

PLANNING AND ZONING COMMISSION AGENDA

Monday, October 10, 2016 Ketchum City Hall 480 East Avenue North, Ketchum, ID 83340

- 5:00 PM SITE VIST: Wood River Drive Landscape Remodel Waterways Design Review
 530 Wood River Drive Ketchum, Idaho
- 2. 5:30 PM CALL TO ORDER: City Hall, 480 East Avenue North, Ketchum, Idaho
- 3. PUBLIC COMMENT Communications from the public for items not on the agenda.
- 4. COMMUNICATIONS FROM STAFF
 - a. Wood River Drive Landscape Remodel Waterways Design Review: The applicant is proposing to do a landscape remodel at their existing residence. The landscape remodel is within the floodplain, but there are no proposed structures or work to be executed within the riparian setback. The property is 0.504 acres in size and zoned Limited Residential (LR).
 - b. Continued from Monday June 13, 2016, Monday June 27, 2016, and Monday July 11, 2016: 911 North Main Street, Ketchum, ID (Ketchum AM Lot 5A Block 30 18,590 SF). The applicant is proposing to construct a motor vehicle fueling station with accessory food service. The property is 0.435 acres in size and zoned Light Industrial-1 (LI-1).
- CONSENT CALENDAR
 - a. Findings: Lift Tower Lodgeb. Minutes: September 26, 2016
- 6. FUTURE PROJECTS AND NOTICING REQUIREMENTS
- 7. STAFF REPORTS & CITY COUNCIL MEETING UPDATE
- 8. COMMISSION REPORTS AND EX PARTE DISCUSSION DISCLOSURE
- 9. ADJOURNMENT

Any person needing special accommodations to participate in the meeting should contact the City Clerk's Office as soon as reasonably possible at 726-3841. All times indicated are estimated times, and items may be heard earlier or later than indicated on the agenda.



October 10, 2016

Planning and Zoning Commission City of Ketchum Ketchum, Idaho

Commissioners:

STAFF REPORT KETCHUM PLANNING AND ZONING COMMISSION REGULAR MEETING OF OCTOBER 10, 2016

PROJECT: Belling Driveway/Landscaping Waterways Design Review and Floodplain Development

Permit

FILE NUMBER: #16-070

OWNERS: Phillip Belling, Trustee and Belling Family Trust

REQUEST: Waterways Design Review and Floodplain Development Permit for a driveway

reconstruction and a landscaping remodel

LOCATION: 530 Wood River Drive (Ketchum FR SW SE TL 7527 SEC 13 4N 17E)

NOTICE: Notice was mailed to adjacent property owners on September 27, 2016. Notice was

posted in three locations within the City of Ketchum (City Hall, Community Library,

Town Square Kiosk) on September 27, 2016.

ZONING: Limited Residential (LR)

OVERLAYS: Floodplain Management Overlay with Floodplain and Floodway subdistricts and

Waterways Design Review subdistrict

REVIEWERS: Brittany Skelton, Senior Planner and Jim Zarubica, PE, PG, CFM

ATTACHMENTS:

- A. Application
 - Application Form, dated September 15, 2016
 - Narrative
 - Plan Set
 - o Landscape Plans, L1.0 through L 7.0, dated September 12, 2016
- B. FP #02-011 Documents
 - Findings of Fact, adopted August 25, 2003
 - Riparian Area Tree Brief, dated December 30, 2002
 - o Landscape Plan, dated November 14, 2002
 - Findings of Fact, adopted March 22, 2004

BACKGROUND

- 1. The applicant is requesting a Flood Plain Development Permit and a Waterways Design Review for reconstruction of a driveway and a landscaping remodel. The subject property is located at 530 Wood River Drive and contains an existing single family residence, an accessory dwelling unit, patios, decks, a bocce court, landscaping, and natural vegetation. The property is adjacent to the Big Wood River, is located entirely within the 1% annual floodplain, contains riparian area, and contains floodway. Chapter 17.88, Floodplain Management Overlay District (FP) requires that this project receive a Floodplain Development Permit and Waterways Design Review approval.
- 2. In 2002 a Floodplain Development Review permit application was submitted for the property (FP02-011) for the construction of a new single family home, relocation of an existing cabin on the property and use of the existing cabin as an accessory dwelling unit, and removal of several trees in the riparian zone. The Planning and Zoning Commission approved FP02-011 and adopted the Findings of Fact on August 25, 2003. The findings included approval to remove one double-stem cottonwood tree from the riparian area.

The approval for FP02-011 expired prior to a building permit being issued and the Commission reevaluated the application and adopted Findings of Fact approving the project on March 22, 2004. The March 22, 2004 Findings note that the double-stem cottonwood tree was removed and that while no other trees had been removed from the riparian zone willows were trimmed to ground level and vegetation in the riparian zone was managed. Both Findings of Fact are attached.

- 3. The applicant proposes the following work as part of this application:
 - a. Remove the existing concrete paver driveway and replace it with a cobblestone two-track driveway surrounded by grass-pave pavers and a cobblestone driveway apron. The driveway apron will be bisected by a stone paved walkway connecting to a new stone paved patio abutting the guest house;
 - b. Remove two existing concrete and stone patios at the rear/west of the property, near but outside of the riparian setback, replace the southernmost patio with a sunken garden, replace the northern patio with a reduced-size sand set patio. Each patio will have stone steps leading to the lawn and an 18" or less stone wall on one side. Five (5) new 3-4" caliper Apsens and landscaping boulders will be placed adjacent to the southernmost patio;
 - c. Construct a rustic boardwalk on piers that will connect the reconstructed patio to an existing deck;
 - d. Construct gravel pathways connecting to the rear patios, bocce court, and driveway apron;
 - e. Remove the existing bocce court and construct a new bocce court with a smaller footprint, install two wood block benches adjacent to the new bocce court;
 - f. Plant three (3) 3-4" caliper Aspens to the northeast of the bocce court; and
 - g. Revegetate areas in the riparian zone that are not currently naturalized with native vegetation (fifteen (15) 5-gallon Serviceberry Shrubs, twenty-nine (29) 5-gallon Red Twig Dogwoods, eleven (11) 10-gallon Drummond's willows;
 - a. Staff notes that there is a discrepancy between trees proposed for removal shown on landscape plan dated November 14, 2002 and included in the Riparian Area Tree Brief that was submitted with the FP02-011 application, removal of which the Commission denied, and the plans dated September 12, 2016, which reflect removal of the trees. The Riparian Area Tree Brief prepared by Bill Josey, Certified Arborist, dated December 30, 2002 and the landscape plan dated November 14, 2002 are attached.
- 4. There will be no new fill as part of this project; the applicant proposes to maintain or lower the grades of the driveway, patio, and bocce court, creating a net gain of floodplain volume. The scope of work will result

in a 579.8 additional square feet of pervious surface on the property, equating to a 3% decrease in impervious surface on the property, as calculated on Landscape Plan sheet L4.0.

- 5. As of Monday, October 3, 2016, no written public comment regarding this project was received.
- 6. Due to the discrepancy between trees shown in the riparian zone on plans submitted with FP02-011 and trees shown to be on the property in the plans dated September 12, 2016 and the fact that the lawn has encroached into the riparian zone, staff recommends conditions stating the riparian setback shall be allowed to naturalize and that that inspections shall be conducted to ensure that condition has been met.

Table 1: General Requirements for all Floodplain Development applications

	General Requirements for all FPDP Applications					
Compliant		Standards and Staff Comments				
Yes	No	N/A	City Code	City Standards and Staff Comments		
\boxtimes			17.88.060.C	Complete Application		
				Fire Department		
				 Approved address numbers shall be placed in such a position to be plainly visible and legible from the road fronting the property. Numbers and letters shall be a minimum of four (4) inches tall, contrast with their background and be positioned a minimum of forty-eight (48) inches above final grade. Vehicle parking and material storage during construction shall not restrict or obstruct public streets or access to any building. A minimum twenty-foot travel lane for emergency vehicle access shall be maintained clear and unobstructed at all times. All required Fire Lanes, including within 15 feet of fire hydrants, shall be maintained clear and unobstructed at all times. An approved access roadway per 2012 International Fire Code Appendix D (www.ketchumfire.org) shall be installed prior to any combustible construction on the site. The road shall be a minimum of twenty (20) feet in width and capable of supporting an imposed load of at least 75,000 pounds. The road must be an all-weather driving surface maintained free, clear, and unobstructed at all times. Grades shall not exceed 7%. Dead end access roadways exceeding 150 feet in length shall be provided with an approved turnaround. Gates, if installed, are required to be siren activated for emergency vehicle access. 		

 Public Works The right-of-way (ROW) should be brought up to the new ROW standards and the cable box or utility box at the south end of the ROW should be moved back to the property line by the utility company. The landscaping in the ROW is creating a safety issue for the traveling public. Several years ago the City had to paint a shoulder line on the roadway because there is not any place for a pedestrian to walk except the roadway. Plans of the ROW work will need to be submitted with the building permit application and ROW plans will need to be approved by the city before any ROW work can take place.
City Arborist 1. The riparian planting list is adequate.
 Utilities 1. If a curb stop is present in the construction area it will need to be protected with a street ring/valve box from snow plowing or traffic.
Building Official O No comment.
Police O No comment.

Table 2: Zoning Standards Analysis

	Compliance with Zoning District and Overlay Requirements				
C	omplia	nt		Standards and Staff Comments	
Yes	No	N/A	Guideline	City Standards and Staff Comments	
\boxtimes			17.12.030.C	Lot Area	
				No change	
\boxtimes			17.12.030.C	Setbacks	
			Staff Comments	" INO CHANGE	
\boxtimes			17.12.030.C Building Height		
			Staff Comments	No Change	
\boxtimes			17.12.030.C Maximum Building Coverage		
			Staff Comments	No change	
\boxtimes			17.125.030.H Curb Cut		
			Staff Comments	The existing carb car has been reduced.	
\boxtimes			17.125.050.6 Parking Spaces		
			Staff Comments	No change	

Table 3: Floodplain Design Review Requirements

			Floo	odplain Design Review Requirements		
1. E	VALU	ATION	STANDARDS:	17.88.050(E)		
	omplia			Standards and Staff Comments		
Yes No N/A Guideline			Guideline	City Standards and Staff Comments		
			17.88.050(E)1 FLOODPLAIN DEVELOPMENT /WATERWAYS DESIGN REVIEW	Preservation or restoration of the inherent natural characteristics of the river and creeks and floodplain areas. Development does not alter river channel.		
			Staff Comments	No development is proposed in the river channel or riparian area. Areas of lawn in the riparian setback will be revegetated with native species. Work in the floodplain consists of upgrading and existing driveway, bocce ball court, and deck. The applicant proposes to maintain or lower the grades of these features, therefore, creating a net gain of floodplain volume.		
\boxtimes			17.88.050(E)2	Preservation or enhancement of riparian vegetation and wildlife habitat, if any, along the stream bank and within the required minimum twenty-five (25) foot setback or riparian zone. No construction activities, encroachment or other disturbance into the twenty five foot (25') riparian zone shall be allowed at any time without written City approval per the terms of this ordinance.		
			Staff Comments	No development is proposed in the riparian setback. Existing lawn will be revegetated with city approved native species.		
			17.88.050(E)3	No development other than development by the City of Ketchum or development required for emergency access shall occur within the twenty-five (25) foot riparian zone with the exception of approved stream stabilization work. The Planning and Zoning Commission may approve access to property where no other primary access is available. Private pathways and staircases shall not lead into or through the riparian zone unless deemed necessary by the Planning and Zoning Commission.		
			Staff Comments	No development is proposed in the riparian setback. Existing lawn will be revegetated with city approved native species.		
		17.88.050(E)4	Plan and time frame are provided for restoration of riparian vegetation damaged as a result of the work done.			
			Staff Comments	No development is proposed in the riparian setback.		
			17.88.050(E)5 New or replacement planting and vegetation includes plantings that are low-and have dense root systems for the purpose of stabilizing stream banks and damage previously done to riparian vegetation. Examples of such plantings i red osier dogwood, common choke cherry, service berry, elder berry, river bi skunk bush sumac, beb's willow, drummond's willow, little wild rose, gooseb honeysuckle. Staff Existing lawn will be revegetated with city approved native species.			
	<u> </u>		Comments	, , , ,		
			17.88.050(E)6	Landscaping and driveway plans to accommodate the function of the floodplain to allow for sheet flooding. Flood water carrying capacity is not diminished by the proposal. Surface drainage is controlled and does not adversely impact adjacent properties including driveways drained away from paved roadways. Culvert(s) under driveways may be required. Landscaping berms are designed to not dam or otherwise obstruct floodwaters or divert same onto roads or other public pathways.		
			Staff Comments	The applicant has submitted a grading and drainage plan that shows the removal and replacement of an existing deck, driveway, and bocce ball court. All of these features will remain at the current grade of be lowered. This will increase the carrying capacity of the floodplain on this site. Prior to issuance of a building permit, the applicant shall		

Compliant		Standards and Staff Comments				
Yes	No	N/A	Guideline	City Standards and Staff Comments		
				provide civil engineered plans, for evaluation by city staff, showing the		
				following:		
				Minimum % of slopes detailed in the ROW.		
				Driveway should follow ROW standards and shall not drain into		
				the street.		
				Private property shall not dewater into the ROW, and the		
	ļ <u></u>			project will need to provide drainage in the ROW.		
			17.88.050(E)7	Impacts of the development on aquatic life, recreation, or water quality upstream, downstream or across the stream are not adverse.		
			Staff	It appears there will be no adverse impact from the development on		
			Comments	aquatic life, recreation or water quality.		
\boxtimes		П	17.88.050(E)8	Building setback in excess of minimum required along waterways is encouraged.		
			Staff Comments	This project is a remodel and existing setbacks will be maintained		
		\boxtimes	17.88.050(E)9	The top of the lowest floor of a building located in the 1% annual chance floodplain		
				shall be a minimum of twenty-four inches (24") above the base flood elevation of the subject property.		
			Staff	No building construction is proposed as part of this action.		
			Comments			
		\boxtimes	17.88.050(E)10	The back fill used around the foundation in the floodplain provides a reasonable		
				transition to existing grade, but is not used to fill the parcel to any greater extent.		
				Compensatory storage shall be required for any fill placed within the floodplain. A LOMA-F shall be obtained prior to placement of any additional fill in the floodplain.		
			Staff	No fill will be placed as part of this project. The applicant proposed to		
			Comments			
				maintain or lower the elevations of new landscape and hardscape		
	ļ <u></u>			features.		
\boxtimes			17.88.050(E)11	All new buildings shall be constructed on foundations that are approved by a licensed		
			Staff	professional engineer. No new buildings are proposed.		
			Comments			
			17.88.050(E)12	Driveways comply with effective Street Standards; access for emergency vehicles has been adequately provided for.		
			Staff	Street and Fire Departments have commented on this application. As a		
			Comments	condition of approval, the building permit application shall address all		
				of the Street and Fire Department comments as listed in the conditions		
				below.		
\boxtimes		П	17.88.050(E)13	Landscaping or revegetation conceals cuts and fills required for driveways and other		
				elements of the development.		
			Staff Comments	All disturbed areas shall be revegetated.		
		\boxtimes	17.88.050(E)14	(Stream Alteration) The proposal is shown to be a permanent solution and creates a stable situation.		
			Staff	N/A		
<u> </u>			Comments			
		\boxtimes	17.88.050(E)15	Stream Alteration) No increase to the 100-year floodplain upstream or downstream has been certified by a registered Idaho engineer.		
			Staff	N/A		
Comments 17.88.050(E)16 (Stream Alteration) The recreational use of the stream is		(Stream Alteration) The recreational use of the stream including access along any and				
		\boxtimes	17.00.030(L)10	all public pedestrian/fisherman's easements and the aesthetic beauty is not		
				obstructed or interfered with by the proposed work.		
			Staff	N/A		
			Comments			
		\boxtimes	17.88.050(E)17	Where development is proposed that impacts any wetland, first priority shall be to		
				move development from the wetland area. Mitigation strategies shall be proposed at time of application that replace the impacted wetland area with a comparable		
				time of application that replace the impacted wetiand area with a comparable		

Compliant		Standards and Staff Comments			
Yes	No	N/A	Guideline	City Standards and Staff Comments	
				amount and/or quality of new wetland area or riparian habitat improvement.	
			Staff Comments	N/A	
		\boxtimes	17.88.050(E)18	(Stream Alteration) Fish habitat is maintained or improved as a result of the work proposed.	
			Staff Comments	N/A	
including, but not limited to, property values, fish and wildlife habitat, a		(Stream Alteration) The proposed work is not in conflict with the local public interest, including, but not limited to, property values, fish and wildlife habitat, aquatic life, recreation and access to public lands and waters, aesthetic beauty of the stream and water quality.			
			Staff Comments	N/A	
			17.88.050(E)20	(Stream Alteration) The work proposed is for the protection of the public health, safety and/or welfare such as public schools, sewage treatment plant, water and sewer distribution lines and bridges providing particularly limited or sole access to areas of habitation.	
			Staff Comments	N/A	

STAFF RECOMMENDATION

Staff recommends continuance of the proposed Waterways Design Review, finding that it meets nearly all applicable floodplain management and zoning standards but does not satisfactorily address on-site drainage and drainage in the right-of-way, as indicated in the Public Works Department's comments. However, a revised drainage plan may be submitted that addresses the Public Works Department's concerns.

The Commission should consider the full record of facts and evidence brought forward on this application based on staff reports, applicant information, public comments, and other relevant information. Based on the information presented and received, the following options should be determined:

- 1. On the whole, this application is in compliance with the floodplain management and zoning ordinances and other adopted or enforced city policies or codes and approve the floodplain development permit/waterways design review request with conditions 1-8 below.
- 2. On the whole, this application is not in compliance with the floodplain management and zoning ordinances and other adopted or enforced city policies or codes and deny the request for a townhouse final plat because the following standards (Commission to insert reasons for denial).
- 3. If the Commission is not opposed to the entire application but only with certain aspects of the proposal, the Commission may amend and revise the proposal and/or modify conditions to address their concerns and proceed with approving the t proposed Waterways Design Review application.
- 4. If the Commission does not feel they have all the information they need to make a decision they may require additional information to be brought forth at a future meeting.
- 5. The Commission may determine some other option based on the information presented at the meeting.

Based on the information submitted to date, staff recommends approval of this project, Belling Driveway/Landscaping Waterways Design Review and Floodplain Development Permit, subject to the conditions 1 - 15 below.

FOR MOTION PURPOSES

- 1. "I move to approve this project, Belling Driveway/Landscaping Waterways Design Review and Floodplain Development Permit, because **does** meet the standards for approval under Chapter 17.88 of Ketchum Code Title 17 only if the following conditions of approval are met.
- "I move to deny this project, Belling Driveway/Landscaping Waterways Design Review and Floodplain Development Permit, because it does not meet the standards for approval under Chapter 17.88 of Ketchum Zoning Code Title 17 because of the following standards (Commission to insert reasons for denial); or,

PROPOSED CONDITIONS

- 1. Waterways Design Review/ Floodplain Development Permit approval shall expire one (1) year from the date of signing of approved Findings of Fact per the terms of KMC, Section 17.88.050.G, Terms of Approval;
- 2. This Waterways Design Review and Floodplain Development Permit approval is based on the plans, as dated in the list of attachments above, and information presented and approved at the meeting on the date noted herein. Any building or site discrepancies which do not conform to the approved plans will be subject to removal;
- 3. Pursuant to Chapter 17.88.050.C, no chemicals or soil sterilants are allowed within 100 feet of the mean high water mark. No pesticides, herbicides, or fertilizers are allowed within 25 feet of the mean high water mark unless approved by the City Arborist 5. All applications of herbicides and/or pesticides within one hundred feet (100') of the mean high water mark, but not within twenty five feet (25') of the mean high water mark, must be done by a licensed applicator and applied at the minimum application rates. Application times for herbicides and/or pesticides will be limited to two (2) times a year; once in the spring and once in the fall unless otherwise approved by the city arborist. The application of dormant oil sprays and insecticidal soap within the riparian zone may be used throughout the growing season as needed;
- 4. Prior to commencement of any work in the riparian setback, a silt fence shall be installed to keep all silt and debris out of the Big Wood River. Said fence shall remain in place for the duration of the riparian landscaping work;
- 5. All excavated materials must be exported off site;
- 6. The riparian plantings shall conform to the application and Landscape Plan sheet L7.0, dated September 12, 2016, and shall be inspected for approval by the Planning and Building Department staff upon completion of the project;
- 7. No maintenance, including the mowing, trimming, and removal of vegetation, and no replacement or repair of existing non-conforming decks or patios within the riparian zone shall take place without approval from the Planning and Building Department;
- 8. A follow up inspection to confirm compliance with the conditions shall occur two (2) years after the adoption of the Findings of Fact. Planning and Building Department staff may schedule subsequent inspections to ensure that the riparian zone is not maintained and is allowed to naturalize;

- 9. A permit is required for any subsequent work in the riparian setback occurring after the duration of this approval.
- 10. Any irrigation system installed shall be a temporary installation and shall be removed within three years of completion of the landscaping installation;
- 11. The above project shall meet all 2012 International Fire Code requirements in addition to specific City Building and Fire Ordinances.
- 12. Approved address numbers shall be placed in such a position to be plainly visible and legible from the road fronting the property. Numbers and letters shall be a minimum of four (4) inches tall, contrast with their background and be positioned a minimum of forty-eight (48) inches above final grade. Vehicle parking and material storage during construction shall not restrict or obstruct public streets or access to any building. A minimum twenty-foot travel lane for emergency vehicle access shall be maintained clear and unobstructed at all times. All required Fire Lanes, including within 15 feet of fire hydrants, shall be maintained clear and unobstructed at all times.
- 13. An approved access roadway per 2012 International Fire Code Appendix D (www.ketchumfire.org) shall be installed prior to any combustible construction on the site. The road shall be a minimum of twenty (20) feet in width and capable of supporting an imposed load of at least 75,000 pounds. The road must be an all-weather driving surface maintained free, clear, and unobstructed at all times. Grades shall not exceed 7%. Dead end access roadways exceeding 150 feet in length shall be provided with an approved turnaround. Gates, if installed, are required to be siren activated for emergency vehicle access.
- 14. The right-of-way (ROW) should be brought up to the new ROW standards and the cable box or utility box at the south end of the ROW should be moved back to the property line by the utility company. Plans of the ROW work will need to be submitted with the building permit application and ROW plans will need to be approved by the city before any ROW work can take place;
- 15. If a curb stop is present in the construction area it will need to be protected with a street ring/valve box from snow plowing or traffic.

File Number: 16-070

FLOODPLAIN MANAGEMENT OVERLAY APPLICATION

Use for:

• Floodplain Development Permit

• Waterways Overlay Design Review

Stream Alteration Permit

	RTIFIED MPLETE	
9	15/6/10	

Project Name: WOOD RIVEY Drive Landscape Remodel.
Owner: Phil 7 Showy Belling. Phone No.: 949. 933. 3497
Mailing Address:
Email: SG Belli ngC Yahoo · Com Architect/Representative: ASh RU Boan D. Phone No.: 208 720 2089 ·
Architect/Representative: ASh Ru Boand. Phone No.: 208 720 2081.
Mailing Address: PO BOX 594, Ketchum, ID, 83340.
Email: ashky @ byla. us'
Engineer of Record: Don Sewey (BMA) Engineer Email: Jon C EMA5B. Corv
Floodplain Management Review Fee: Date Paid:
Legal Land Description: LO+ 7527
SEC. 13 TWNSHP AN., RNG 17 E.
Street Address: 530 WOOD RIVER Drive
Lot Area: 121,932 Zoning District: LK\$ Flood Anticipated Use: DV 1 Vale
Type Construction: New Remodel X — Addition Other
Total Floor Area: Proposed Basements: Existing Landscape
Basements:
1st Floor:
2nd Floor:
3rd Floor:
Mezzanine:
Total:
Percent of Building Coverage: 25% (cyisting). Curb Cut: existing
Setbacks: Front 15' Side 10' Side 10' Rear 20'
Height: existing Parking Spaces Provided: For Construction.
Will fill or excavation be required? If yes, amount in cubic yards- Fill Excavation
Will existing trees or vegetation be removed? Yes X No
The Applicant agrees in the event of a dispute concerning the interpretation or enforcement of the
Floodplain Management Overlay Application in which the City of Ketchum is the prevailing party to pay the
reasonable attorney fees, including attorney fees on appeal, and expenses of the City of Ketchum.
reasonable accorney rees, malaling accorney rees on appear, and expension or the enty or resistant
I, the undersigned, certify that all information submitted with and upon this application form is true and
accurate to the best of my knowledge and belief.
Signature of Owner: 12.2016
Approved/Denied: Date:
Approved/ Defiled

Pursuant to Resolution No. 08-123, any direct costs incurred by the City of Ketchum to review this application will be the responsibility of the applicant. Costs include but are not limited to: engineer review, attorney review, legal noticing, and copying costs associated with the application. The City will require a retainer to be paid by the applicant at the time of application submittal to cover said costs. Following a decision or other closure of an application, the applicant will either be reimbursed for unexpended funds or billed for additional costs incurred by the City.



BEN YOUNG LANDSCAPE ARCHITECT

PROJECT: Wood River Drive Landscape Remodel

ADDRESS: 530 Wood River Drive

LEGAL: SEC 13, Township 4N, Range 17 E

PROJECT DESCRIPTION:

Landscape remodel at an existing residence. Project scope of work within the floodplain. Project will reduce impervious surface, and increase pervious by 4% from existing. There are no proposed structures, and no work will be executed within the riparian setback.

WRITTEN CONTENTS:

Provided narrative in response to criteria 1-19 of Addendum 1 for Floodplain Management Overlay Application

Addendum 1 Narrative:

- **1.** The proposed landscape renovations promote the natural characteristics of the Big Wood River, and are outside of the riparian setback. The plan proposes to revegetate areas within the riparian setback on the property that are currently lawn.
- **2.** All riparian vegetation will be preserved with the addition of re-vegetating lawn areas within the riparian setback with city recommended plantings.
- **3.** No development within the scope of work will take place within the 25' riparian setback.
- **4.** Sheet L1.1 of the provided landscape set highlights riparian re-vegetation within the 25' setback. Re-vegetation will occur to restore areas of existing lawn, back to riparian vegetation.
- **5.** All new plantings will conform to the approved plantings provided by the City of Ketchum listed on criteria 5, Addendum1, of the Criteria for Evaluation of Applications.
- **6.** All proposed landscape and hardscape accommodate the function of the floodplain. The proposed renovations keep in character with the existing landscape features, and are mere improvements and upgrades. No water will be diverted. Drainage patterns and surface drainage are a 4% improvement from existing.
- **7.** There will be no impact or adverse reactions to aquatic life, recreation, or water quality from the improvements. All improvements are intended to promote riparian health and diversity.
- **8.** All setbacks are to remain as existing, and as required by the City of Ketchum.
- **9.** Does not apply.

- **10.** Backfill will only take place at two proposed stone veneer walls. All backfill used will allow finished grade to match that of existing. No regrading from existing will take place within the riparian setback.
- **11.** The proposed driveway renovations comply with the effective street standards of the City of Ketchum. All driveway improvements have been reviewed and approved by the Fire Department. Access for emergency vehicles will be provided for by the proposed two-track cobblestone driveway, with grass-pave reinforcements on each side to meet the 20' W requirement. All drivable surfaces will comply with the 75,000lb weight requirement.
- **12.** No prominent cuts or fills to take place.
- **13.** Does not apply.
- **14.** Does not apply.
- **15.** Does not apply.
- 16. Does not apply.
- 17. Does not apply.
- **18.** Does not apply.
- 19. Does not apply.

BELLING RESIDENCE

530 WOOD RIVER DRIVE KETCHUM, ID 83340

SHEET	SHEET TITLE
L1.0	Cover
L2.0	Survey
L3.0	Existing Condtions + Demo
L4.0	Landscape Overview
L5.0	Grading + Drainage Plan
L6.0	Lighting + Utility Plan
L7.0	Planting Schedule

P&Z FLOODWAY DESIGN REVIEW

GENERAL NOTES

1. <u>Design Intent</u>: These drawing represent the general design intent to be implemented on-site. Contractor shall be responsible for contacting the Landscape Architect for any additional clarification or details necessary to accommodate site conditions or architectural

2. Contractor Coordination: Each contractor shall coordinate and otherwise integrate work with that of others in and efficient and timely manner, so as to provide the owner with a well-constructed, easily maintainable project. Each Contractor shall notify others at least two working day in advance of completing, covering, or exposing work to be installed by

3. <u>Contractor's Job Site Conditions</u>: Contractor agrees to assume sole and complete responsibility for site conditions during construction, including safety of all persons and property; that this requirement shall apply continuously and not be limited to normal working hours; and that the contractor shall defend, indemnify, and hold the owner and the Landscape Architect harmless from any and all liability, real or alleged, in connection with the performance of work on this project, excepting for liability arising from the sole negligence of the owner or the Landscape Architect.

<u>Composite Base Sheet</u>: The proposed improvement shown on these drawing are superimposed on a base sheet. This base sheet is compiled from the topographic survey. other architectural and/or Engineering documents, and other data as made available to the Landscape Architect. This base sheet information is shown in half tone on the plans. The Landscape Architect shall not be held liable for changes, inaccuracies, omissions, or other errors on these documents. The composite base sheet is provided as an aid only and the contractor shall be responsible for reviewing these documents and incorporating/integrating all construction as required to accommodate same.

The base sheet source for these drawings is: 16120 Worksheet email (DWG File, prepared by BMA, date 31 August 2016)

5. <u>Utilities:</u> Prior to construction, the Contractor is responsible for contacting the Utility Companies involved and requesting a visual verification of the location of their underground facilities. The Utility Companies are member of the Underground Service Alert (USA) on-call program. The contractor or any subcontractor for this contract shall notify members of the USA 48 hours in advance of performing excavation work by calling the toll free number 800.642.2444. Excavation is defined as being 18 or more inches in depth below the existing surface.

The Contractor is cautioned that only excavation will reveal the types, extent, sizes, location and depths of such underground utilities. However, the consultant can assume no responsibility for the completeness or accuracy of its delineation of such underground utilities, nor for the existence of other buried objects or utilities which are not shown on these drawings

6. <u>Tree Protection and Maintenance Requirements</u>: All trenching within the dripline of existing trees, to remain, shall be by hand, with care taken not to cut or damage roots over 1-inch diameter. Trees to remain shall be fenced with temporary fencing, such as steel stakes (max. 5 feet O.C.) with wire mesh fabric (6x6 open), chainlink or similar, height to be 5 feet minimum.

OPTIONAL 7. The Contractor shall be solely and completely responsible for furnishing, installing and maintaining all warning signs and devices necessary to safeguard the general public and the work. This requirement shall apply continuously and not be limited to normal working hours. The use of flagmen, barricades and construction signing shall comply with the current edition of work area and traffic control handbook (WATCH).

8. The Contractor shall be responsible for the protection of all existing survey

monuments or markers during construction. 9. The Contractor shall maintain all existing drainage and sewer facilities within the construction area until new drainage and sewer improvements are in place and functioning. 10. Cost to remove and replace existing pavement over the water, storm, and sanitary line trenches shall be included in the price bid for pipe in place. Replacement shall be 4"

11. Pipe and manhole dimensions are to the centerline, unless otherwise noted.

AC and 12" AB, minimum, or as indicate on the plans.

12. A storm water permit must be obtained when construction activity results in soil disturbance of X or more acres.

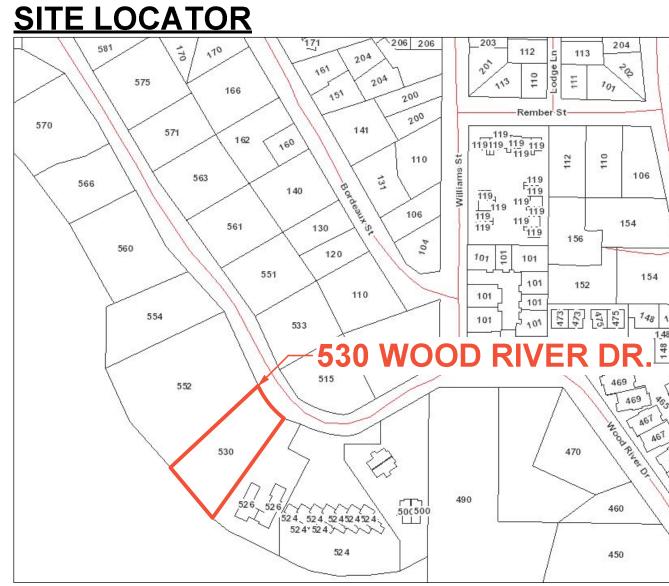
13. The Contractor is responsible for determining the exact location of all existing utilities and for the protection of and repair of damage to them. The contractor shall contact underground service alert 1.800.642.2444, two (2) working days before work is to begin.

14. <u>Shop Drawings</u>: Contractor shall submit shop drawings, to Landscape Architect for review and approval, of all custom materials/details at least one week prior to ordering materials or beginning construction.



VICINTIY MAP





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09.12.2016 Drawn By: Checked By: AB, BY Belling LA Base.vwx

Sheet No.

THE PURPOSE OF THIS MAP IS TO SHOW LIMITED/SITE INFORMATION AS IT EXISTED ON THE DATE THE FIELD S URVEY WAS PERFORMED. CHANGES MAY HAVE OCCURRED TO SITE CONDITIONS SINCE SURVEY DATE.

THIS MAP WAS PREPARED FOR THE EXPRESS USE OF THE CLIENT AND IS NOT TRANSFERABLE TO OTHERS W

A TITLE POLICY HAS NOT BEEN SUBMITTED TO BENCHMARK ASSOCIATES, NOR HAS A TITLE SEARCH BEEN R EQUESTED. CERTAIN INFORMATION CONTAINED WITHIN SAID POLICY MAY NOT APPEAR ON THIS MAP OR MAY AF

ELECTRONIC DATA: BENCHMARK ASSOCIATES ACCEPTS NO RESPONSIBILITY OR LIABILITY FOR THE REUSE, DISTRIBUTION OR ACCURACY OF DATA CONTAINED ON ELECTRONIC COPIES OF THIS DRAWING. THE STAMPED H ARD COPY OF THIS DRAWING IS THE FINAL PRODUCT. ANY DATA THAT APPEARS ON THE ELECTRONIC DRAWING,

BUILDING AREA: MINIMUM SETBACKS AS SHOWN ARE PER KETCHUM ORDINANCE # 17.12.030: DIMENSIONAL S

GENERAL RESTRICTIONS: EXCEPT AS SPECIFICALLY STATED OR SHOWN ON THIS MAP, THIS SURVEY DOES N OT PURPORT TO REFLECT ANY OF THE FOLLOWING WHICH MAY APPLICABLE TO THE SUBJECT OF REAL ESTATE: EASEMENTS, OTHER THAN THOSE SHOWN OR LISTED HEREON, BUILDING SETBACK LINES, RESTRICTIVE COVENA NTS, SUBDIVISION RESTRICTIONS, ZONING, WETLANDS OR ANY OTHER LAND-USE REGULATIONS.

WETLANDS AND RIPARIAN: CERTAIN AREAS WITHIN THIS MAP MAY CONTAIN RIPARIAN, RIVERINE OR WETLAN DS CONDITIONS. SAID AREAS AND THE LOCATION OF ORDINARY HIGH WATER (OHW), IF SHOWN HEREON, ARE SU BJECT TO INTERPRETATION. IT IS RECOMMENDED THAT THE LANDOWNER OR CLIENT RETAIN THE SERVICES OF A N ENVIRONMENTAL SPECIALIST IN ORDER TO ASCERTAIN IF SAID CONDITIONS EXIST, AND TO FURTHER IDENTIFY SAID AREAS IF THEY DO EXIST. PERMITS MAY BE REQUIRED FROM LOCAL, STATE OR FEDERAL AGENCIES PRIOR TO CONSTRUCTION, EXCAVATION OR FILL ACTIVITIES.

STORM WATER PLAN: IF SOIL DISTUPBANCE, CLEARING, GRADING AND/OR EXCAVATION OF ONE (1) ACRE OR

UTILITIES MAY EXIST. LOCATION OF UNDERGROUND UTILITIES AND SERVICES SHOULD BE CONFIRMED PRIOR TO

MAP SCALE: DUE TO ELECTRONIC MAP DELIVERY AND ALTERNATE PRINTING METHODS, PLEASE USE BAR SC

A.O. BOX 733 - 100 BELL DRIVE, KETCHUM, IDAHO, 83340

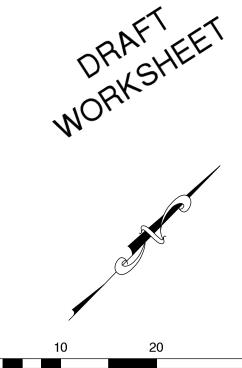
—OHW——OHW——OHW——OH —— RS —— RS —— RS — _____

(NXX°XX'XX"E X.XX') NXX°XX'XX"E XX.XX' ADJOINING PROPERTY LINE EXTERIOR BUILDING FOOTPRINT

MINIMUM BUILDING SETBACK

FOUND 1/2" REBAR, AS NOTED

CONIFEROUS TREE





AND NOT SHOWN ON THE STAMPED HARD COPY, SHALL BE INTERPRETED BY BENCHMARK ASSOCIATES ONLY.

TANDARDS, DISTRICTS MATRIX. IT IS RECOMMENDED THAT SAID ORDINANCES BE REVIEWED PRIOR TO DESIGN.

MORE IS TO TAKE PLACE A FEDERAL GENERAL CONSTRUCTION PERMIT, INCLUDING A STORM WATER POLLUTION PREVENTION PLAN (SWPPP), MUST BE PREPARED AND SUBMITTED TO AND APPROVED BY THE U.S. ENVIRONMEN TAL PROTECTION AGENCY PRIOR TO ANY SITE DISTURBANCE.

BASIS OF BEARINGS IS PER A MOMENT FREE SUBDIVISION, INST. NO. 372146. BOUNDARY LINES ARE PER FOU ND MONUMENTS. ALL DISTANCES SHOWN ARE GROUND DISTANCES IN U.S. SURVEY FEET.

UTILITIES AND DRAIN PIPES IF SHOWN HEREON ARE PER SURFACE EVIDENCE ONLY. OTHER UNDERGROUND

12. BUILDING WALLS IF SHOWN HEREON ARE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED. 13. SPRINKLER HEADS AND IRRIGATION LINES ARE NOT SHOWN HEREON.

FEATURES OBSCURED BY DEBRIS, SNOW OR VEHICLES AT THE TIME OF SURVEY, DO NOT APPEAR ON THIS M

PHONE (208)726-9512 FAX (208)726-9514 EMAIL:mail@bma5b.com

PROPERTY LINE **PAVERS** EDGE OF LAWN

APPROXIMATE ORDINARY HIGH WATER RIPARIAN SETBACK

RECORD BEARINGS AND DISTANCES MEASURED BEARINGS AND DISTANCES FOUND 5/8" REBAR,

DECIDUOUS TREE

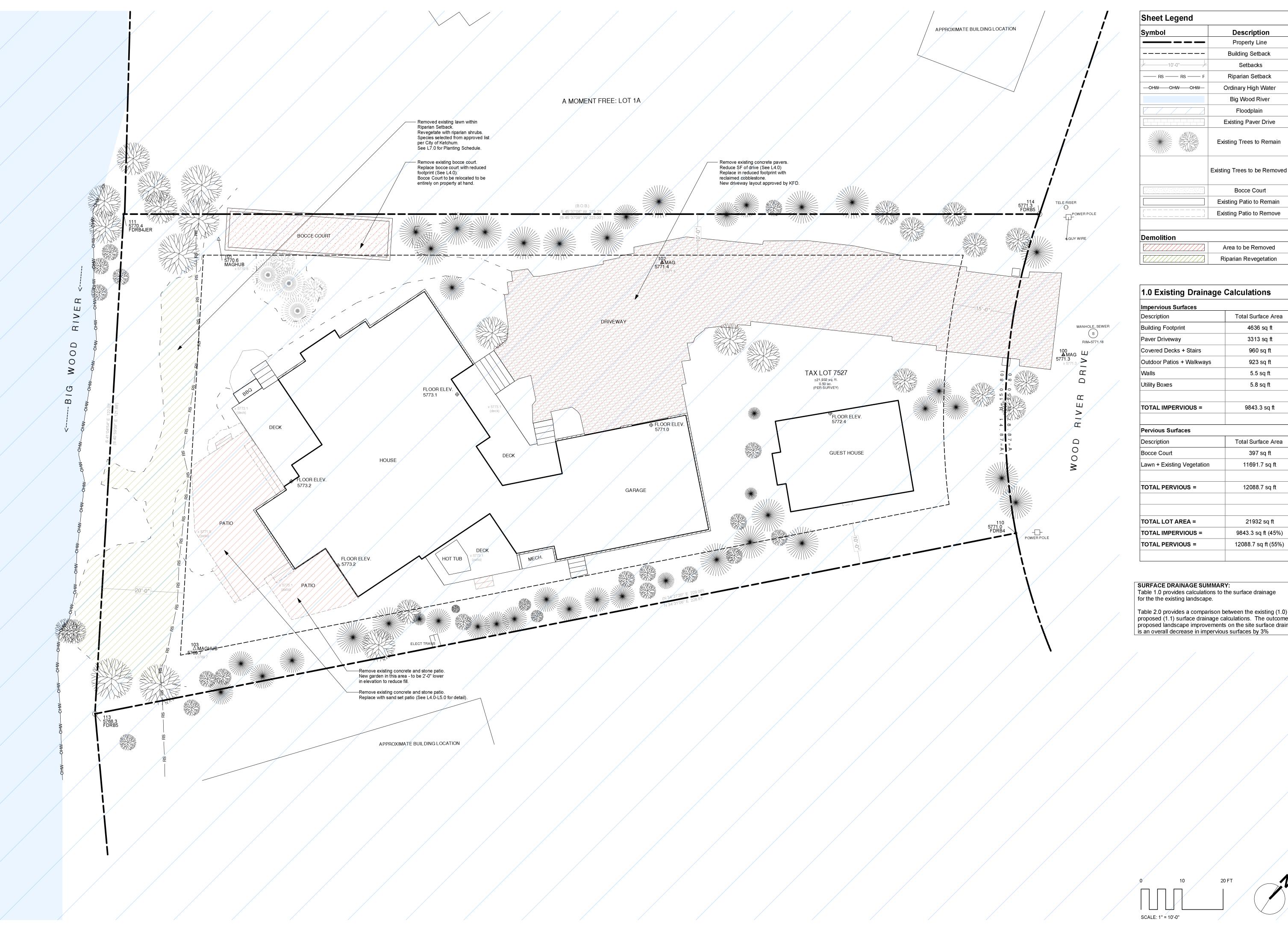
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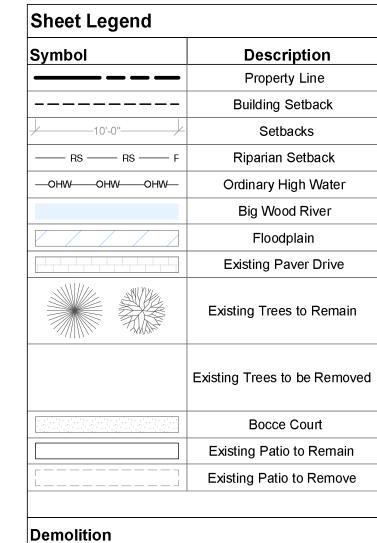
Survey

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Total Surface Area
4636 sq ft
3313 sq ft
960 sq ft
923 sq ft
5.5 sq ft
5.8 sq ft
9843.3 sq ft

Pervious Surfaces				
Description	Total Surface Area			
Bocce Court	397 sq ft			
Lawn + Existing Vegetation	11691.7 sq ft			

TOTAL PERVIOUS =	12088.7 sq ft
TOTAL LOT AREA =	21932 sq ft
TOTAL !!!DED\#0!!0	0040 0

Table 2.0 provides a comparison between the existing (1.0) and proposed (1.1) surface drainage calculations. The outcome of the proposed landscape improvements on the site surface drainage

Existing Conditions + Demo

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SCALE: 1" = 10'-0"

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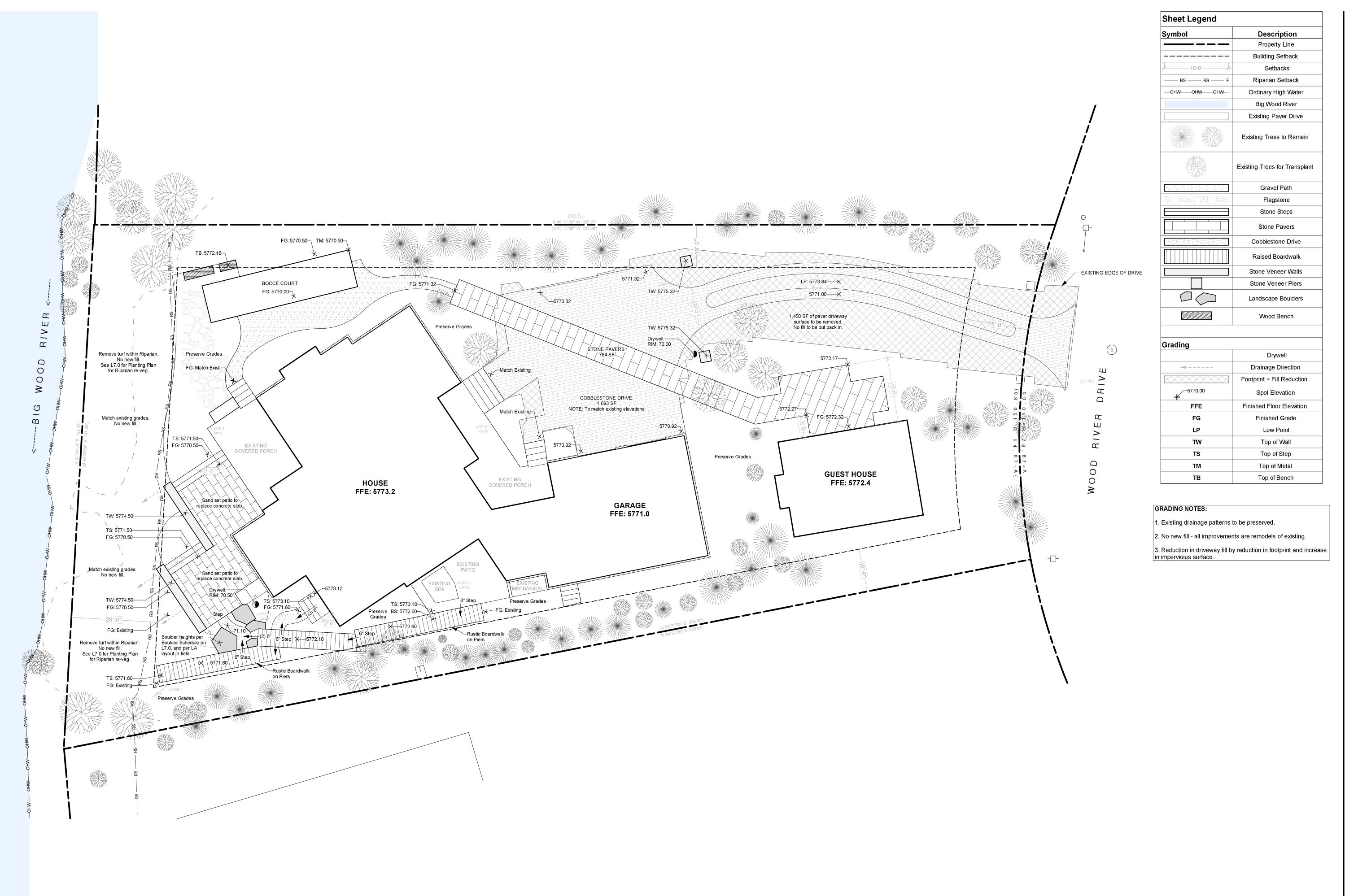
Landscape Overview

Date: 09.12.2016 Drawn By: AB

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Grading + Drainage Plan

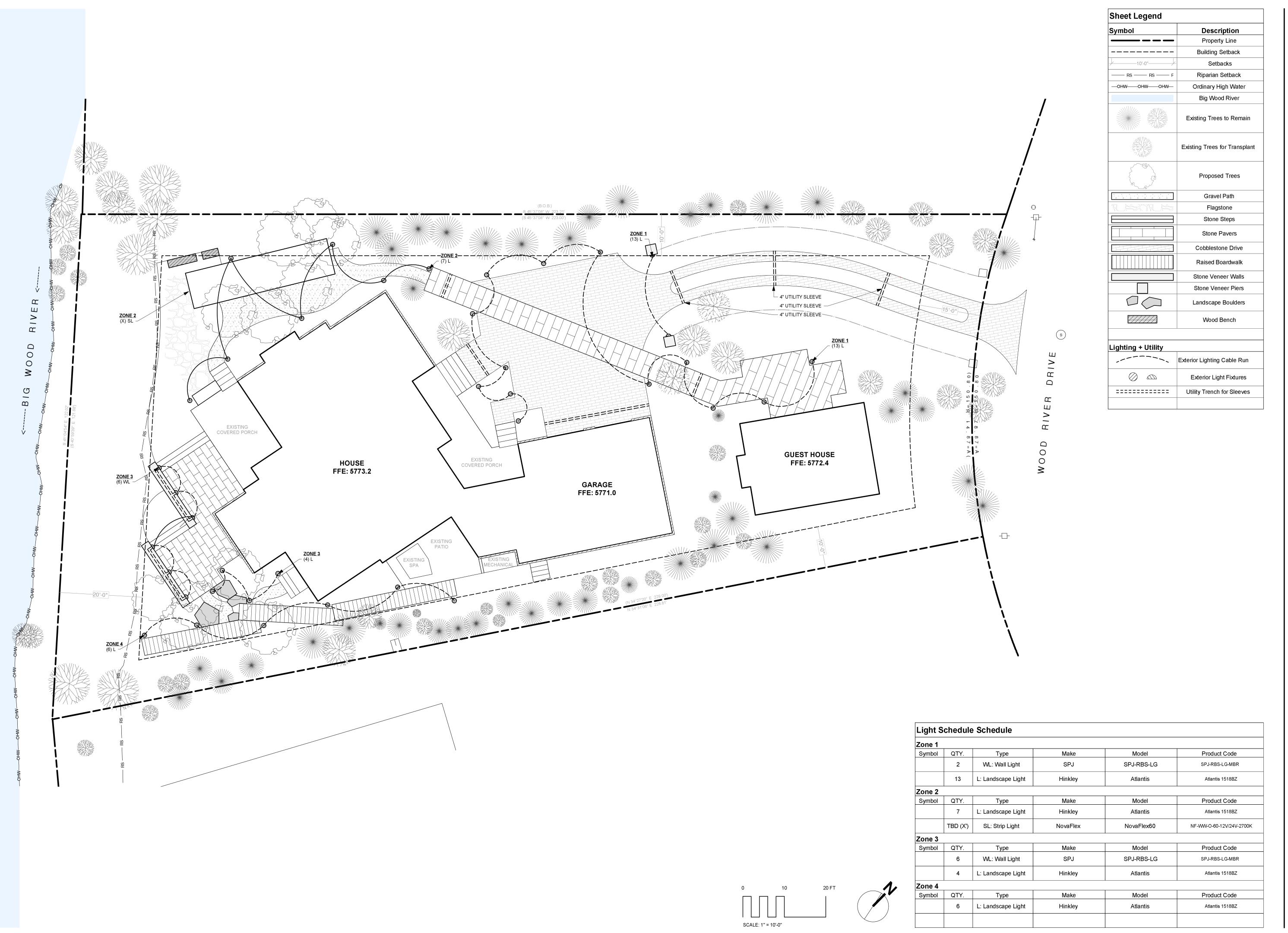
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Sheet No.

SCALE: 1" = 10'-0"





o. Description Si

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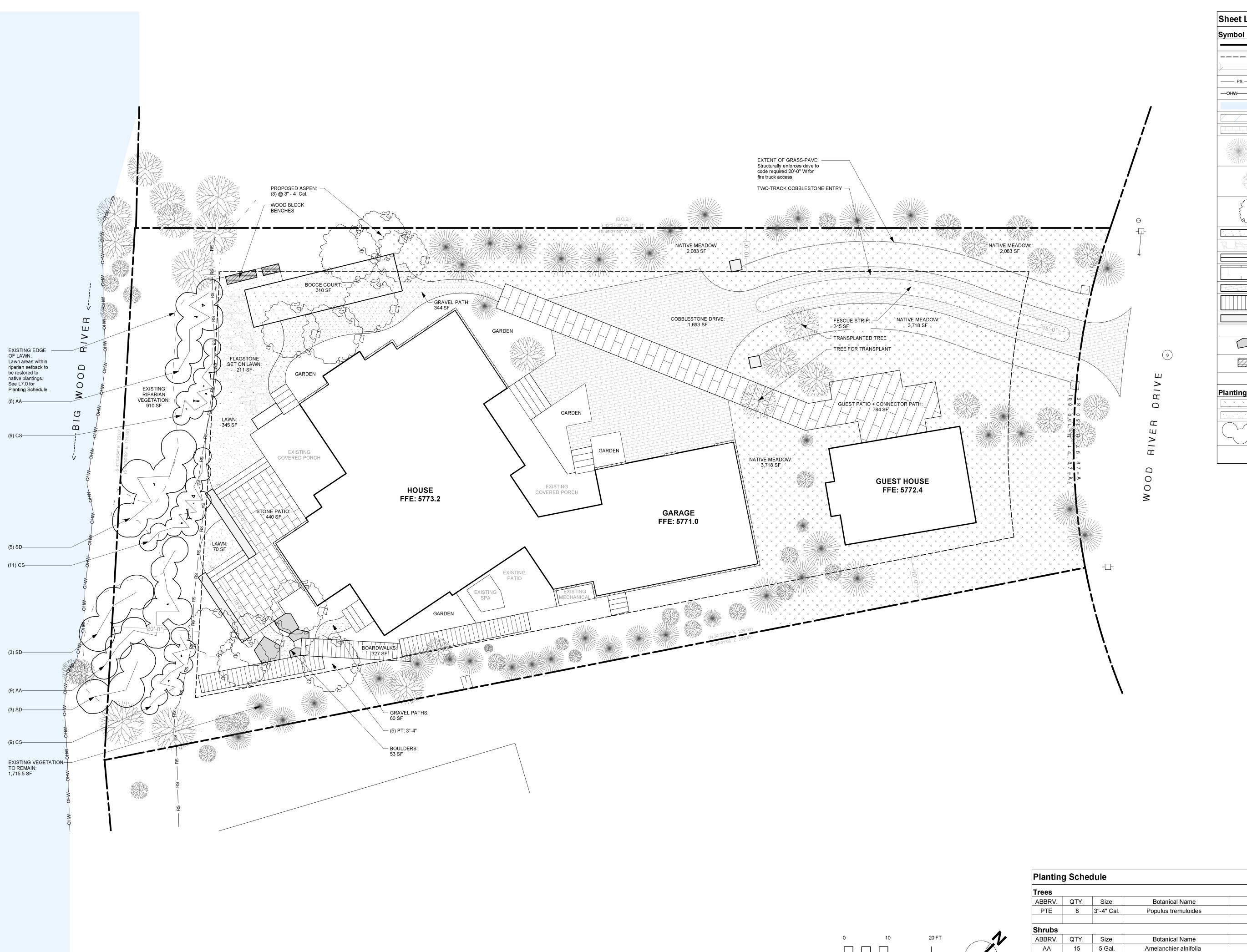
F REMODEL

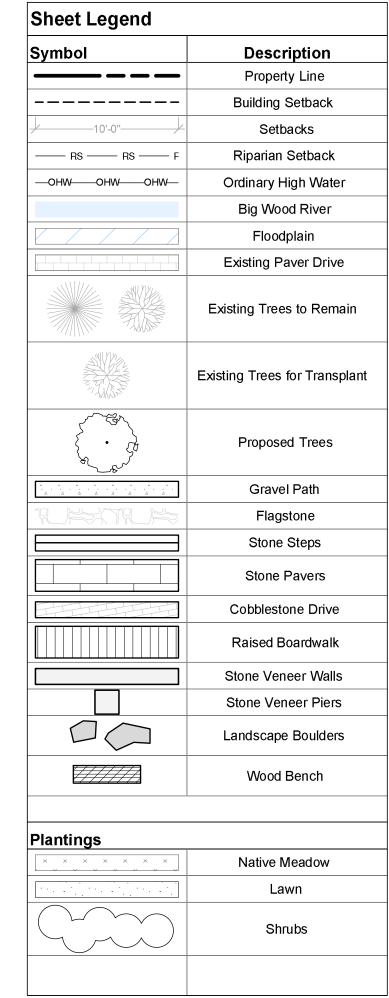
Lighting + Utility Plan

Date:
09.12.2016
Drawn By:
AB
Checked By:
AB, BY
File:
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Sheet No.

L6.0





BELLING RESIDENCE
LANDSCAPE REMODEL
WOOD RIVER DRIVE | KETCHUM, ID | 83340

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Planting Schedule

Date: 09.12.2016 Drawn By:

Drawn By:
AB
Checked By:
AB, BY

File:
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Sheet No.

Spacing

Per Plan

Spacing Per Plan

Per Plan

Per Plan

Common Name

Serviceberry Shrub

Cornus sericea

Drummond's Willow

5 Gal.

SD

SCALE: 1" = 10'-0"

Red Twig Dogwood

Salix drummondiana

L7.0

BACKGROUND FACTS

OWNER:

Philip Belling, verified July 15, 2003 with Assessor

REQUEST:

Waterways Design Review approval of a single family residence and an

accessory dwelling unit (ADU)

LOCATION:

Tax Lot 7527 (530 Wood River Drive)

NOTICE:

Adjacent property owners

ZONING:

Limited Residential (LR)

OVERLAY:

Floodplain (FP) Zone

FLOOR AREA:

EXISTING 630 sq.ft.

LOT AREA:

21,867 square feet

LOT COVERAGE:

19 percent (35 percent allowed)

BUILDING HEIGHT:

approximately 15 feet

PROPOSED SETBACKS:

FRONT (ADU): 27 feet REAR (riparian-main house): 32 feet

SIDE(S): 15 feet SIDE(N): 48 feet

REQUIRED SETBACKS:

FRONT: 15 feet REAR (riparian): 25 feet SIDE: 10 feet SIDE: 10 feet

CURB CUT (ADU): 11.5 perc

11.5 percent (35 percent allowed)

* Driveway must be increased to twenty (20) feet pursuant to Ketchum Fire Department requirements.

PARKING SPACES: 4 (2.5 required - 1.5 for primary residence and one (1) for ADU)

REVIEWER: Stefanie Webster, City Planner

GENERAL FINDINGS OF FACT

- 1. The applicant is requesting waterways design review approval of a 5,613 square foot single family dwelling and a 990 square foot, one (1) bedroom accessory dwelling unit (ADU). The 21,867 square foot lot is allowed an ADU up to 1,000 square feet in size pursuant to Section 17.108.010 of Title 17, Ketchum Zoning Code.
- 2. The ADU is an existing cabin that presently straddles the northern side of the subject property and an adjacent property to the north. The applicant is planning to remove a portion of the cabin and relocate the cabin to the southern part of the subject property.
- 3. An existing garage structure adjacent to Wood River Drive will be either demolished or relocated off-site.
- 4. At their meeting of July 28, 2003, the Commission continued this item to their August 11, 2003 meeting.

EVALUATION STANDARDS

STANDARD 17.108.010(M)(1):

Preservation of the site's significant natural features such as hillsides, mature trees and landscaping shall be encouraged. Cuts and fills shall be minimized and shall be concealed with landscaping, revegetation and/or natural material.

FOUND:

The cabin will be relocated to the south side of the property. It presently straddles the property line between the subject property and the property to the north. The relocation site is approximately where the garage structure sits. No mature trees will be removed for the site of the ADU. The riparian zone is addressed below in these Findings.

STANDARD 17.108.010(M)(2)(a):

The project shall be compatible with the townscape, the primary dwelling and surrounding neighborhoods with respect to height, bulk, setbacks and relationship to the street.

FOUND:

The cabin is an existing log structure. Neighboring properties have similar guesthouses or apartments over a garage.

STANDARD 17.108.010(M)(2)(b):

The project's materials and colors shall be compatible with the townscape, the primary dwelling and surrounding neighborhoods.

FOUND:

The existing structure is log siding painted gray. Neighboring structures have wood or other natural materials for the exteriors.

STANDARD 17.108.010(M)(3)(a):

There shall be continuity among accessory structures, fences, walls and landscape features within the project.

FOUND:

No fencing is indicated on the plans. Additional landscaping includes aspens, willows and dogwood.

STANDARD 17.108.010(M)(3)(b):

Exterior lighting shall not have an adverse impact upon other properties and/or public streets.

FOUND:

Exterior lighting shall conform to the Dark Sky Ordinance.

STANDARD 17.108.010(M)(3)(c):

Building design should include weather protection which prevents water from dripping or snow from sliding on areas where pedestrians gather and circulate or onto adjacent properties.

FOUND:

The existing cabin has a sloped roof. Weather protection devices are not indicated on the plans.

STANDARD 17.108.010(M)(4)(a):

Traffic shall flow safely within the project and onto adjacent streets. Traffic includes vehicle, bicycle, pedestrian and equestrian use. Consideration shall be given to adequate sight distances and proper signage.

FOUND:

A curb cut exists off of Wood River Drive. Traffic flows safely into the project.

STANDARD 17.108.010(M)(4)(b):

Parking areas have functional aisle dimensions, backup space and turning radius.

FOUND:

The backup space (and space for two [2] additional vehicles) for the garage is twenty (20) feet in length and twenty-eight (28) feet in width. The Ketchum Fire Department has commented that the driveway width must be increased to twenty (20) feet in order to meet their requirements.

STANDARD 17.108.010(M)(4)(c):

Location of parking areas is designed for minimum adverse impact upon adjacent properties with regard to noise, lights and visual impact.

FOUND:

Location of parking areas does not adversely impact neighbors.

STANDARD 17.108.010((M)(5)(a):

Landscaping shall provide a substantial buffer between land uses, including, but not limited to, structures, streets and parking areas.

FOUND:

New aspens, willows, dogwoods and pines will be planted to buffer the ADU from the street and adjacent properties.

STANDARD 17.108.010(O)(1):

The accessory dwelling unit shall be secondary in nature to the primary residence.

FOUND:

The ADU is secondary in nature to a main residence that will be constructed beginning Fall of 2003 or Spring of 2004.

STANDARD 17.108.010(O)(2):

The accessory dwelling unit shall not be sold separately from the primary residence.

FOUND:

The ADU will not be sold separately.

STANDARD 17.108.010(O)(3):

The accessory dwelling unit shall be limited to long-term rental (60 day minimum).

FOUND:

The ADU will not be used for a vacation rental or short-term rental.

STANDARD 17.108.010(O)(4):

Parking requirement – one (1) parking space per studio and one bedroom units; two (2) parking spaces for two and three bedroom units.

FOUND:

The plans indicate one (1) parking space adjacent to the ADU is allocated for the one (1) bedroom unit.

STANDARD 17.108.010(O)(5):

Designated storage shall be provided for all accessory dwelling units.

FOUND:

A storage closet within the ADU is indicated on the plans.

STANDARD 17.88.060(E)(1):

Preservation or restoration of the inherent natural characteristics of the river and creeks and floodplain areas. Development does not alter river channel.

FOUND:

Development does not alter the river channel. One (1) two-stemmed cottonwood tree is proposed to be removed by the applicant due to a weak stem. All other riparian vegetation will be preserved.

STANDARD 17.88.060(E)(2):

Preservation of riparian vegetation and wildlife habitat, if any, along the stream bank and within the required minimum twenty-five (25) foot setback or riparian zone.

FOUND:

All of the riparian vegetation, except for one (1) two-stemmed cottonwood tree, will be preserved. According to a "Riparian Area Tree Brief" dated December 30, 2002, and authored by Bill Josey, Certified Arborist, of Arborcare Resources, Inc., the two-stemmed cottonwood has a significant lean towards the site of the new residence and has a weak stem. Mr. Josey recommends removal of the tree due to the fact that cottonwoods are prone to failure and the tree's proximity to the new residence. Mr. Josey recommends leaving the low cut stumps and root system for river bank stabilization. If the Commission deems that the removal of the cottonwood is necessary, Staff recommends that the stumps and root system remain intact.

STANDARD 17.88.060(E)(3):

Plan and time frame are provided for restoration of riparian vegetation damaged as a result of the work done.

FOUND:

No restoration or new planting is proposed in this application.

STANDARD 17.88.060(E)(4):

New or replacement planting and vegetation includes plantings that are low-growing and have dense root systems for the purpose of stabilizing stream banks and repairing damage previously done to riparian vegetation. Examples of such plantings include: red osier dogwood, common choke cherry, service berry, elder berry, river birch, skunk bush sumac, beb's willow, drummond's willow, little wild rose, gooseberry, and honeysuckle.

FOUND:

No new landscaping is proposed in the riparian zone. The applicants are requesting removal of one (1) two-stemmed cottonwood tree within the riparian zone. Staff found that the

northern section of the riparian zone needs some additional landscaping with native grasses and riparian shrubs. Staff recommends that a detailed landscaping plan, prepared by a landscape architect, for the twenty-five (25) foot riparian zone be submitted to the Planning Department for review and approval prior to issuance of a building permit.

STANDARD 17.88.060(E)(5):

Landscaping and driveway plans to accommodate the function of the floodplain to allow for sheet flooding. Flood water carrying capacity is not diminished by the proposal. Surface drainage is controlled and does not adversely impact adjacent properties including driveways drained away from paved roadways. Culvert(s) under driveways may be required. Landscaping berms are designed to not dam or otherwise obstruct floodwaters or divert same onto roads or other public pathways.

FOUND:

A drainage plan has been submitted to the City. Flood water carrying capacity is not diminished by the proposed project.

STANDARD 17.88.060(E)(6):

Impacts of the development on aquatic life, recreation, or water quality upstream, downstream or across the stream are not adverse.

FOUND:

The project does not have adverse impacts on the waterway.

STANDARD 17.88.060(E)(7):

Building setback in excess of minimum required along waterways is encouraged.

FOUND:

The rear terrace of the new residence is set back thirty-two (32) feet from the mean high water mark, in excess of the twenty-five (25) foot setback requirement.

STANDARD 17.88.060(E)(8):

The bottom of the lowest floor in the floodplain is a minimum of one (1) foot above the I.R.F.

FOUND:

The bottom of the lowest floor of the structure is 1 foot 8 inches above the I.R.F. of 5,766 feet.

STANDARD 17.88.060(E)(9):

The back fill used around the foundation in the floodplain provides a reasonable transition to existing grade, but is not used to fill the parcel to any greater extent.

FOUND:

The backfill to existing grade is at an approximately two (2) percent grade transition.

STANDARD 17.88.060(E)(10):

Driveways comply with effective Street Standards; access for emergency vehicles has been adequately provided for.

FOUND:

The Ketchum Fire Department states that the driveway must increase to twenty (20) feet in width from the curb cut on Wood River Drive to within 150 feet of the furthest point of the structure.

STANDARD 17.88.060(E)(11):

Landscaping or revegetation conceals cuts and fills required for driveways and other elements of the development.

FOUND:

Cut and fill calculations will be verified at building permit stage.

STANDARDS 17.88.060(E)(12), (13), (14), (16), (17) and (18):

These standards are for stream alteration projects.

FOUND:

The project does not propose a stream alteration, therefore, these standards were not applicable.

STANDARD 17.88.060(E)(15):

Wetlands are not diminished.

FOUND:

No wetlands exist on the site.

CONCLUSIONS OF LAW

- 1. The City of Ketchum is a municipal corporation organized under Article XII of the Idaho Constitution and the laws of the State of Idaho, Title 50, Idaho Code.
- 2. Under Chapter 65, Title 67 of the Idaho Code, the City has passed a land use and zoning code, Title 17.
- 3. The Commission has authority to hear the applicant's Design Review Application pursuant to Chapters 17.108 and 17.88 of Ketchum Code Title 17.

- 4. The City of Ketchum Planning Department provided adequate notice for the review of this application.
- 5. The project **does** meet the standards of approval under Chapters 17.108 and 17.88 of Zoning Code Title 17.

DECISION

THEREFORE, the Ketchum Planning and Zoning Commission approves this Design Review Application this 11th day of August, 2003, subject to the following conditions:

- 1. Ketchum Water, Sewer, Fire and Building Department requirements shall be met;
- 2. Design review approval shall expire six (6) months from the date of approval;
- 3. Design review elements shall be completed prior to occupancy;
- 4. Construction fencing shall be located at the 25 foot setback line prior to issuance of a building permit, and shall remain in place for the duration of construction to limit the disturbance to the riparian areas;
- 5. This Design Review approval is based on the plans and information presented and approved at the meeting on the date noted herein. Building Permit plans must conform to the approved Design Review plans unless otherwise approved in writing by the Commission or Planning and Zoning Administrator. Any building or site discrepancies which do not conform to the approved plans will be subject to removal;
- 6. Riparian vegetation and other landscaping are maintained in perpetuity as shown on approved plans;
- 7. Restoration of damaged riparian vegetation within the riparian zone shall be required prior to completion of the proposed project. A bond to assure such restoration may be required prior to commencement of such work;
- 8. The accessory dwelling unit shall be secondary in nature to the primary residence;
- 9. The accessory dwelling unit shall not be sold separately from the primary residence;
- 10. The accessory dwelling unit shall be limited to long-term rental (60 day minimum);

- 11. One (1) parking space shall be maintained for use by residents of the ADU;
- 12. Designated storage shall be provided for all accessory dwelling units;
- 13. The driveway shall be increased to twenty (20) feet in width from the curb cut to 150 feet of the farthest point of the structure;
- 14. The low cut stump and root system of the two-stemmed cottonwood shall remain in place after removal of the tree. Staff shall be notified of the date when the tree will be removed;
- 15. All exterior lighting shall conform to the City of Ketchum Dark Sky Ordinance;
- 16. No exterior lighting fixtures, patios, or decking shall extend into the twenty-five (25) foot riparian zone. All grasses and vegetation within the riparian zone shall remain natural and the riparian zone shall not be mowed; and,
- Prior to issuance of a building permit for the primary residence, a detailed landscaping plan, prepared by a landscape architect, for the twenty-five (25) foot riparian zone shall be submitted to the Planning Department for review and approval.

Findings of Fact adopted this 25th day of August, 2003.

Peter Ripsom, Chairman

Ketchum Planning and Zoning Commission

530 Wood River Drive Ketchum, Idaho

Riparian Area Tree Brief

Prepared for:
Phil Belling
&
Jarvis Group

Prepared By:
Bill Josey, Certified Arborist
ArborCare Resources, Inc.
PO Box 397
Hailey, Idaho 83333

December 30, 2002

ARBORIST DISCLOSURE STATEMENT

Arborists are tree specialists who use their education, knowledge, training and experience to examine trees, recommend measures to enhance the beauty and health of trees, and attempt to reduce the risk of living near trees. Clients may choose to accept or disregard the recommendations of the arborists, or to seek additional advice.

Arborists cannot detect every condition that could possibly lead to the structural failure of a tree. Trees are living organisms that fail in ways we do not fully understand. Conditions are often hidden within trees and below ground. Arborists cannot guarantee that a tree will be healthy or safe under all circumstances, or for a specified period of time. Likewise, remedial treatments, like any medicine, cannot be guaranteed.

Treatment, pruning and removal of trees may involve considerations beyond the scope of the arborist's services such as property boundaries, property ownership, site lines, disputes between neighbors, and other issues. Arborists cannot take such considerations into account unless complete and accurate information is disclosed to the arborist. An arborist should then be expected to reasonably rely upon the completeness and accuracy of the information provided.

Trees can be managed, but they cannot be controlled. To live near trees is to accept some degree of risk. The only way to eliminate all risk associated with trees is to eliminate all trees.

Observations:

The two stem Cottonwood has stems ~12 and 14 inches diameter breast height (DBH) in size (refer to photos A and C). Both stems have significant leans, one to the southeast the other to the east. A large dead Cottonwood stump is also located riverside of the two stems and has decay all the way from the top down to snow level. This large stump appears to have been a parent tree and accounts for the lean of the other stems. All three stems come to the ground in close proximity with no other tall trees nearby, so consequently I suspect an interconnected root system, which raises concern. A vertical stress crack was found in the northern tree stem.

The native Alder (see photo B and D) is a nice multiage stand with some broken stems, some dead stems, and a lot of young newly developing stems that is very typical of this species. Intermixed in this stand are some Cottonwood, Dogwood and Current. This stand is located on the north property line area and also surrounds the large Cottonwood on the north property.

The Cottonwood on the north property, (see photo D), is ~ 30" DBH with a slight lean to the east-northeast (towards the home on the north lot). The tree has been topped in the past but the stem appears solid above the snowline. There are no other tall trees in the area.

Discussion and Recommendations:

Two Stem Cottonwood

Given the nature of Cottonwood species being weak wooded and prone to failure, add in the facts that normal supportive stand trees are missing, the stems lean, the suspected root issues with the parent tree and finally the proximity to the proposed construction, it is my recommendation to remove the two stem Cottonwood and the dead stump before construction begins, leaving the low cut stumps and root system for river bank stabilization.

Alder Stand

I suggest we work with the Alder stand by cleaning up the stand by removal of full select weak, damaged, and leaning stems, again leaving all stumps and root systems for bank stabilization, and strive to promote a multi aged regenerative stand for the future.

North Property Cottonwood

The north property Cottonwood has been "topped" in the past, which is not an accepted pruning practice by The International Society of Arboriculture. The topping created multiple branches at the topping points, sometimes referred to as a "witches broom", which create weak connections between branches and are very prone to rot, decay, and failure. There are no actions that can be done to correct this once the topping has been done. Given the nature of Cottonwood species as previously discussed, and the current condition of the tree, it is my recommendation that no structure or personal high use development is done within branch trough of the tree and consideration be given to removal as an option.

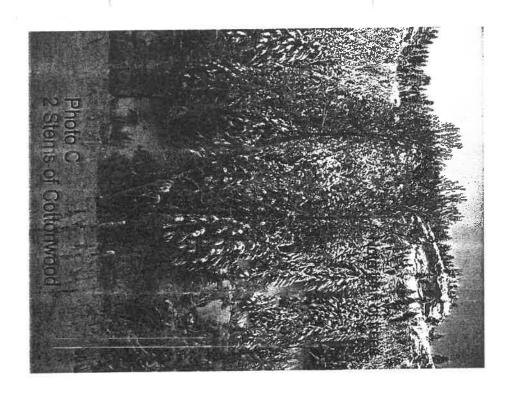
Bill Josey, Certified Arborist ArborCare Resources, Inc.

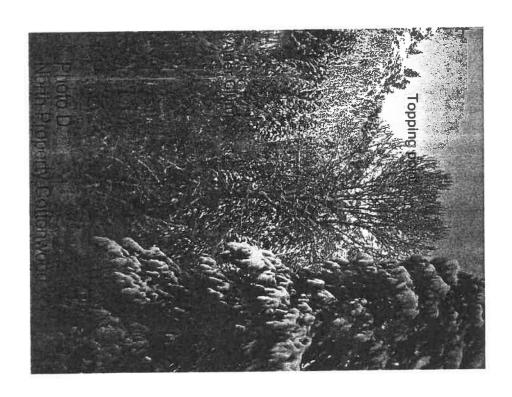
December 30, 2002





Bill Josey, Certified Arborist ArborCare Resources, Inc.





Bill Josey, Certified Arborist ArborCare Resources, Inc.

530 Wood River Drive, Ketchum, Idaho Addendum to Riparian Area Tree Brief Dated December 30, 2002

January 2, 2003

This addendum is intended to be additional clarifications relating to questions addressed at an on-site meeting with Phil Belling and Mark of the Jarvis Group on December 30, 2002 and a phone conservation with Daniel "Tipp" Cullum, owner of 550 Wood River Drive, Ketchum, Idaho, which is the property to the north of 530 Wood River Drive.

"Discussion And Recommendations"

Alder Stand

To clarify the stems recommended for removal in the Alder stand. Removal would be up to, but not exceed, 40% of the stand, however specific tagging at this point is not feasible due to the amount of snow.

North Property Cottonwood

The word "trough" should have been "throw" referring to "branch throw", or approximately 1.5 times the tree's drip line radius. In this specific case this equates to a majority of backyard of 550 Wood River Drive.

It was observed that an approximately 4-5" diameter branch in the tree has failed and is hanging.

Property owner, Tipp Cullum, has indicated that this area in the branch throw zone of the north property Cottonwood, is frequently used by his six grandchildren as a playground as well as used by other family and guests. Consequently he is **greatly** concerned about the safety of family and guests with respect to this tree and the potential for tree or branch failure. Since the majority of the Cullum backyard is in the branch throw area and avoidance of the area is unlikely, removal of the tree is now recommended due to current and future safety reasons.

Additional Comments:

Once any work has been approved, it can commence at any time.

Addendum to Belling Riparian Setback Vegetation Management Plan December 6, 2002

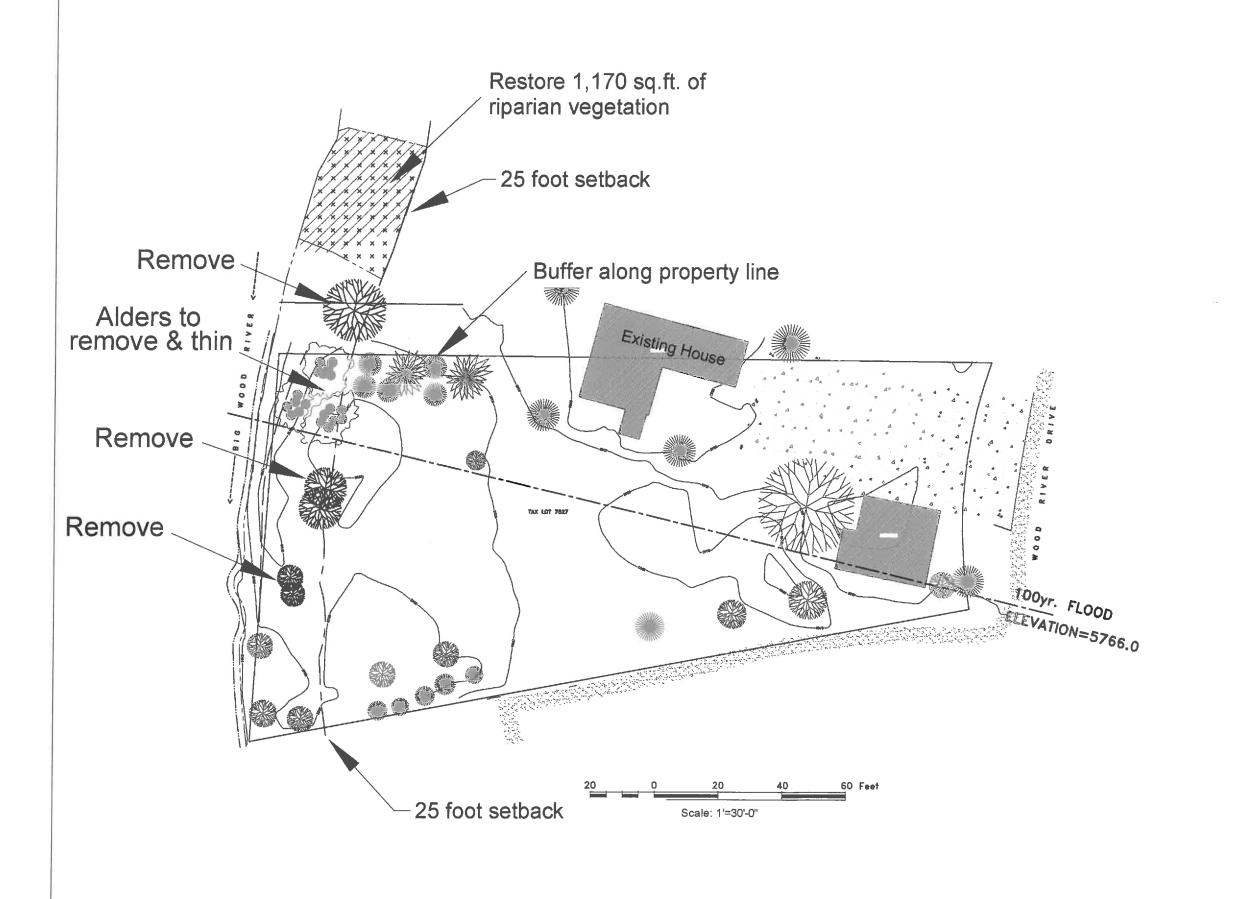
This document is submitted to help clarify some the details of Mr. Belling's proposed riparian management plan. The plan and map submitted on November 14, 2002, shows an 1,170 ft² (square foot) portion of bank in the riparian setback where restoration will occur. Photo #1 also shows the presently shrub-less area highlighted with yellow arrows. This area is on the neighboring owner's property and they are agreeable to the proposed plan.

We propose to plant 71 one-gallon shrubs (one per each four square feet) in this area. Each "x" in the restoration area is meant to represent one plant. Bebb's Willow (Salix bebbiana) and Drummond willow (Salix drummondiana) will be mixed 50/50 and planted along the bank parallel to the water and back into the land ten to fifteen feet. Inland of that line of shrubs we will change the mix to 50% Red osier dogwood (Cornus stolonifera) and 50% Bebb's willow.

The attached map shows an enlargement of the area. It also shows how the plants will be mixed according to the above description.

This plan prescribes installing the following plants:

36 1-gallon Bebb's willow (Salix bebbiana)	@ \$13.25/plant	\$477.00
18 1-gallon Drummond willow (Salix drummondiana)	238.50	
25 1-gallon Red osier dogwood (Cornus stolonifera)	@ \$16.00/plant	272.00
Installation - \$7.50/plant		<u>532.50</u>
*		
To	otal cost	\$1,520.00



GRASS MIX -

Apply in disturbed areas within 25 foot setback:

Species	lbs./1000 sq.ff	:.
Garrison creeping foxtail	0.5	10%
Annual rye grass	2.0	34%
Western wheatgrass	1.0	18%
Mountain brome	1.0	18%
Wild Rye (greatbasin, etc	e.) 0.5	10%
Idaho Fescue	0.5	10%

NATIVE RIPARIAN SHRUB & TREE LIST!

Species for use in 25 foot setback: Willow Salix spp.

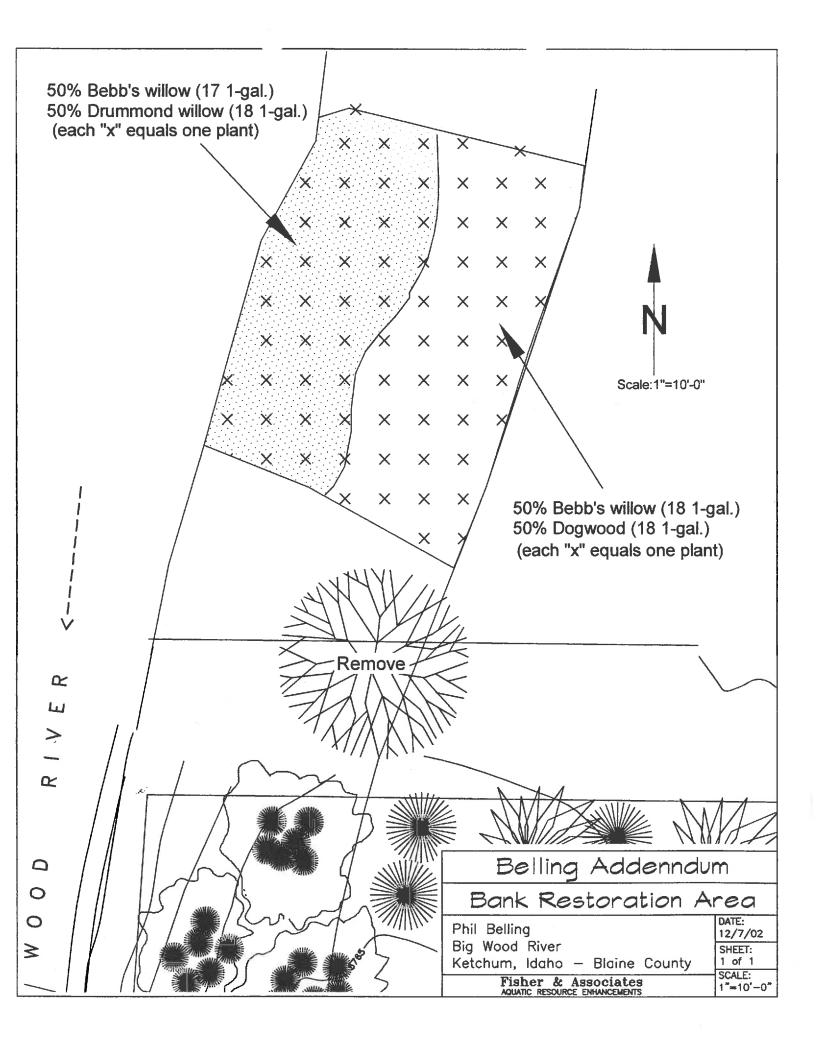
Dogwood Cornus stolonifera Cottonwood Populus trichocarpa

Species for use in uplands and outside setback: Wood's rose Rosa woodsii

Golden current Ribes aureum P. tremuloides Common

chokecherry Prunus virginiana Spruce - salvaged from neighbor

Belling - Tax Lot 752	27
Riparian Management	Plan
PHIL BELLING - 530 WOOD RIVER DRIVE	DATE: 11/14/02
ON BANK OF BIG WOOD RIVER KETCHUM, IDAHO — BLAINE, CO.	PAGE 6
Fisher & Associates AQUATIC RESOURCE ENHANCEMENTS	SCALE: 1"=30'-0"



BACKGROUND FACTS

OWNER: Philip Belling, verified July 15, 2003 with Assessor

REQUEST: Waterways Design Review approval of a single family residence and an

accessory dwelling unit (ADU)

LOCATION: Tax Lot 7527 (530 Wood River Drive)

NOTICE: Adjacent property owners

FLOOR AREA ADU: EXISTING 630 sq.ft.

LOT AREA ADU: 21,867 square feet

LOT COVERAGE ADU: 19 percent (35 percent allowed)

BUILDING HEIGHT ADU: approximately 15 feet

PROPOSED SETBACKS:

FRONT (ADU): 27 feet REAR (riparian-main house): 32 feet

SIDE(S): 15 feet SIDE(N): 48 feet

REQUIRED SETBACKS:

FRONT: 15 feet REAR (riparian): 25 feet SIDE: 10 feet SIDE: 10 feet

CURB CUT: 11.5 percent (35 percent allowed)

* Driveway must be increased to twenty (20) feet pursuant to Ketchum Fire Department requirements.

PARKING SPACES: 4 (2.5 required - 1.5 for primary residence and one (1) for ADU)

ZONING: Limited Residential (LR)

OVERLAY: Floodplain (FP)

REVIEWER: Stefanie Webster, City Planner

GENERAL FINDINGS OF FACT

- 1. The applicant is requesting waterways design review approval of a 5,613 square foot single family dwelling and a 990 square foot, one (1) bedroom accessory dwelling unit (ADU). The 21,867 square foot lot is allowed an ADU up to 1,000 square feet in size pursuant to Section 17.108.010 of Title 17, Ketchum Zoning Code.
- 2. The ADU is an existing cabin that presently straddles the northern side of the subject property and an adjacent property to the north. The applicant is planning to remove a portion of the cabin and relocate the cabin to the southern part of the subject property.
- 3. An existing garage structure adjacent to Wood River Drive will be either demolished or relocated off-site.
- 4. The applicant received Waterways Design Review approval from the Planning and Zoning Commission on August 11, 2003 and this approval has expired. The applicant must have a valid design review approval prior to receiving a building permit from the City.
- 5. Since the original design review approval, the two-stemmed cottonwood tree addressed under the Waterways Design Review evaluation standards and approved by the Commission for removal has been removed by a certified arborist.
- At their meeting of February 23, 2004, the Planning and Zoning Commission 6. continued this project to the meeting of March 8, 2004, in order for Staff to evaluate the degree of maintenance that has taken place within the riparian zone. The Jarvis Group, representing the applicant, submitted photographs of the two-stemmed cottonwood tree prior to removal and after removal. Apparently, this is the only tree that has been removed from the riparian zone. The photographs taken by Staff illustrate the trimming of the willow trees within the riparian zone that has occurred. Staff photographs indicated that the trees were trimmed to near ground level and the property owner has managed the vegetation within the riparian zone. The prior design review approval of August 11, 2003, included the following condition: "prior to issuance of a building permit for the primary residence, a detailed landscaping plan, prepared by a landscape architect, for the twenty-five (25) foot riparian zone shall be submitted to the Planning Department for review and approval." Staff recommends that this condition remain as part of this approval. If the snow is still too deep at the time of building permit submittal, then prior to final framing inspection, said plan shall be submitted to the Planning Department. This plan shall be reviewed and approved by the Planning Director and the Chair of the Planning and Zoning Commission prior to further approval of any construction work. Any restoration of the riparian zone shall take place prior to further approval by the City for construction work or the City will accept a bond for 150 percent of the estimated cost of landscaping improvements.

EVALUATION STANDARDS

STANDARD 17.108.010(M)(1):

Preservation of the site's significant natural features such as hillsides, mature trees and landscaping shall be encouraged. Cuts and fills shall be minimized and shall be concealed with landscaping, revegetation and/or natural material.

FOUND:

The cabin will be relocated to the south side of the property. It presently straddles the property line between the subject property and the property to the north. The relocation site is approximately where the garage structure sits at the time of these Findings. No mature trees will be removed for the site of the ADU.

STANDARD 17.108.010(M)(2)(a):

The project shall be compatible with the townscape, the primary dwelling and surrounding neighborhoods with respect to height, bulk, setbacks and relationship to the street.

FOUND:

The cabin is an existing log structure. Neighboring properties have similar guesthouses or apartments over a garage.

STANDARD 17.108.010(M)(2)(b):

The project's materials and colors shall be compatible with the townscape, the primary dwelling and surrounding neighborhoods.

FOUND:

The existing structure is log siding painted gray. Neighboring structures have wood or other natural materials for the exteriors.

STANDARD 17.108.010(M)(3)(a):

There shall be continuity among accessory structures, fences, walls and landscape features within the project.

FOUND:

No fencing is indicated on the plans. Additional landscaping includes aspens, willows and dogwood.

STANDARD 17.108.010(M)(3)(b):

Exterior lighting shall not have an adverse impact upon other properties and/or public streets.

FOUND:

Exterior lighting is not indicated on the plans.

STANDARD 17.108.010(M)(3)(c):

Building design should include weather protection which prevents water from dripping or snow from sliding on areas where pedestrians gather and circulate or onto adjacent properties.

FOUND:

The existing cabin has a sloped roof. Weather protection devices are not indicated on the plans.

STANDARD 17.108.010(M)(4)(a):

Traffic shall flow safely within the project and onto adjacent streets. Traffic includes vehicle, bicycle, pedestrian and equestrian use. Consideration shall be given to adequate sight distances and proper signage.

FOUND:

A curb cut exists off of Wood River Drive. Traffic flows safely into the project.

STANDARD 17.108.010(M)(4)(b):

Parking areas have functional aisle dimensions, backup space and turning radius.

FOUND:

The backup space (and space for two [2] additional vehicles) for the garage is twenty (20) feet in length and twenty-eight (28) feet in width. The Ketchum Fire Department has commented that the driveway width must be increased to twenty (20) feet in order to meet their requirements.

STANDARD 17.108.010(M)(4)(c):

Location of parking areas is designed for minimum adverse impact upon adjacent properties with regard to noise, lights and visual impact.

FOUND:

Location of parking areas does not adversely impact neighbors.

STANDARD 17.108.010(M)(5):

Landscaping shall provide a substantial buffer between land uses, including, but not limited to, structures, streets and parking areas.

FOUND:

New aspens, willows, dogwoods and pines will be planted to buffer the ADU from the street and adjacent properties.

STANDARD 17.108.010(O)(1):

The accessory dwelling unit shall be secondary in nature to the primary residence.

FOUND:

The ADU is secondary in nature to a main residence that will be constructed beginning in the spring of 2004.

STANDARD 17.108.010(O)(2):

The accessory dwelling unit shall not be sold separately from the primary residence.

FOUND:

The ADU will not be sold separately.

STANDARD 17.108.010(O)(3):

The accessory dwelling unit shall be limited to long-term rental (60 day minimum).

FOUND:

The ADU will not be used for a vacation rental or short-term rental.

STANDARD 17.108.010(O)(4):

Parking requirement – one (1) parking space per studio and one bedroom units; two (2) parking spaces for two and three bedroom units.

FOUND:

The plans indicate one (1) parking space adjacent to the ADU is allocated for the one (1) bedroom unit.

STANDARD 17.108.010(O)(5):

Designated storage shall be provided for all accessory dwelling units.

FOUND:

A storage closet within the ADU is indicated on the plans.

STANDARD 17.88.060(E)(1):

Preservation or restoration of the inherent natural characteristics of the river and creeks and floodplain areas. Development does not alter river channel.

FOUND:

Development does not alter the river channel. All other riparian vegetation will be preserved.

STANDARD 17.88.060(E)(2):

Preservation of riparian vegetation and wildlife habitat, if any, along the stream bank and within the required minimum twenty-five (25) foot setback or riparian zone.

FOUND:

The applicants are not proposing additional removal of vegetation at this time. According to a "Riparian Area Tree Brief' dated December 30, 2002, and authored by Bill Josey, Certified Arborist, of Arborcare Resources, Inc., the two-stemmed cottonwood had a significant lean towards the site of the new residence and had a weak stem. Mr. Josey recommended removal of the tree due to the fact that cottonwoods are prone to failure and the tree's proximity to the new residence. Mr. Josey recommended leaving the low cut stumps and root system for river bank stabilization. Based on Mr. Josey's recommendation, the Commission approved the removal of the two-stemmed cottonwood tree at their meeting of August 11, 2003.

STANDARD 17.88.060(E)(3):

Plan and time frame are provided for restoration of riparian vegetation damaged as a result of the work done.

FOUND:

No restoration or new planting is proposed in this application.

STANDARD 17.88.060(E)(4):

New or replacement planting and vegetation includes plantings that are low-growing and have dense root systems for the purpose of stabilizing stream banks and repairing damage previously done to riparian vegetation. Examples of such plantings include: red osier dogwood, common choke cherry, service berry, elder berry, river birch, skunk bush sumac, beb's willow, drummond's willow, little wild rose, gooseberry, and honeysuckle.

FOUND:

No new landscaping is proposed in the riparian zone. Staff found that the northern section of the riparian zone needs some additional landscaping with native grasses and riparian shrubs. The Commission recommended that a detailed landscaping plan, prepared by a landscape architect, for the twenty-five (25) foot riparian zone be submitted to the Planning Department for review and approval prior to issuance of a building permit.

STANDARD 17.88.060(E)(5):

Landscaping and driveway plans to accommodate the function of the floodplain to allow for sheet flooding. Flood water carrying capacity is not diminished by the proposal. Surface drainage is controlled and does not adversely impact adjacent properties including driveways drained away from paved roadways. Culvert(s) under driveways may be required. Landscaping berms are designed to not dam or otherwise obstruct floodwaters or divert same onto roads or other public pathways.

FOUND:

A drainage plan has been submitted to the City. Flood water carrying capacity is not diminished by the proposed project.

STANDARD 17.88.060(E)(6):

Impacts of the development on aquatic life, recreation, or water quality upstream, downstream or across the stream are not adverse.

FOUND:

The project does not have adverse impacts on the waterway.

STANDARD 17.88.060(E)(7):

Building setback in excess of minimum required along waterways is encouraged.

FOUND:

The rear terrace of the new residence is set back thirty-two (32) feet from the mean high water mark, in excess of the twenty-five (25) foot setback requirement.

STANDARD 17.88.060(E)(8):

The bottom of the lowest floor in the floodplain is a minimum of one (1) foot above the I.R.F.

FOUND:

The bottom of the lowest floor of the structure is 1 foot 8 inches above the I.R.F. of 5,766 feet.

STANDARD 17.88.060(E)(9):

The back fill used around the foundation in the floodplain provides a reasonable transition to existing grade, but is not used to fill the parcel to any greater extent.

FOUND:

The backfill to existing grade is at an approximately two (2) percent grade transition.

STANDARD 17.88.060(E)(10):

Driveways comply with effective Street Standards; access for emergency vehicles has been adequately provided for.

FOUND:

The Ketchum Fire Department states that the driveway must increase to twenty (20) feet in width from the curb cut on Wood River Drive to within 150 feet of the furthest point of the structure.

STANDARD 17.88.060(E)(11):

Landscaping or revegetation conceals cuts and fills required for driveways and other elements of the development.

FOUND:

Cut and fill calculations will be verified at building permit stage.

STANDARDS 17.88.060(E)(12), (13), (14), (16), (17) and (18):

These standards are for stream alteration projects.

FOUND:

The project does not propose a stream alteration, therefore, these standards were not applicable.

STANDARD 17.88.060(E)(15):

Wetlands are not diminished.

FOUND:

No wetlands exist on the site.

CONCLUSIONS OF LAW

- 1. The City of Ketchum is a municipal corporation organized under Article XII of the Idaho Constitution and the laws of the State of Idaho, Title 50, Idaho Code.
- 2. Under Chapter 65, Title 67 of the Idaho Code, the City has passed a land use and zoning code, Title 17.
- 3. The Commission has authority to hear the applicant's Design Review Application pursuant to Chapters 17.88 and 17.108 of Ketchum Code Title 17.
- 4. The City of Ketchum Planning Department provided adequate notice for the review of this application.
- 5. The project **does** meet the standards of approval under Chapters 17.88 and 17.108 of Zoning Code Title 17.

DECISION

THEREFORE, the Ketchum Planning and Zoning Commission approves this Design Review Application this 8th day of March, 2004, subject to the following conditions:

- 1. Ketchum Water, Sewer, Fire and Building Department requirements shall be met;
- 2. Design review approval shall expire six (6) months from the date of approval;
- 3. Design review elements shall be completed prior to occupancy;

- 4. Construction fencing shall be located at the 25 foot setback line prior to issuance of a building permit, and shall remain in place for the duration of construction to limit the disturbance to the riparian areas;
- 5. This Design Review approval is based on the plans and information presented and approved at the meeting on the date noted herein. Building Permit plans must conform to the approved Design Review plans unless otherwise approved in writing by the Commission or Planning and Zoning Administrator. Any building or site discrepancies which do not conform to the approved plans will be subject to removal;
- 6. Riparian vegetation and other landscaping are maintained in perpetuity as shown on approved plans;
- 7. Restoration of damaged riparian vegetation within the riparian zone shall be required prior to completion of the proposed project. Prior to final framing inspection for the primary residence, a detailed landscaping plan, prepared by a landscape architect, for the twenty-five (25) foot riparian zone shall be submitted to the Planning Department for review and approval by Staff and the Planning and Zoning Chairperson. A bond to assure such restoration shall be required prior to commencement of such work. Any restoration of the riparian zone shall take place prior to further approval by the City for construction work or the City will accept a bond for 150 percent of the estimated cost of landscaping improvements;
- 8. The accessory dwelling unit shall be secondary in nature to the primary residence;
- 9. The accessory dwelling unit shall not be sold separately from the primary residence;
- 10. The accessory dwelling unit shall be limited to long-term rental (60 day minimum);
- 11. One (1) parking space shall be maintained for use by residents of the ADU;
- 12. Designated storage shall be provided for all accessory dwelling units;
- 13. The driveway shall be increased to twenty (20) feet in width from the curb cut to 150 feet of the farthest point of the structure;
- 14. The low cut stump and root system of the two-stemmed cottonwood shall remain in place after removal of the tree;

- 15. All exterior lighting shall conform to the City of Ketchum Dark Sky Ordinance; and,
- 16. No exterior lighting fixtures, patios or decking shall extend into the twenty-five (25) foot riparian zone. All grasses and vegetation within the riparian zone shall remain natural and the riparian zone shall not be mowed. No removal of vegetation or maintenance of vegetation shall be performed without approval from the Planning Department.

Findings of Fact adopted this 22nd day of March, 2004

Rod Sievers, Chairman

Ketchum Planning and Zoning Commission



October 10, 2016

Planning and Zoning Commission City of Ketchum Ketchum, Idaho

Commissioners:

STAFF REPORT KETCHUM PLANNING AND ZONING COMMISSION REGULAR MEETING OF OCTOBER 10, 2016

PROJECT: Bracken Station Conditional Use Permit (CUP)

FILE NUMBERS: #16-034

OWNER: North Town Partners LLP

REPRESENTATIVE: Steve Cook, AIA

REQUEST: Conditional Use Permit (CUP) for a motor vehicle fueling station and food service

establishment

LOCATION: 911 N. Main Street (Ketchum, AM Lot 5A, Block 30)

ZONING: Light Industrial District Number 1 (LI-1)

NOTICE: Property owners within 300 foot radius of subject property were mailed notice on

May, 16, 2016. A public hearing notice was published in the Legal Notices of the Idaho Mountain Express on May 25, 2016. Notice was posted on the subject property and in

three public City locations on May, 17, 2016.

Continuation of the hearing to June 27, 2016 was announced during the June 13, 2016 hearing. Continuation of the hearing to July 11, 2016 was announced during the June 27, 2016 meeting. Continuation of the hearing to July 25, 2016 was announced at the July 11, 2016 meeting. Continuation of the hearing to October 10, 2016 was

announced at the July 25, 2016 meeting.

REVIEWER: Brittany Skelton, Senior Planner

Introduction

The applicant is requesting a Conditional Use Permit (CUP) to allow redevelopment of 911 N. Main for a motor vehicle fueling station and a food service establishment. Motor vehicle fueling stations and food service (subject to limitations on hours of operation and size) are only allowed in the LI-1 District if a Conditional Use Permit (CUP) is approved. The definition of motor vehicle fueling station permits retail sales of items of convenience to the motoring public. The Planning and Zoning Commission (Commission) has complete discretionary authority to approve, deny, or conditionally approve either use (fueling station or restaurant) or approve, deny, or conditionally approve both uses on the site, basing the decision upon findings of fact.

The first public hearing for this proposal occurred on June 13, 2016. The hearing was continued to June 27, 2016 and July 11, 2016. After receiving verbal public comment on July 11, 2016 the Commission closed the hearing and continued the application to July 25, 2016 to allow for the applicant's rebuttal and the Commission's deliberation. During the July 25, 2016 meeting the applicant provided rebuttal, the Commission deliberated and requested additional information of the applicant, and the application was continued to October 10, 2016. The Commission also re-opened the hearing to accept public comment on the new information.

Following the July 25, 2016 meeting on August 3, 2016 staff transmitted a letter to the applicant summarizing additional information requested by the Commission and new information requested by staff. This letter is attached, Attachment B. On September 30, 2016 staff transmitted an additional request for information to the applicant in a list format. This list is attached, Attachment E.

The report that follows contains an analysis of the new information submitted by the applicant as of Tuesday, October 4, 2016. The report addresses the implications of the proposed motor vehicle fueling station and food service on the proposed location and contains recommendations for how the Planning and Zoning Commission may mitigate impacts. Public comment received by 5:00 p.m. on Tuesday, October 4, 2016 for the October 10, 2016 public hearing is attached to the staff report.

Current Report

The location proposed for a motor vehicle fueling station and food service establishment is located on Lot 5A, Block 30, Ketchum Townsite, otherwise known as 911 N. Main Street. Three buildings currently exist on the site that are proposed to be substantially altered or removed for the project. Building "A" is the northernmost building, "B" is located in the center, and "C" is the southernmost building. The applicant proposes to partially demolish building "B" and to remodel and add an addition and a trellis patio to the remaining portion of the building. The applicant is also proposing to construct a canopy structure associated with the motor vehicle fueling station. The applicant is proposing to entirely demolish buildings "A" and "C" along with installing sidewalks, crosswalks, landscaping, lighting, parking, and drainage improvements to accommodate the development. The site does not currently meet city standards for the existing or proposed development and the site will require significant upgrades for the proposed project if the Planning and Zoning Commission determines a conditional use permit can be approved. Recommended improvements to meet city standards are contained within this report.

All city departments have completed their review of the applicant's submittals. This report contains an analysis of new information submitted by 5:00 p.m. Tuesday, October 4, 2016 for the October 10, 2016 hearing and contains an analysis of the information previously submitted for the July 11, 2016 hearing, which the Commission considered at the July 11, 2016 and July 25, 2016 meetings.

Currently there are three fueling stations in the LI District, two restaurants, and one food mart to service the area. In total there are five existing fueling stations within a 1.5 mile radius of the proposed site. The

Commission must decide if the proposed uses are appropriate for the site and location and if the uses are necessary to serve the LI district.

<u>Summary of New Information Received and Analysis</u>

- Attachment A. summarizes comments from all departments on the proposed development. *Updated* from the July 25, 2016 staff report
- Attachment B. is a letter from staff to the applicant dated August 3, 2016 requesting additional information and also contains a table summarizing the applicant's responses to the August 3, 2016 letter.
- Attachment C. summarizes and is an analysis of the new vehicle circulation exhibits received.
- Attachment D. consists of the new vehicle circulation exhibits.
- Attachment E. is a list of additional information and clarification requested by staff on September 30, 2016.
- Attachment F. is a memo from Hales Engineering dated October 4, 2016 responding to staff's September 30, 2016 request.
- Attachment G. is an analysis of the proposed project and the zoning standards required for all projects.
- Attachment H. is an analysis of the proposed project and the Conditional Use Permit required standards. *Updated from the July 25, 2016 staff report*
- Attachment I. summarizes the applicant's proposed public and private improvements. *Updated from the July 25, 2016 staff report*
- Attachment J. summarizes staff's additional recommended public improvements. *Updated from the July 25, 2016 staff report*

Summary of Prior Information Received and Analysis and Excerpts from Title 17: Zoning

- Attachment K. is an image depicting the subject property and its relationship to an existing footpath extending north from the Frenchman's Place condominium development to the subject property
- Attachment L. is a summary and analysis of new plans, studies and information received for the July 11, 2016 meeting that the Commission considered at the July 11 and July 25, 2016 meetings. No new analysis is contained in this attachment.
- Attachment M. lists the uses permitted in the LI-1 zoning district. *No new analysis is contained in this attachment.*
- Attachment N. describes the dimensional standards required in the LI-1 zoning district. *No new analysis is contained in this attachment.*
- Attachment O. describes the maximum building height and lot coverage permitted in the LI-1 zoning district and the building height and lot coverage proposed for the Bracken Station project. No new analysis is contained in this attachment.

Summary of Exhibits Submitted by the Applicant

- All other plans, studies and exhibits submitted by the applicant and not specified above are attached.
 Plans, studies and exhibits are arranged in reverse chronological order with the newest submittals appearing first.
- The Traffic Impact Study dated October 3, 2016 is attached as Appendix A.
- The Traffic Impact Study dated May 2016 is attached as Appendix B.

Summary of Public Comment Received

 All public comment submitted for the record is attached as Appendix C. Public comments received for the October 10, 2016 meeting appear first.

Conditional Use Permit Overview

The Planning and Zoning Commission must determine if a Conditional Use Permit can be approved for the fueling station and restaurant proposed for the LI-1 district. According to the Zoning Ordinance, conditional uses by definition possess characteristics that require review and appraisal by the Commission to determine whether or not the use would cause any public health, safety or welfare concerns. Conditional uses may only be allowed if the Commission determines there would be no impact to the public health, safety and welfare of the community.

A conditional use permit may be granted by the commission only if the applicant demonstrates that:

- The characteristics of the conditional use will not be unreasonably incompatible with the types of uses permitted in the applicable zoning district;
- The conditional use will not materially endanger the health, safety and welfare of the community;
- The conditional use is such that pedestrian and vehicular traffic associated with the use will not be hazardous or conflict with existing and anticipated traffic in the neighborhood;
- The conditional use will be supported by adequate public facilities or services and will not adversely
 affect public services to the surrounding area, or conditions can be established to mitigate adverse
 impacts;
- The conditional use is not in conflict with the policies of the comprehensive plan or the basic purposes of the Zoning Ordinance.

Should the Commission agree a CUP can be approved, they may attach additional conditions to the application approval as it determines necessary in order to make the uses more compatible with the vicinity and adjoining uses, mitigate impacts, and allow for health, safety and welfare. Such conditions may include, but are not limited to:

- A. Minimizing adverse impact on other development.
- B. Controlling the sequence and timing of development.
- C. Controlling the duration of development.
- D. Assuring that development is maintained properly.
- E. Designating the exact location and nature of development.
- F. Requiring the provision for on site or off site public facilities or services.
- G. Requiring more restrictive standards than those generally required in an ordinance.
- H. Requiring mitigation of effects of the proposed development upon service delivery by any political subdivision, including school districts, providing services within the city. (Ord. 1135, 2015)

STAFF RECOMMENDATION

Staff's analysis and concerns regarding the proposed Conditional Use are detailed in the attachments. In summary staff's concerns are as follows:

- 1. Staff has concerns with on-site circulation and resulting external impacts to traffic on Main Street. These concerns have not been resolved by the circulation exhibits submitted. See Attachment C, Attachment D, and Attachment H for detail.
- 2. Staff has concerns with traffic queuing that remain unresolved. See Attachment C, Attachment E, Attachment F, and Attachment H for detail.

After considering the above concerns raised by staff, the Commission must consider the Bracken Station CUP application as it relates to the criteria used for evaluating conditional use permits and has the option of approval or denial. If the Planning and Zoning Commission chooses to approve the application, staff recommends requiring the conditions of approval as identified in this report as a minimum. The Commission

may require additional conditions based on findings received through public comment, testimony, or other discovery.

COMMISSION OPTIONS

- 1. **Denial of the Application**: "Motion to deny the application from North Town Partners LLP for a Conditional Use Permit application for a motor vehicle fueling station and food service, finding the application **does not** meet the standards for approval under Chapter 17.116 of Ketchum Zoning Code Title 17, for the following reasons: [cite findings for denial]."
- 2. **Approval of the Application**: "Motion to approve the application from North Town Partners LLP for a Conditional Use Permit application for a motor vehicle fueling station and food service, finding the application meets the standards for approval under Chapter 17.116 of Ketchum Zoning Code Title 17 with the following conditions: [insert conditions of approval here]"
- 3. **Continuation of the Application**: "Motion to continue the application from North Town Partners LLP to a date certain of [insert date of meeting]."

RECOMMENDED CONDITIONS

Ketchum City Engineer, Streets, Utilities, Fire and Building Department requirements shall be met, including:

- 1. All departmental conditions as described in Table 1.
- 2. All building and fire code requirements as dictated by 2012 family of international building codes shall apply to all construction onsite.
- 3. Snow removal outside the travel lanes of Highway 75 shall be the responsibility of the property owner.
- 4. All light fixtures mounted on or recessed into the lower surface of the service station canopy shall be fully shielded and utilize flat lenses. Such shielding must be provided by the fixture itself; shielding by surrounding structures, such as canopy edge, is not permitted.
- 5. The applicant shall construct the public improvements recommended by staff described in Table 1.
- 6. The applicant shall construct the public improvements recommended by staff as described in Table 6.
- 7. The applicant shall construct the public improvement recommended by staff described in Table 7.
- 8. All storm water retention improvements shall meet the latest standards for motor vehicle fueling stations and shall be approved by the Public Works Director.
- 9. Per Title 17, Section 17.116.080: TERM OF PERMITS: Conditional Use Permit approval shall expire one (1) year from the date of approval if not acted upon within that time frame; and
- 10. This Conditional Use Permit approval is based on representations made and other components of the application presented and approved at the meeting on October 10th, 2016.

ATTACHMENTS:

- A. Table 1: Requirements for All Applications
- B. Table 2: Summary of additional information requested in the August 3, 2016 letter from staff as submitted by the applicant by October 4, 2016
- C. Table 3: Analysis of Vehicle Circulation Exhibits dated September 16, October 3 and October 4, 2016
- D. Circulation Exhibits dated September 16, 2016, October 3, 2016 and October 4, 2016
- E. Additional Information Requested by Staff on September 30, 2016
- F. "Bracken Station Additional Information Requested Response" memo from Hales Engineering, dated October 4, 2016
- G. Table 4: Table 4. Zoning Standards Analysis
- H. Table 5: Conditional Use Permit Requirements
- I. Table 6: Required Public and Private Improvements
- J. Table 7: Recommended Additional Public Improvements
- K. Aerial Photo of subject property and footpath connecting to Frenchmen's Place
- L. Table 8. Summary and Analysis of New Plans, Studies and Information Received for the July 11, 2016, meeting
- M. Table 9: Uses in the LI-1 Zone
- N. Table 10: Dimensional Standards for the LI-1 Zone
- O. Table 11: Potential Build Out for 911 N. Main Street
- P. Application
- Q. Revised Site Plan, dated September 16, 2016
- R. Revised Overall Site Plan, dated September 16, 2016
- S. Site Plan Changes exhibit, dated September 16, 2016
- T. Kellerstrass Oil Company letter, dated September 23, 2016
- U. Kellerstrass Oil Company letter, dated October 1, 2016
- V. Kellerstrass Delivery Truck Dimensions
- W. Letter from Ned Williamson, applicant's attorney, dated July 22, 2016
- X. Plans as submitted for the July 11, 2016 meeting
 - a. A.0 Coversheet, dated May 23, 2016
 - b. Existing Site Plan
 - c. A-2 Conditional Use / Preapplication Site Plan, dated June 30, 2016
 - d. A-2.1 Overall Conditional Use / Preapplication Site plan, dated June 30, 2016
 - e. A.3 North Elevation, dated May 23, 2016
 - f. A.5 Proposed Flood Plan and Proposed East Elevation, dated May 23, 2016
 - g. A.6 Proposed Retaining Walls at Alley, dated May 23, 2016
 - h. EX Preliminary Improvements Plan, dated June 3, 2016
 - i. EX Preliminary Grading & Drainage Plan, dated June 3, 2016
 - j. On-Site Vehicle Turn Exhibit, dated July 11, 2016
 - k. 10th Street Vehicle Turn Exhibit, dated July 11, 2016
 - I. Highway 75 Frenchman Sidewalk Connection, dated July 11, 2016
 - m. Profile From North of 10th Street to South of 10th Street, dated July 11, 2016
 - n. L1.0 Landscape Plan, dated July 1, 2016
 - o. Proposed North Elevation 10th Street View
 - p. L.1 Lighting Plan, dated June 30, 2016
 - q. Site lighting fixtures, types A-F
 - r. Photometric Plan, black and white, dated June 20, 2016
 - s. Photometric Plan, color, no date
 - t. Radiosity Plan, dated June 20, 2016
- Y. Motor Fueling Station Pedestrian Analysis, dated June 29, 2016
- Z. Connector Sidewalk from Bracken Station to Frenchman's e-mail, dated June 27, 2016

- AA. Retail S Analysis, dated January 2016
- BB. Renderings existing conditions and proposed development, north and south views
- CC. Chevron monument sign example
- DD. Idaho Department of Environmental Quality's Rules Regulating Underground Storage Tank Systems
- EE. Seismic Behavior of Xerxes Underground Tanks memorandum, dated September 11, 2007
- FF. Xerxes Fiberglass Underground Storage Tanks brochure
- GG. Ketchum Bracken Station TIS, Additional Information memorandum, dated July 6, 2016

Appendix A - "Bracken Station Traffic Impact Study Updated" dated October 3, 2016

Appendix B – "Ketchum Gas Station Traffic Impact Study Updated" dated May 2016

Appendix C – Public Comment Received June 25, 2016 through October 4, 2016

Attachment A

Table 1: Requirements for All Applications

	General Requirements for All Applications				
Compliant			Standards and Staff Comments		
Yes	No	N/A	City Code	City Standards and Staff Comments	
\boxtimes			17.116.040(A)	Complete Application	
			Department and Boards/ Commissions Comments	Public Works Department: 1. The On Site Vehicle Turn Exhibit, dated July 11, 2016, did not adequately indicate that the fueling station would not cause congestion on Main Street/HWY 75. It appeared that north-bound trucks with trailers or box trucks would not be able to maneuver the site when other vehicles were positioned at the fueling islands. Additional exhibits depicting recreational vehicles, commercial delivery trucks, and fuel delivery trucks circulating the site within the context of the composition of vehicles observed at a comparable development were requested. An analysis of the new exhibits is contained in Attachment C., Table 3. As detailed in the table, the exhibits do not adequately prove that the size and configuration of the site can simultaneously accommodate the circulation of the fueling and delivery trucks, whose deliveries are necessary to operate the business, and patron	
				of the business in such a manner that will not cause queuing or backing up on Main Street/HWY 75 during peak times. A number of exhibits illustrate optimal conditions in which commercial delivery vehicles, fuel delivery trucks, passenger vehicles towing RVs, and other oversize vehicles can circulate the site, but in many cases the optimal condition is dependent on one particular or two tandem fuel pumps being available. If an oversize vehicle is forced to queue and wait on site for an optimal fuel pump to become available, and vehicles in position at fueling pumps are not parked optimally, pinch points are created at the entrance to the site. The pinch points could constrain or prevent vehicles in the travel or turn lane from entering the site thus causing traffic backups on Main Street/HWY 75.	
				 Each circulation exhibit is described in detail in Attachment C., Table 3, and all circulation exhibits are attached as Attachment D. The configuration of the sidewalk design creates a challenge for the City's snow removal operations. If the project is approved, a condition of approval will require the owner to remove the snow to the west of the valley gutter and the snow may not be placed back out in the roadway. 	
				 The additional crosswalk crossing Main Street at the northern end of the site, as proposed in the Motor Fueling Station Pedestrian Analysis and with ADA compliant ramps, is recommended. Colored pedestrian areas, as proposed #4 in Figure 2 in the 	

- Pedestrian Analysis, is recommended; a Maintenance Agreement stating that owner shall maintain the pedestrian areas will be required if the conditional use permit is approved.
- To address pedestrian traffic from the southwestern pedestrian catchment area referenced in the Pedestrian Analysis, further analysis of the need for the Rectangular Rapid Flashing Beacon at the intersection of Warm Springs Road and 10th is needed.
- 6. As proposed in the Pedestrian Analysis, further study of the feasibility of defining the gap in the sidewalk on the north side of 10th Street between Warm Springs Road and Main Street is needed.
- 7. The property owner will need to maintain the landscaping in the right-of-way, according to ITD standards.
- 8. The initial On Site Vehicle Turn Exhibit only illustrates turn movements in an empty parking lot, which does not adequately prove turn movements can be made in real world conditions. In order to recommend approval of the conditional use permit the On-Site Vehicle Turn Exhibit needs to be revised to include turn movements, vehicles in the parking lot, and the location(s) where vehicles can stack on site.
- 9. The Preliminary Grading and Drainage Plan has been reviewed and is acceptable. Prior to issuance of a building permit a seepage test will need to be conducted and clarification regarding the infiltration rate and storm intensity and number of dry wells will be required.
- 10. The 5' sidewalk connecting to Frenchman's Place is acceptable. The existing drywell indicated on the plan is a catch basin and it shall be abandoned after installation of the new drywells.

Fire Department:

- 1. The project shall meet all 2012 International Fire Code requirements in addition to specific City Building and Fire Ordinances.
- 2. An approved fire detection system shall be installed per City of Ketchum Ordinance #1125 (www.ketchumfire.org) and the requirements of NFPA 72. Two (2) sets of alarm system plans shall be submitted to the Ketchum Fire Department for approval and a permit is required prior to installation of alarm systems. Inspections of fire detection systems by the Fire Chief or an appointee are required and shall be scheduled at least 48 hours in advance.
- 3. An approved access roadway per 2012 International Fire Code Appendix D (www.ketchumfire.org) shall be installed prior to any combustible construction on the site. The road shall be a minimum of twenty (20) feet in width and capable of supporting an imposed load of at least 75,000 pounds. The road must be an all-weather driving surface maintained free, clear, and unobstructed at all times.
- 4. Fire extinguishers shall be installed and maintained per 2012 IFC Section 906 both during construction and upon occupancy of the building.
- 5. An approved key box shall be installed, with the appropriate keys, for emergency fire department access in a location approved by the fire department. The key box shall be a Knox box brand and sized to accommodate keys to every door of the project.
- 6. The underground fuel tanks will be installed and tested following the

- 2012 International Fire Code, Sections 5704.2.11 through Section 5704.2.12.2.
- 7. Motor fuel dispensing stations will be installed following the 2012 International Fire Code, Section 2306.7 through Section 2306.7.7.2.
- 8. The Liquefied Petroleum Gas fuel dispensing will be installed following the 2012 International Fire Code, Section 2307.1 through Section 2307.7

Building:

• Building plans must meet 2012 International Building Code.

Police Department:

No comment.

Utilities:

No comment.

Parks/Arborist:

- 1. The owner shall maintain the landscaping in the right-of-way, which is managed by ITD.
- 2. The southeastern-most Abies lasiocarpa is in close proximity to the overhead transmission line, substitute a more hardy bristlecone pine.
- 3. The other species are good and the diversity and placement are appreciated.
- 4. Staff recommends retaining the tree that is adjacent to the existing power pole in the right-of-way on Main Street if ITD will allow it.

Attachment B

Table 2. Summary of additional information requested in the August 3, 2016 letter from staff as submitted by the applicant by October 4, 2016

Information requested by staff and the Commission in the letter to the applicant dated August 3, 2016, is detailed in the rows shaded in yellow. An analysis of the items submitted is detailed in the rows with a white background.

Traffic Study

1. Design Horizon

a. Use a design horizon of 10 years, or as defined by the Public Works Director.

The Public Works Director requested design horizons of 2020 and 2026; these horizons were used in the "Bracken Station Traffic Impact Study Updated," prepared by Hales Engineering, dated October 3, 2016, and attached to the staff report in Appendix A.

2. Peak time periods

- a. Use 7:00 9:00 a.m. instead of 8:00 9:00 a.m.
- b. Use 4:00 6:00 p.m. instead of 4:15-5:15 p.m.

Traffic counts were taken at the intersection of 10th Street and SH-75 (Main Street) and 5th Street and SH-75 (Main Street) on Thursday, September 1st, 2016 and Monday, September 5th, 2016 (Labor Day). The traffic volumes were highest on Thursday, September 1st with the a.m. peak hour being 8:00 a.m. to 9:00 a.m. and the p.m. peak hour being 4:15 to 5:15 p.m. Detailed traffic count data is included as Appendix A to the "Bracken Station Traffic Impact Study Updated," dated October 3, 2016, by Hales Engineering.

3. Trip Generation

- a. The study must identify the ITE trip generation rated used. The existing study only shows the total daily trips and total trips in a.m. and p.m. periods. The study does not show the factor used to compute the trips—for example; ITE has a trip generation factor for hourly, a.m. peak and p.m. peak. Indicate the factor used to verify the project trip generation is correct.
- b. Trip generation also needs to include daily, a.m., and p.m. trips associated with a high turnover restaurant, per the ITE generation rates, in addition to the trip generation for Gasoline/Service Station with Convenience Market 8 Vehicle Fueling Positions.
 - i. Include tables indicating average length of stay in a parking space for such uses.
 - ii. Address whether the proposed number of on-site parking spaces adequately meets the demand for parking spaces based on the uses and average length of stay in a parking space.

Staff directed the applicant to collect data at the Chevron gas station located at 209 S. Main Street in Hailey, Idaho because the Chevron was the closest, most comparable operation in the Wood River Valley for which permission to collect observational data could be obtained. The Chevron in Hailey, Idaho has fuel pumps, accessory retail, and a mobile food vendor often operates on the property. The mobile food vendor was parked on the premises during the time period data was collected on September 1, 2016.

Hales Engineering contracted with L2 Data Collection of Boise, ID to collect trip generation, length of stay, and observational data at the Hailey Chevron on Thursday, September 1, 2016 and Monday, September 5, 2016. Data was collected by filming the location and then analyzing the film to quantify trip generation, vehicle composition, and lengths of stay. Trip generation was higher on Thursday, September 1, 2016 than Monday, September 5, 2016 and staff directed Hales Engineering to use the September 1 data.

On page ii of the Executive Summary to the "Bracken Station Traffic Impact Study Updated," dated October 3, 2016, Hales Engineering reports:

"As directed by Ketchum City staff, trip generation for the development was calculated using data collected at an existing gas station in the area that was determined to be characteristically similar to the proposed Bracken Station. Data was gathered in the morning (7:00 to 9:00 a.m.) and afternoon (3:00 to 7:00 p.m.) peak periods on Thursday, September 1, 2016. The number of entering and exiting vehicles, the vehicle classification, and the duration of time that each vehicle remained on-site was recorded. A summary of these data can be found in Appendix E, however for information purposes, the average dwell time for a fueling vehicles was 5 minutes and 05 seconds, average dwell time for someone using the C-store was 5 minutes and 51 seconds, and the average dwell time for someone fueling and using the C-store was 9 minutes and 37 seconds. These data were used to determine a trip generation rate using the number of fueling positions as the independent variable (similar to the method used in the Institute of Transportation Engineers (ITE) Trip Generation (9th Edition, 2012)).

Trip generation for the proposed project is as follows:

- p.m. Peak Hour Trips: 90"
 - c. It appears the existing traffic analysis uses "net" trips generated by the project instead of the ITE trip generation rates. This needs more explanation; we need to understand and agree with any reduction factor used to determine net vs. ITE trips generated.

As stated in the preceding section, trip generation for the study was determined by observing trips to the Hailey Chevron, as directed by staff.

4. Existing Background Traffic Conditions

- a. Background/cumulative traffic period should extend 10 years and include daily trips and a.m. and p.m. peak hour trips projected for Warm Springs Ranch development and Community School, and any other projects filed or approved but not yet built.
- i. Background traffic/cumulative should be based on the 1.1% traffic growth factor, plus the projected traffic from approved developments.

Trip generation information for the Community School, Warm Springs Ranch Resort, and redevelopment of the Stock Lumberyard site (1000 Warm Springs Road) were included in the 2020 and 2026 traffic analyses. Trip generation during the p.m. peak hour at the 10th Street / Main Street intersection for the projects is as follows:

- Community School 4 trips
- Warm Springs Ranch Resort 15 trips
- Stock Lumberyard Redevelopment 38 trips

This information is provided in Appendix G of the Traffic Impact Study dated October 3, 2016.

- b. Show LOS and impact to turning movements at 10th Street & Highway 75 and 5th Street & Highway 75 intersections with the following charts:
 - 1. Existing LOS and turning movements
 - 2. Future / cumulative LOS and turning movements
 - 3. Future / cumulative with project traffic added and impact to LOS and turning movements

LOS and turn movements for the 10th Street and 5th Street intersections with Highway 75 were included in the updated Traffic Information Study, dated October 3, 2016, as directed.

5. Project Trip Distribution

a. It appears that the existing traffic study projects 15% of the trips come from the north and 85% of the trips come from the south and that these projections impact the turning analysis. Due to the nature of the use it is likely that a greater share of trips – perhaps 30-40%

- are coming from the north.

The revised traffic study projects 40% of trips coming from the north and 60% coming from the south. Hales Engineering's memo "Bracken Station – Additional Information Request Responses," dated October 4, 2016, states that the 40%/60% split was used because the August 3, 2016 letter from city staff to the applicant postulated that perhaps 30-40% of trips may be generated from the north.

b. Indicate how the left and right turns into and out of the project site are derived

"The Highway Capacity Manual 2010 (HCM 2010) methodology was used in this study to remain consistent with "state-of-the-practice" professional standards. This methodology has different quantitative evaluations for signalized and unsignalized intersections. For signalized and all-way stop intersections, the LOS is provided for the overall intersection (weighted average of all approach delays). For all other unsignalized intersections LOS is reported based on the worst approach."

"Using Synchro/SimTraffic, which follow the Highway Capacity Manual (HCM) 2010 methodology introduced in Chapter I, the p.m. peak hour LOS was computed for the study intersection. Multiple runs of SimTraffic were used to provide a statistical evaluation of the intersection. These results serve as a baseline condition for the impact analysis of the proposed development during existing (2016) conditions."

"Project traffic is assigned to the roadway network based on the type of trip and the proximity of project access points to major streets, high population densities, and regional trip attractions.

Existing travel patterns observed during data collection also provide helpful guidance to establishing these distribution percentages, especially in close proximity to the site. The resulting distribution of projected generated trips is as follows:

To/From Project:

- · 40% North
- 60% South

These trip distribution assumptions and the prevailing movements at each intersection were used to assign the evening peak hour generated traffic at the study intersections to create trip assignment for the proposed development."

6. Intersection Analysis – Turn Movements

- a. Evaluate the 10th Street & Highway 75 intersection.
- b. Evaluate the 5th Street and Highway 75 intersection; activity at this intersection directly impacts the turning movements at 10th & Highway 75, particularly southbound traffic on Highway 75 which often backs up to the project site during the p.m. peak due to the signal at 5th Street & Highway 75.

7. Project Vehicle Composition

a. Based on comparable uses (gas station with accessory food service and retail) and comparable sites (gas stations located along Main Street in neighboring or similar mountain/resort communities, for example) provide an analysis of the composition of vehicles utilizing the sites during weekday, weekend, and a.m. and p.m. peak time periods. Categorize vehicles as passenger vehicles, recreational vehicles, other oversize passenger vehicles (Sprinters, SUVs towing boats, etc.) and commercial oversize vehicles (trucks towing trailers, semi-trucks, etc.).

Staff directed the applicant to analyze the Chevron gas station located at 209 S. Main Street in Hailey, Idaho as a comparable project. The Hailey Chevron has fuel pumps, accessory retail sales, and a food truck parks on

the premises. L2 Data Collection filmed the Chevron on Thursday, September 1, 2016 and Monday, September 5, 2016 during the hours of 7:00 – 9:00 a.m. and 3:00 p.m. – 7:00 p.m. and reviewed the film in order to categorize vehicles as passenger vehicles, recreational vehicles, commercial vehicles, motorcycles, and pedestrians/cyclists. Data from Thursday, September 1, 2016 was used to inform the vehicle composition depicted in the circulation exhibits.

L2 classified the data as follows:

Commercial Vehicles

- All vehicles, regardless of make or model, that bore a company logo
- All vehicles towing construction/landscaping trailers

Recreational Vehicles

- Any personal vehicle towing a boat, trailer with ATVs or motorcycles, or any other trailer that was not
 obviously a construction/landscaping trailer
- Recreational vehicles

Passenger Vehicles

All other passenger vehicles, regardless of size

Motorcycles

Motorcycles

Cyclists/Pedestrians

- Pedestrians and cyclists
 - b. This composition count must be approved by a licensed engineer. Any self-reporting will be rejected.

The composition data was provided by L2 Data Collection, a professional traffic data collection firm based in Boise, ID.

8. Queuing Analysis

- a. The existing traffic study assumes there is an 80 foot stacking lane available for cars turning left. However, the stacking lane for cars entering into the project appears to be less than 80 feet as indicated on the Site Plan. Additionally, it appears the analysis only focuses on the delay and stacking for cars turning left onto 10th Street and does not take into account cars turning left into the project and how that impacts cars turning left on 10th Street. For instance, a car turning left onto 10th Street is not going to que behind a car waiting to turn left into the project.
 - The analysis needs to look at those two turning movements separately and determine if the stacking lane is sufficient to accommodate each independent turning movement.
 - ii. The study needs to address impacts to through traffic if one of the two turning lanes is over capacity and cars are stacked up in the travel lane.
 - iii. Show how the traffic light at 5th Street and the current traffic issues merging onto Warm Springs will be impacted by the gas station. Currently, this is one of the most congested areas of the city and the Bracken project has yet to address these concerns satisfactory to staff.

Hales Engineering reports the following in the October 4, 2016, memo "Bracken Station – Additional Information Request Response" :

"Although it is debatable whether or not a vehicle turning left onto 10th Street would queue behind a vehicle waiting to turn left into the project, the two turning movements were in fact analyzed separately in each version of the TIS report. In each instance, it was determined that the stacking distance for each movement is sufficient to accommodate each independent turning movement.

If the left-turn queuing demand were to exceed the capacity at either 10th Street or the project access, then the left-turn queue would block the thru lane, just as happens currently at the 10th Street / Main Street (SH-75) intersection. The addition of the left-turn lane along the project frontage would cause the thru lane to be blocked less often than it is currently with the current lane configuration.

With the addition of the gas station, approximately 27 vehicles would be added to the existing volumes on the southbound approach to the 5th Street / Main Street (SH-75) intersection during the evening peak hour (an increase of less than 4%). Queues of several hundred feet have been reported on this approach, and it is likely that vehicles leaving the gas station heading south on Main Street (SH-75) would end up in this existing queue. It is difficult to say how the Warm Springs Road / Main Street (SH-75) intersection will be impacted, as this intersection was not in the scope outlined by city staff. However, based on the analyses of the 5th Street / Main Street (SH-75) intersection, it is likely that the impact of the traffic added by the gas station would be insignificant."

Traffic Study – Pedestrian and Cyclists

9. Pedestrian and Cyclist Traffic Study

a. Either incorporated into the revised Traffic Study or as a separate document analyze projected pedestrian and cyclist trip generation for the proposed uses.

Hales Engineering projects that the project will generate six pedestrian/bicycle trips during the p.m. peak hour, or that 6% of all trips would be pedestrian/bicycle trips. This projection is based on observations at the comparable Hailey Chevron site.

b. Provide existing conditions - pedestrian and cyclist counts.

Hales Engineering reports that significant pedestrian/bicycle volumes were observed on September 1st at the 5th Street / Main Street intersection and that no pedestrians or bicyclists were observed at the 10th Street / Main Street intersection on September 1st. On the alternative data collection day 17 pedestrians/bicycles were observed at the 10th Street / Main Street intersection, with 3 observed during the peak p.m. hour.

c. Analyze pedestrian and cyclist circulation to and within the site.

The Site Plan has been revised and now includes enhanced pedestrian access.

Southern Access: From south of the site to the proposed store/food service, a pedestrian at Frenchmen's Place could take the proposed connector sidewalk to the southeast corner of the site and walk on an newly proposed on-site sidewalk bordering the southwest property line to access the store/food service. A pedestrian on the east side of Main Street/HWY 75 could cross Main Street just south of the 9th Street / Main Street intersection, using the crosswalk with rapid flashing beacon that the applicant previously proposed, and could then use the same on-site sidewalk to reach the store/food service.

Western Access: From west of the site on 10th Street a pedestrian can use the previously proposed staircase to access the site at the southwest corner of the site. The alignment of the staircase has been revised in order to allow better vehicle circulation in the parking lot, however no pedestrian functionality was lost. The pedestrian staircase directly connects to the on-site sidewalk that provides access to the store/food service.

Northern Access: The previously proposed crosswalk spanning 10th Street remains.

Eastern Property Line: The eastern property line of the site is adjacent to Main Street/HWY 75. ITD requires an 84' wide boulevard approach to the site. The applicant previously proposed installing surface materials across

the pedestrian zone of the 84' boulevard approach that would distinguish the pedestrian zone from the asphalt; this proposal remains.

Additionally, Hales Engineering recommends that if the city requires crosswalks at intersections that are not stop controlled, pedestrian activated rapid flashing beacons should be installed to increase the visibility of pedestrians. Hales Engineering notes that by installing rapid flashing beacons or other pedestrian crossing enhancements at 9th Street and/or 10th Street that the relative visibility of the existing mid-block pedestrian crosswalks at approximately 8th Street and 7th Street may be reduced; Hales recommends that the City consider installing rapid flashing beacons at these locations or consider removing those crossings due to redundancy.

d. Provide warrants for all recommended improvements, including the pedestrian signals, crosswalks, and other infrastructure.

The Traffic Impact Study dated October 3, 2016 recommends construction of a two-way left-turn lane from a location north of 10th Street to a location south of the project in order to allow northbound left-turning vehicles to 10th Street or into the project access to decelerate and/or queue without blocking the flow of through traffic.

On page vii of the Executive Summary the Traffic Impact Study states, "Although pedestrian volumes during the p.m. peak hour are anticipated to be relatively low, if pedestrian crosswalks are installed on Main Street (SH-75) at 9th Street, it should be done in accordance with the Idaho Transportation Department with their permit process. It is also recommended that pedestrian activated rectangular rapid flashing beacon signs be installed to increase visibility of the crossings." The Traffic Impact Study does not provide warrants for the pedestrian crosswalks, signals, or other infrastructure.

Circulation Diagrams

10. Revised Circulation Exhibits

- a. Revise circulation exhibits to show:
 - i. Additional turn movements of large vehicles circulating the site with large vehicles parked on site in, and with a fuel delivery truck on site, order to show site is large enough to accommodate a variety of vehicles without causing traffic to back up into the travel right-of-way.
 - ii. Queuing scenarios based on projected composition of vehicles that will visit the site. iii. All on-site circulation exhibits must reflect the traffic study, updated vehicle composition numbers, and other revised data.

Benchmark Associates provided additional circulation exhibits for the abovementioned scenarios. The circulation exhibits are detailed in Attachment C, Table 3.

10th Street Parallel Parking

- 11. Examine the slope on 10th Street and discuss the feasibility of parallel parking during the winter.
 - a. If on-site parking spaces are proposed to be eliminated and parallel parking spaces on 10th Street are proposed to make up the difference address the feasibility of parking on 10th Street during winter conditions with respect to the slope of 10th Street.

The revised site plan dated September 16, 2016 reflects the removal of several previously proposed parking spaces in order to provide additional circulation space on the site. Parking spaces required for the project are still provided entirely on site.

Attachment C:

Table 3: Analysis of Vehicle Circulation Exhibits dated September 16, October 3 and October 4, 2016

Exhibit	Conditions	Comments
#1 – On-Site Circulation – Worst Case Scenario	This exhibit depicts a northbound car towing a camper trailer 48.7' in length circulating onto the site and queuing north of the fuel pumps, 2 passenger cars 19' in length circulating onto the site and maneuvering between the queuing RV and the passenger cars fueling at pumps #1 and #3, and a box truck 30' in length queuing in the turn lane because the location of the queuing passenger car towing the RV prohibits the box truck from circulating onto the site. There are 9 stationary passenger cars 19' in length on site with 6 positioned at fuel pumps and 3 parked.	Benchmark notes that 5 passenger cars and 2 commercial vehicles were observed simultaneously on site at the Hailey Chevron for a total of 2 minutes on September 1, 2016 and 5 passenger cars and 1 commercial vehicle were observed simultaneously on site at the Hailey Chevron for a total of 4 minutes on September 1, 2016. Staff requested this exhibit, which depicts 1 commercial vehicle and 1 RV, to reflect a scenario more congested than conditions observed at the Hailey Chevron. This exhibit illustrates that if a passenger car towing an RV arrives to the site and must queue while waiting for an available fuel pump a box truck or other commercial vehicle must queue in the turn lane. The exhibit illustrates that northbound and southbound passenger vehicles can circulate onto the site while a passenger car towing an RV is queued north of the fueling island. Staff's concerns include: 1. The maneuvers of passenger cars onto the site while a passenger car towing an RV queues and passenger cars are fueling at pumps #1 an #3 are constrained; and 2. A southbound passenger vehicle and the northbound box truck and RV must drive onto the curb to successfully maneuver onto the site; and 3. Circulation of the box truck to the loading zone is constrained when fuel pumps #1, #2, #3, and the ADA parking space are occupied.
#1A – On- Site Circulation – Worst Case Scenario	This exhibit depicts a northbound box truck 30' in length circulating onto the site and parking in the loading zone, followed by a northbound car towing a camper trailer 48.7' in length circulating onto the site and exiting, and a southbound passenger car 19' in length	Benchmark notes that 5 passenger cars and 2 commercial vehicles were observed simultaneously on site at the Hailey Chevron for a total of 2 minutes on September 1, 2016 and 5 passenger cars and 1 commercial vehicle were observed simultaneously on site at the Hailey Chevron for a total of 4 minutes on September 1, 2016. Benchmark notes that a commercial vehicle and RV were never observed on site the same time during the period observational data was collected.

circulating onto the site and exiting.

There are 9 stationary passenger cars 19' in length on site with 6 positioned at fuel pumps and 3 parked.

Staff requested this exhibit, which depicts 1 commercial vehicle and 1 RV, to reflect a scenario more congested than conditions observed at the Hailey Chevron.

This exhibit illustrates that if a 30' box truck enters the site and parks in the loading space a passenger car and a passenger car towing an RV can circulate through the site. Based on the overlapping turn movements for the box truck and the passenger vehicle towing an RV a box truck could not circulate onto the site if the passenger car towing the RV were queued on site north of the fuel pumps.

Staff's concerns include:

- A northbound box truck and car with RV must drive onto the curb to successfully maneuver onto the site while a vehicle is fueling at pump #3, passenger vehicles fueling at pumps #1 and #2 create constrained circulation; and
- A southbound passenger vehicle must drive onto the curb to successfully maneuver onto the site while a vehicle is fueling at pump #3, passenger vehicles fueling at pumps #1 and #2 create constrained circulation.

#2 – On-Site Circulation – RV Fueling; Commercial Vehicle Queuing

This exhibit depicts a northbound passenger car towing an RV circulating onto the site and fueling at pumps #1 and #2, a northbound box truck arriving to the site afterwards and quieting north of the fuel pumps, and a southbound passenger car circulating onto the site and maneuvering between the queued box truck and fueling passenger cars.

Benchmark notes that 5 passenger cars and 2 commercial vehicles were observed simultaneously on site at the Hailey Chevron for a total of 2 minutes on September 1, 2016 and 5 passenger cars and 1 commercial vehicle were observed simultaneously on site at the Hailey Chevron for a total of 4 minutes on September 1, 2016. Benchmark notes that a commercial vehicle and RV were never observed on site the same time during the period observational data was collected.

Staff requested this exhibit, which depicts 1 commercial vehicle and 1 RV, to reflect a scenario more congested than conditions observed at the Hailey Chevron.

There are 8 stationary passenger cars on site with 5 positioned at fuel pumps and 3 parked.

This exhibit illustrates that a passenger car can circulate through the site while a box truck is queued north of the fueling island and passenger cars and a passenger car towing an RV fuel.

Staff's concerns include:

- 1. Based on this exhibit and the "Recreational Vehicle Access and Circulation" exhibit described later in this table, a northbound passenger vehicle towing an RV can only maneuver into fuel pumps #1 and #2. The conditions in this exhibit are based on the RV circulating onto the site when the optimal condition of both pumps #1 and #2 being vacant. If both pumps #1 and #2 are not vacant the RV will be forced to queue on site which will result in a northbound box truck and/or RV and any northbound passenger vehicles queuing in the turn lane; and
- Northbound box trucks and passenger vehicles towing RVs must drive onto the curb to successfully maneuver onto the site and circulate around a passenger vehicle fueling at pump #3.

#3 – On-Site Circulation – RV Queuing; Commercial Vehicle Fueling

This exhibit depicts a northbound passenger car towing an RV circulating onto the site, queuing, and exiting the site, a southbound box truck circulating onto the site and fueling on the west side of pump #4, and a southbound passenger car circulating onto the site and maneuvering between the queued RV and the fueling passenger cars.

There are 8 stationary passenger cars on site with 5 positioned at fuel pumps and 3 parked.

Benchmark notes that 5 passenger cars and 2 commercial vehicles were observed simultaneously on site at the Hailey Chevron for a total of 2 minutes on September 1, 2016 and 5 passenger cars and 1 commercial vehicle were observed simultaneously on site at the Hailey Chevron for a total of 4 minutes on September 1, 2016. Benchmark notes that a commercial vehicle and RV were never observed on site the same time during the period observational data was collected.

Staff requested this exhibit, which depicts 1 commercial vehicle and 1 RV, to reflect a scenario more congested than conditions observed at the Hailey Chevron.

This exhibit illustrates that a northbound passenger vehicle towing an RV can queue and circulate onto the site when a southbound box truck is fueling at pump #4. Additionally, a passenger vehicle can circulate through the site while a passenger vehicle towing an RV is queuing.

Staff's concerns include:

 Based on the "Recreational Vehicle Access and Circulation" exhibit described later in this table a southbound box truck can maneuver to all fuel pumps when there are no other vehicles on site.
 Based on this exhibit a box truck can maneuver to the west side of pumps #3 and #4 if other pumps are

occupied. If the box truck fuels at pump #3 rather than #4, depending on the length of the box truck that extended north beyond the fuel pump, circulation of other vehicles onto the site would be constrained or may be impossible; 2. Circulation of a passenger vehicle is constrained when a passenger car towing an RV is queued north of the fuel island and pump #1 is occupied; and 3. Northbound passenger vehicles towing RVs and southbound passenger vehicles and box trucks will be required to drive on the curb to successfully maneuver onto the site. Recreational This exhibit illustrates three possible maneuvers for This exhibit depicts several Vehicle circulation scenarios of southbound passenger vehicles towing RVs and three **Access and** northbound and southbound possible maneuvers for northbound passenger vehicles Circulation passenger vehicles towing RVs. towing RVs to circulate the site. Staff's concerns: There are no stationary vehicles parked on site. 1. Northbound - There is only one possible scenario for fueling available, which requires two pumps in tandem to be available at the same time; 2. Southbound - There are only two scenarios for fueling available, which both require two pumps in tandem to be available at the same time; 3. Multiple scenarios require the vehicle to drive onto the curb; and 4. If the vehicle fueled at the northern pumps rather than the southern pumps circulation on the north side of the pumps would be further constrained. **Box Truck** This exhibit depicts several This exhibit illustrates four possible maneuvers of Access and circulation scenarios of northbound box trucks and two possible maneuvers of Circulation northbound and southbound southbound box trucks to circulate the site. box trucks. Staff's concerns: There are no stationary vehicles 1. Northbound - There are only two fueling positions parked on site. possible, fueling on the east side of pump #1 and fueling on the east side of pump #2, with fueling at pump #2 dependent on pump #1 being open in order for the box truck to maneuver into position;

- Southbound While there are several scenarios for fueling available, half of the scenarios require two pumps in tandem to be available at the same time in order to access the southernmost pumps;
- 3. Maneuvering into the loading zone is dependent the east side of pump #4 to be open;
- 4. Multiple scenarios require the box truck to drive onto the curb;
- 5. If a box truck fuels at pump #3 or #4 the length of the vehicle may encroach into the area north of the fueling island that all vehicles use as ingress to circulate the site; and
- Fueling at pump #2 as depicted in the exhibit encroaches into the area south of the fueling island that vehicles use to circulate to the parking spaces.

Fuel Truck Delivery – RV Circulation

This exhibit depicts a northbound passenger vehicle towing an RV circulating the site while a fuel delivery truck is on site.

There are 5 passenger vehicles positioned at fuel pumps.

This exhibit illustrates that when a fueling truck is on site in the fueling location and the east and west sides of pump #1 are closed and the west side of pump #2 is closed a passenger vehicle towing an RV can circulate onto the site and maneuver past the fuel truck to queue near the exit or exit the site.

Staff's concerns:

- Benchmark did not have enough information to accurately model the fuel delivery truck in motion and was not able to show the turn movements of the fuel delivery truck entering the site and maneuvering into position; staff cannot analyze the ability of the fuel delivery truck to make the maneuvers while pumps #1 and #3 are occupied and therefore is concerned about the queue in the turn lane while the fuel delivery truck circulates on the site;
- 2. The applicant has provided a letter from Dallas Green, Director of Operations of Kellestrass Oil, stating that the timing of fuel delivery truck trips to the site can be scheduled. However, conditioning approval of the Conditional Use Permit to include conditions of the times that fuel and retail deliveries can occur is a challenging and burdensome condition to continually enforce in perpetuity.

Fuel Truck Delivery – Box Truck Circulation

This exhibit depicts a northbound box truck circulating the site while a fuel delivery truck is on site.

There are 5 passenger vehicles positioned at fuel pumps.

This exhibit illustrates that when a fueling truck is on site in the fueling location and the east and west sides of Pump #1 are closed and the west side of pump #2 is closed a box truck can circulate onto the site and maneuver past the fuel truck to queue near the exit or exit the site.

Staff's concerns:

- Benchmark did not have enough information to accurately model the fuel delivery truck in motion and was not able to show the turn movements of the fuel delivery truck entering the site and maneuvering into position; staff cannot analyze the ability of the fuel delivery truck to make the maneuvers while pumps #1 and #3 are occupied and therefore is concerned about the queue in the turn lane while the fuel delivery truck circulates into position on site;
- 2. The applicant has provided a letter from Dallas Green, Director of Operations of Kellestrass Oil Company, stating that the timing of fuel delivery truck trips to the site can be scheduled for non-high traffic times. However, condoning approval of the Conditional Use Permit to include conditions of the times that fuel and retail deliveries can occur is a challenging and burdensome condition to continually enforce in perpetuity.

Semi-Truck Delivery Circulation

This exhibit depicts a northbound semi-tuck 45.5' in length circulating the site.

There are 6 passenger vehicles positioned at fuel pumps.

The exhibit notes that deliveries made by trucks larger than a WB-40, an Intermediate Semi-Trailer 45.5' in length, shall be made in the alley. The exhibit also notes that deliveries will be scheduled so no more than two trucks (one in the surface level loading zone and one in the alley) are on site at once.

This exhibit illustrates that a northbound WB-40 semi-trailer can circulate onto the site and maneuver into the loading zone when the west sides of pumps #1 and #2 are closed and passenger vehicles occupy all other fuel pumps.

Staff's concerns:

 While the exhibit illustrates that a semi-truck can circulate the site and maneuver into the loading zone while passenger vehicles occupy the fuel pumps circulation onto the site is constrained by a 19' passenger vehicle fueling on the east side of pump #3; a larger vehicle at this fueling location would prohibit a ingress into the site and would impact queue lengths in the turn lane; and

2. While the semi-truck is in position in the loading zone a vehicle parked in the southernmost parking space would not be able to exit the parking space;

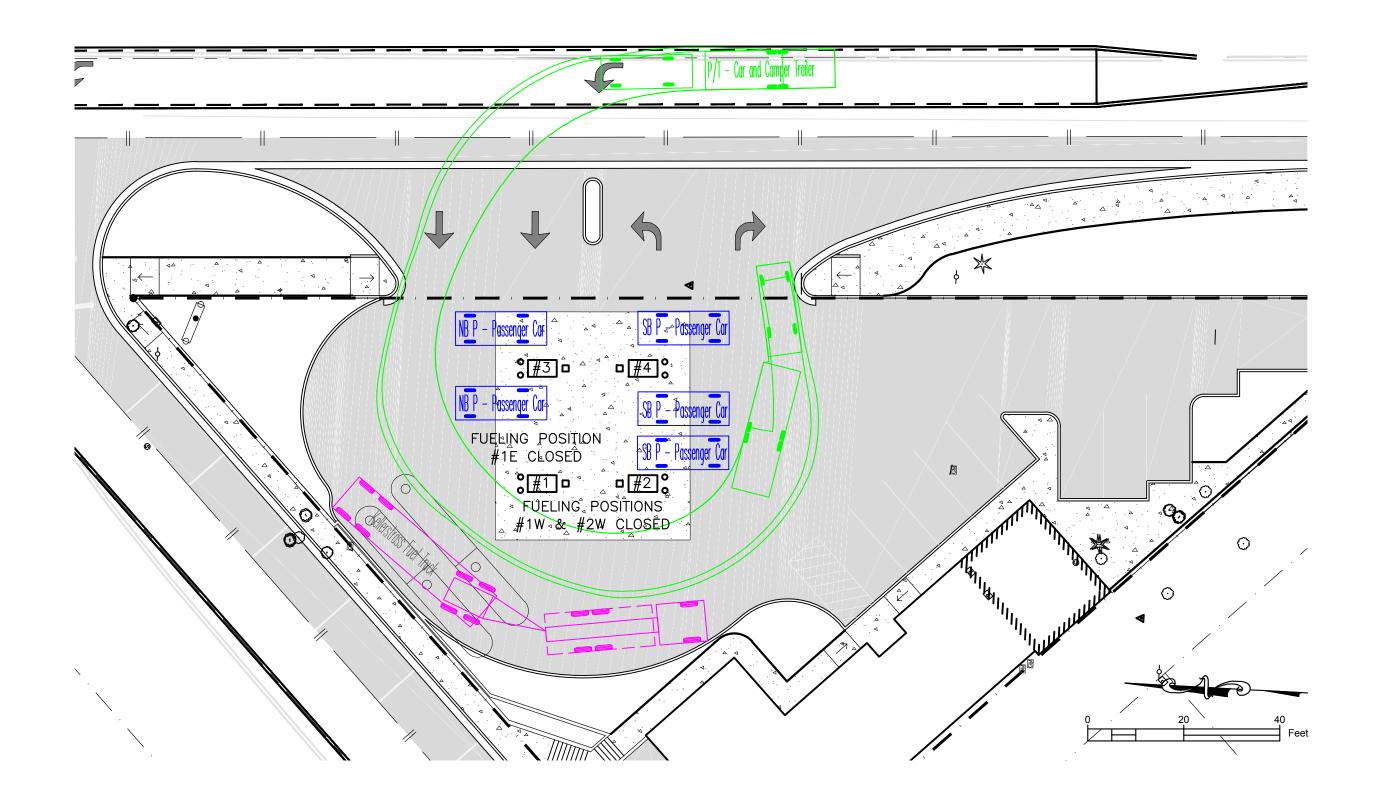
3. The applicant has provided a letter from Dallas Green, Director of Operations of Kellestrass Oil Company, stating that the timing of fuel delivery truck trips to the site can be scheduled for non-high traffic times. However, condoning approval of the Conditional Use Permit to include conditions of the times that fuel and retail deliveries can occur is a

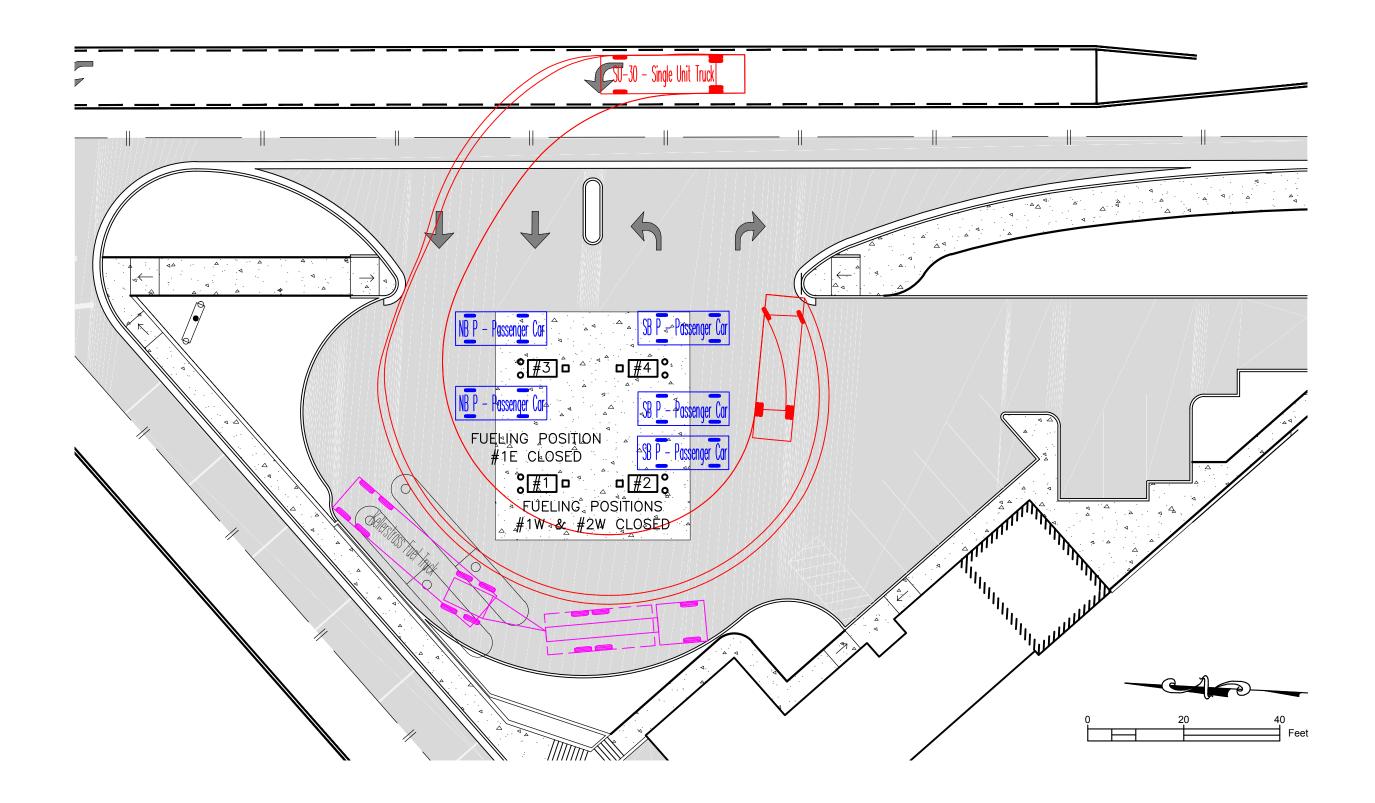
challenging and burdensome condition to continually

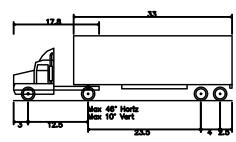
enforce in perpetuity.

Attachment D:

Circulation Exhibits dated September 16, 2016, October 3, 2016 and October 4, 2016





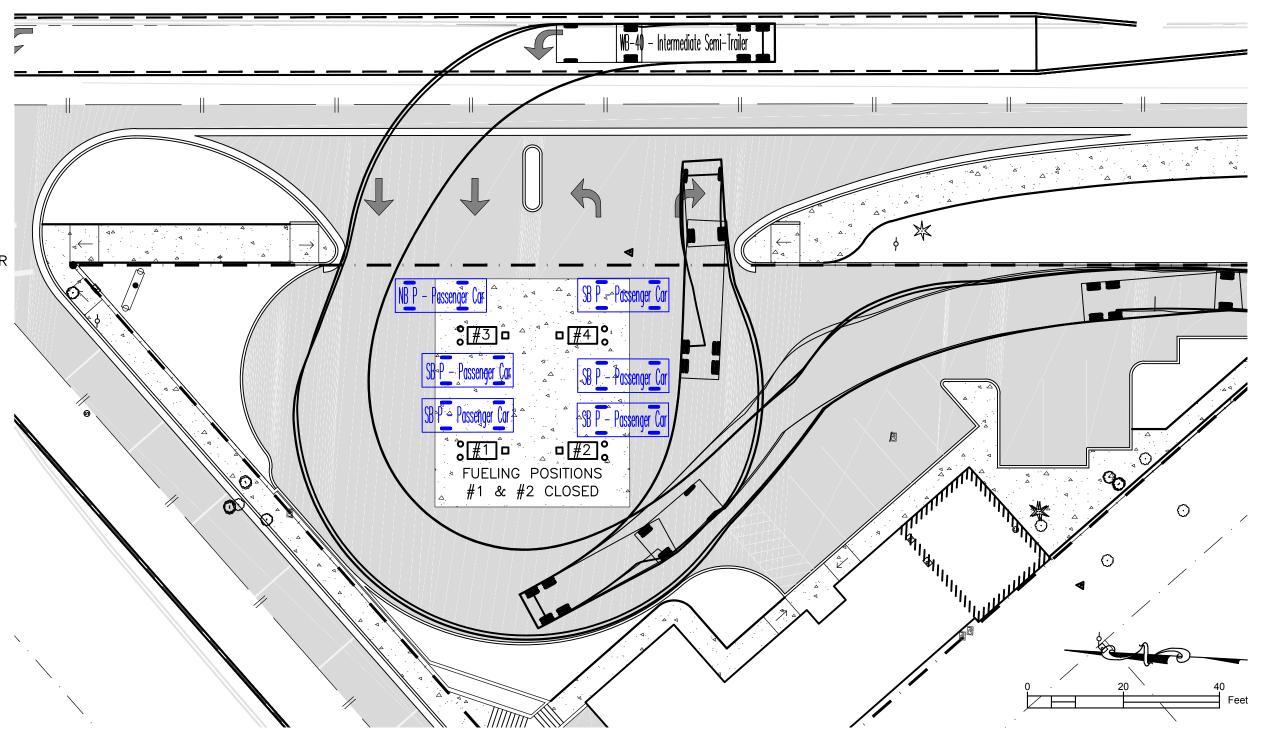




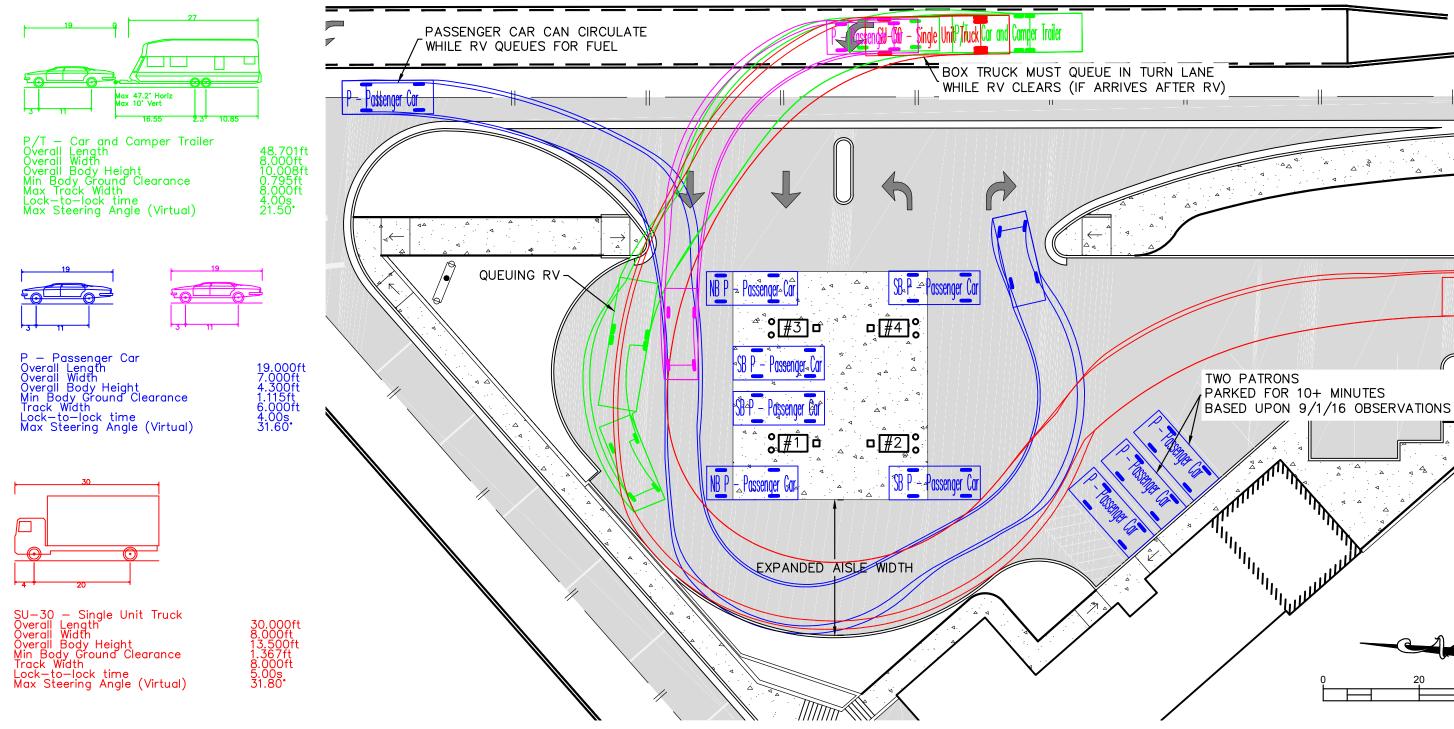


- NOTES:

 1. DELIVERIES MADE BY TRUCKS LARGER THAN A WB-40 SHALL BE MADE IN THE ALLEY.
- 2. DELIVERIES WILL BE SCHEDULED SO NO MORE THAN TWO TRUCKS (ONE IN LOADING ZONE AND ONE IN ALLEY) ARE ON SITE AT ONCE.







NOTES:

- 1. VEHICLE COMPOSITION SHOWN IS BASED UPON OBSERVATIONS MADE AT THE MAIN STREET ACCESS OF THE CHEVRON GAS STATION IN HAILEY, ID ON THURSDAY, 9/1/16 WITH THE ADDITION OF A ONE (1) RECREATIONAL VEHICLE (CAR+TRAILER) PER THE CITY OF KETCHUM STAFF'S REQUEST.
- 2. A COMMERCIAL VEHICLE (BOX TRUCK)
 AND RECREATIONAL VEHICLE
 (CAR+TRAILER) WERE NEVER OBSERVED
 ON SITE AT THE SAME TIME DURING DATA
 COLLECTED ON 9/1/16 AND 9/5/16
 FROM 7:00AM-9:00AM AND
 3:00PM-7:00PM.
- 3. THE WORST CASE (BUSIEST) SCENARIO ON—SITE TRAFFIC COMPOSITION SHOWN WAS REQUESTED BY THE CITY OF KETCHUM STAFF. THE WORST CASE COMPOSITION SHOWN INCLUDES:
 - (5) PASSENGER VEHICLES
 - (1) COMMERCIAL VEHICLES-BOX TRUCK
 - (1) RECREATIONAL VEHICLE PER THE CITY OF KETCHUM STAFF'S REQUEST

OBSERVED VEHICLE COMPOSITION A

(5) PASSENGER VEHICLES + (2) COMMERCIAL VEHICLES OBSERVED FOR 2 MINUTES ON 9/1/16 OCCURRED <1% (0.56%) OF OBSERVED TIME ON 9/1/16

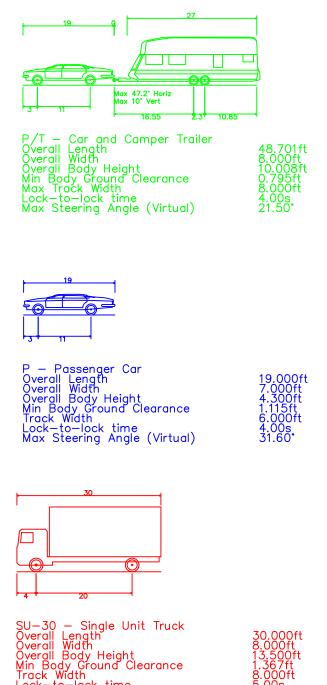
OBSERVED VEHICLE COMPOSITION B

(5) PASSENGER VEHICLES + (1) COMMERCIAL VEHICLES OBSERVED FOR 4 MINUTES ON 9/1/16 OCCURRED 1.1% OF OBSERVED TIME ON 9/1/16

SEPTEMBER 16TH, 2016

ON-SITE CIRCULATION EXHIBIT #1- WORST CASE SCENARIO

RECREATIONAL VEHICLE QUEUING; COMMERCIAL VEHICLE DELIVERY



2. A COMMERCIAL VEHICLE (BOX TRUCK) AND RECREATIONAL VEHICLE (CAR+TRAILER) WERE NEVER OBSERVED ON SITE AT THE SAME TIME DURING DATA COLLECTED ON 9/1/16 AND 9/5/16 FROM 7:00AM-9:00AM AND 3:00PM-7:00PM.

- 3. THE WORST CASE (BUSIEST) SCENARIO ON-SITE TRAFFIC COMPOSITION SHOWN WAS REQUESTED BY THE CITY OF KETCHUM STAFF. THE WORST CASE COMPOSITION SHOWN INCLUDES:
 - (5) PASSENGER VEHICLES

□ #2 °

EXPANDED AISLE WIDTH

- (1) COMMERCIAL VEHICLES-BOX TRUCK
- (1) RECREATIONAL VEHICLE PER THE CITY OF KETCHUM STAFF'S REQUEST

TWO PATRONS

PARKED FOR 10+ MINUTES

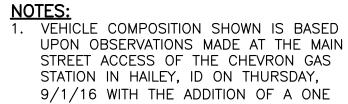
BASED UPON 9/1/16 OBSERVATIONS

(5) PASSENGER VEHICLES + (2) COMMERCIAL VEHICLES OBSERVED FOR 2 MINUTES ON 9/1/16 OCCURRED <1% (0.56%) OF OBSERVED TIME ON 9/1/16

OBSERVED VEHICLE COMPOSITION B

(5) PASSENGER VEHICLES + (1) COMMERCIAL VEHICLES OBSERVED FOR 4 MINUTES ON 9/1/16 OCCURRED 1.1% OF OBSERVED TIME ON 9/1/16/

SEPTEMBER 16TH, 2016



Lock—to—lock time Max Steering Angle (Virtual)

PER THE CITY OF KETCHUM STAFF'S REQUEST.

(1) RECREATIONAL VEHICLE (CAR+TRAILER)

PASSENGER CAR CAN CIRCULATE WHILE RV QUEUES FOR FUEL

ON-SITE CIRCULATION EXHIBIT #1A- WORST CASE SCENARIO

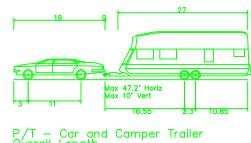


OBSERVED VEHICLE COMPOSITION A

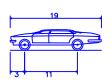
BOX TRUCK MUST QUEUE IN TURN LANE WHILE RV CLEARS (IF ARRIVES AFTER RV)

> QUEUING RV LEAVES SITE

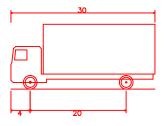
RECREATIONAL VEHICLE QUEUING; COMMERCIAL VEHICLE DELIVERY



P/T — Car and Camper Trailer Overall Length Overall Width Overall Body Height
Min Body Ground Clearance
Max Track Width
Lock—to—lock time
Max Steering Angle (Virtual)



 Passenger Car Overall Length
Overall Width
Overall Body Height
Min Body Ground Clearance
Track Width
Lock—to—lock time
Max Steering Angle (Virtual)



SU-30 — Single Unit Truck Overall Length Overall Width Overall Body Height Min Body Ground Clearance Track Width Lock—to—lock time Max Steering Angle (Virtual)

NOTES:

REQUEST.

2. A COMMERCIAL VEHICLE (BOX TRUCK) AND RECREATIONAL VEHICLE (CAR+TRAILER) WERE NEVER OBSERVED ON SITE AT THE SAME TIME DURING DATA COLLECTED ON 9/1/16 AND 9/5/16 FROM 7:00AM-9:00AM AND 3:00PM-7:00PM.

- 3. THE WORST CASE (BUSIEST) SCENARIO ON-SITE TRAFFIC COMPOSITION SHOWN WAS REQUESTED BY THE CITY OF KETCHUM STAFF. THE WORST CASE COMPOSITION SHOWN INCLUDES:
 - (5) PASSENGER VEHICLES
 - (1) COMMERCIAL VEHICLES-BOX TRUCK
 - (1) RECREATIONAL VEHICLE PER THE CITY OF KETCHUM STAFF'S REQUEST

OBSERVED VEHICLE COMPOSITION A

(5) PASSENGER VEHICLES + (2) COMMERCIAL VEHICLES OBSERVED FOR 2 MINUTES ON 9/1/16 OCCURRED <1% (0.56%) OF OBSERVED TIME ON 9/1/16

OBSERVED VEHICLE COMPOSITION B

(5) PASSENGER VEHICLES + (1) COMMERCIAL VEHICLES OBSERVED FOR 4 MINUTES ON 9/1/16 OCCURRED 1.1% OF OBSERVED TIME ON 9/1/16/

SEPTEMBER 16TH, 2016

VEHICLE COMPOSITION SHOWN IS BASED

STREET ACCESS OF THE CHEVRON GAS

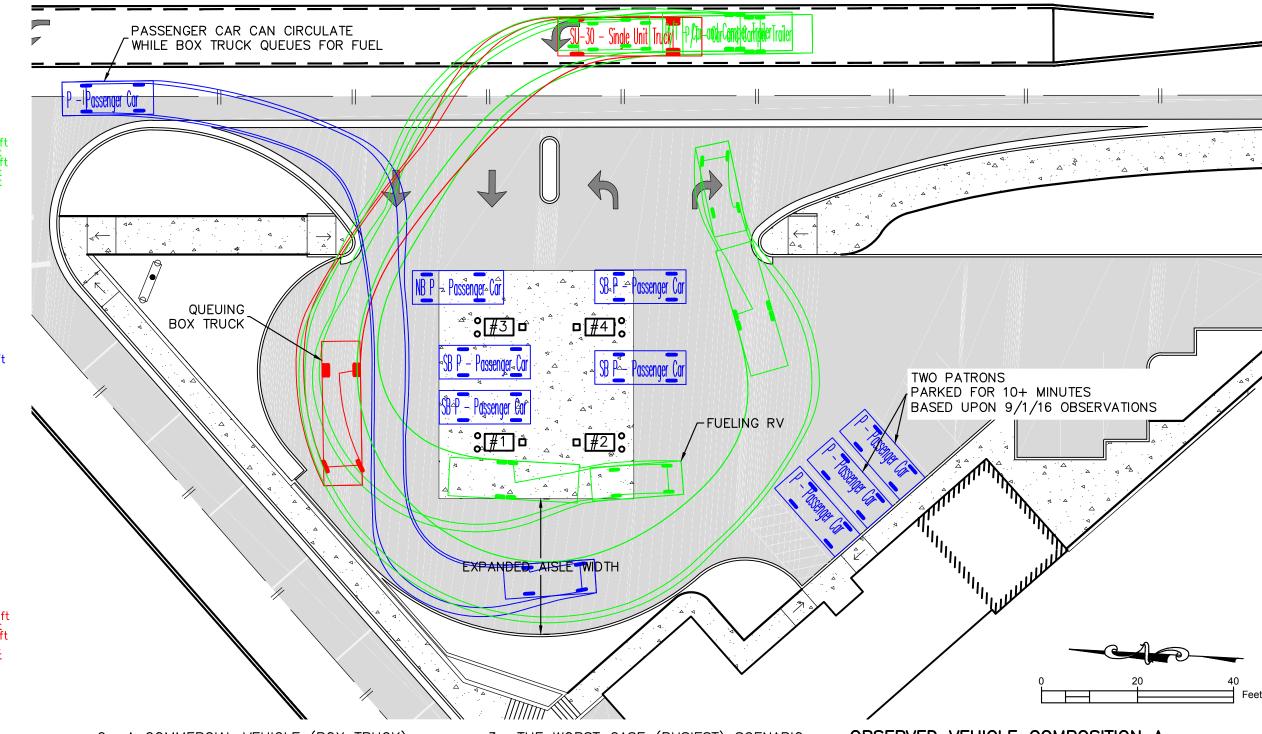
9/1/16 WITH THE ADDITION OF A ONE

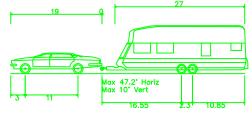
(1) RECREATIONAL VEHICLE (CAR+TRAILER)

STATION IN HAILEY, ID ON THURSDAY,

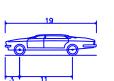
PER THE CITY OF KETCHUM STAFF'S

UPON OBSERVATIONS MADE AT THE MAIN

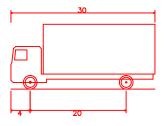




P/T — Car and Camper Trailer Overall Length Overall Width Overall Body Height
Min Body Ground Clearance
Max Track Width
Lock—to—lock time
Max Steering Angle (Virtual)



 Passenger Car Overall Length
Overall Width
Overall Body Height
Min Body Ground Clearance
Track Width
Lock—to—lock time
Max Steering Angle (Virtual)



SU-30 — Single Unit Truck Overall Length Overall Width Overall Body Height Min Body Ground Clearance Track Width Lock—to—lock time Max Steering Angle (Virtual)

NOTES:

REQUEST.

2. A COMMERCIAL VEHICLE (BOX TRUCK) AND RECREATIONAL VEHICLE (CAR+TRAILER) WERE NEVER OBSERVED ON SITE AT THE SAME TIME DURING DATA COLLECTED ON 9/1/16 AND 9/5/16 FROM 7:00AM-9:00AM AND

3:00PM-7:00PM.

- 3. THE WORST CASE (BUSIEST) SCENARIO ON-SITE TRAFFIC COMPOSITION SHOWN WAS REQUESTED BY THE CITY OF KETCHUM STAFF. THE WORST CASE COMPOSITION SHOWN INCLUDES:
 - (5) PASSENGER VEHICLES
 - (1) COMMERCIAL VEHICLES-BOX TRUCK
 - (1) RECREATIONAL VEHICLE PER THE CITY OF KETCHUM STAFF'S REQUEST

OBSERVED VEHICLE COMPOSITION A

(5) PASSENGER VEHICLES + (2) COMMERCIAL VEHICLES OBSERVED FOR 2 MINUTES ON 9/1/16 OCCURRED <1% (0.56%) OF OBSERVED TIME ON 9/1/16

OBSERVED VEHICLE COMPOSITION B

(5) PASSENGER VEHICLES + (1) COMMERCIAL VEHICLES OBSERVED FOR 4 MINUTES ON 9/1/16 OCCURRED 1.1% OF OBSERVED TIME ON 9/1/16/

SEPTEMBER 16TH, 2016



VEHICLE COMPOSITION SHOWN IS BASED

UPON OBSERVATIONS MADE AT THE MAIN

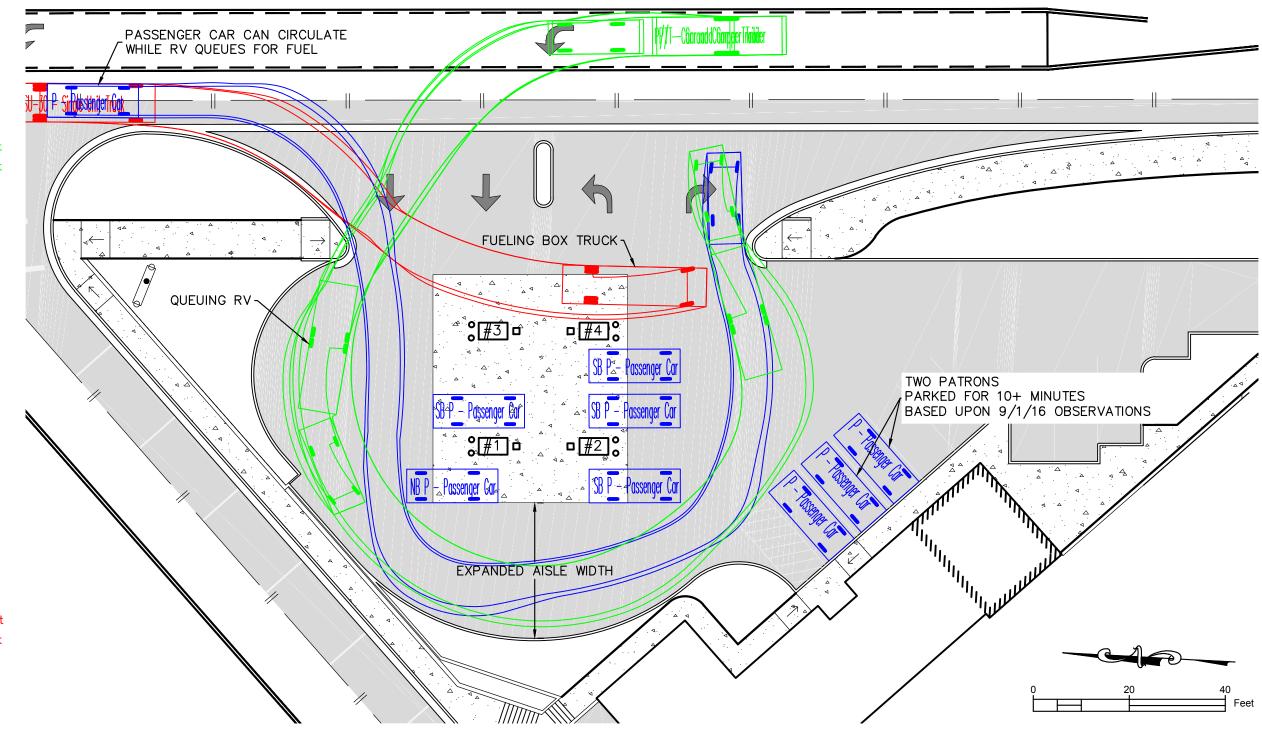
STREET ACCESS OF THE CHEVRON GAS

9/1/16 WITH THE ADDITION OF A ONE

(1) RECREATIONAL VEHICLE (CAR+TRAILER)

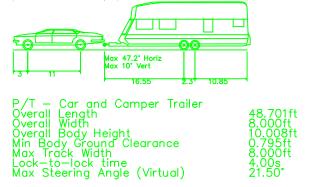
STATION IN HAILEY, ID ON THURSDAY,

PER THE CITY OF KETCHUM STAFF'S

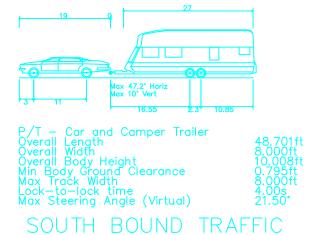


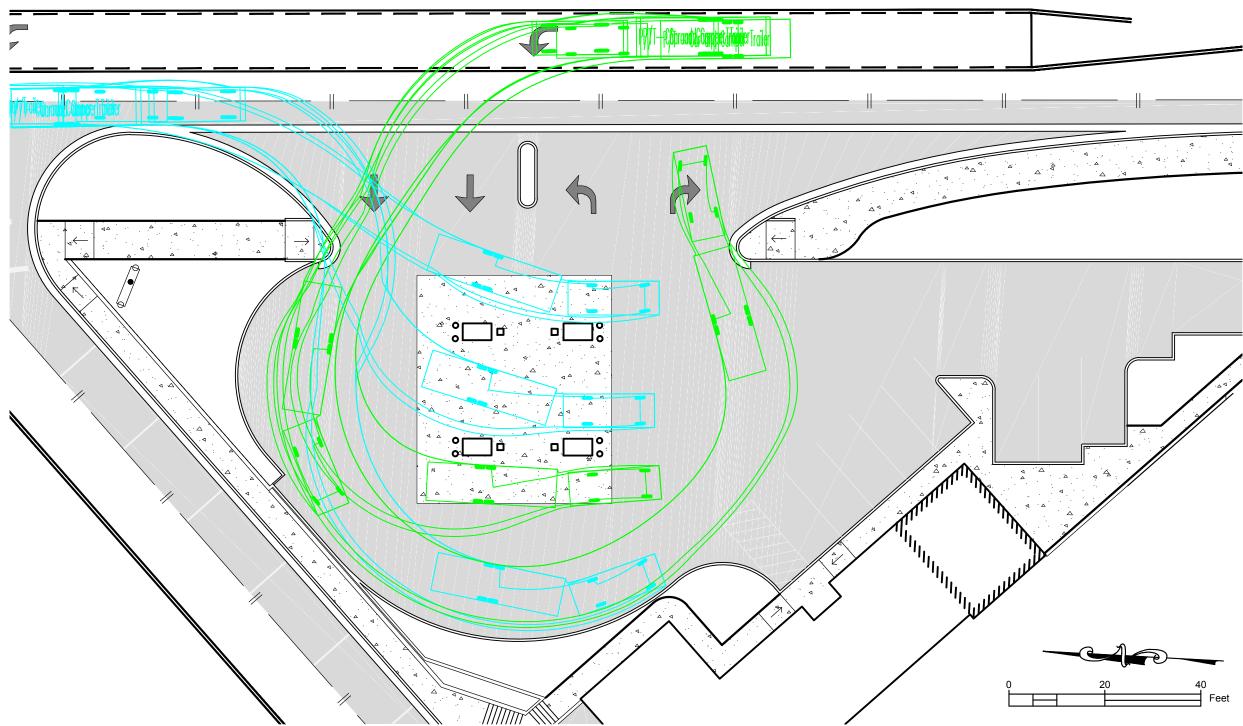


NOTE: VEHICLE TURNS SHOW ACCESS POINTS FOR A PASSENGER VEHICLE AND TRAILER FUELING, QUEUING, AND PARKING.



NORTH BOUND TRAFFIC

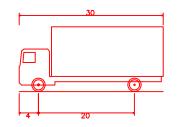






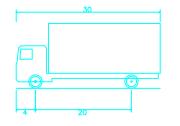
- NOTES:

 1. TRUCK TURNS SHOW ACCESS POINTS FOR BOX TRUCK FUELING, QUEUING, AND MAKING DELIVERIES.
- 2. DELIVERIES WILL ALSO TAKE PLACE IN THE ALLEY.



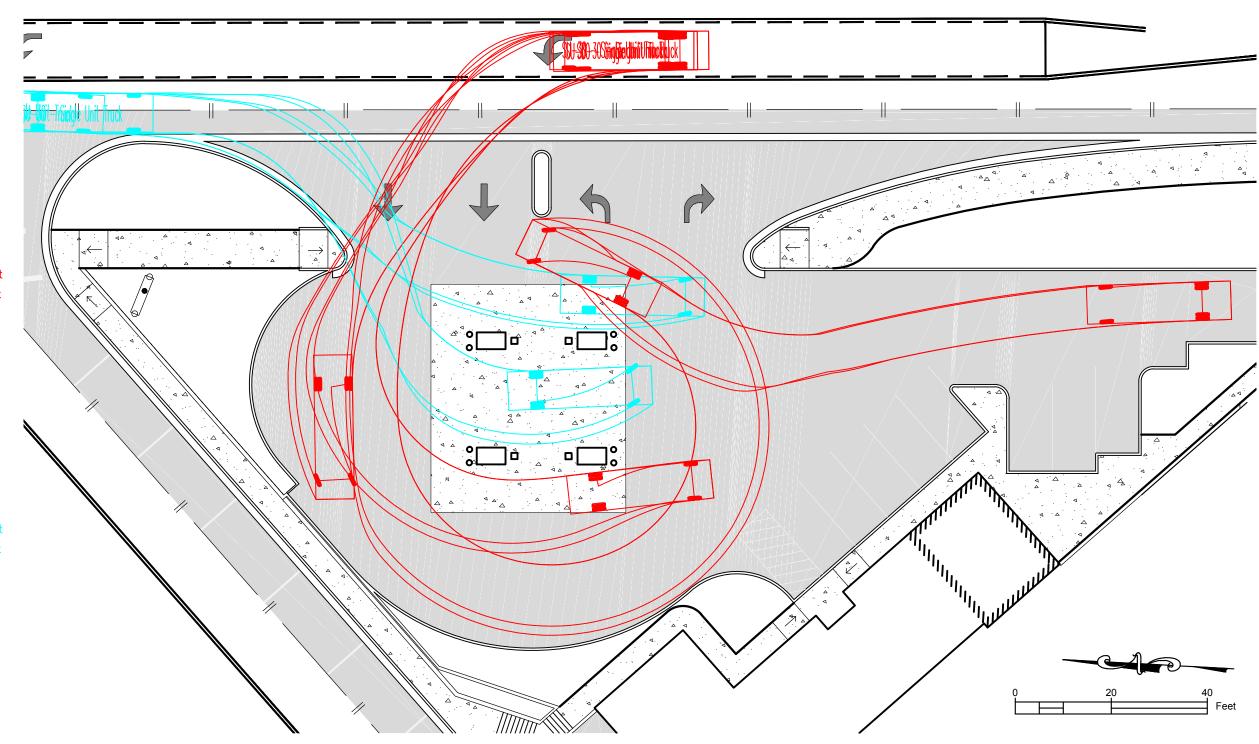
SU-30 — Single Unit Truck Overall Length Overall Width Overall Body Height Min Body Ground Clearance Track Width Lock—to—lock time Max Steering Angle (Virtual)

NORTH BOUND TRAFFIC

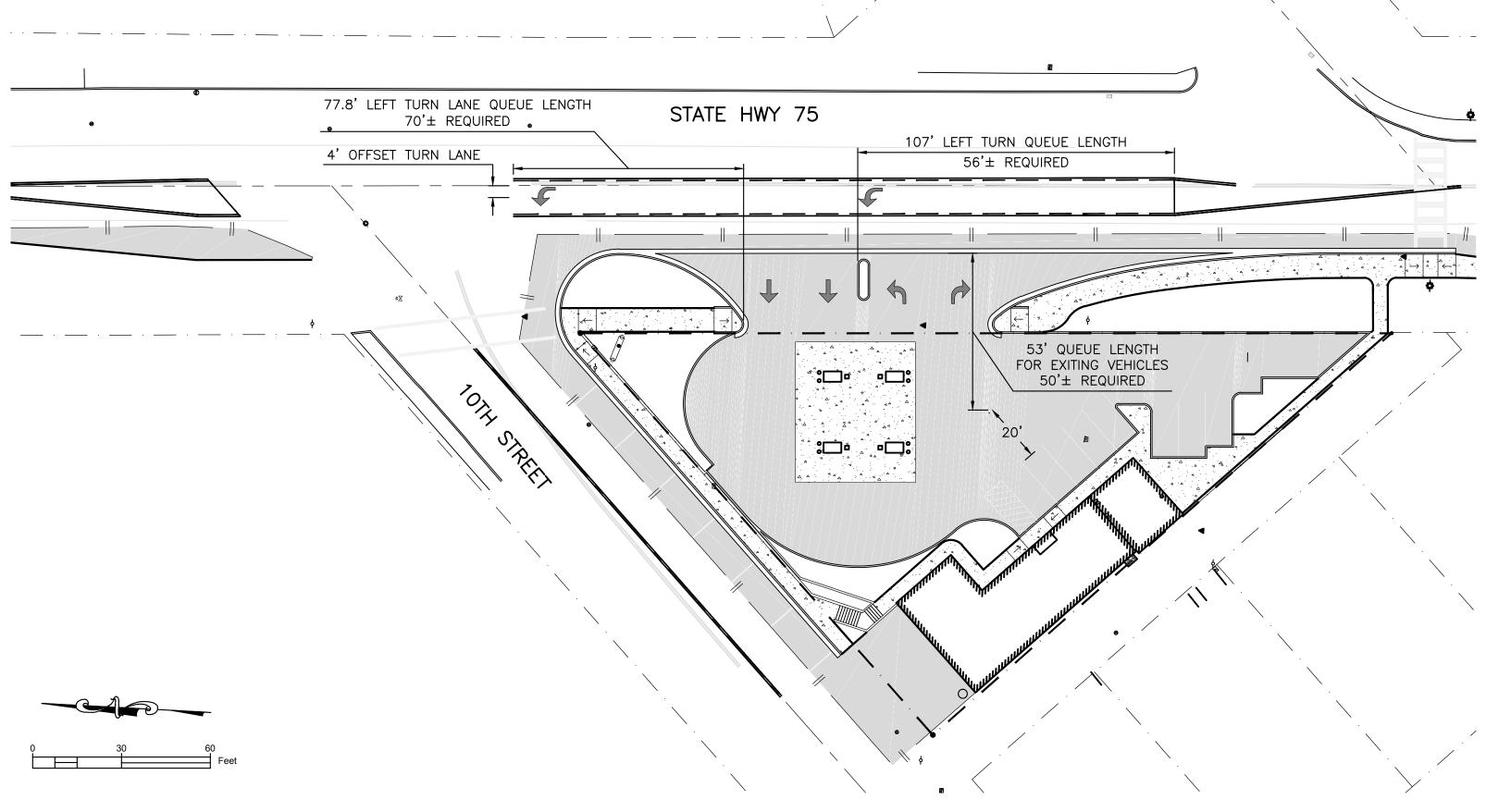


— Single Unit Truck Length Vidth all Body Height Body Ground Clearance Lock-to-lock time Max Steering Angle (Virtual)

SOUTH BOUND TRAFFIC







Attachment E:

Additional Information Requested by Staff on September 30, 2016

Bracken Station – Conditional Use Permit application

Additional Information Requested by City of Ketchum staff

September 30, 2016

Vehicle Observation and Classification

- 1. Please verify that the food truck was on site and operating at the Chevron during the entire time period observation data was collected.
- 2. Please clarify vehicle classification:
 - a. How were the following oversized passenger vehicles classified?
 - i. Personal Sprinter or other make/model of oversize vans(without an commercial logos)
 - ii. Personal vehicles towing boats
 - iii. Personal vehicles towing flatbed trailers with ATVs, motorbikes, or other items
 - iv. Personal vehicles, without company logos, towing construction trailers
 - b. Please confirm that passenger vehicles with company logos, such as Chevrolet/Ford/etc. trucks, were classified as commercial vehicles.
- 3. Please detail the total number of minutes each combination of vehicles was present on site during the 360 minute observation period:

Vehicles located on site	Total number of minutes this composition of vehicles was present on site
1 passenger vehicle	
2 passenger vehicles	
3 passenger vehicles	
4 passenger vehicles	
5 passenger vehicles	
6 passenger vehicles	
1 passenger vehicle + 1 RV or commercial	
2 passenger vehicles + 1 RV or commercial	
3 passenger vehicles + 1 RV or commercial	
4 passenger vehicles + 1 RV or commercial	
5 passenger vehicles + 1 RV or commercial	
6 passenger vehicles + 1 RV or commercial	
1 passenger vehicle + 2 RV or commercial	
2 passenger vehicles + 2 RV or commercial	
3 passenger vehicles + 2 RV or commercial	
4 passenger vehicles + 2 RV or commercial	
5 passenger vehicles + 2 RV or commercial	
6 passenger vehicles + 2 RV or commercial	
Other (describe)	
Other (describe)	

4. Using the above information, please provide a bar chart or pie chart displaying the data.

Traffic Study

- 5. Are the queue lengths calculated based on traffic counts and turn movements taken September 1st on HWY 75, the observation data collected at the Chevron, or some other data? Please explain. Queue lengths should be based on the L2 counts and turn movements from September 1st.
- 6. Is LOS based on traffic counts and turn movements taken on September 1st on HWY 75, the observation data collected at the Chevron, or some other data? Please explain. LOS should be based on the L2 counts and turn movements from September 1st.
- 7. Please explain why the following occur:
 - a. Existing (2016) Background Conditions queue lengths for NB 10th / HWY 75 and EB 10th / HWY 75 are both 85'; with the Existing (2016) Plus Project Conditions, the NB 10th / HWY 75 queue length reduces to 80' and the EB 10th / HWY 75 queue length reduces to 70'. Why is there a reduction in queue length for both intersections?
 - b. The Existing (2016) Plus Project Conditions Analysis reports that the anticipated NB project access queue length will be 50' and the level of service for the EB approach to the project is A. Why was an EB level of service cited rather than a NB level of service for the project access? Similarly, why were NB access queue lengths referenced for the 2020 and 2026 Project Plus Conditions analyses while EB levels of service were reported?
 - c. The Future (2020) Background Conditions for the NB 10th / HWY 75 and EB 10th / HWY 75 queue lengths for are80' and 70' respectively, which are less than the Existing (2016) Background Condition queue lengths of 85'. Why are the queue lengths less in 2020?
 - d. The Future (2020) Plus Project Conditions Analysis reports the NB 10th / HWY 75 queue length to be 50', which is less than NB 10th / HWY 75 queue for the 2020 Background Conditions and the 2016 Plus Project conditions. Please explain why.
 - e. The Future (2020) Plus Project Conditions Analysis reports that the EB 10^{th} / HWY 75
- 8. Address the methodology used to collect the data at the gas station/convenience store/Gandolfo's in Provo and how the methodology and the classification of the use compares to ITE's process for incorporating new uses in the ITE manual. In other words, address whether the gas station/convenience store/Gandolfo's data will be used, or if it is intended to be used, in forthcoming editions of the ITE manual.
- 9. Include discussion of the Provo example in the Executive Summary section.
- 10. Provide the date of revision or label the study "revised" on the front cover of the Traffic Impact Study.
- 11. Based on the updated traffic study, our interpretation is that left and right turn movements into and out of the project site were derived by assigning 40% of trips to the site from the north and 60% of trips to the site from the south, prevailing movements at each intersection determined by data collected in September 2016 and permitted turn movements, and Snychro/SimTraffic. Please confirm and explain more thoroughly or explain otherwise.
- 12. Address these points from the August 3, 2016 information request letter in narrative form:
 - "4a. The existing traffic study assumes there is an 80 foot stacking lane available for cars turning left. However, the stacking lane for cars entering into the project appears to be less than 80 feet as indicated on the Site Plan. Additionally, it appears the analysis only focuses on the delay and stacking for cars turning left onto 10th Street and does not take into account cars turning left into the project and how that impacts cars turning left on 10th Street. For instance, a car turning left onto 10th Street is not going to que behind a car waiting to turn left into the project.
 - i. The analysis needs to look at those two turning movements separately and determine if the stacking lane is sufficient to accommodate each independent turning movement.
 - ii. The study needs to address impacts to through traffic if one of the two turning lanes is over capacity and cars are stacked up in the travel lane.

- iii. Show how the traffic light at 5th Street and the current traffic issues merging onto Warm Springs will be impacted by the gas station. Currently, this is one of the most congested areas of the city and the Bracken project has yet to address these concerns satisfactory to staff."
- 13. Address the impact of traffic coming from 9th Street and turning northbound onto the highway, into the turn lane, and into the project site. Address the impact of such traffic on overall traffic flow in the vicinity of the project site.
- 14. Provide information about the characteristics of the Community School, Warm Springs Ranch Resort, and Stock Lumberyard proposal as used to generate future conditions. Provide a table summarizing trips each use will generate or an appendix.
- 15. Explain the implications of queuing in the 77.8' NB L turn lane potentially exceeding the 77.8' length and vehicles queening in the area of the turn lane that NB vehicles use to exit the project site.

Exhibits

- 16. Exhibit #1 Provide a variation showing two commercial delivery vehicles on the site. Show a semi-truck sized delivery truck circulating into the site, into location at the loading zone, and circulation out of the loading zone (including backing up maneuvers). The second commercial delivery vehicle may be a 30' box truck.
- 17. Exhibit #2 Provide a variation, with all passenger vehicles at the same locations, showing a semi-truck sized delivery truck circulating to the loading zone and circulating out of the loading zone (including backing up maneuvers).

Attachment F

"Bracken Station – Additional Information Requested Response"	' memo from Hales Engineering, dated
October 4, 2016	



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MEMORANDUM

Date: October 4, 2016

To: Brittany Skelton, Senior Planner

City of Ketchum

From: Hales Engineering

Benchmark Associates

Subject: Bracken Station – Additional Information Request Response

UT16-851

This memorandum addresses the questions and requests for clarification or information posed in a memo titled "Bracken Station – Conditional Use Permit application, Additional Information Requested by City of Ketchum staff," dated September 30, 2016. Each question/request posed by the city staff will be reproduced below in italics, followed by a response from Hales Engineering and/or Benchmark Associates as appropriate.

Vehicle Observation and Classification

1. Please verify that the food truck was on site and operating at the Chevron during the entire time period observation data was collected.

The food truck was onsite and operating during the September 1st data collection activities.

- 2. Please clarify vehicle classification:
 - a. How were the following oversized passenger vehicles classified?
 - i. Personal Sprinter or other make/model of oversize vans(without an commercial logos)
 - ii. Personal vehicles towing boats
 - iii. Personal vehicles towing flatbed trailers with ATVs, motorbikes, or other items
 - iv. Personal vehicles, without company logos, towing construction trailers
 - b. Please confirm that passenger vehicles with company logos, such as Chevrolet/Ford/etc. trucks, were classified as commercial vehicles.



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Personal "Sprinters" or other oversize vans without company logos were classified as passenger vehicles. Any personal vehicle towing a boat, trailer with ATVs or motorcycles, or any other trailer that was not obviously a construction/landscaping trailer was classified as an RV. All vehicles towing construction/landscaping trailers were classified as commercial vehicles. All vehicles, regardless of make or model, that bore a company logo were classified as commercial vehicles.

3. Please detail the total number of minutes each combination of vehicles was present on site during the 360 minute observation period:

The worst case scenarios that Benchmark Associates and City Staff agreed on represent the most constrained site conditions. Their durations are shown below:

Vehicle Composition	Duration	Percentage of Occurance
		During 360 Observation Period
5 Passenger + 2 Commercial	2 minutes	0.56%
6 Passenger + 1 Commercial	1 minute	0.28%
5 Passenger + 1 Commercial	4 minutes	1.1%
4 Passenger + 2 Commercial	3 minutes	0.83%

Durations for the additional vehicle combinations requested by city staff do not affect the design of Bracken Station because all lesser combinations would show fewer constraints and improve site circulation.

4. Using the above information, please provide a bar chart or pie chart displaying the data.

See the above table.

Traffic Study

5. Are the queue lengths calculated based on traffic counts and turn movements taken September 1st on HWY 75, the observation data collected at the Chevron, or some other data? Please explain. Queue lengths should be based on the L2 counts and turn movements from September 1st.

As is stated in the "Bracken Station Traffic Impact Study UPDATED" report submitted to Ketchum City staff on September 26, 2016, in Chapter II Section C, the traffic counts used for the Existing (2016) Background Conditions analysis of the 10th Street / Main Street (SH-75) and 5th Street / Main Street (SH-75) intersections were collected on September 1st as directed by city staff. These data collected on September 1st were also used to project future 2020 and 2026 background traffic volumes at the study intersections (explained in Chapter V Section C and in Chapter VII Section C) that were used for the



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Future (2020) Background Conditions and Future (2040) Background Conditions analyses.

Also as directed by city staff, the data from the September 1st counts at the Hailey Chevron were used to calculate trip generation for the proposed Bracken Station. These generated trips were added to each of the observed or projected background volumes at the study intersections to study "Plus Project" conditions. This is explained in more detail in the previously mentioned document submitted to Ketchum City staff on September 26, 2016 (see Chapter III, Chapter IV Section B, Chapter VI Section B, and Chapter VIII Section B).

All 95th percentile queue lengths and levels of service (LOS) presented in the previously mentioned report are based on data collected at the 10th Street / Main Street (SH-75) and 5th Street / Main Street (SH-75) intersections and the Hailey Chevron on September 1, 2016.

6. Is LOS based on traffic counts and turn movements taken on September 1st on HWY 75, the observation data collected at the Chevron, or some other data? Please explain. LOS should be based on the L2 counts and turn movements from September 1st.

Please see response to question/comment 5.

- 7. Please explain why the following occur:
- a. Existing (2016) Background Conditions queue lengths for NB 10th / HWY 75 and EB 10th / HWY 75 are both 85'; with the Existing (2016) Plus Project Conditions, the NB 10th / HWY 75 queue length reduces to 80' and the EB 10th / HWY 75 queue length reduces to 70'. Why is there a reduction in queue length for both intersections?

The 10th Street / Main Street (SH-75) intersection was studied with the existing lane configuration for the Existing (2016) Background Conditions analysis (see Chapter II Section A as well as Figure 2). The intersection currently consists of a shared thru/left-turn lane on the northbound approach, a shared thru/right-turn lane on the southbound approach, and a shared right/left-turn lane on the eastbound approach. In the current configuration, if a vehicle traveling northbound on Main Street (SH-75) wishes to turn left onto 10th Street, they must slow down and/or stop in the single northbound lane until an acceptable gap in the southbound traffic is available before they can execute the turning movement. In turn, vehicles wishing to continue traveling north on Main Street (SH-75) must queue behind the stopped left-turning vehicle.

Vehicles on 10th Street waiting to turn left onto Main Street (SH-75) must yield to both left-turning and thru vehicles on Main Street (SH-75), and wait for an acceptable gap to execute the turning movement. Vehicles wishing to turn right onto Main Street (SH-75) must also queue behind the left-turning vehicles and vice versa.



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The Existing (2016) Plus Project Conditions analysis assumed that a similar shared thru/left-turn lane would serve the project access (see Figure 4). While a northbound vehicle waits for an acceptable gap to execute a left-turn movement into the project access, all thru vehicles must queue behind the left-turning vehicle. This left-turn ingress movement at the project access shifts some of the queueing that normally would have occurred at 10th Street south to the project access resulting in a shorter 95th percentile queue length on the northbound approach to the 10th Street / Main Street (SH-75) intersection.

The northbound queuing at the project access also creates more or longer gaps in northbound traffic at the 10th Street / Main Street (SH-75) intersection, resulting in a reduction in the time vehicles have to wait for an acceptable gap to execute the turning movement. This also results in a shorter 95th percentile queue length on the eastbound approach to the 10th Street / Main Street (SH-75) intersection.

b. The Existing (2016) Plus Project Conditions Analysis reports that the anticipated NB project access queue length will be 50' and the level of service for the EB approach to the project is A. Why was an EB level of service cited rather than a NB level of service for the project access? Similarly, why were NB access queue lengths referenced for the 2020 and 2026 Project Plus Conditions analyses while EB levels of service were reported?

As is stated in Chapter I Section C of the "Bracken Station Traffic Impact Study UPDATED" report submitted to Ketchum City staff on September 26, 2016, level of service for signalized and all-way stop controlled intersections is based on the weighted average of all approach delays. For all other unsignalized intersections, LOS is based on the worst approach. The LOS for the eastbound approach was reported because the average delay for vehicles on that approach was greater than the average delay for vehicles on either of the other two approaches.

The northbound queue length was referenced because queuing on the northbound approach was a point of emphasis in the memo from Brittany Skelton to Steve Cook dated August 3, 2016, outlining the scope for the TIS as directed by Ketchum City staff, and because queues blocking thru lanes are generally of more concern to governing entities than queues backing onto a project site due to potential impacts to the surrounding transportation network.

Average delay and 95th percentile queue lengths are not necessarily correlated, especially when volumes on the approaches are unbalanced (meaning the volumes on the project access are much smaller than the volumes on Main Street (SH-75)), and one approach is stop-controlled while another is uncontrolled.



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As stated in Chapter IV Section D, (as well as Chapter VI Section D and Chapter VII Section D) detailed queuing reports are provided in Appendix D. (Detailed LOS reports are also provided in Appendix B.)

c. The Future (2020) Background Conditions for the NB 10th / HWY 75 and EB 10th / HWY 75 queue lengths for are 80' and 70' respectively, which are less than the Existing (2016) Background Condition queue lengths of 85'. Why are the queue lengths less in 2020?

As shown in Appendix D of the previously mentioned report, the 85th percentile queue lengths for the nouthbound and eastbound approaches to the 10th Street / Main Street (SH-75) intersection are actually 80 feet and 77 feet, respectively.

As is discussed several times throughout the report, multiple runs of Synchro/SimTraffic are used to provide a statistical evaluation of traffic conditions. Each run of the model uses a different random vehicle generator (while adhering to hourly volumes at each intersection, peak hour factors, and other quantified values) so that results can be calculated from a variety of possible conditions, as opposed to one single statistical sample. As such, small variations in calculated values (such as 5-7 feet of 95th percentile queue length) are statistically insignificant, especially when traffic volumes increased by such a small number. For this reason, it was reported in Chapter V Section E that no significant changes to the 95th percentile queues are anticipated with future (2020) background conditions.

d. The Future (2020) Plus Project Conditions Analysis reports the NB 10th / HWY 75 queue length to be 50', which is less than NB 10th / HWY 75 queue for the 2020 Background Conditions and the 2016 Plus Project conditions. Please explain why.

As is stated in Chapter VI Section B (as well as shown in Figure 6), for the Future (2020) Plus Project Conditions analysis it was assumed that a center two-way left-turn lane had been installed along the project frontage (as recommended in Chapter IV Section E). This two-way left-turn lane allows northbound vehicles on Main Street (SH-75) wishing to turn left onto 10th Street to slow down, stop, and queue without blocking the northbound thru lane. In the Future (2020) Background Conditions and the Existing (2016) Plus Project Conditions analyses, all left-turning vehicles were required to slow down and queue in the shared thru/left-turn lane. Since thru vehicles do not have to wait for turning vehicles to execute left-turn movements, fewer cars end up queuing at the intersection and thus the 95th percentile queue lengths are shorter.

e. The Future (2020) Plus Project Conditions Analysis reports that the EB 10th / HWY 75

This question/comment appears to be incomplete.



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8. Address the methodology used to collect the data at the gas station/convenience store/Gandolfo's in Provo and how the methodology and the classification of the use compares to ITE's process for incorporating new uses in the ITE manual. In other words, address whether the gas station/convenience store/Gandolfo's data will be used, or if it is intended to be used, in forthcoming editions of the ITE manual.

As is stated in Chapter 9 Section B of the previously discussed report, this data was collected by university students for the Institute of Transportation Engineers (ITE), and not by or for Hales Engineering. The observed hourly volumes, directional distributions (vehicles entering and exiting), and a brief description of the site were provided to Hales Engineering as a courtesy. ITE paid for this data collection to be used in a future edition of their *Trip Generation Manual*.

9. Include discussion of the Provo example in the Executive Summary section.

This has been added to the Executive Summary section of the report dated October 3, 2016, and is noted as a hypothetical scenario analyses.

10. Provide the date of revision or label the study "revised" on the front cover of the Traffic Impact Study.

The date has been added to the cover page of the document.

11. Based on the updated traffic study, our interpretation is that left and right turn movements into and out of the project site were derived by assigning 40% of trips to the site from the north and 60% of trips to the site from the south, prevailing movements at each intersection determined by data collected in September 2016 and permitted turn movements, and Snychro/SimTraffic. Please confirm and explain more thoroughly or explain otherwise.

In the original TIS report, we distributed 85% to/from the south and 15% to/from the north. This calculation was based on the existing directional distribution of traffic on Main Street (SH-75). In the previously mentioned memo from Brittany Skelton to Steve Cook dated August 3, 2016, outlining the scope for the TIS as directed by Ketchum City staff, staff explained that 30-40% of trips would be coming from the north, and thus 60%/40% distribution should be used in subsequent analyses.

12. Address these points from the August 3, 2016 information request letter in narrative form: "4a. The existing traffic study assumes there is an 80 foot stacking lane available for cars turning left. However, the stacking lane for cars entering into the project appears to be less than 80 feet as indicated on the Site Plan. Additionally, it appears the analysis only



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focuses on the delay and stacking for cars turning left onto 10th Street and does not take into account cars turning left into the project and how that impacts cars turning left on 10th Street. For instance, a car turning left onto 10th Street is not going to que behind a car waiting to turn left into the project.

- i. The analysis needs to look at those two turning movements separately and determine if the stacking lane is sufficient to accommodate each independent turning movement.
- ii. The study needs to address impacts to through traffic if one of the two turning lanes is over capacity and cars are stacked up in the travel lane.
- iii. Show how the traffic light at 5th Street and the current traffic issues merging onto Warm Springs will be impacted by the gas station. Currently, this is one of the most congested areas of the city and the Bracken project has yet to address these concerns satisfactory to staff."

Although it is debatable whether or not a vehicle turning left onto 10th Street would queue behind a vehicle waiting to turn left into the project, the two turning movements were in fact analyzed separately in each version of the TIS report. In each instance, it was determined that the stacking distance for each movement is sufficient to accommodate each independent turning movement.

If the left-turn queuing demand were to exceed the capacity at either 10th Street or the project access, then the left-turn queue would block the thru lane, just as happens currently at the 10th Street / Main Street (SH-75) intersection. The addition of the left-turn lane along the project frontage would cause the thru lane to be blocked less often than it is currently with the current lane configuration.

With the addition of the gas station, approximately 27 vehicles would be added to the existing volumes on the southbound approach to the 5th Street / Main Street (SH-75) intersection during the evening peak hour (an increase of less than 4%). Queues of several hundred feet have been reported on this approach, and it is likely that vehicles leaving the gas station heading south on Main Street (SH-75) would end up in this existing queue. It is difficult to say how the Warm Springs Road / Main Street (SH-75) intersection will be impacted, as this intersection was not in the scope outlined by city staff. However, based on the analyses of the 5th Street / Main Street (SH-75) intersection, it is likely that the impact of the traffic added by the gas station would be insignificant.

13. Address the impact of traffic coming from 9th Street and turning northbound onto the highway, into the turn lane, and into the project site. Address the impact of such traffic on overall traffic flow in the vicinity of the project site.

The analysis of the 9th Street / Main Street (SH-75) intersection was not included in the scope that was provided by city staff. Therefore, no data was collected for that intersection and the intersection was not analyzed. However, it can be assumed that vehicles turning



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right from 9th Street will have to wait for an acceptable gap in traffic, just as they do now, before executing the turning movement.

14. Provide information about the characteristics of the Community School, Warm Springs Ranch Resort, and Stock Lumberyard proposal as used to generate future conditions. Provide a table summarizing trips each use will generate or an appendix.

This has been added to the Appendix section of the report dated October 3, 2016.

15. Explain the implications of queuing in the 77.8' NB L turn lane potentially exceeding the 77.8' length and vehicles queening in the area of the turn lane that NB vehicles use to exit the project site.

Vehicles exiting the project site, whether they are turning left or right, must yield to traffic on 10th Street. When vehicles are queued and waiting to turn left, at either 10th Street or at the project access, left-turning vehicles leaving the site must wait for the queue to clear before executing the turning movement. While it is possible that queues on Main Street (SH-75) will block the left-turn egress movements from the site, this is only anticipated to happen on rare occasions.

Exhibits

16. Exhibit #1 – Provide a variation showing two commercial delivery vehicles on the site. Show a semi-truck sized delivery truck circulating into the site, into location at the loading zone, and circulation out of the loading zone (including backing up maneuvers). The second commercial delivery vehicle may be a 30' box truck.

Deliveries to Bracken Station will take place both in the loading space provided in the southern corner of the site and in the alley accessed from 10th Street. Two commercial delivery vehicles are not anticipated on site.

17. Exhibit #2 – Provide a variation, with all passenger vehicles at the same locations, showing a semi-truck sized delivery truck circulating to the loading zone and circulating out of the loading zone (including backing up maneuvers).

"Semi-Truck Delivery Circulation" exhibit provided via email 10/4/16, 8:28 AM.

Attachment G

Table 4. Zoning Standards Analysis

				Compliance with Zoning Standards
С	omplia	nt		Standards and Staff Comments
Yes	No	N/A	Guideline	City Standards and Staff Comments
\boxtimes	П		17.12.030.C	Lot Area
			Staff Comments	8,000 square feet minimum is required. The lot is 0.4267 acres or 18,590
				square feet.
\boxtimes		П	17.12.030.C &	Setbacks and Supplementary Yard Requirements
			17.128.020.C	
			Staff Comments	Buildings "A" and "C" currently have non-conforming setbacks on the front
				(eastern) property line. Building "B" currently conforms to setbacks. The
				applicant is proposing to demolish buildings "A" and "C" and to build an
				addition to building "B" which will result in a site with structures that meet
				setback requirements.
				Proposed Front (north – 10 th Street) –20'
				Proposed Side (east – Main Street) – 13–'4"
				Proposed Rear – (west 0 alley) – 0'
				The proposed setbacks meet setback requirements.
\boxtimes			17.12.030.C	Building Coverage
			Staff Comments	Permitted - 75% Proposed – 23% (including gas station canopy)
\boxtimes			17.12.030.C	Building Height
			Staff Comments	Maximum building height permitted is 35'; the existing buildings are 13'-8"
				above grade on Main Street and 24'-8" above grade on 10 th Street; the
				proposed addition to building "B" is 13'-8" above grade on Main Street and
				24-8" above grade on 10 th Street. The proposed canopy is 18' above grade on
				Main Street and 20' above grade from 10 th Street at the eastern edge of the
				structure and 24' above grade from 10 th Street at the western edge of the
				structure.
\boxtimes			17.125.030.H	Curb Cut
			Staff Comments	A maximum of thirty five percent (35%) of the linear footage of any street
				frontage can be devoted to access off street parking.
				The curb cut design was recommended by ITD is 84' (40' entrance, 4' island, 4'
				exit) in width, which equates to 30.6% of the linear footage frontage of the
				lot. (The linear footage of lot frontage is 273.97'.)
\boxtimes			17.124.060.M	Parking Spaces
			Staff Comments	Required:
				The off street parking standards apply when an existing structure or use is
				expanded or enlarged. Additional off street parking spaces shall be required
				only to serve the enlarged or expanded area, not the entire building or use.
				2 spaces per fuel pump at fuel pump; 4 pumps require 8 spaces.
				1 space per 250 square feet retail;
				1 space per 250 square feet restaurant

			There is a 508 square foot addition to the existing 2,084 square foot building proposed; 3 spaces are required.
			Proposed: • 8 for temporary holding at the fuel pumps • 12 to serve retail/restaurant (4 spaces are lower level accessed from 10 th Street) • There are 4 additional lower level parking spaces accessed from 10 th
			Street to serve the existing uses.
\boxtimes		17.125.040	Off Street Parking and Loading Areas
			17.125.040 - In the LI-1, LI-2 and LI-3 districts, off street loading areas (containing 180 square feet with no 1 dimension less than 10 feet) shall be required as an accessory use for new construction or major additions involving an increase in floor area, as follows: One off street loading space for floor area in excess of two thousand (2,000) square feet, provided no loading space occupies any part of a public street, alley, driveway or sidewalk; except, that where practicable to do so, an alley may be used in lieu of the requirement of this section if prior permission is granted by the commission.
			The project consists of 2,592 square feet on the second floor of the building, which is at grade when accessed from Main Street. The existing first floor of the building is 2,084 and is accessible from 10 th Street and the alley behind the building. With 4,676 square feet 2 off-street loading spaces are require for the site.
			The minimum permitted size of an off-street loading space is 10' x 18'; the site plan indicates 1 off-street loading space of 10' x 26' which satisfies this requirement. The Commission may grant permission for use of the alley to satisfy the requirement for a second loading space.
\boxtimes		17.18.140, 17.12.020 and 17.08.020	Zoning Matrix & Definitions
		17.00.020	17.18.140 - A. Purpose: The LI-1 light industrial district number 1 is established as a transition area providing limited commercial service industries, limited retail, small light manufacturing, research and development, and offices related to building, maintenance and construction and which generate little traffic from tourists and the general public. (Ord. 1135, 2015)
			Staff notes that uses in the LI-1 district are intended to generate little traffic from tourists and the general public.
			17.12.020 – Motor Vehicle Fueling Stations are allowed in the LI-1 zone with a Conditional Use Permit. The applicant is proposing a motor vehicle fueling station with 4 fuel pumps, two electric vehicle charging stations, and retail sales for the convenience of the motoring public. Food Service is allowed in the LI-1 zone with a Conditional Use Permit when the conditions described in footnote #15 are adhered to.
			The applicant is proposing to remodel the existing building, consisting of 2,084 square feet, and to add an addition of 508 square feet and an attached outdoor patio area with seating. The applicant is proposing to utilize the

			remodeled and expanded building for a retail store associated with the motor vehicle fueling station and for a deli service restaurant. The site plan indicates a food service area of 280 square feet. Footnote #15 limits the hours of operation of restaurants that require a conditional use permit to no later than 9:00 p.m. but gives the Commission the authority to expressly permit operation of the restaurant past 9:00 p.m. as part of the conditional use permit approval. The zoning code does not specify hours of operation for fuel pumps or retail sales for the convenience of the motoring public that are associated with motor vehicle fueling stations. However, the Commission may condition hours of operation in order to minimize adverse impact on other development.
			17.08.020 – Definitions: Motor Vehicle Fueling Station - A facility providing the retail sale and direct delivery to motor vehicles of fuel, including electric charging stations, lubricants and minor accessories, and retail sales for the convenience of the motoring public.
			Food Service - An establishment where food and drink are prepared, served and consumed on site with associated outdoor dining, or distributed to customers through take out, delivery or catering. Typical uses include, but are not limited to restaurants, cafes, delis, catering services and brewpubs that do not distribute beer produced for off-site consumption.
			Footnote #15. Catering and food preparation is permitted. Restaurants require a conditional use permit and shall not exceed 1,000 square feet and serve no later than 9:00 P.M. unless expressly permitted through approval of the conditional use permit.
\boxtimes		17.132.020J & 17.132.020K	Dark Skies
			J. The average foot-candle lighting for service stations is required to be no greater than 30 foot-candles, as set by the IESNA for urban service stations. K. [Canopy lights] shall be recessed sufficiently as to ensure that no light source is visible from or causes glare on public rights of way or adjoining property.
			As indicated in the Photometric Plan, the average foot-candle lighting for the canopy is 28.51 foot-candle.
			As indicated by the Lighting Fixtures exhibit, all canopy lights are CRUS-SC-LED and CRUS-AC-LED fixtures. The light source Is recessed within the fixture and the fixtures themselves will be flush mounted to the underside of the canopy.

Attachment H

Table 5: Conditional Use Permit Requirements

Conditional Use Requirements 1. EVALUATION STANDARDS: 17.116.030 and § 67-6512 of Idaho Code A conditional use permit shall be granted by the commission only if the applicant demonstrates that: **Compliance and Analysis** Yes No N/A Code City Standards and Staff Comments 17.116.030(A) The characteristics of the conditional use will not be unreasonably incompatible with the \boxtimes CONDITIONAL types of uses permitted in the applicable zoning district. USE Staff Comments Staff's analysis from the July 25, 2016 staff report remains unchanged and is as follows: The LI-1 district allows for one of the widest varieties of uses in the zoning code use matrix; uses ranging from manufacturing to personal service to warehousing and wholesaling to automotive uses are permitted. The LI-1 and LI-2 districts are the only districts that permit motor vehicle fueling stations within the City of Ketchum and in both the LI-1 and LI-2 districts motor vehicle fueling stations are permitted only with a conditional use permit. The city has ten districts classified as commercial or light industrial; food service is permitted in six districts of those districts and is permitted conditionally in two districts (LI-1 and LI-2). The proposed uses of a motor vehicle fueling station with associated food service are generally compatible with the types of uses permitted in the LI-1 district. However, Ketchum zoning code section 17.18.140 defines the purpose of the Light Industrial District Number 1 as: "A. Purpose: The LI-1 light industrial district number 1 is established as a transition area providing limited commercial service industries, limited retail, small light manufacturing, research and development, and offices related to building, maintenance and construction and which generate little traffic from tourists and the general public. (Ord. 1135, 2015)" The Retail S Analysis, dated January 2016 and conducted by Gmap USA and provided by the applicant states, "The population is around 3,200 people within 2.0 miles and the median age is about 47 years old. The population is somewhat lighter than ideal for this type of site location and the median age is a little high for ideal C-store customer base population. However the focus for this site is the winter and especially the summer tourists that pass through the town." With respect to business projections, the Retail S Analysis states, "One of the keys for this site is to provide a good operation with a good offering that will bring in the commuter that passes by the intersection on a consistent basis...The focus on the merchandising should be having a quality offering that entices the commuter/tourist traffic that passes by the site on a regular basis.

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			The site should have a large fountain and coffee offering to entice the commuters to use the site as their refreshment spotOverall the site is on a good corner is[sic] the area and has good potential. The traffic passing by the site is strong and along with the residential backup the location should do well."
			As such, while the proposed uses are generally compatible with the types of uses permitted in the LI-1 zone, the proposed uses on this specific site are dependent on traffic from tourists and the general public, which is in conflict with the purpose of the LI-1 zone.
\boxtimes		17.116.030(B)	The conditional use will not materially endanger the health, safety and welfare of the
		Staff Comments	In regards to health, safety and welfare concerns of the underground fuel
			storage tanks associated with the use, as noted by the Fire Department, the
			underground fueling tanks and fueling stations must be constructed to meet
			applicable Fire Code. Additionally, state and federal environmental standards
			for the construction of fuel storage tanks and operation of fuel pumps will have to be met. The applicant has provided a copy of the Idaho Department of
			Environmental Quality's "Rules Regulating Underground Storage Tank
			Systems", IDAPA 58.01.07.
			The applicant has also submitted an exhibit from J.M. Plenik, P.E., regarding
			the Xerxes Corporation underground fuel storage tanks proposed for the site.
			The exhibit states that seismic activity occurring at a distance away from the
			tanks could be withstood but that seismic activity occurring at or very near the
			tanks would rupture the tanks. The applicant has also submitted a brochure
			for the proposed Xerxes underground tanks, which notes safety features.
			The applicant addressed concerns regarding gas spillage from fuel pumps
			onto snow or ice and drainage into the on-site oil/water separator at the July
			11, 2016 meeting and no further information was requested by the
			Commission or staff.
			Additionally, as analyzed in Attachment I, Table #: Required Public and Private
			Improvements and Attachment J: Recommended Additional Public
			Improvements, the majority of pedestrian and vehicular safety and welfare
			concerns could be addressed by the sidewalks, crosswalks, rapid flashing
			beacon, turning lane, and reduced curb cut width proposed by the applicant.
			However, after review and analysis of the new vehicle circulation exhibits
			prepared for the October 10, 2016, meeting, concerns still exist regarding on-
			site circulation and potential negative externalities. These concerns as
			discussed in detail in the next section.
			As such, at this time the applicant has not proved that the conditional use will
\boxtimes		17.116.030(C)	not materially endanger the health, safety, and welfare of the community. The conditional use is such that pedestrian and vehicular traffic associated with the use will
			not be hazardous or conflict with existing and anticipated traffic in the neighborhood.
		Staff Comments	The applicant has submitted an updated Traffic Study prepared by Hales

Engineering, dated October 3, 2016, which analyzes existing traffic levels of service at the Main Street and 10th Street, Main Street and 5th Street, and Main Street and Project Access intersections based on existing conditions, projected future conditions with background conditions but without the proposed use, and projected future conditions with background conditions and with the proposed use. The Traffic Study recommends improvements, namely the proposed turning lane, in order to maintain level of service.

The applicant previously submitted a Pedestrian Study prepared by Alta Planning + Design. The Pedestrian Study analyzes three pedestrian catchment areas where pedestrians traveling to the site are anticipated to be drawn from and recommends specific improvements to enhance pedestrian safety. Some recommendations reinforce recommendations previously made by staff, some recommendations are new, and some are slight variations to recommendations previously made by staff. These recommendations are discussed in depth in Attachment L, Table 8, Summary and Analysis of New Plans, Studies and Information Received for the July 11, 2016 meeting. Staff finds that some of the recommended improvements in Table 6: Required Public and Private Improvements and Table 7: recommended Additional Public Improvements could adequately mitigate the majority of potential hazards or conflict with existing and anticipated pedestrian and vehicular traffic associated with the use in the context of travel to the site, however, some of the recommendations in the Pedestrian Study will require further study and analysis before staff can make a recommendation.

The applicant previously submitted several circulation exhibits for the July 11, 2016 meeting. Staff's analysis was that neither the initial On-Site Vehicle Turn Exhibit nor the revised On-Site Vehicle Turn Exhibit-Circulation adequately proved turn movements could be made in real world conditions in such a manner that did not cause traffic to back up on Main Street/Highway 75. For the October 10th, 2016 meeting staff requested additional exhibits indicating the turn movements and queuing locations of delivery trucks, recreational vehicles, and fueling trucks based on conditions observed at a comparable site.

The analysis of the new exhibits is detailed in Attachment C, Table 3 Analysis of Vehicle Circulation Exhibits dated September 16, October 3 and October 4, 2016: As noted in Table 3, while there are scenarios illustrated in each exhibit where optimal conditions enable commercial or recreational vehicles to maneuver to a fuel pump, adequate circulation is often dependent on 1 particular pump of the 8 pumps on site being available or 2 pumps in tandem both being available at the same time. Additionally, the ability of oversize vehicles to enter the site and circulate past the fueling island to park in a parking space or park in the loading space depends on each vehicle parking at a fuel pump in such a way that the vehicle's encroachment into the circulation areas is minimized; the encroachment into the circulation area of just one oversize vehicle parked at just one fuel pump could constrain or prevent vehicles in the travel lane or turn lane from entering the site. Furthermore, during the lengths of time that a fuel delivery truck is on site the ability of a box truck 30' in length or a passenger vehicle towing an rv to enter the site and circulate is dependent on the east side of pump #1 and the west side of

			pumps #1 and #2 being closed for the box truck or the west side of pumps #1 and #2 being closed for the rv because it is physically impossible for the box truck or rv to circulate through the site if vehicles are fueling at the pumps noted while a fuel delivery truck is on site. Although Hales Engineering notes in their memo dated October 4, 2016 that the occasions when 2 commercial vehicles and 4 or 5 passenger vehicles were observed on site were 3 minute (0.83% of the time observed) and 2 minute (0.56% of the time observed) lengths of time respectively, L2 Data Collection observed that 26.9% of all trips to the Hailey Chevron between the hours of 7:00 – 9:00 a.m. were commercial vehicles and during the hours of 3:00 – 7:00 p.m. 10.5% of all trips were commercial vehicles. The ability of the proposed development to adequately accommodate on site circulation in such a manner that does not adversely impact traffic on Main Street in the southbound travel lane or in the proposed northbound turn lane cannot be evaluated by assuming the only potential impact will occur the 0.56% or 0.83% of the time that 4 or 5 passenger vehicles and 2 commercial vehicles were observed on site. Rather, the ability of the site to mitigate external traffic impacts should be evaluated more comprehensively. For example, a northbound box truck can only maneuver to the west side of pumps #1 and #2 to fuel unless a 3, 4, or 5 point turn is made to maneuver to pump #3 or #4. The box truck's ability to fuel at pump #2 is dependent on pump #1 also being open. The percentage of time that the east side of pumps #1 and #2 to fuel unless a 3, 4, or 5 point turn is made to maneuver to pump #1 and #2 to fuel unless a 3, 4, or 5 point turn is made to maneuver to pump #1 and #2 to fuel unless a 3, 4, or 5 point turn is made to maneuver to pump #1 and #2 to fuel unless a 3, 4, or 5 point turn is made to maneuver to pump #1 and #2 to fuel unless a 3, 4, or 5 point turn is made to maneuver to pump #1 and #2 to fuel unless a 3, 4, or 5 point turn is m
			As such staff can not recommend approval of the Conditional Use Permit because it has not been proved that vehicular traffic associated with the use
			will not be hazardous or conflict with existing and anticipated traffic in the neighborhood.
\boxtimes		17.116.030(D)	The conditional use will be supported by adequate public facilities or services and will not
			adversely affect public services to the surrounding area or conditions can be established to mitigate adverse impacts.
		Staff Comments	Due to the proposed pedestrian and vehicular public improvements, and
			review of the proposed use and the site, the conditional uses can be supported
			by adequate public facilities or services and will not adversely affect public
<u> </u>		47.446.000/5\	services to the surrounding area.
		17.116.030(E)	The conditional use is not in conflict with the policies of the Comprehensive Plan or the basic purposes of this Section.
		Staff Comments	Staff's analysis from the July 25, 2016 staff report remains unchanged and is

as follows:

The Comprehensive Plan designates the property for mixed-industrial use. Primary uses specified include light manufacturing, wholesale, services, automotive, workshops, studios, research, storage, construction supply, distribution and offices make up the bulk of development within this district. Secondary uses specified include a limited range of residential housing types and supporting retail. Uses should generate little traffic from tourists and the general public.

Similar to the compatibility of the proposed uses with the purpose of the LI-1 zone as stated in the zoning code, the proposed motor vehicle fueling station and food service as uses are generally consistent with the Comprehensive Plan. However, due to the location of the specific site, the use proposed will generate additional traffic from both the public and visiting tourists. This is a conflict between the Comprehensive Plan and the zoning code, which only conditionally allow for the motor vehicle fueling station and food service uses in the LI-1 and LI-2 zones.

Further, the introduction of a new fueling station and restaurant into the LI-1 District is a discretionary decision. There are currently three fueling stations in the LI District, two restaurants, and one food mart to service the area. The Commission must decide if the uses proposed are appropriate for the site and the location and are necessary to serve the LI district.

Attachment I

All developments are required to install a minimum amount of infrastructure, however conditional uses may be required to contribute more than the minimum due to the nature of the use and projected impacts. The following table represents the public and private improvements as proposed by the applicant.

Table 6: Required Public and Private Improvements

	Analysis of Proposed Public and Private Improvements
Improvement	Description
New - On Site Sidewalk	The revised site plan dated September 16, 2016 indicates new on-site sidewalk that extends from the southern corner of the property, borders the southwest side of the property, connects to the structure where the accessory retail and foodservice are proposed, and connects to the proposed staircase leading to 10 th Street. This internal, on-site sidewalk proposed by the applicant will allow pedestrians accessing the site to connect to the retail and foodservice without entering the parking lot where vehicles will be circulating.
Main Street – Sidewalk and Landscaping	The existing buildings "A" and "C" currently have a 0' setback from Main Street/Hwy 75. There is no defined curb cut on Main Street and the entire frontage is utilized for vehicular egress to the site and parking. No sidewalk currently exists.
	The applicant is proposing to construct a new 8' sidewalk and landscaping in the right-of-way adjacent to Main Street spanning the entire property frontage. The applicant proposes to maintain the landscaping.
	The 8' sidewalk will have a 84' gap between the northern and southern segments of the sidewalk in order to accommodate the boulevard approach for vehicles. The applicant is proposing to install a surface material that is 8' in width, in alignment with the sidewalk, and visually distinguishable from the surface of the parking lot in order to provide a visual cue to pedestrian and motorists that pedestrians will be utilizing the area.
Main Street – Turn Lane	The applicant retained Hales Engineering to prepare a traffic study for the proposed use and redevelopment of the site. The traffic study recommended constructing a new turn lane on Main Street to facilitate vehicular access to the site.
	The applicant is proposing to construct the turn lane and staff has accepted the design. An existing valley gutter on the eastern side of Main Street/Hwy 75 across from the southern end of the site prevents the turn lane from extending further south. Circulation at the 10 th Street intersection prevents the turn lane from extending further north.
Main Street and 10 th Street, southwest	The applicant has proposed reconfiguring the curb radius at the southwest corner of the Main Street and 10 th Street intersection in order to better accommodate vehicular southbound turns from 10 th Street to Main Street. ITD has approved the

corner curb radius	curb radius.
10 th Street - Sidewalk	There is not currently a sidewalk on the 10 th Street frontage of the site. The applicant is proposing to construct a 5' paved sidewalk in the right-of-way adjacent to the property for the length of the property frontage on 10 th Street.
10 th Street - Staircase	The applicant is proposing to construct a new staircase at the western property corner that will provide access to the sidewalk that will be constructed on 10 th Street. The staircase will be lit with six (6) wall mounted 4" diameter, cylinder shaped light fixtures that point downward and fully shield the LED bulbs in order to enhance pedestrian safety and draw pedestrians from 10 th Street to the staircase in order to access the site.

Attachment J

In addition to the public improvement the applicant is proposing in the table above, staff and the Pedestrian Analysis have identified several other necessary public improvements that are required to mitigate negative impacts of the proposed development. Staff recommends the following improvements as a minimum and other improvements or conditions may be appropriate or discovery through the public process.

Table 7: Recommended Additional Public Improvements

Recon	nmended Public Improvements to Mitigate Impacts of Development
Public Improvement	Description
Boulevard Approach	As proposed in the Pedestrian Analysis, staff recommends visually differentiating the
Pedestrian Definition	pedestrian zone spanning across the boulevard approach with the use of color pavers
	or an alternative material. The owner shall enter into a Maintenance Agreement with
	the City to maintain the pedestrian zone. The applicant agrees to install this
	improvement as indicated on the Overall Site Plan (A-2.1).
Main Street –	Staff is recommending the applicant to construct a painted pedestrian crosswalk
Pedestrian Crosswalk	across Main Street/Hwy 75 at the southeast corner of the site. The crosswalk will
	include a new ADA compliant ramp to provide access to the sidewalk at the
	southeast corner of the site and will utilize an existing ramp on the opposite side of
	Main Street/Hwy 75. The applicant agrees to install this improvement as indicated on
	the Overall Site Plan (A-2.1).
Main Street & 9 th	Staff is recommending the applicant to install a rapid flashing beacon at the Main
Street - Rapid	Street/Hwy 75 crosswalk. The rapid flashing beacon will contain sensors that can be
Flashing Beacon at	activated by pedestrians seeking to use cross. The applicant agrees to install this
Crosswalk	improvement as indicated on the Overall Site Plan (A-2.1).
Main Street & 10 th	Staff is recommending a painted pedestrian crosswalk across 10 th Street at the
Street Intersection –	intersection of 10 th Street and Main Street/HWY 75. The applicant agrees to install
Pedestrian Crosswalk	this improvement as indicated on the Overall Site Plan (A-2.1).
across 10 th Street	
10 th Street & Main	As proposed in the Pedestrian Analysis, staff is recommending a painted pedestrian
Street Intersection –	crosswalk across Main Street at the intersection of 10 th Street and Main Street/HWY
Pedestrian Crosswalk	75.
across Main Street	
10 th Street Pedestrian	As proposed in the Pedestrian Analysis, this recommendation requires further review
Zone definition	and analysis to determine feasibility in light of the existing right of way and current conditions.
10 th Street and Warm	As proposed in the Pedestrian Analysis, this recommendation requires further study
Springs Road Rapid	and preparation of pedestrian warrants to assess if this is an appropriate device for
Flashing Beacon	this location.

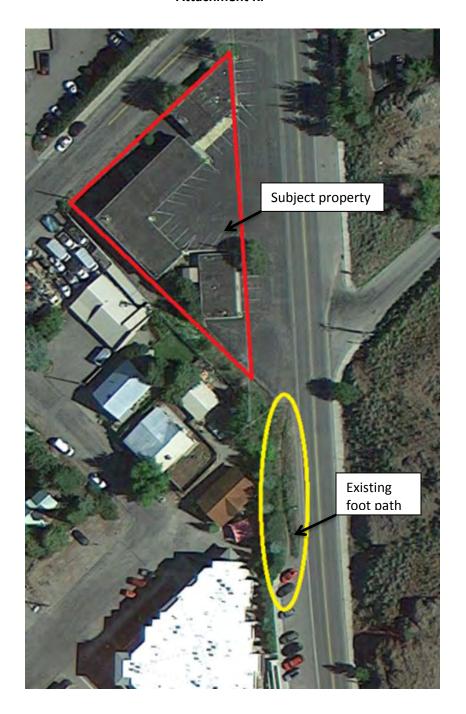
Main Street Sidewalk Extension

Staff recommends extending the 5' sidewalk on Main Street an additional 175' in length (approximately) to connect to the existing public sidewalk located adjacent to the Frenchmen's Place condominium development.

There is not currently a sidewalk connecting the two properties but there is an informally created and well-worn pedestrian foot path; the new uses proposed for the site will generate additional pedestrian trips and a 5', paved, and ADA compliant sidewalk is recommended for safety purposes. See Attachment F.

The applicant agrees to install 5' sidewalk connecting to Frenchmen's Place, which has been approved by ITD, as indicated on the Overall Site Plan (A-2.1).

Attachment K.



Attachment L:

Table 8. Summary and Analysis of New Plans, Studies and Information Received for the July 11, 2016, meeting

Staff Note: No new analysis for the October 10, 2016 meeting is contained in this table.

Information Requested by	Submittal from	Analysis
Commission	applicant	
1. Produce a pedestrian study. a. Address the locations of all proposed crosswalks. b. Address the rapid flashing beacon. c. Address whether a different/additional location for a	"Motor Vehicle Fueling Station Pedestrian Analysis", dated June 29, 2016, Alta Planning + Design	Three major pedestrian catchment areas were defined to be associated with the site: an eastern catchment area, a southwestern catchment area, and a northwestern catchment area. Major pedestrian routes were determined to be 10 th Street and Main Street with the major crossings identified as Main Street at 10 th Street and Main Street at 9 th Street. Recommendations to accommodate pedestrian traffic were given for specific locations; some recommendations aligned with public improvements already proposed by the City and other recommendations were new or were alternatives to recommendations proposed by the City.
crosswalk may be better or feasible (across Main Street at Frenchman's, for example).		In the eastern catchment area the study recommends a 5' wide sidewalk connection from the site to Frenchman's Place and the rapid flashing beacon, ramps, and crosswalk across Main Street near 9 th Street. These improvements align with recommendations previously made by the City that the applicant has agreed to and are indicated in the site and civil plans that have been submitted.
		Southwestern Catchment Area The southwestern catchment area includes Hemingway Elementary School. A pedestrian route identified to the site is the northern side of 10 th Street; the northern side is identified as being more desirable due to the existing sidewalk, which contains only a small gap between Warm Springs Road and Main Street. Among the study recommendations are defining a pedestrian zone through the gap in the sidewalk on the northern side of 10 th Street. This will require further analysis to determine if this recommendation is feasible given the right of way and current conditions. Options to define the pedestrian zone may include changing the pavement material or color in order to increase visibility of the pedestrian zone. At this time, more analysis is necessary to determine feasibility.
		The study reaffirms the recommendation of a crosswalk at

the intersection of Main Street and 10th Street that spans 10th Street, which the applicant has agreed to and indicates on the site and civil plans. The planning and public works departments concur with the recommendation to install an additional crosswalk spanning Main Street at the Main Street and 10th Street intersection.

The study recommends crosswalks at the Warm Springs

The study recommends crosswalks at the Warm Springs Road and 10th Street intersection as well as consideration of a rectangular rapid flashing beacon at the intersection; crosswalks at this location already exist. The feasibility and installation of a flashing beacon requires further study and pedestrian counts to determine if pedestrian use warrants this type of device. At this time, staff cannot recommend installation of the beacon without future study.

Northwest Catchment Area

The recommendations in the Pedestrian Analysis for the southwest catchment area cover the northwest catchment area as well. Recommendations for the northwestern catchment are the same as the recommendations for the southwestern catchment area.

Motor Fueling Station

Of the recommendations for the Motor Fueling Station, the Public Works Department agrees with the recommendation to install materials to differentiate the pedestrian zone and to install the crosswalk crossing Main Street at the intersection of Main Street and 10th Street already described in the southeastern catchment area improvements section. Due to the boulevard approach being ITD's standard, the rolled curbs described in the study are not recommended, and due to the proposed sidewalk and parking improvements on 10th Street being the city's standards, the landscaped area and reduced travel lane on 10th Street are not recommended. Finally, the applicant has proposed a slight realignment of the crosswalk crossing Main Street at the southern end of the site rather than moving the crosswalk further south; the Public Works Department recommends the realignment of the crosswalk as indicated on A.2 – Site Plan.

2. Obtain traffic counts at 10th Street/Main Street intersection in order to corroborate the 2008 data in the traffic study already conducted. If the traffic engineer wants to make the case that the need for new data is superfluous, and submits a narrative

"Ketchum – Bracken Station TIS, Additional Information" memorandum, dated July 6, 2016 Peak hour turning movement counts were collected on June 29, 2016; when compared to the data from February 2008, which was adjusted 30% to reflect peak seasonal conditions and was adjusted at a 1.1% growth rate per year, the estimated counts were 5% higher than the volumes collected on June 29, 2016.

explaining why, that would be acceptable. However, the request for current data at the 10th Street/Main Street intersection is driven by public comment and providing this data also serves the purpose of addressing public concern, so obtaining the new counts is recommended. On Saturday, July 2nd, 2016 between 10 a.m. and 5 p.m. and 3. Address the projected "Ketchum on Sunday, July 3rd between 12 p.m. and 5 p.m. Roy Bracken makeup of vehicles that will Bracken analyzed vehicle types patronizing the Shell gas station be using the gas station. Station TIS, a. What percentage Additional located at 211 Lewis Street in the LI-1 zone and reported the Information" findings to Hales Engineering. The memo from Hales will be oversized Engineering reports that during those time periods 7% of memorandum, vehicles (RVs, dated July 6, vehicles observed were large vehicles (i.e. trucks pulling construction 2016 trailers or recreational vehicles) and 93% were passenger trailers, et cars and pickup trucks. As such, Hales Engineering cetera)? determined it was not necessary to modify their i) Address how assumptions of 20' of length per vehicle queuing in the the proposed turn lane. proportion of oversized The memo reiterates that the traffic study found that with vehicles future (2020) conditions plus traffic conditions generated by the project the 95th percentile queue at the intersection impacts the would extend approximately 105' and that the proposed amount of turn lane is more than adequate to accommodate gueues of vehicles that such length. can queue in the turn lane. The memo states that it is unlikely that the left-turn queue b. Address would overflow into the thru-traffic lane but in such cases, potential backevents would likely have minimal short-term impacts on thru up of traffic. Further, delays for northbound left-turning vehicles northbound at the gas station access and for vehicles at 10th Street are traffic lining up anticipated to be short and that when delays are short queues dissipate quickly. to make a left turn into the gas station and the implications of exceeding the length of the turn lane (e.g. traffic backed up further south than the turn lane extends). 4. Obtain the Idaho Letter from The letter from Dave Jenson of ITD confirms that the ITD

Transportation Department (ITD)'s approval for the Frenchman's Place connector sidewalk. 5. Address the potential for northbound (left) and southbound (right) turn lanes on 10 th Street to facilitate left and right turns onto Main Street.	Dave Jensen, ITD District 4 Permit Coordinator, dated June 27, 2016 "Ketchum – Bracken Station TIS, Additional Information" memorandum, dated July 6, 2016	permit committee has approved the design of the sidewalk proposed to connect Bracken Station to the Frenchmen's Place development. The memorandum states, "A separate right-turn lane is not recommended at this location. Turning movement wheel path analyses show that with the current approach geometry, larger vehicles are able to execute right-turn movements with minimal encroachments into opposing traffic lanes. It is likely that the addition of a separate right-turn lane would constrain the right-turn movement such as to require significant encroachment into opposing traffic lanes. The traffic impact study found that delays at this intersection are anticipated to be relatively low, and therefore a separate right turn lane would not provide
6. Provide information addressing fuel spillage onto snow and snow removal from the site; what are the implications and how will	No exhibit submitted.	significant benefit." Applicant will address this issue during the hearing.
they be mitigated? 7. Provide site circulation/turning radii information for vehicles of various sizes within the site.	On-Site Vehicle Turn Exhibit, dated July 11, 2016 and On- Site Vehicle Turn Exhibit- Circulation, dated July 11, 2016	The exhibit depicts turning radii on the site for two vehicles: a 30' length single unit truck and a 48.7' camper trailer connected to a passenger car. The exhibit depicts the circulation of each vehicle entering the site, navigating around the fueling island canopy, and exiting the site. The first On-Site Vehicle Turn Exhibit illustrates unimpeded circulation of each of the two vehicle types when no other vehicles are present on the site and does not adequately prove turn movements can be made in real world conditions. The revised On-Site Vehicle Turn Exhibit-Circulation does not adequately indicate that the fueling station will not cause
8. Provide a section drawing	Site Profile,	congestion on Main Street/HWY 75. It appears that northbound trucks with trailers or box trucks would not be able to maneuver the site when other vehicles are positioned at the fueling islands. Submission of additional figures or modeling to show that cars and trucks will not end up queuing or backing up on Main Street/HWY 75 during peak times would be necessary to adequately address concerns regarding queuing. The site profile illustrates the grade change between the
showing Bracken Station site, canopy, and the Tenth Street Light Industrial	dated July 11, 2016	Tenth Street Light Industrial Development (491 E. 10 th Street) and the proposed Bracken Station property (911 N. Main) at the 10 th Street and Main Street intersection. The

development.		height of the proposed gas station canopy and the
		landscaping proposed to buffer the canopy are shown.
		This cross section was requested so that the height of the
		proposed canopy and the canopy lighting could be evaluated
		with respect to the lower grade of 491 E. 10 th Street. The site profile, in conjunction with the new LS 1.1 plan illustrating
		additional landscaping and the revised L 1.0 plan, indicates
		that the majority of the 50' length of the canopy facing 10 th
		Street will be screened and buffered by 9 Spartan Juniper
		trees that are 10' at the time of planting, 1 Lodge Pole Pine that is 14' at time of planting and 1 Lodge Pole Pine that is
		16' at time of planting.
Information Requested by	Submittal from	Analysis
Staff	applicant C.2.1	The Dublic Works Department has reviewed this also and
1. Provide a conceptual drainage plan that indicates	Preliminary	The Public Works Department has reviewed this plan and finds it acceptable. The drainage plans include a proposed
the site has the capacity to	Drainage	oil/water separator at the southernmost corner of the site
retain all storm water.	Exhibit, dated	that the on-site drywell and catch basins drain to. However,
retuin an storm water.	June 3, 2016	prior to issuance of a building permit a seepage test will
		need to be conducted and clarification regarding the infiltration rate and storm intensity and number of dry wells
		will be required.
2. Indicate Frenchman's	A.2.1 Overall	Both plans indicate a new sidewalk connecting the proposed
connector sidewalk on site	Site Plan, dated	Bracken Station property to the Frenchmen's Place
plan and landscape and civil plans to the same level of	June 30, 2016 and C.2.4	development to the south. The sidewalk is indicated on C.2.4 to be 5' in width. The Public Works Department finds the 5'
detail as the already	Preliminary	width to be acceptable.
proposed sidewalks have	Frenchman's	·
been shown on those plans.	Sidewalk	
	Exhibit, dated July 11, 2016	
3. Provide photometric data	Photometric	The applicant submitted a Radiosity exhibit that illustrates
for proposed site lighting,	Lighting	illuminance from canopy lighting at night.
including canopy.	Proposal (black	
	and white), Photometric	The Photometric plans indicate foot-candles calculated at a grid of points overlaid on the site plan. The Photometric
	Lighting	plans indicate a range of 0.0 to 0.9 foot-candles measured
	Proposal	on the northern and western property lines and a range of
	(color), dated	0.0 to 4.9 foot-candles along the eastern property line,
	June 30, 2016,	nearest the canopy. The average foot-candles under the
	Radiosity exhibit dated	canopy are calculated to be 28.51, with the minimum measurement calculated at 11.3 and the maximum at 41.3.
	June 30, 2016	measurement calculated at 11.5 and the maximum at 41.5.
	,	Ketchum code 17.132.020 J. states that the average foot-
		candle lighting level for new and existing service stations
		shall be no greater than 30 foot-candles, as set by the IESNA standards for urban service stations.
4. Provide a copy of Idaho	Idaho	The applicant submitted IDAPA 58.01.07, "Rules Regulating
Department of	Department of	Underground Storage Tank Systems". The rules establish
Environmental Quality	Environmental	standards and procedures necessary for the regulation of

Quality's (DEQ) /and Environmental underground storage tank systems and the rules state Protection Agency (EPA) "Rules compliance with IDAPA 58.01.07 shall not relieve persons regulations for gas stations. Regulating from the obligation to comply with other applicable state or Underground federal laws. Storage Tank Systems", IDAPA 58.01.07 contains rules for protecting ground water **IDAPA** from contamination, rules for reporting when an 58.01.07, underground storage tank releases (spills) petroleum, submitted June requirements for training of primary and daily on-site 20, 2016, operators, and information on inspections and penalties for violations. Seismic Behavior of Xerxes The memorandum addresses seismic activity occurring at a Underground distance away from the tank, which the tanks can withstand, Tanks and seismic activity occurring at or very near the location of memorandum, the tank, which would cause the tank to rupture just as the Xerxes ground ruptures at and near the location of seismic activity. **Fiberglass** Underground The brochure addresses construction and safety features of **Storage Tanks** Xerxes double-wall underground storage tanks. brochure

Attachment M.

Table 9: Uses in the LI-1 Zone

Table 7: Uses in the LI-1 Zone

"P" = PERMITTED "C" CONDITIONAL "A" = ACCESSORY

r :	= PERMITTED "C" CONDITIONAL "A" =	ACCESSORY
	USES	LI-1
	Dwelling, Multi-family	C ¹⁴
RES	Dwelling, One-Family	
	Residential Care Facility	
	Agriculture, Commercial Adult Only Business	
	Business Support Service	Р
	Convenience Store	P ¹²
	Daycare Center	C ¹⁷
	Daycare Facility	C ¹⁷
	Drive-Through Facility	
	Equestrian Facility	
	Food Service	PC ¹⁵
	Golf Course	, 0
	Grocery Store	
	Health and Fitness Facility	С
	Hotel	
		D
	Hybrid Production Facility Instructional Service	P P
	Kennel, Boarding	P
	Laundry, Industrial	P
	Lodging Establishment	Г
	Maintenance Service Facility	Р
	Manufacturing	P
٩L	Mortuary	·
COMMERCIAI	Motor Vehicle Fueling Station	С
ME	Motor Vehicle Sales	С
N	Motor Vehicle Service	Р
ŏ	Office, Business	
	Outdoor Entertainment	
	Personal Service	P ¹³
	Professional Service	Р
	Recreation Facility, Commercial	
	Repair Shop	Р
	Retail Trade	P ¹²
	Self-Service Storage Facility	Р
	Ski Facility	
	Storage Yard	Р
	Studio, Commercial	Р
	Tourist House	
	Tourist Housing Accommodation	
	Truck Terminal	Р
	TV and Radio Broadcasting Station	Р
	Veterinary Service Establishment	Р
	Warehouse	Р
	Wholesale	Р
	Wireless Communication Facility	C ²³

	Assembly, Place of					
	Cemetery					
	Cultural Facility					
_	Geothermal Utility					
¥	Hospital					
2	Medical Care Facility					
₽	Nature Preserve	Р				
PUBLIC & INSTITUTIONAL	Parking Facility, Off-Site					
2	Parking, Shared					
<u>2</u>	Performing Arts Production					
JBL	Public Use	С				
곱	Public Utility	Р				
	Recreation Facility, Public	Р				
	Recycling Center					
	Semi-Public Use					
	Agriculture, Urban	A ²²				
	Daycare Home	C⁴				
	Daycare, Onsite Employees	Α				
	Dwelling Unit, Accessory					
ВY	Energy System, Solar	Α				
ACCESSORY	Energy System, Wind	Α				
ä	Fallout Shelter					
¥	Guesthouse					
	Home Occupation	Α				
	Recreation Facility, Residential	Α				
	Equestrian Facility, Residential					
	Sawmill, Temporary					

- 1. A multi-family development containing up to two (2) dwelling units is permitted.
- 2. Two (2) one-family dwellings are permitted.
- 3. Religious institutions are allowed through the provision of a conditional use permit. No other assembly uses as defined in Chapter 17.08 are permitted.
- 4. Use is not permitted in the Avalanche Zone. Reference Zoning Map.
- 5. Retail trade is permitted but must not exceed 2,500 square feet.
- 6. Uses must be subordinate to and operated within tourist housing and not to exceed ten percent (10%) of the gross floor area of the tourist housing facility.
- 7. Utility for offsite use.
- 8. See section 17.125.070 for shared parking standards.
- 9. Drive-throughs are not allowed in association with food service establishments.
- 10. This is a permitted use, however offices and professional services on the ground floor with street frontage require a conditional use permit.
- 11. Tourist houses shall only be located in existing one-family dwellings. Additions to the home shall not exceed 20 percent (20%) of the existing square footage.
- 12. The following forms of retail trade are permitted: (a) Equipment rental, including sporting equipment and entertainment equipment, (b) Building, construction and landscaping materials; small engines with associated sales (c) Retail in conjunction with manufacturing, warehousing or wholesaling not to exceed 30% gross floor area or 800 square feet, whichever is less; no advertising is displayed from windows or building facades; and no access onto a major arterial is allowed if an alternative access is available.
- 13. Personal service is not allowed except for laundromats and dry cleaning establishments.
- 14. See section 17.124.090 of this title for industrial districts residential development standards.
- 15. Catering and food preparation is permitted. Restaurants require a conditional use permit and shall not exceed 1,000 square feet and serve no later than 9:00 P.M. unless expressly permitted through approval of the conditional use permit.
- 16. The following forms of retail trade are permitted: (a) Equipment rental, including sporting equipment and entertainment equipment (b) Building, construction and landscaping materials; small engines with associated sales (c) Furniture and appliances in conjunction with warehousing not to exceed 18% gross floor area or 900 square feet, whichever is less; (d) Other retail in conjunction with manufacturing, warehousing or wholesaling; it is limited to 10% gross floor area or 500 square feet, whichever is less. ---- Retail uses (c) & (d) shall have no advertising displayed from windows or building facades; and no access will be permitted onto a major arterial if an alternative access is available.
- 17. See section 17.124.120.C of this title for industrial districts daycare development standards.
- 18. See section 17.124.070 of this title for accessory dwelling unit development standards.
- 19. A maximum of five (5) dwelling units are allowed through a conditional use permit and shall be a minimum of 400 square feet and not exceed 1,200 square feet in size.
- 20. Indoor only.
- 21. Only allowed in conjunction with an equestrian facility.
- 22. See section 17.124.080 of this title for urban agriculture development standards.
- 23. See chapter 17.140 for wireless communications facility provisions.
- 24. Allowed on the ground floor only.
- 25. See section 17.124.050 of this title for hotel development standards.
- 26. Ground floor street frontage uses are limited to retail and/or office uses. In subdistrict A office uses require a conditional use permit.
- 27. Ground floor only.
- 28. Through the provision of a conditional use permit, the planning and zoning commission may approve a 20% increase to the total existing square footage of an existing nonconforming one-family dwelling.
- 29. Use is allowed as an accessory use through the provision of a conditional use permit.

Attachment N.

Table 10: Dimensional Standards for the LI-1 Zone

Table 8: Dimensional Standards for the LI-1 Zone

Dimensional Standards for LI-1

Minimum Lot Area	Minimum Lot Area w/ PUD. See Title 16.	Minimum Lot Area, Townhouse Sublot	Lot Width	Building Height	Maximum Building Coverage	Minimum Open Space	Front Setback	Side setback	Rear Setback	Lot Lines created by Townhouse Sublots	Setbacks from Hwy 75	Any setback along Warm Springs Road	Setbacks along 200' Former Railroad ROW
8,000 SF	N/A	N/A	80' Min.	35'	75%	N/A	20'	Zero (0') ¹ for internal side yards and a min. of 10' for street side yards.	Zero (0') ¹	N/A	N/A	N/A	N/A

^{1.} If the lot adjoins a more restrictive district on the side or rear, the more restrictive setbacks of that district shall apply.

Attachment O.

Table 11: Potential Build Out for 911 N. Main Street

Lot size	18,590 square feet			
Maximum coverage in LI- 1 zone	75%			
Potential Lot Coverage	13, 942 square feet			
Proposed Project, Lot Coverage	23%			
Proposed Project, Lot Coverage Square Footage	2,592 square feet ground floor building; 1,720 square foot canopy. Total lot coverage 4,312 square feet.			

Maximum building height in LI-1 zone	35'
Proposed building height, Main Street grade	13'-8"
Proposed building height, 10th Street grade	24'-8"
Proposed canopy height, Main Street grade	18'
Proposed canopy height, 10th Street grade	20' at eastern edge of canopy and 24' at western edge of canopy

RECEIVED

APR 29 2016

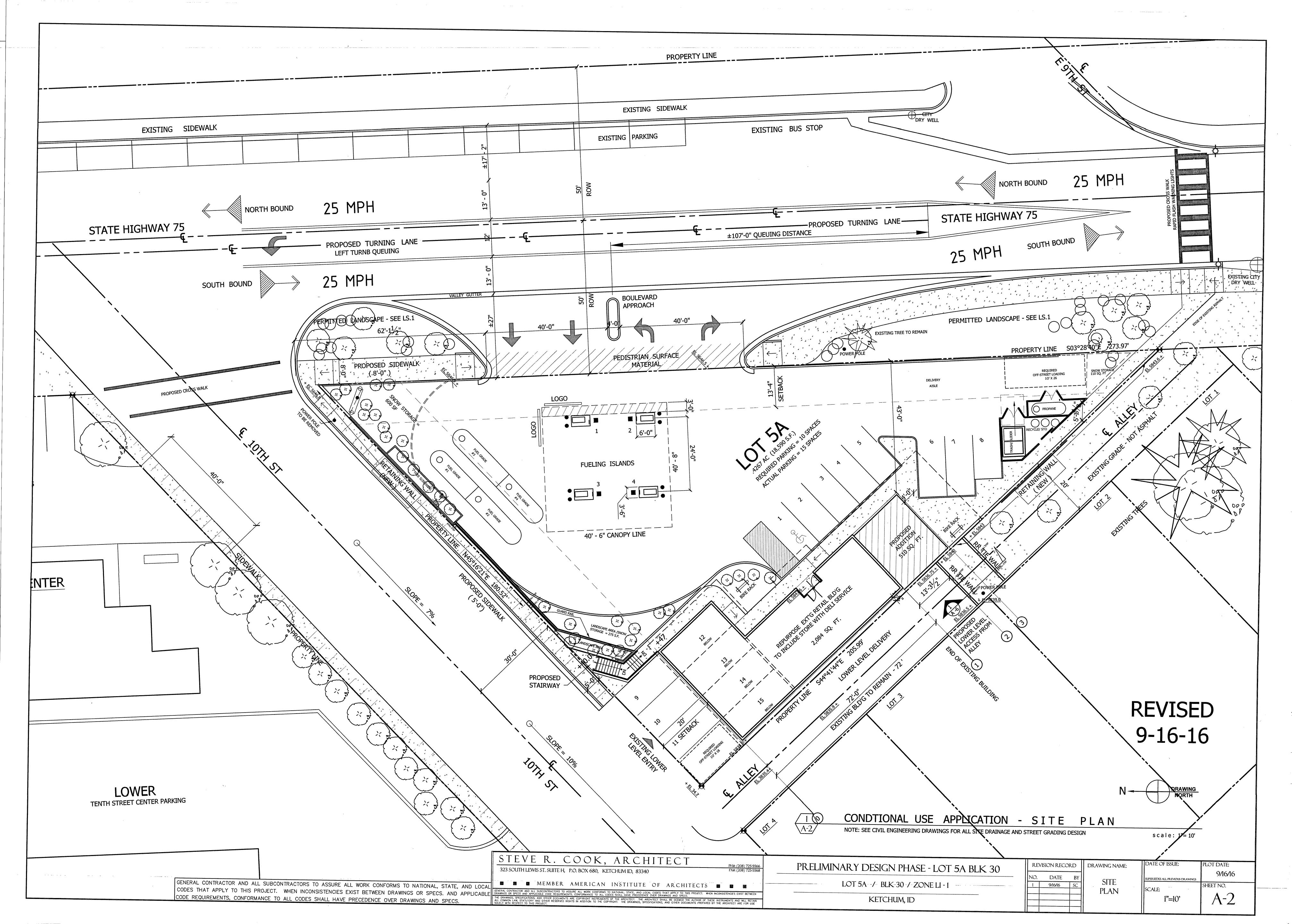
CITY OF KETCHUM

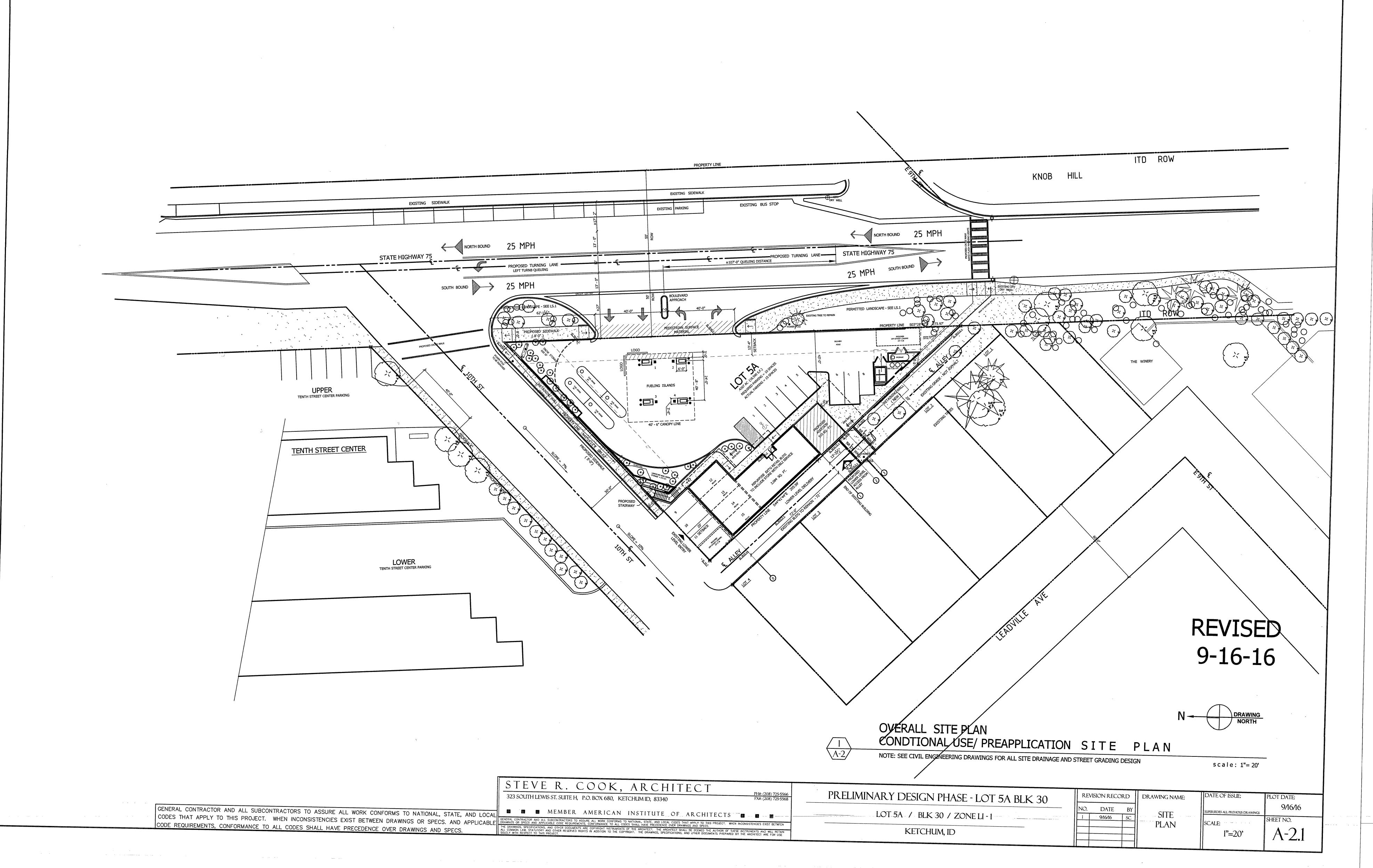
File No.: 16-634
Reference Design Review
ON 16-035

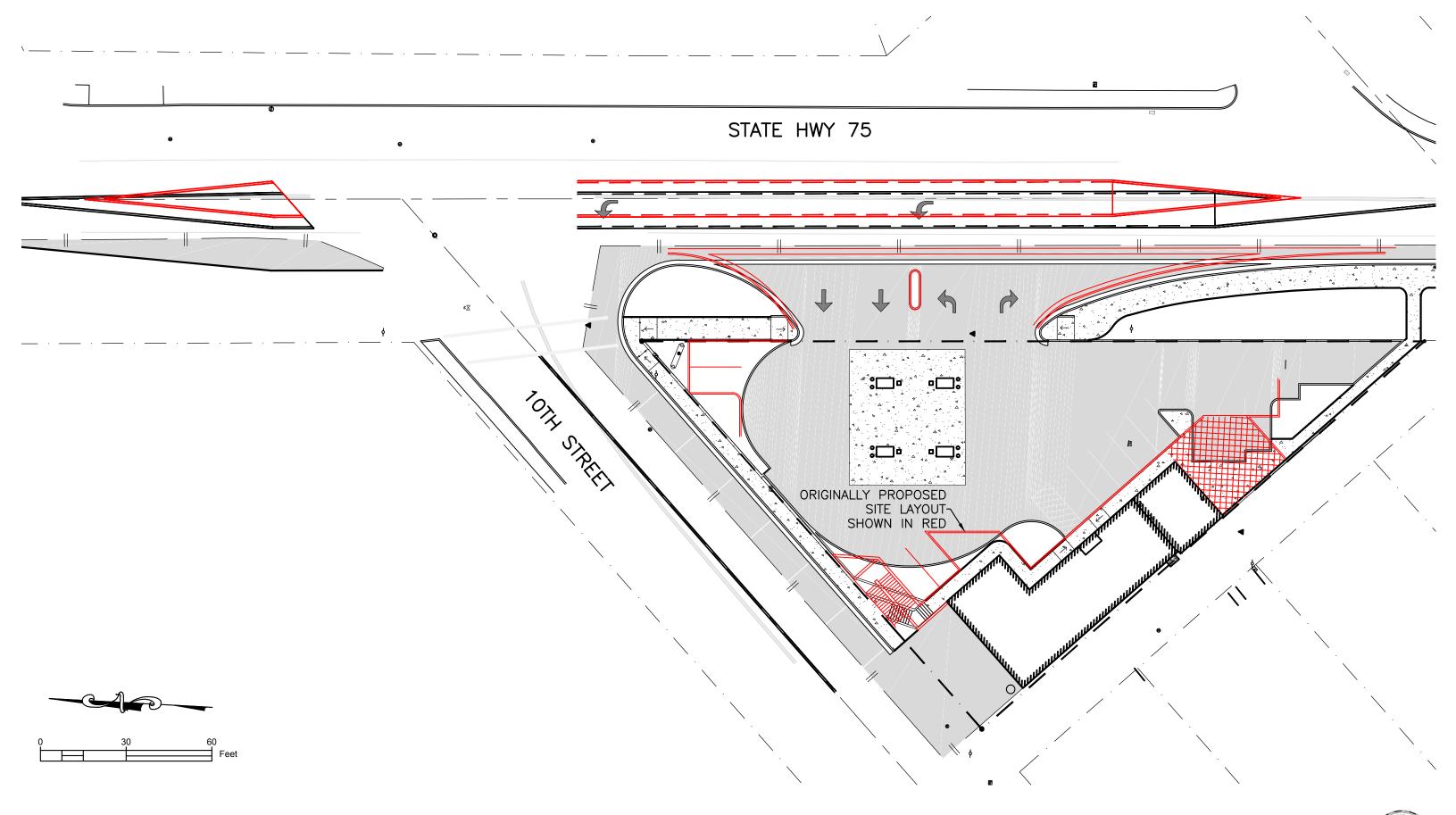
CITY OF KETCHUM CONDITIONAL USE PERMIT APPLICATION

Name of Applicant: RRJ-LLC
Name of Owner of Record: NORTH TOWN PARTNERS LLC P.O.BOY 5277
Mailing Address: P. 6. 1807 5277 KETCHUM, ID. 83340
Contact Phone Number: 208.721.0080
Street Address of Property Requiring a CUP: 911 N MAIN STREET
Legal Description of Property Requiring a CUP: FETCHUM AMENDED LOT GA BLK-30
RPK0000030005A
Description of Proposed Conditional Use: MOTOR VEHICLE FUELING STATION AND RETAIL STORE
PROPOSED: FECESSED SOFFIT DO DESCRIPTION OF Proposed and Existing Exterior Lighting: <u>UGHTS IN EMSTING ROOF</u> OVERHANG/NEW LED, FLUSH MOUNT 16"X16" SQ FIXTURES IN CANOP 1 - 6 TO S IN NUMBER - SOFT WARM COLOR AS PER (KELVIN TEMP.)
Zoning District: LI.
Overlay District: Flood Avalanche Pedestrian Mountain
The Applicant agrees in the event of a dispute concerning the interpretation or enforcement of the Conditional Use Permit Application in which the City of Ketchum is the prevailing party to pay reasonable attorney fees, including attorney fees on appeal, and expenses of the City of Ketchum. I hereby acknowledge I have filled in this application accurately and provided the required information to the best of by knowledge.
Jog KACJES Date 4-28-16
Applicant's Signature

Pursuant to Resolution No. 08-123, any direct costs incurred by the City of Ketchum to review this application will be the responsibility of the applicant. Costs include but are not limited to: engineer review, attorney review, legal noticing, and copying costs associated with the application. The City will require a retainer to be paid by the applicant at the time of application submittal to cover said costs. Following a decision or other closure of an application, the applicant will either be reimbursed for unexpended funds or billed for additional costs incurred by the City.







SITE CHANGES

SEPTEMBER 16TH, 2016



A DBA OF KELLERSTRASS ENTERPRISES INC.

Corporate Office P.O. Box 1067 • 1500 West 2550 South • Ogden, Utah 84402 (801) 392-9516 • Fax (801) 392-9589

> September 23, 2016 Date

To The City Of Ketchum,

Kellerstrass Oil Company has been approved by Chevron to supply the proposed Bracken Chevron Station to be located in Ketchum, Idaho with quality Chevron Fuels. As a Chevron distributor we currently supply over 75 stations in Utah, Wyoming, Montana, Nevada and Idaho. Many of these stations are similar in site layout and traffic follow as this proposed station. As the Director of Operations for Kellerstrass Oil I can assure you that with our Transportation Department running 24/7, 365 days a year, all fuel delivers to this proposed station would be done during non-high traffic times. This is a customary practice in our industry and is required for safe and efficient fuel drops. These scheduled fuel deliveries would not include Holidays as well. We will work with Roy Bracken to make sure our fuel delivers are planned well in advance so that his staff is prepared and that our drivers can make quick and safe deliveries as to not interfere with customers and traffic flow on site and with the highway traffic. Any questions please feel free to call me.

Best Regards,

Kellerstrass Oil Company

Dallas Green

Director of Operations

Cell- (801) 648-4969

Office- (801) 392-9516 ext. 42

Email- DGreen@Kellerstrassoil.com

P.O. Box 460



A DBA OF KELLERSTRASS ENTERPRISES INC.

Corporate Office P.O. Box 1067 • 1500 West 2550 South • Ogden, Utah 84402 (801) 392-9516 • Fax (801) 392-9589

Date October 1, 2016

To The City Of Ketchum,

As the Director of Operations for Kellerstrass Oil Company I have reviewed the exhibits showing a truck and trailer making a fuel delivery at the proposed Bracken Chevron C-store to be located in Ketchum, Idaho. It is my opinion that this exhibit is very accurate and that our delivery trucks with this same combination of truck and trailer can make deliveries to the fuel tanks shown. These combinations of truck and trailer have two pivot points one at the pintle hitch and one at the turn table located under the front of the pull trailer. They are designed to be able to maneuver in tight places and the trailer will track behind the truck with only a 3.5 foot off-track. These combinations are also very easy to back up to place the unit over fuel tanks located away from fuel dispensers and in such places as the exhibit shows. I would be available to answer any questions the commission might have.

Best Regards,

Kellerstrass Oil Company

Dallas Green

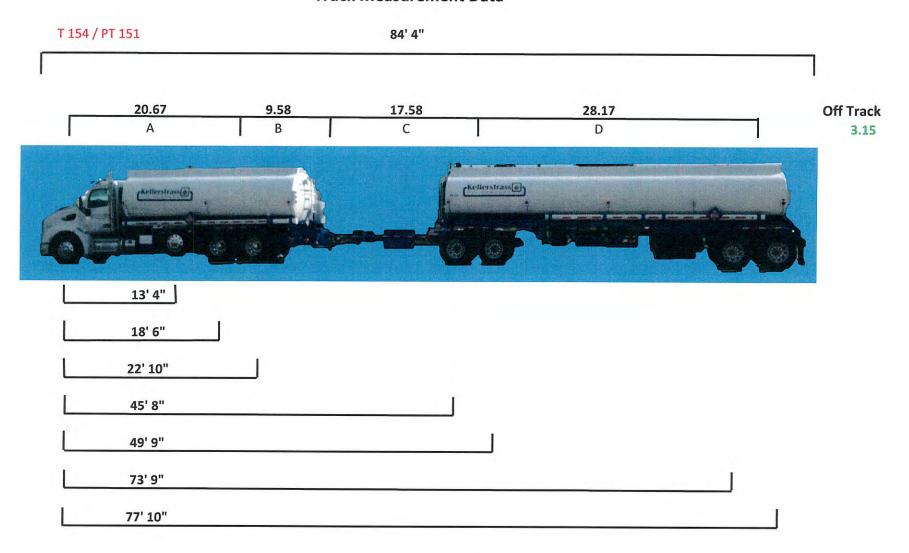
Director of Operations

Cell- (801) 648-4969

Office- (801) 392-9516 ext. 42

Email- DGreen@Kellerstrassoil.com

Truck Measurement Data



NED C. WILLIAMSON

ATTORNEY AT LAW 115 SECOND AVENUE SOUTH HAILEY, IDAHO 83333 (208) 788-6688 FAX (208) 788-7901

July 22, 2016

Ketchum Planning and Zoning Commission City of Ketchum P.O. Box 2315 Ketchum, ID 83340-2315

Re: Bracken CUP Application

Dear Commission Members:

As you know, I am the attorney for Roy Bracken, the applicant for a conditional use permit for a motor vehicle fueling station and food service establishment. This letter is intended to provide rebuttal or clarification to some of the public comment and to portions of the staff report.

A. Standards

We have consistently urged the Commission to approve the CUP application because we felt the evidence showed the applicant has met all of the five applicable criteria set forth in § 17.116.030 of the Ketchum Municipal Code. At the first hearing on June 13, 2016, I pointed out that the CUP staff report showed that the application met four out of five criteria and that the CUP staff report stated there was non-compliance with the standard requiring compliance with the comprehensive plan. At the first hearing, based on *Urrutia v. Blaine County*, 134 Idaho 353, 2 P.3d 738 (2000) and several other appellate cases, I argued that it was inappropriate to deny this application based on an alleged incompatibility with the Comprehensive Plan. At the last hearing on July 11, 2016, I pointed out that the staff report was modified just before the meeting to show noncompliance with four out of five criteria. Staff argued that their recommendations were modified after receiving the applicant's information. I would like to respond to each revised recommendation by staff.

1. Compatibility of Uses. Standard §17.116.030(A) provides that "[t]he characteristics of the conditional use will not be unreasonably incompatible with the types of uses permitted in the applicable zoning district." In the first staff report, staff noted that a motor vehicle fueling station and a food service establishment were compatible with the uses permitted in the LI-1 zoning district. In the recently revised staff report, staff then relied on a needs analysis by Gmap USA which merely stated the obvious--the site will be served by traffic from the general public and tourists. From that observation, staff then concluded the proposed uses are incompatible with the purpose section of the LI-1 zoning district. The purpose section of the LI-1 zoning district states:

A. Purpose: The LI-1 light industrial district number 1 is established as a transition area providing limited commercial service industries, limited retail, small light manufacturing, research and development, and offices related to building, maintenance and construction and which generate little traffic from tourists and the general public.

Ketchum Municipal Code §17.18.140(A).

Staff's interpretation will gut the allowed uses in this LI-1 zoning district. The LI-1 zoning district allows, as either permitted or conditional uses, many other uses which generate heavy traffic from the general public and/or tourists, such as convenience stores, day care facilities, health and fitness facilities, instructional services, outdoor entertainment, repair shop, retail trade, commercial studios, truck terminals, veterinary service establishments, public use and public recreational facility. *Ketchum Municipal Code § 17.12.020*. If the Commission follows the suggestion of staff, then the purpose section of the LI-1 zoning district will trump the numerous enumerated conditional and permitted uses allowed in the LI-1 zoning district.

A court construes a local ordinance as it construes a statute. Friends of Farm to Market v. Valley County, 137 Idaho 192, 196, 46 P.3d 9, 13 (2002). Statutory construction always begins with the literal language of the statute or ordinance. Id. at 197, 46 P.3d at 14. If an ordinance is unambiguous, a court need not consider rules of statutory construction and the statute will be given its plain meaning. Hamilton ex rel. Hamilton v. Reeder Flying Serv., 135 Idaho 568, 572, 21 P.3d 890, 894 (2001); Canal/Norcrest/Columbus Action Comm. v. City of Boise, 136 Idaho 666, 670, 39 P.3d 606, 610 (2001). Where the language of a statute is ambiguous, a court applies rules of construction for guidance. Friends of Farm to Market, 137 Idaho at 197, 46 P.3d at 14. Courts disfavor constructions that lead to absurd or unreasonably harsh results. Id. All sections of the applicable statute must be construed together to determine the legislative body's intent. Id. (citing Lockhart v. Dept. of Fish and Game, 121 Idaho 894, 897, 828 P.2d 1299, 1302 (1992)). Statutes and ordinances must be construed so as to give effect to all their provisions and not to render any part superfluous or insignificant. Id. (citing Brown v. Caldwell Sch. Dist. No. 132, 127 Idaho 112, 117, 898 P.2d 43, 48 (1995)). When laws conflict, a later or more specific law controls over a more general law. Johnson v. Boundary Sch. Dist. No. 101, 138 Idaho 331, 335, 63 P.3d 457, 461 (2003); Hyde v. Fisher, 143 Idaho 782, 786, 152 P.3d 653, 657 (Ct. App. 2007).

Based on these well-established rules of statutory construction, I believe the specific enumeration of a motor vehicle fueling station and a food service establishment is unambiguous, but if the Commission finds that the purpose section of the LI-1 zoning district creates an ambiguity, then courts require the specific enumeration of a motor vehicle fueling station and a food service establishment to control over the general purpose provision found in the LI-1 zoning district.

Ketchum Planning and Zoning Commission July 22, 2016 Page 3

Notably, the LI-1, LI-2 and LI-3 zoning districts all contain the same language in their respective purpose sections. The purpose sections of these light industrial zoning districts provide that the uses will "generate little traffic from tourists and the general public." *Ketchum Municipal Code* §§17.18.140(A), 17.18.150(A) and 17.18.160(A). If adopted, staff's interpretation would make the existing gas stations in Warm Springs non-conforming.

Moreover, staff's interpretation ignores the positive impact this application would have on eliminating traffic in the congested Lewis Street area. This application would promote the relocation of traffic from the core of the light industrial areas to the fringe of the light industrial zones along a state highway. In reality, this application is consistent with the purpose section of the light industrial districts by reducing traffic in the heart of the light industrial districts.

- 2. Health, Safety and Welfare. Standard §17.116.030(B) provides that "[t]he conditional use will not materially endanger the health, safety and welfare of the community." Again, the first staff report noted that the proposed uses would not materially endanger the public health, safety and welfare. The initial assessment was largely based on an evaluation by the Public Works and Fire Departments. The revised staff report now cites "concerns about on-site circulation and potential negative externalities" and concerns about gas spillage. During the last hearing, we presented testimony from qualified engineers who addressed traffic off- and on-site and petroleum experts about safety and spillage. Benchmark Engineering is still attempting to address any concerns that the Public Works Department raised in the last hearing about circulation and traffic flow. The applicant will construct a state-of-the-art facility which will be safer than any other gas station in Ketchum and will support traffic in and out of the facility better than any other station in Ketchum. When considering this standard, please remember that the non-conforming structures will be removed, that turn lanes will be constructed and that pedestrian safety will be addressed by the traffic crossings, beacons, sidewalks and pathway. This station will also provide the only electric charging station for cars in the city.
- 3. Pedestrian and Vehicular Traffic. Standard §17.116.030(C) provides that "[t]he conditional use is such that pedestrian and vehicular traffic associated with the use will not be hazardous or conflict with existing and anticipated traffic in the neighborhood." The initial staff report noted that the Hales traffic study was reviewed by the Public Works Department and that the applicant met this standard. Now, staff questions the same traffic study and the on-site vehicle turn and 10th Street turn exhibits. For on-site circulation, we have demonstrated that passenger cars, trucks and trailers can maneuver into, on and out of the site. As mentioned above, Benchmark is still working to address any lingering concerns about circulation. Again, we believe this design is safer than the existing site with non-conforming buildings and is superior to other gas stations in Ketchum. For southbound traffic exiting 10th Street, issues were raised about the turning radius. Please remember that ITD approved of the design which is in their right-of-way. Based on comments during the June 13 meeting, the design team modified the design of the improvements at the intersection of 10th Street and Highway 75 to help with the turning radius concerns. As established by Benchmark Engineering, the turning radius meets standards and is adequate for most traffic.

Ketchum Planning and Zoning Commission July 22, 2016 Page 4

Comprehensive Plan. Standard §17.116.030(E) provides that "ft]he conditional use is note in conflict with the policies of the Comprehensive Plan or the basic purposes of this Section." The staff reports have been consistent in stating that the application is noncompliant with this standard. In my capacity as the Hailey City Attorney, I have recommended this type of standard be deleted as a standard for zoning and subdivision permits. I know that many other city and county attorneys agree. Following Blaine County v. Urrutia, supra, the courts have repeatedly stated that the comprehensive plan is not controlling law, that a city or county cannot deny a use allowed by a zoning ordinance based on noncompliance with the comprehensive plan and that if there is a conflict between a comprehensive plan and a zoning ordinance, the zoning ordinance controls. Sanders Orchard v. Gem County, 137 Idaho 695, 700, 52 P.3d 840, 845 (2002). The alleged conflict is based on a statement in the comprehensive plan that "mixedindustrial" uses "should generate little traffic from tourists and the general public." Of course, this is very much like the language found in the purpose section of the LI-1 zoning district. See discussion supra $\delta A(1)$. But as stated above, the designation of a motor vehicle fueling station and a food service establishment in the LI-1 zoning district will control over the general statement found in the comprehensive plan. Sanders Orchard, supra.

In the discussion about the comprehensive plan, the staff report suggests that the Commission should decide whether a new fueling station and restaurant in the LI-1 zoning district is necessary in the LI-1 zoning district. In making this suggestion, staff did not cite to any particular provision of the comprehensive plan. Such a suggestion is not appropriate because the express standards of a conditional use do not address necessity and because it puts the Commission in the position of making an economic decision. I would contend that is not the role of a planning and zoning commission.

B. Wendland's Comments

Mr. Wendland states that the Hales traffic study is materially deficient. To support his contention, Mr. Wendland uses customer counts from his Warm Springs gas station. The documents contain redacted information which only shows the number of customers. Mr. Wendland states his site is 50% smaller than the proposed Bracken site but handles on the average of 1,003 daily customers, which is greater than the Hales projections. As pointed out by Scott Johnson of Hales Engineering, the traffic study used standard traffic counting techniques, not receipts. By way of example, a car with 4 passengers could purchase gas, with each occupant individually buying goods, resulting in five sales receipts. Of course, five receipts would not equal one vehicle trip. On June 13, 2016, Mr. Wendland stated that his Warm Springs station only pumped 600 gallons/day during the peak season. If every one of the 1,003 customers purchased gas, each vehicle would pump on average only .6 gallons. Obviously that cannot be the case. Please rely on industry standards, not an unprecedented date collecting technique. Mr Wendland also states the proposed development poses a "grave fire danger." I would recommend that you rely on your fire department who is more qualified to analyze safety concerns. The fire department has commented that it will have adequate access to the facility in an emergency.

C. Surveys

I previously expressed the problems with City initiated survey. In short, I believe the survey is illegal and only encourages a popularity contest. To comply with procedural due process, the Idaho Courts require the identification of a person who communicates with a decision maker. *See Idaho Historic Preservation Council v. Boise*, 134 Idaho 651, 655-56. 8 P.3d 646, 650-51 (2000) [failure to identify ex parte communications is a violation of procedural due process]. I believe the city-initiated survey is even worse than an improperly disclosed ex parte communication because the city actively solicited anonymous comments.

D. Gas Dispensing Options

At the conclusion of the last hearing, Commissioner Mizell asked whether the applicant would entertain the installation of vaporizer at the nozzle of the gas dispensers. I was informed by Leonard Petroleum that such a system was not as effective as a no drip nozzle, was not commonly installed and was not required by the regulating authorities. Leonard Petroleum suggested and the applicant agrees to install a no drip nozzle on the dispensers. Apparently, the no drip nozzle is very effective at eliminating the spillage of fuel.

E. Nature of Comments

During the hearings, I have been discouraged by the lack of civility by the participants. In contrast, the Commission has been courteous, civil and professional during these contentious hearings. Unfortunately, certain members of the public have taken potshots at the applicant and his representatives. The lack of civility does not aid anyone in the process. Some have questioned the applicant's integrity but have utterly failed to produce independent studies showing that the applicant's studies were flawed. In contrast, I believe the applicant has added objectivity to the process by providing expert testimony, which came at no small expense to the applicant. Roy's goal in this process has been to address the standards and legitimate questions raised by the Commission and public.

F. Conclusion

The applicant is proposing a small to moderate sized facility which is appropriate in scale to this community. This application will better serve the general community and tourists by providing an accessible and state of the art gas station. The city has previously planned this property by zoning it light industrial and allowing the proposed uses in this district. The facility will be conforming and will replace non-conforming structures. The evidence in this case shows substantial compliance with the applicable standards. Evidence has been submitted which allows the Commission to approve the application.

Thank you for your consideration.

Ketchum Planning and Zoning Commission July 22, 2016 Page 6

Sincerely,

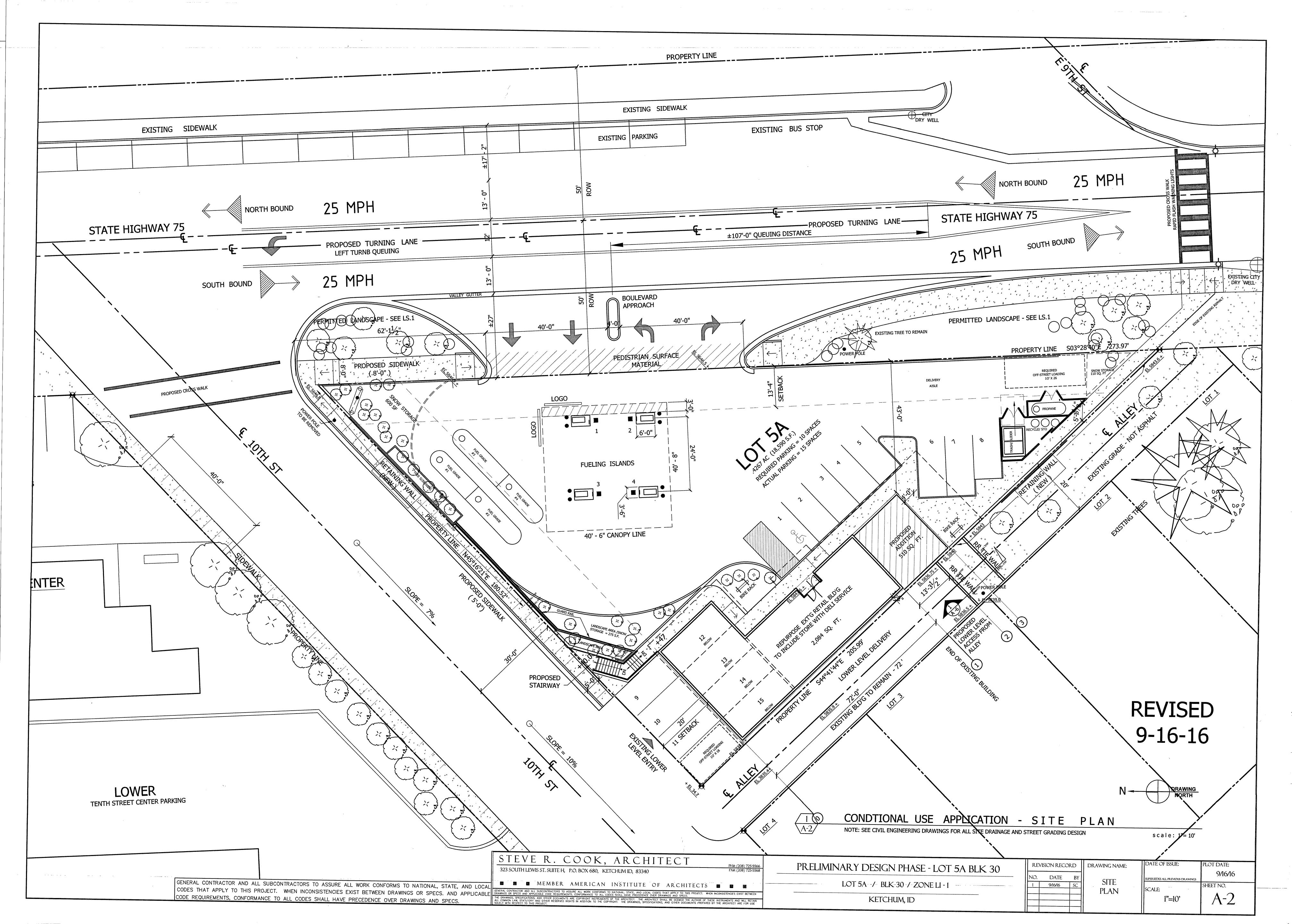
Ned C. Williamson

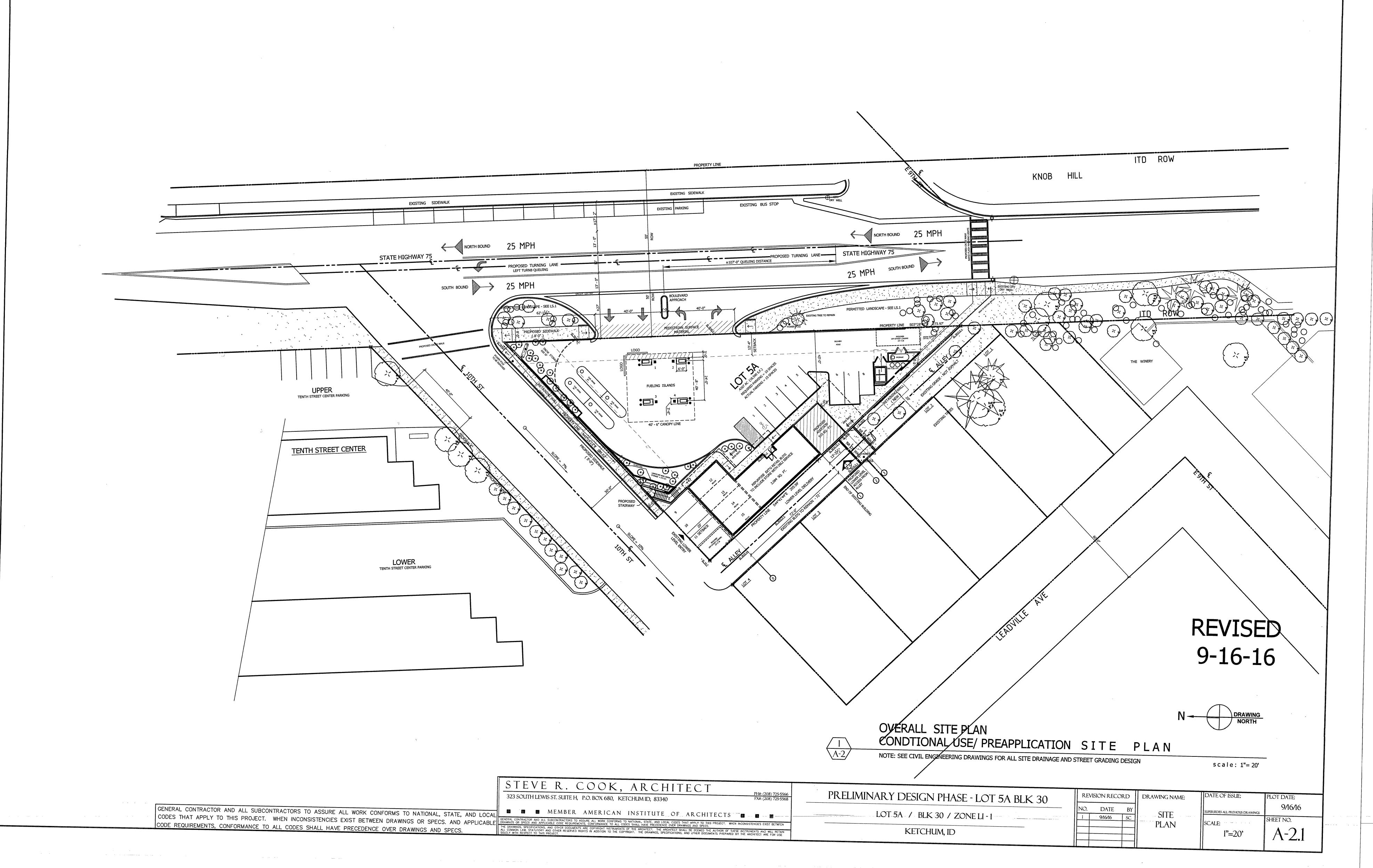
NCW/jrs

cc:

Roy Bracken

Stephanie Bonney

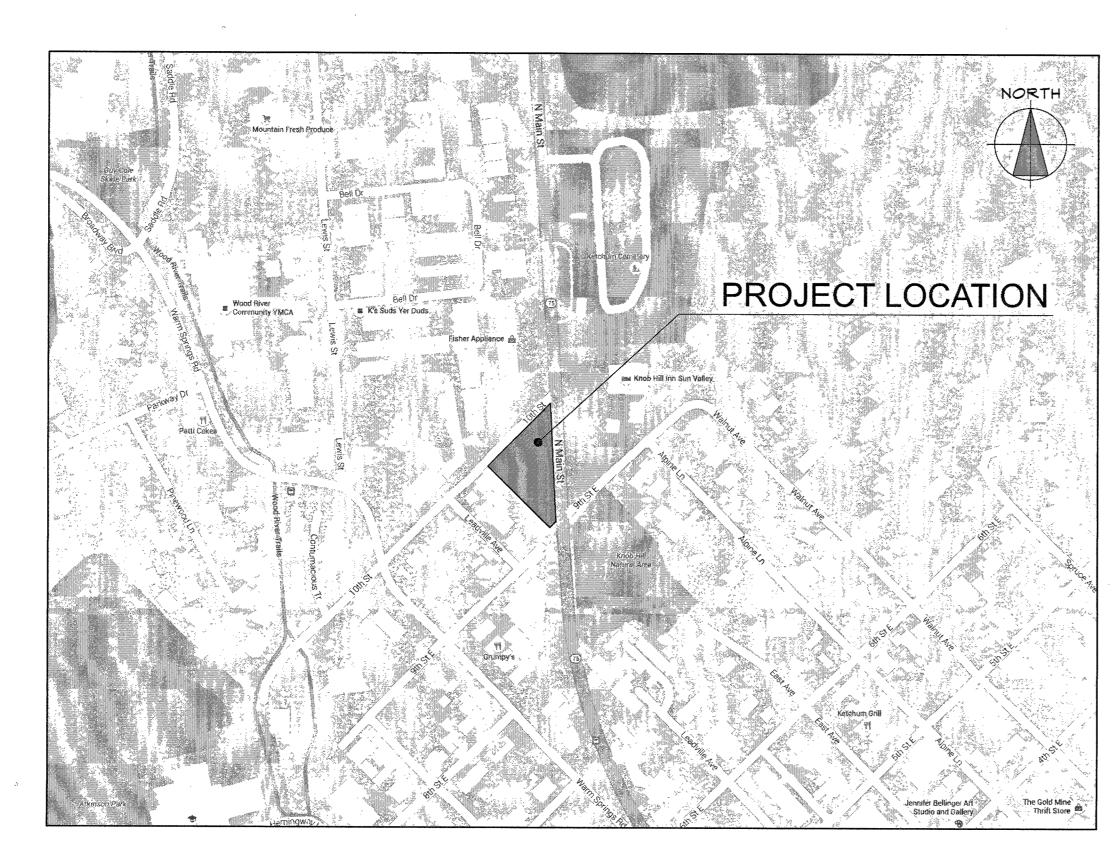




BRACKEN STATION

A CONDITIONAL USE PERMIT / DESIGN REVIEW APPLICATION FOR:

A MOTOR VEHICLE FUELING STATION LOT 5A / BLK 30 / ZONE LI-1



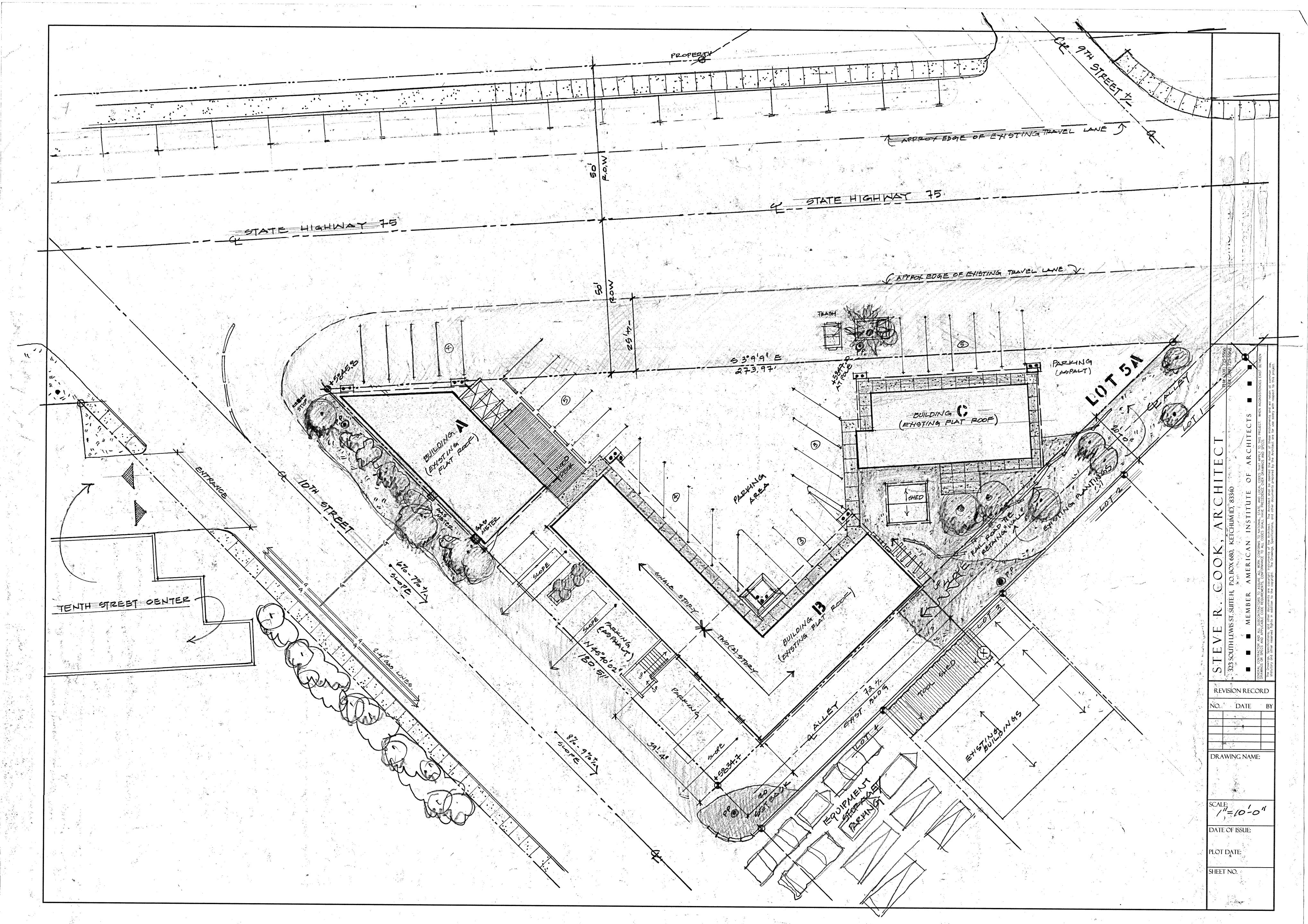
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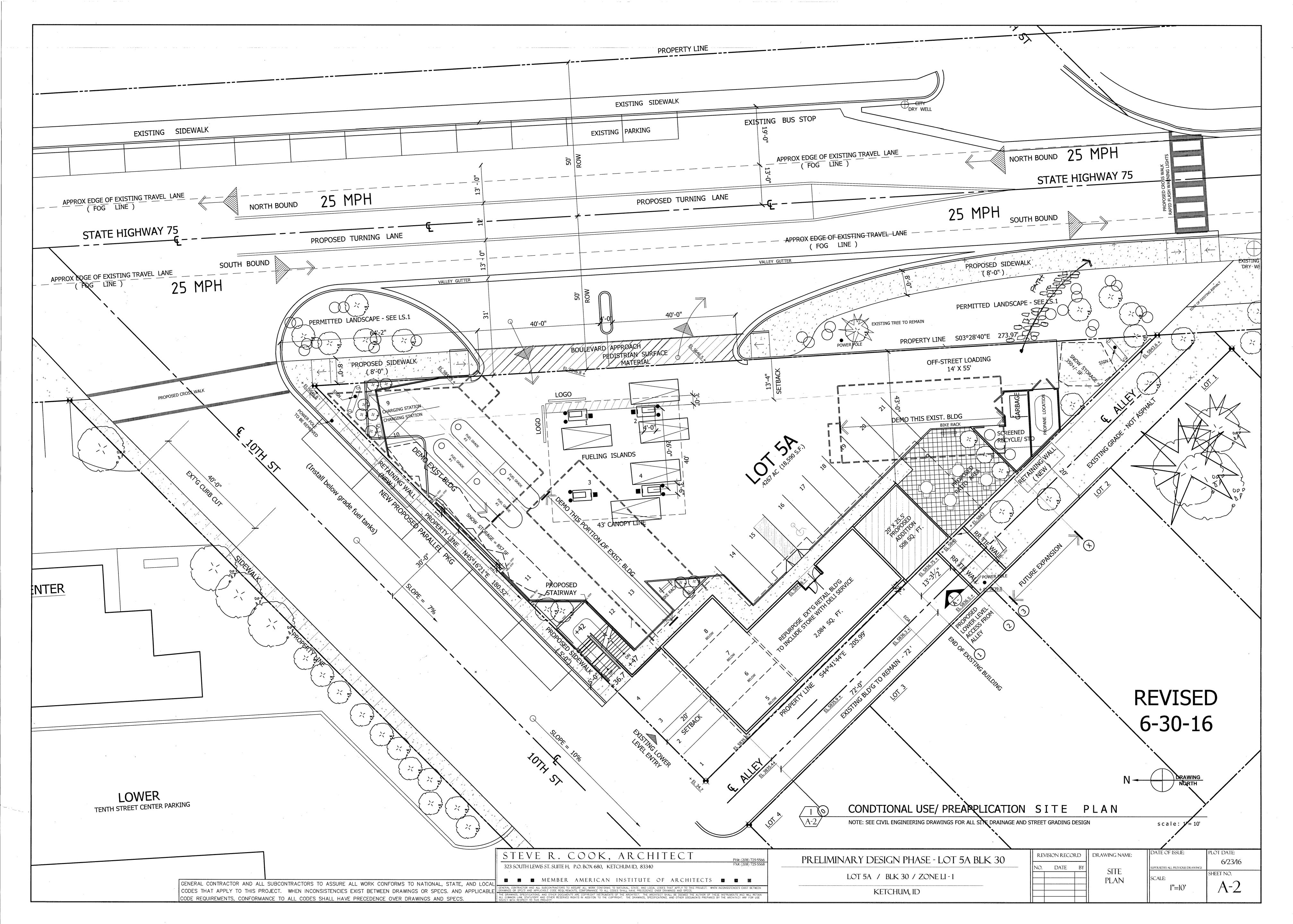


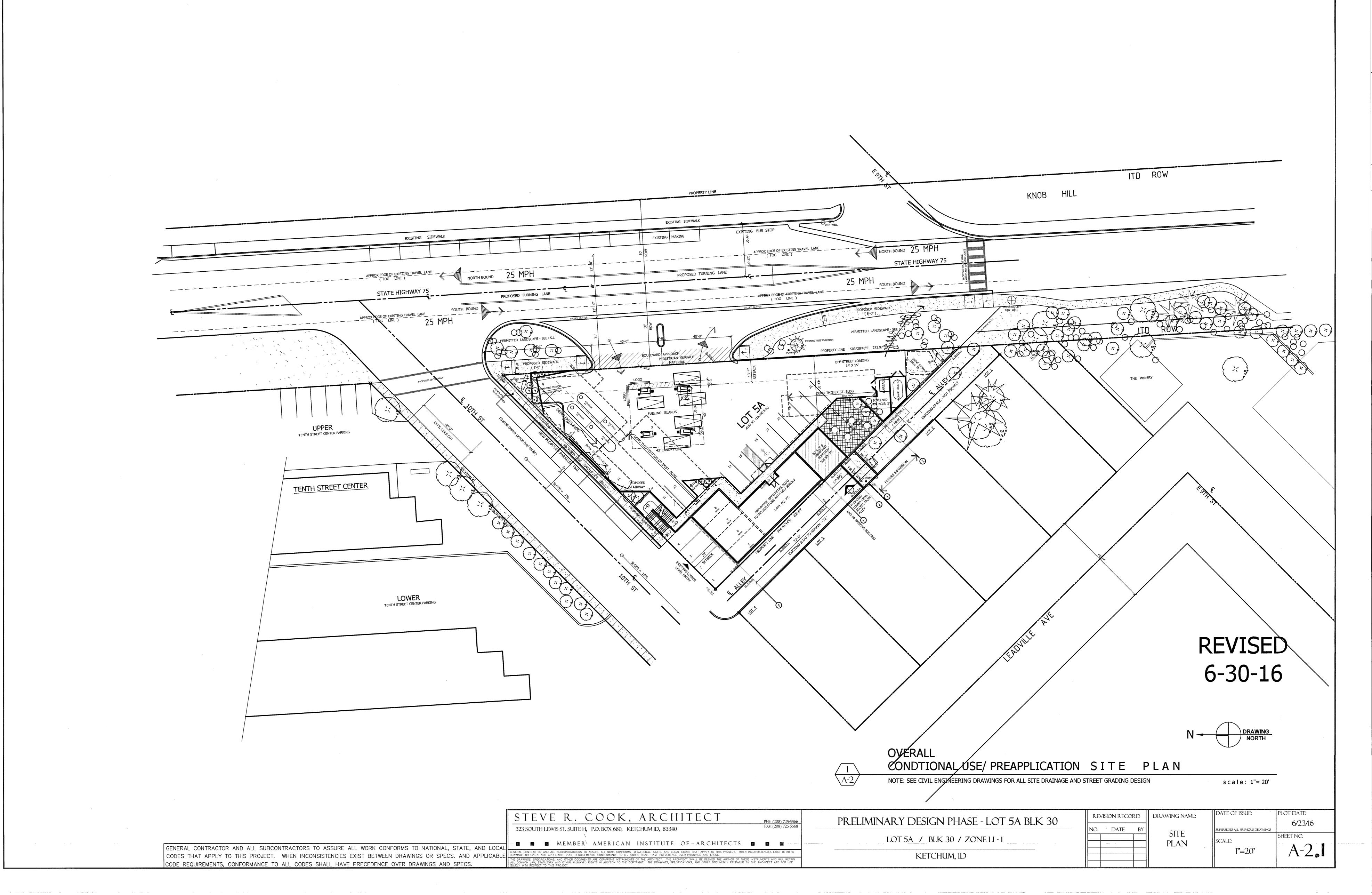
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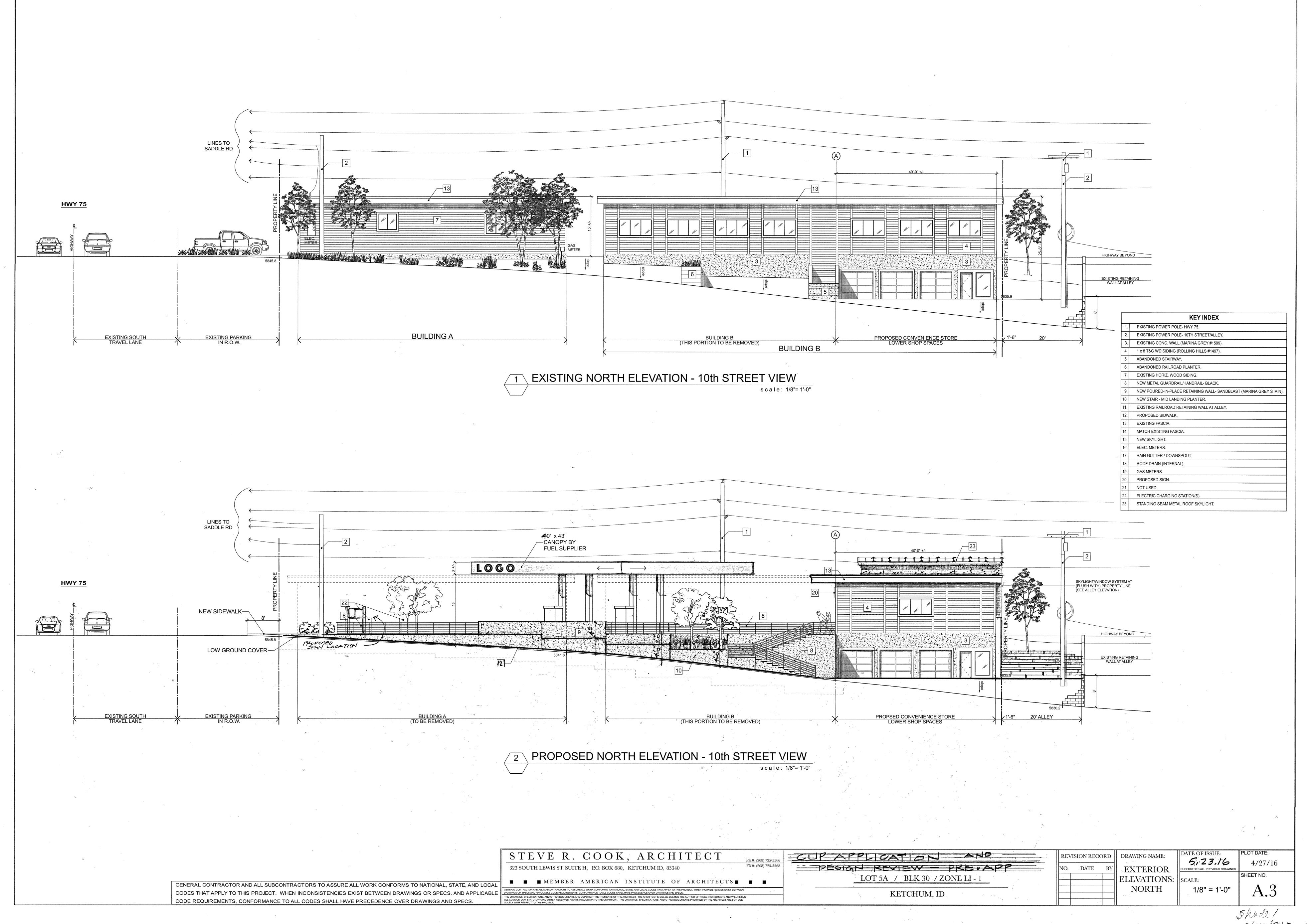
	DRAWING INDEX
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A.0	COVER PAGE.
A.1	EXISTING SITE PLAN. 1"= 10'
A.2	PROPOSED SITE PLAN. 1"= 10"
A.3	10 TH STREET VIEW: EXISTING AND PROPOSED. 1/8"= 1'
A.4	ALLEY VIEW: EXISTING AND PROPOSED. 1/8"= 1'
A.5	STORE FRONT ELEVATION AND FLOOR PLANS WITH AREA SQ. FOOTAGE CALCULATIONS. 1/4"= 1'
A.6	ENLARGED VIEW - ALLEY RETAINING WALLS. 1/4"= 1'
C.1	SITE SURVEY.
C.2	CIVIL ACCESS PLAN TO HWY 75.
L.1.0	LANDSCAPE PLAN.
	COMPUTER GENERATED MODELS:
ž÷	NORTH VIEW – BEFORE / AFTER
: •	• SOUTH VIEW – BEFORE / AFTER
:	NORTH ENLARGED VIEW
	SOUTH ENLARGED VIEW

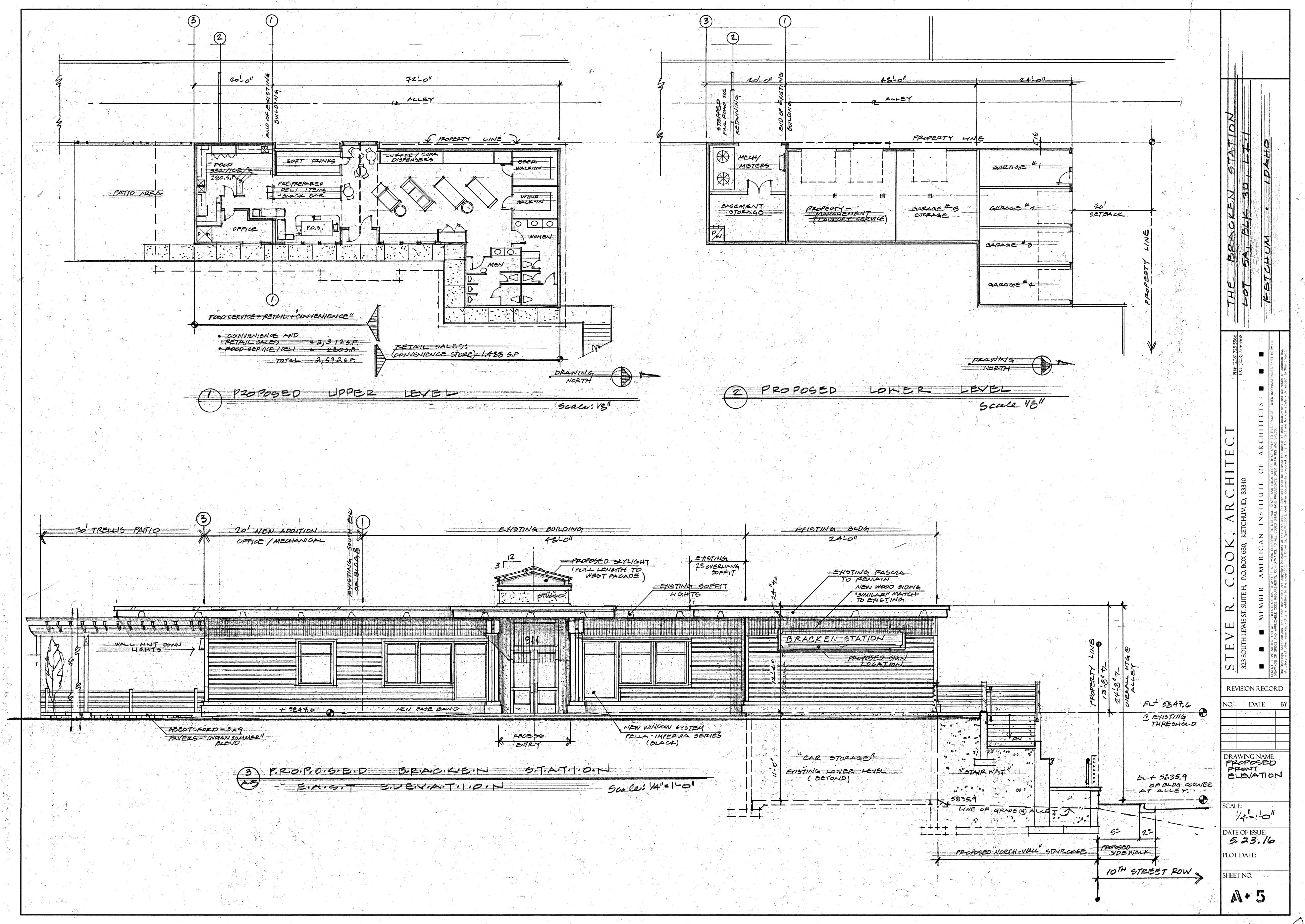
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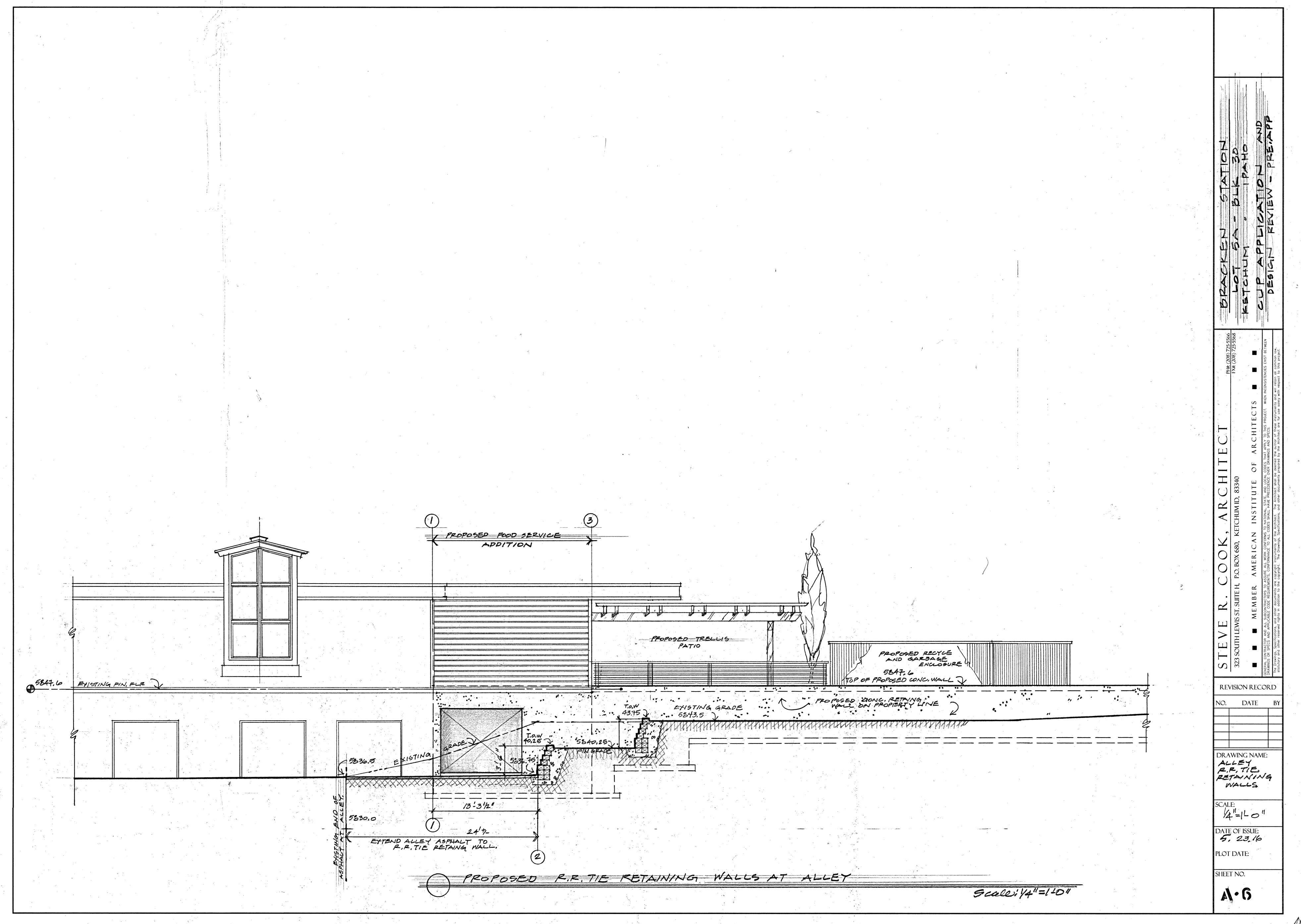




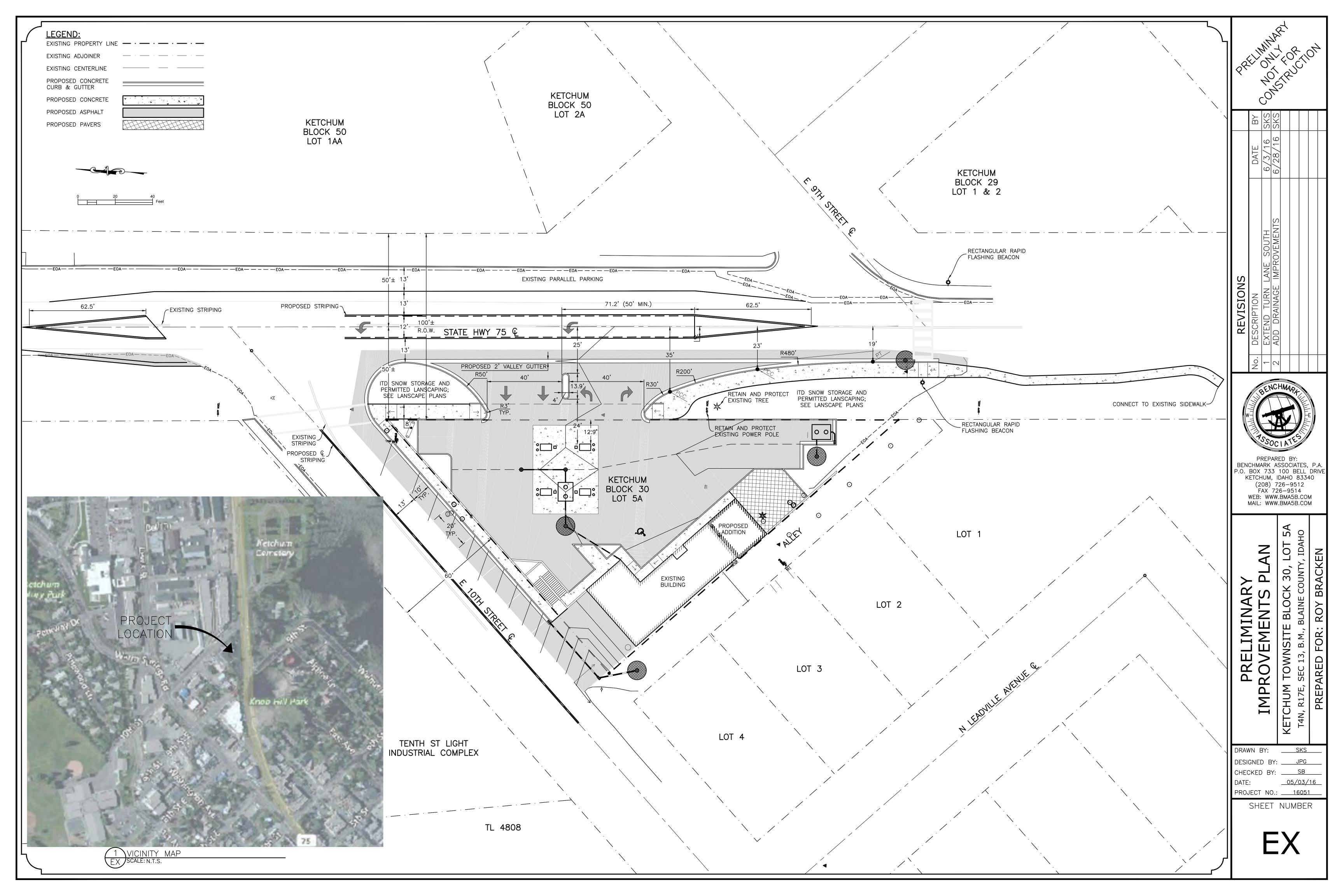


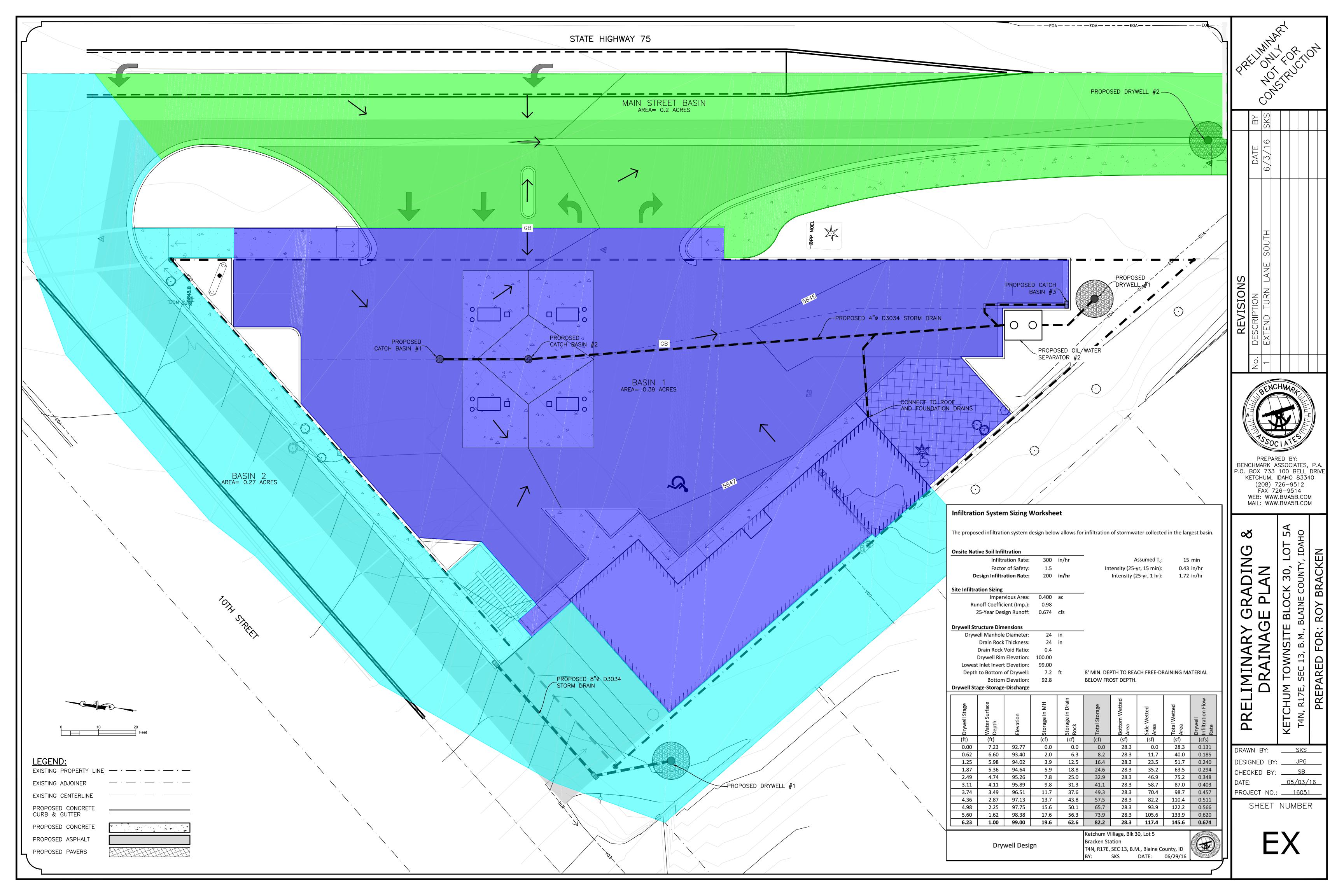


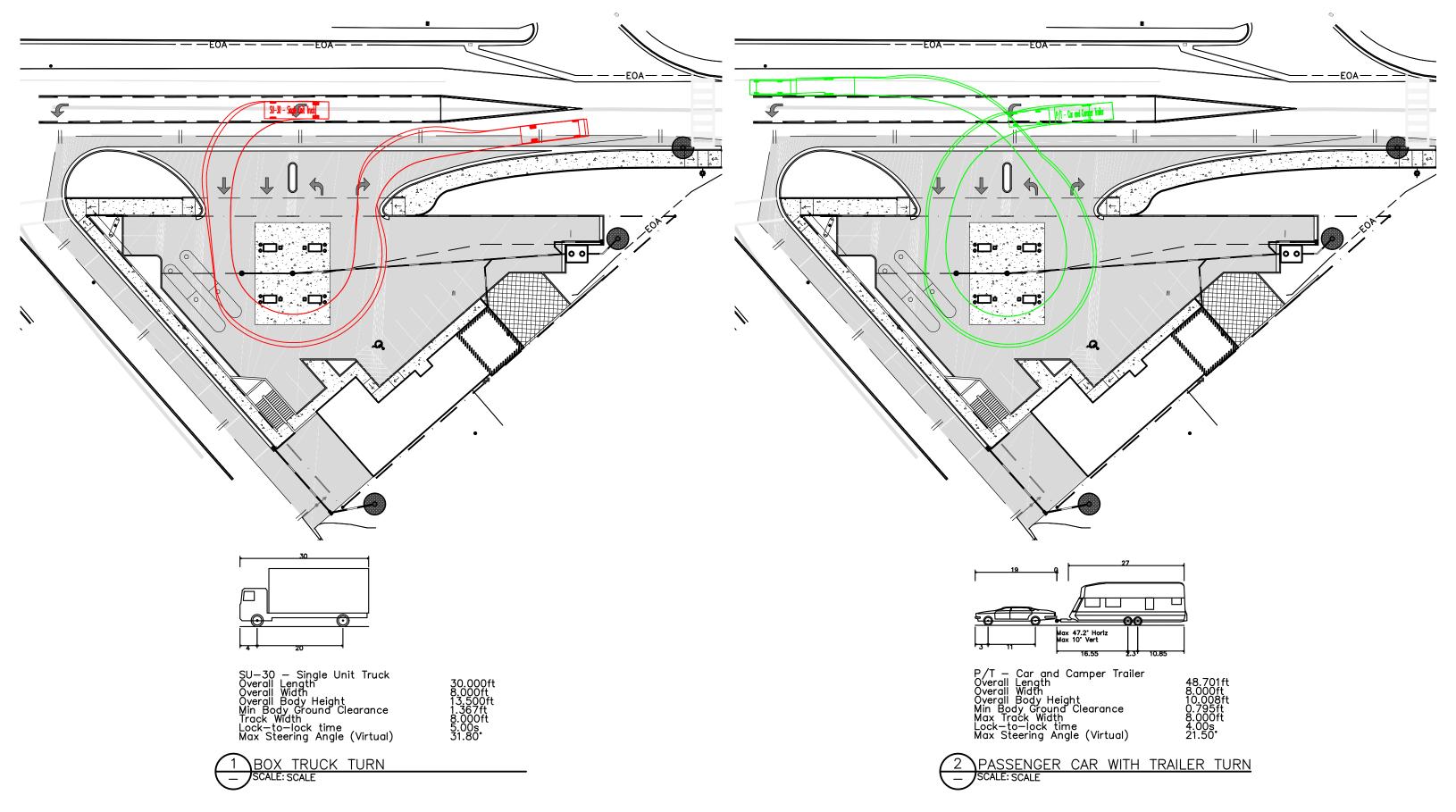


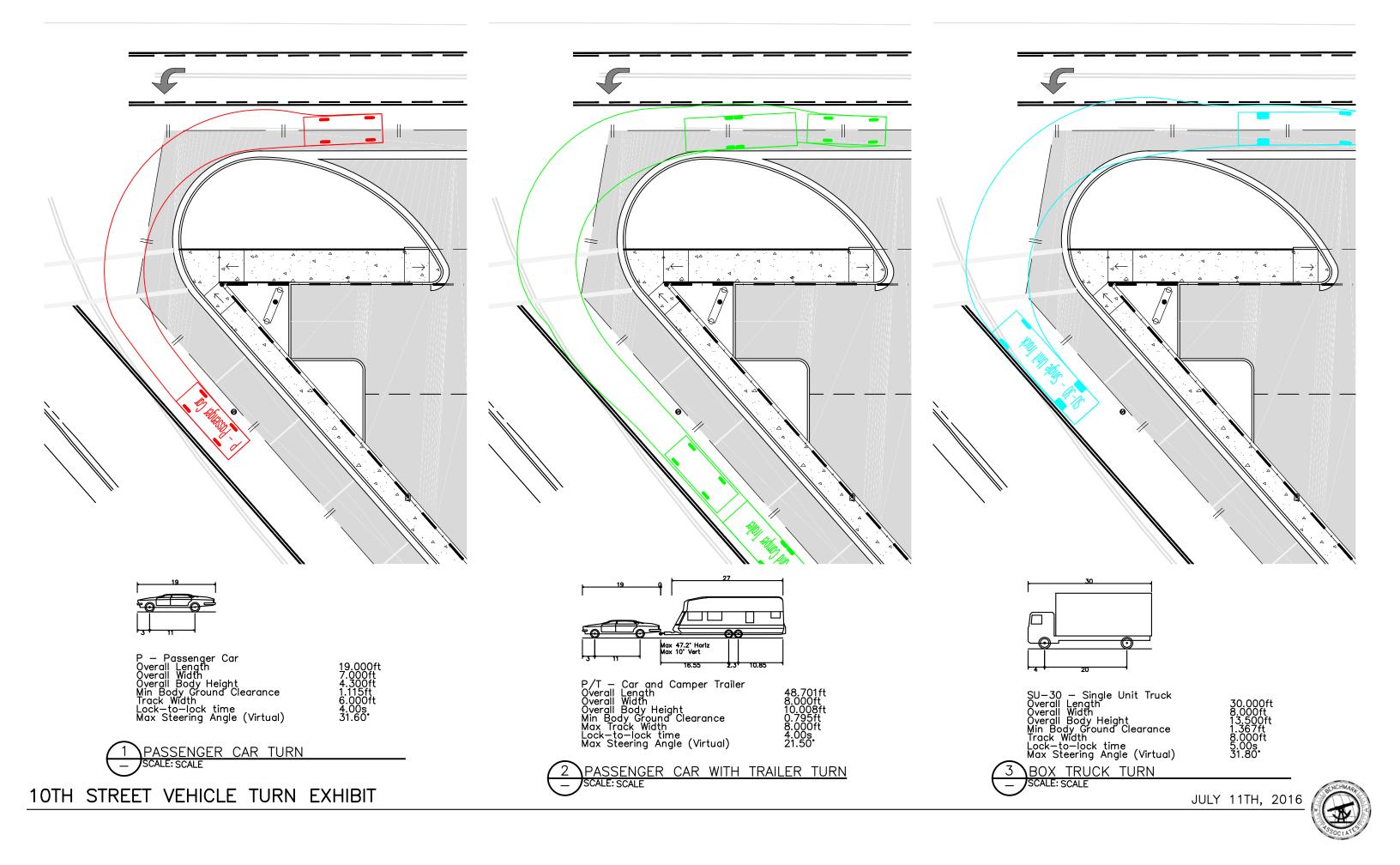


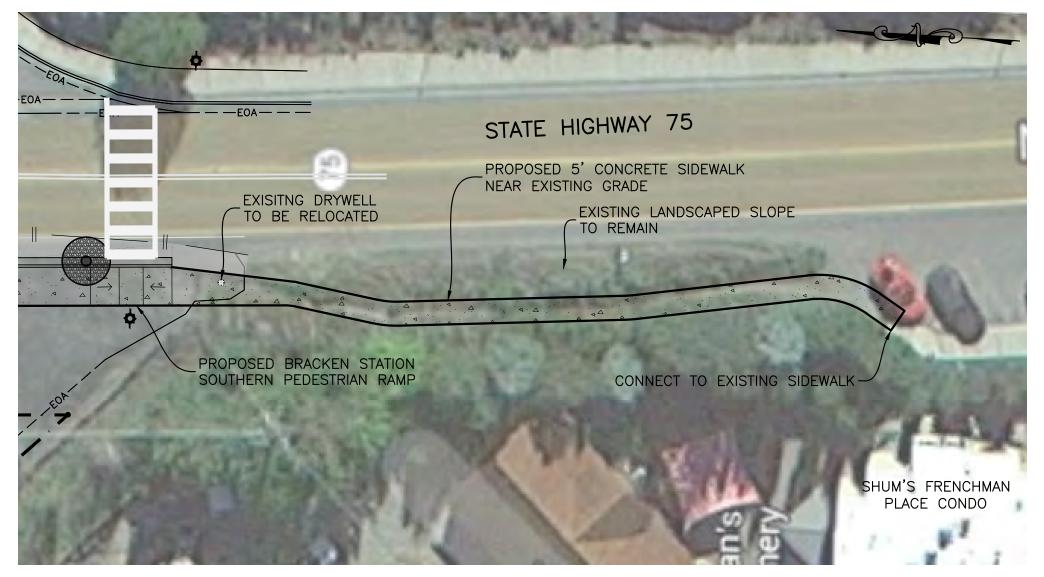
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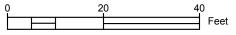


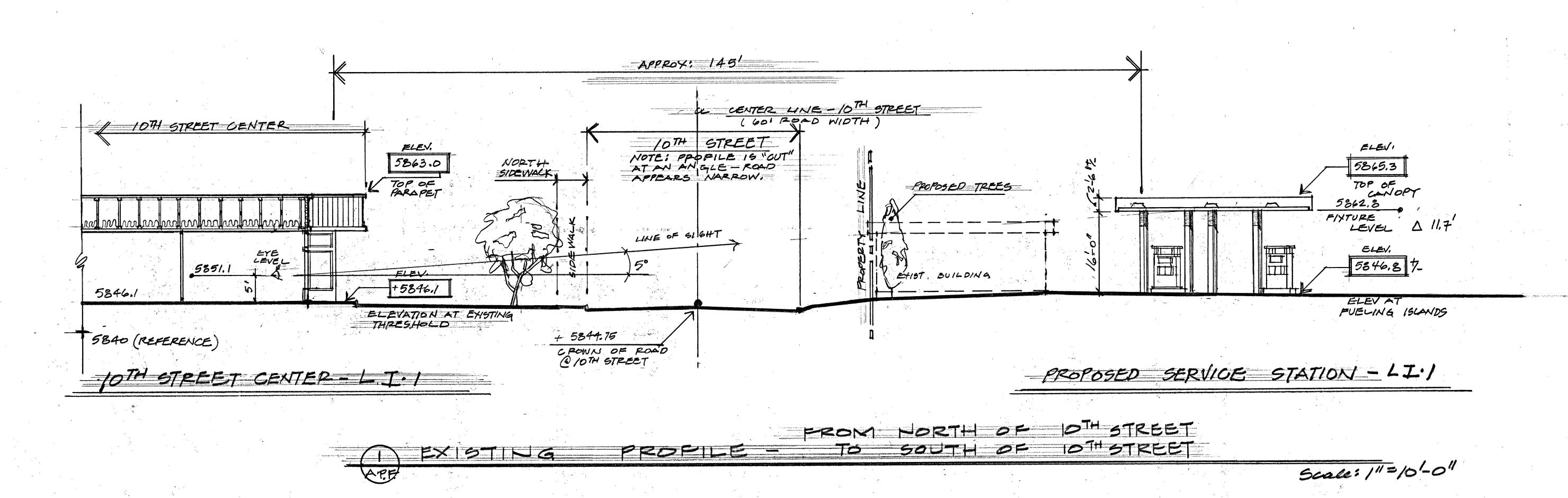












GENERAL CONTRACTOR AND ALL SUBCONTRACTORS TO ASSURE ALL WORK CONFORMS TO NATIONAL, STATE, AND LOCAL CODES THAT APPLY TO THIS PROJECT. WHEN INCONSISTENCIES EXIST BETWEEN DRAWINGS OR SPECS. AND APPLICABLE CODE REQUIREMENTS, CONFORMANCE TO ALL CODES SHALL HAVE PRECEDENCE OVER DRAWINGS AND SPECS.

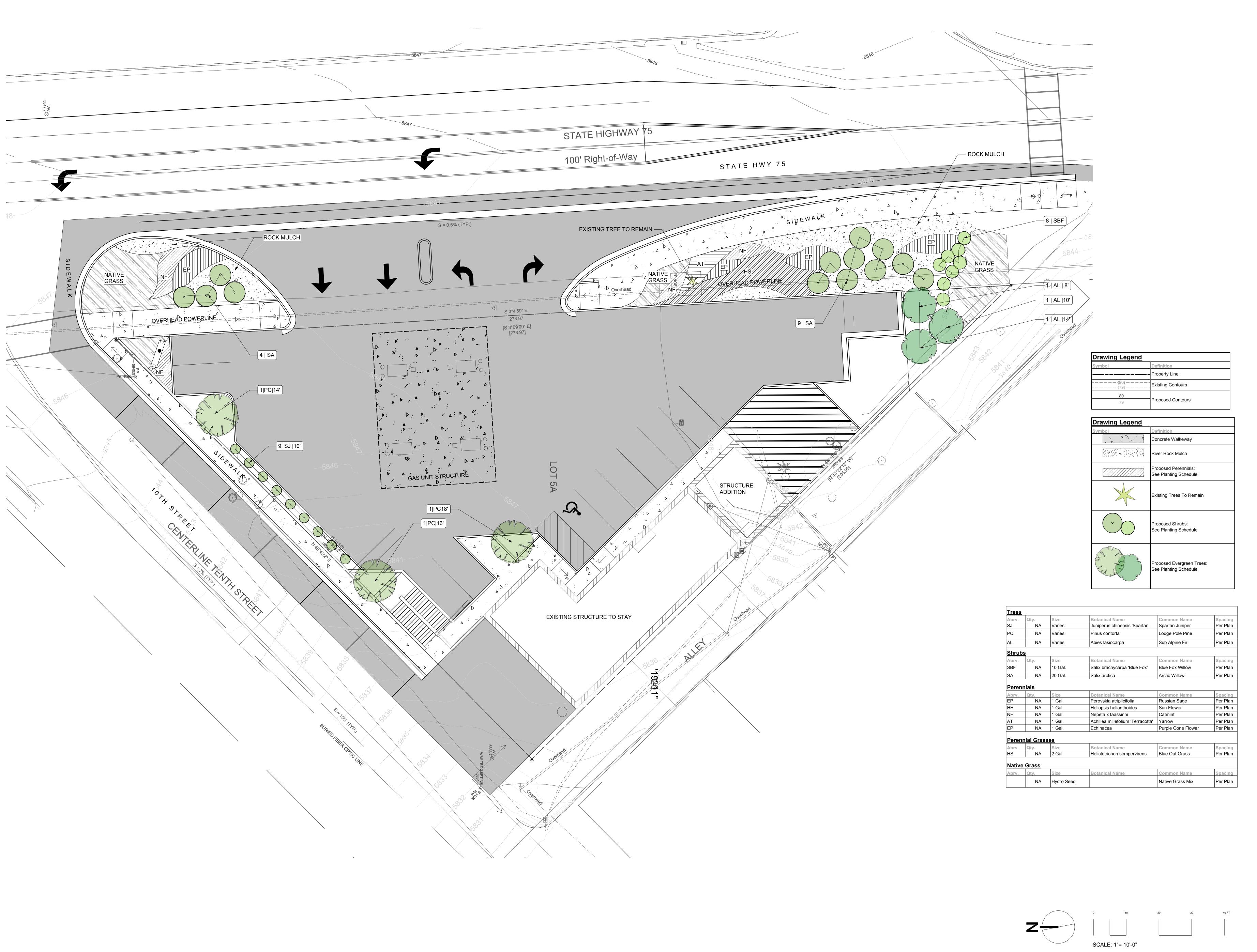
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7,11,16

SUPERSEDES ALL PREVIOUS DRAWING PLOT DATE:

SHEET NO.

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iption Signature Date

No. Description

© copyright 2016 ben young landscape architect

BRACKEN STATION
BRACKEN BUILDING
LOT 5A | BLK 30 | SONE LI-1, KETCHUM, ID

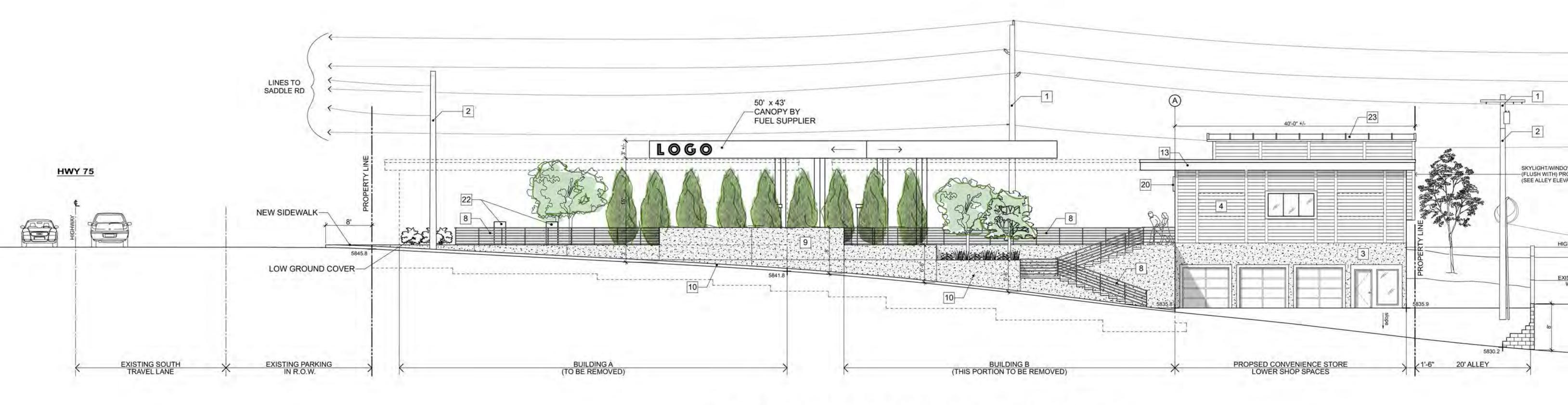
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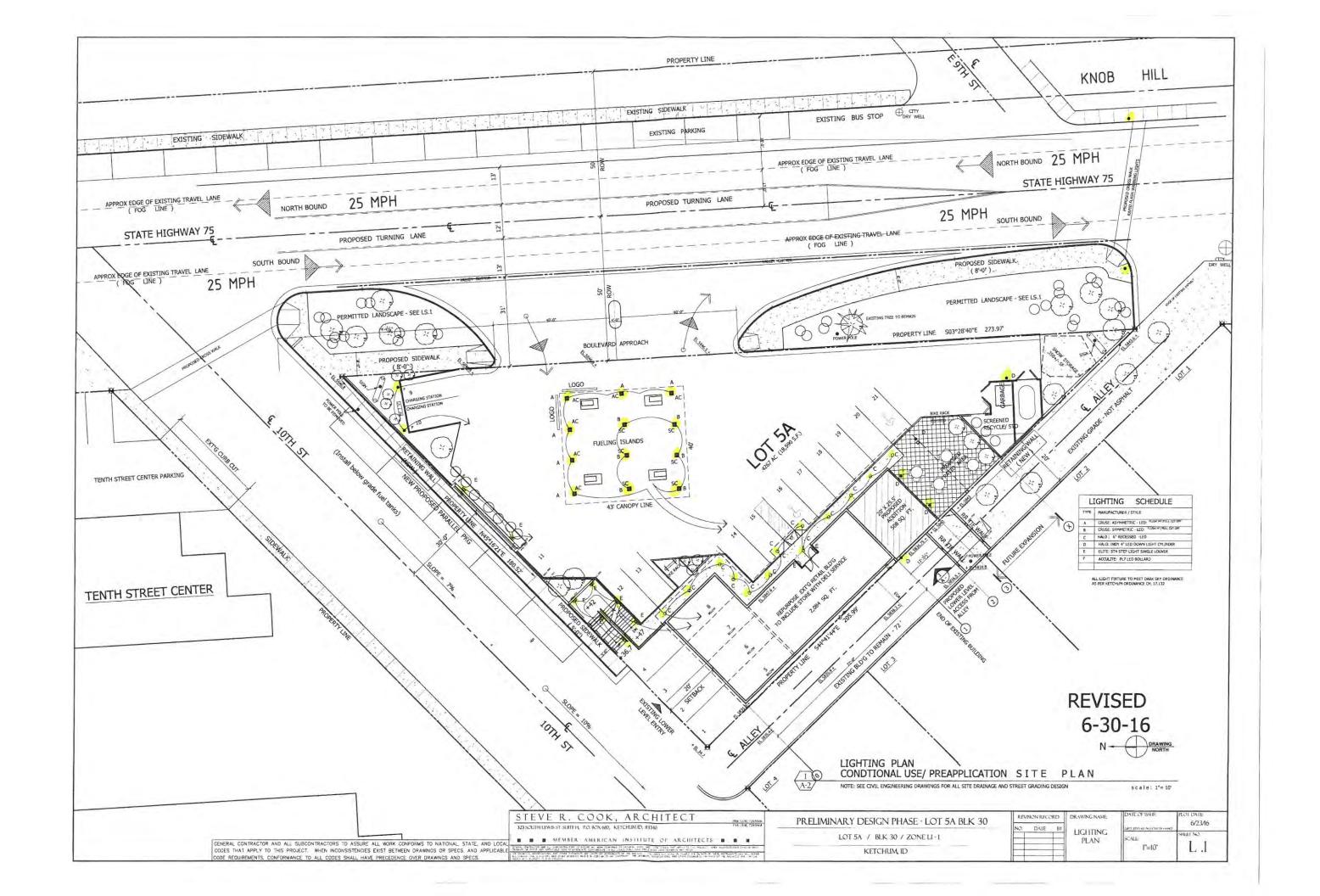
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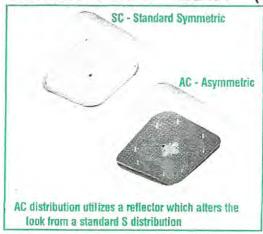
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Sheet No.

L1.0







DOE LIGHTING FACTS

Department of Energy has verified representative product test data and results in accordance with its Lighting Facts Program. Visit www.lightingfacts.com for specific catalog strings.

US & Int'l, patents pending

HOUSING - Low profile, durable die-cast, aluminum construction, providing a reliable weather-tight seal.

LEDS - Features an array of select, mid-power, high brightness, high efficiency LED chips; 5000K color temperature, 70 CRI (nominal).

DRIVE CURRENT - Choice of Very Low Wattage (VLW), Low Wattage (LW), Super Saver (SS), High Output (HO) or Very High Output (VHO).

OPTICS / DISTRIBUTION - Choice of Symmetrical or Asymmetrical, which directs light through a clear tempered glass lens, to provide a uniform distribution of light to vertical and horizontal surfaces.

OPTICAL UNIT - Features an ultra-slim 7/8" profile die-cast housing, with a flat glass lens. Unit is water-resistant, sealed to an IP67 rating. Integral designed heat sink does not trap dirt and grime, ensuring cool running performance over the life of the fixture.

PRESSURE STABILIZING VENT - Luminaire assembly incorporates a pressure stabilizing vent breather to prevent seal fatigue and failure.

HAZARDOUS LOCATION - Designed for lighter than air fuel applications. Product is suitable for Class 1 Division 2 only when properly installed per LSI installation instructions (consult factory).

DRIVER - State-of-the-art driver technology superior energy efficiency and optimum light output. Driver components are fully encased in potting for moisture resistance. Complies with IEC and FCC standards. 0-10 V dimming supplied standard with all drive currents.

DRIVER HOUSING - Die-cast aluminum, wet location rated driver/electrical enclosure is elevated above canopy deck to prevent water entry, provide easy "knock-out" connection of primary wiring and contributes to attaining the lowest operating temperatures available. Seals to optical housing via one-piece molded silicone gasket.

OPERATING TEMPERATURE - -40°C to 50°C (-40°F to +122°F)

ELECTRICAL - Universal voltage power supply, 120-277 VAC, 50/60 HZ input. Drivers feature two-stage surge protection (including separate surge protection built into electronic driver) meets IEEE C62.41.2-2002, Scenario 1, Location Category C.

FINISH - Standard color is white and is finished with LSI's DuraGrip® polyester powder coat process. DuraGrip withstands extreme weather changes without cracking or peeling.

INSTALLATION - One person installation. No additional sealant required. Installs in a 12" or 16" deck pan. Deck penetration consists of a 4" hole, simplifying installation and water sealing. Unit is designed to quickly retrofit into existing Scottsdale (4") hole as well as openings for Encore and Encore Top Access and to reconnect wiring for the SC/ECTA without having to relocate the conduit. Retro'panels are available for existing Encores (see back page) as well as kits for recessed and 2x2 installations (see separate spec sheets). Support brackets are provided standard, to prevent sagging of deck.

SHIPPING WEIGHT - 27 pounds (single pack), 48 pounds (double pack).

EXPECTED LIFE - Minimum 60,000 to 100,000 hours depending upon the ambient temperature of the installation location. See LSI web site for specific guidance.

WARRANTY - Limited 5-year warranty.

LISTING - UL and ETL listed to UL 1598, UL 8750 and other U.S. and International safety standards. Suitable for wet locations.

PHOTOMETRICS - Please visit our web site at <u>www.lsi-industries.com</u> for detailed photometric data.

Consult Factory

Class 1. Division 2 - Standard on SS & LW.

T5 Temperature Classification – The surface temperature of this product will not rise above 100°C, within a 40°C ambient.

Gas Groups A,B,C, and D — Group A: Acetylene / Group B: Hydrogen / Group C: Propane and Ethylene / Group D: Benzene, Butane, Methane & Propane.

This product, or selected versions of this product, meet the standards listed below.

Please consult factory for your specific requirements.















Project Name ______ Fixture Type ______

04/29/16





CRUS SC LED TYPICAL ORDER EXAMPLE: HO 50 UE WHT

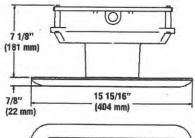
Prefix	Distribution ¹	Light Source	Drive Current	Color Temperature	Input Voltage	Finish	Options
CRUS	SC - Standard Symmetric AC - Aymmetric	LED	VLW - Very Low Watt LW - Low Watt SS - Super Saver HO - High Output VHO - Very High Output	50 - 5000K	UE - Universal Voltage (120-277V) 347 - 480V	WHT ~ White BRZ - Bronze BLK - Black	None

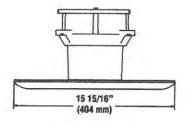
FOOTNOTES:

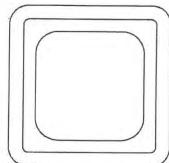
1- AC distribution utilizes a reflector which alters the look from a standard S distribution.

ACCESSORY ORDERING INFORMATION (Access	sories are field installed)		
Description	Order Number	Description	Order Number
Retrofit Panels - EC / ECTA / SCF to CRU, for 16" Deck Panel	525946	Kit - Hole Plugs and Silicone (enough for 25 retrofits) ¹	1320540
Retrofit Panels - ECTA / SCF to CRU, for 12" Deck Panel	530281	1- Consists of (25) 7/8" hole plugs and (1) 10.3 oz tube of RTV	1020040
Retrofit 2x2 Cover Panel Blank (no holes)	357282	And the second of the	
Retrofit RIC Cover Panel Blank (no holes)	354702		

DIMENSIONS







		Lun	nens	Wa	tts	LI	W
		SC	AC	SC	AC	SC	AC
9 _	VLW - Very Low Watt	8842	-	79	-	112	-
l White	LW - Low Watt	10871	8746	88	83	124	105
C00	SS - Super Saver	13554	11518	114	111	119	104
	HO - High Output	18633		132		141	-
_	VHO - Very High Output	22418	17262	159	157	141	110



Masmanie Gasto

Project Name

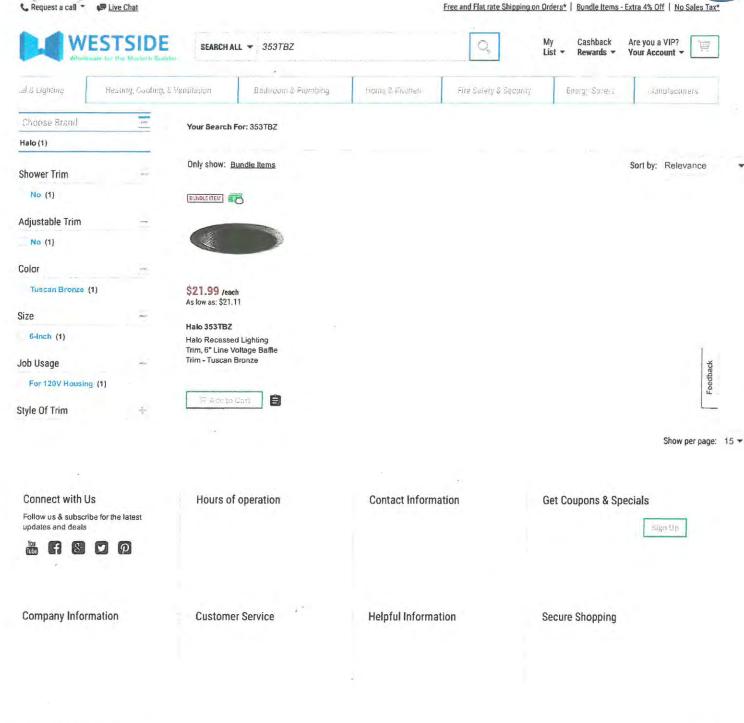
→ Fixture Type

04/29/16

Catalog #_

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HOME /ELECTRICAL & LIGHTING /RECESSED LIGHTING /RECESSED LIGHT CANS /HALO H7ICT

Print

Halo H7ICT 6" Recessed Lighting Can, AIRTITE Housing for New Construction

BRAND: HALO MODEL: H7ICT CONDITION: NEW WARRANTY: 1 YEAR SKU: 166218 CLEARANCE ITEM!

0

STATUS: IN STOCK



Regular Price: \$11.99

On Sale: \$9.15

+ FREE Shipping! on qualified orders

₩ Add to Cart

SAVE TO LIST



Get 10% Cashback in Westside Dollars

HALO.

View All Halo Recessed Lighting

Promo: 10% Off recessed cans when you buy at least 10 Lutron LI

nmers

Feedback

Description
Features
Specifications
Warranty
Customer Reviews
Questions & Answers

Description

The Halo H7ICT is a 6" insulated recessess housing capable of beuing installed againat ceiling insulation, its self regulating thermal protector deactives the light should overheating occur. Many homeowners who are looking to retrofit their homes with new accent lighting often run into challenges when installing new fixtures: insulation in their ceilings that results in a potential fire hazard when exposed to a light fixture's excess heat.

The Halo H7ICT remedies this problem by utilizing Halo's integral thermal protector technology. The Halo H7ICT is also easy to install no matter where you're going to use it. The wiring connections cab be made outside the junction box. Automatic flange leveling aligns the housing allowing one to hold the housing in one hand while simultaneously drive in nails with your otehr hand. Its a snap.

Allows wiring connections to be made outside thejunction box. It features simple captive bar hangers, which enable the housing to be used anywhere within the full 24* joist span. The socket also fastens tightly into the unit's trim for proper lamp positioning.

Perfect for any home's established decor, this unit is offered in a variety of colors, including black, bronze, copper, white and brushed nickel. The Halo H7ICT is also UL listed and CSA certified, making it one of the most popular choices for modern homes.

Use with 6" Lighting Recessed Trims

- Black
- Bronze
- Brushed Nickel
 Copper
- White

Features

- Thermally Protected IC Housing
- Housing may be covered with insulation
- For use with 120V Incandescent lamps

Specifications

- · 10 1/2" Long
- 7 1/2" Wide
- 7 1/2" Tall

Warranty

One (1) year through the manufacturer

Customer Reviews

There are no reviews for this product yet. Be the first to review the product!

Write Your Own Review:

Nickname*

1 star 2 stars 3 stars 4 stars 5 stars

Quality

Summary of Your Review*

Value

Value

Review Text*





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Revit Files Instructions Compatible Trims Accessories

Indy 4in LC4B Series, LED, Wall Mount Cylinder



FINISHES / TYPES LINE ART Black Silver White















Indy™ 4-inch L-Series LED Cylinders are part of the latest generation of Indy L-Series luminaires, offering a fully coordinated family of commercial-grade quality luminaires, from LED color rendering to aperture sizes and trim finishes. These cylinders can be surface, cable, pendant, or wall-mounted. They are the energy-efficient choice for a wide range of commercial applications. Efficacies up to 111lm/W.

Compact light source delivers uniform illumination without pixilation, enabling excellent beam control. Light levels range from 800 to 2,300 lumens

LEDs are energy-efficient and offer exceptionally long service life

Available in CCTs of 2700K, 3000K, 3500K and 4000K, high CRI of 80 Im/w. Optional 90 CRI and 97 CRI availble

Lumen packages suitable for ceiling heights ranging from 8' to over 35'

L-Series Cylinder fixtures offer hyperbolic, parabolic, baffle or wall wash reflectors and multiple trim color and finish options

One piece heavy gauge aluminum with white, black or silver backed textured powder coat finish

Passive Cooling; cast aluminum heat sink integrated directly with housing provides superior thermal management to ensure the long life of the LED

LEDs contain no environmentally harmful mercury, lead, or PCBs and generate no UV or IR emissions

LED driver accommodates 120V, 277V, or 347V inputs with 0-10V dimming standard; also available with DMX, DALI, phase cut, forward and reverse phase and Lutron drivers for use with EcoSystem®

U/CSA Listed; RoHS compliant; ARRA compliant; ENERGY STAR® qualified

BRANDS

JUNO

PRODUCT SELECTOR

INDY10

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LITERATURE

LIGHTING PRODUCT

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INSTRUCTOR-LED TRAINING

CONTACT US

CUSTOMER SERVICE: 800-367-5865

TECHNICAL SUPPORT: 888-387-2212

PRODUCT ASSISTANCE

FIND AN AGENT

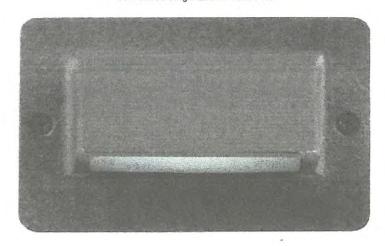
FIND A DISTRIBUTOR



- Products
- LED Step Light
- ST4 Series
- ST403



ST4 Series Single Louver Face Plate



Housing only:

- Specs
- B K.
- · Catalog

Features

Technical

Colors

Housing

Application

For concrete use only

High grade die-cast aluminum with 0.3% Copper to prevent corrosion in concrete applications.

Supplied standard with four 3/4? NPS holes.

Face Plate

High-grade die-cast aluminum

Gasket

Single piece molded silicon for wet location

Lens

Frosted tempered glass

External Louver

Anodized 45" cut-off louver

Reflector

High performance aluminum reflector assembly for superb light output

Listing

UL/cUL for wet locations





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Description Catalog Numbers

AccuLite PL7 Series, LED, Parklume Bollards











AccuLite™ Parklume area lighting bollards provide effective, energy-efficient pathway lighting in a variety of attractive styles. PL7- LED Series bollards offer a clean, contemporary look with a slim 7-inch diameter that blends easily with the landscape. LED version provides up to 1,000 lumens from just 25W input, while other efficient alternatives include HID and CFL light sources. All bollards feature vandal-resistant polycarbonate lenses and durable powder coat

LEDs feature a top reflector that collects light and a lower cone that spreads it to the surroundings; anodized reflectors smooth light for uniform distribution

LEDs have an aluminum heat sink that reduces heat and helps assure rated service life of 50,000 hours; universal 120-277VAC LED driver, 60Hz input

7-inch diameter housings feature rugged extruded-aluminum construction with cast-aluminum top and base plate

160-degree house-side shield is optional

15W or 25W LED light source; other models accept HID or CFL lamps of various types and wattages

UL/CSA Listed; suitable for wet locations



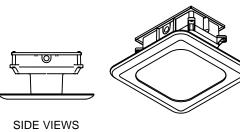




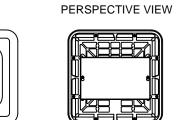
CUSTOMER SERVICE: 808-367-5866 TECHNICAL SUPPORT: 888-287-2212

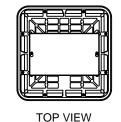
FIND A DISTRIBUTOR

CRUS-SC-LED LED CANOPY LIGHT - LEGACY

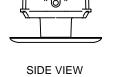


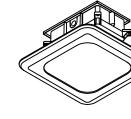




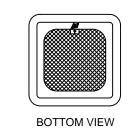


CRUS-AC-LED LED CANOPY LIGHT - LEGACY





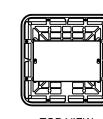
PERSPECTIVE VIEW



LLF

0.850 N.A.

0.850 N.A.



Lumens/Lamp | Arr. Lum. Lumens

8746

8842

*	6	В	SINGLE		CRUS-SC-LED-	VLW-CW D	IMMED 15%	MTD @ 15′	
Calculation S	ımmary								
Label			CalcType	Units	Avg	Max	Min	Avg/Min	Ma×/Min
ALL CALC POI	VTS		Illuminance	Fc	1.62	41.3	0.0	N.A.	N.A.

Description

28,51

CRUS-AC-LED-LW-CW DIMMED 15% MTD @ 15'

41.3

11.3

Arrangement

SINGLE

Illuminance

Total Project Watts Total Watts = 969.6

Arr. Watts

82.9

78.7



LO-133509-2 LIGHTING PROPOSAL 43 X 40 CAN□PY

STATE HWY 75 KETCHUM,ID DATE:6-20-16 REV:6-30-16

SCALE: 1"=16'

This lighting plan represents illumination levels calculated from laboratory data taken under controlled conditions in accordance with The Illuminating Engineering Society (IES) approved methods. Actual performance of any manufacturer's luminaires may vary due to changes in electrical voltage, tolerance in lamps/LED's and other variable field conditions. Calculations do not include obstructions such as buildings, curbs, landscaping, or any other architectural elements unless noted.

Based on the information provided, all dimensions and luminaire locations shown represent recommended positions. The engineer and/or architect must determine the applicability of the layout to existing or future field conditions.

5.0 5.0 5.1 5.1 b. 1.4 5.4 8.6 196 20.3 19.0 17.0 13.1 6.7 2.9 1.4 1.7 1.7 5.0 5.0 5.0 5.1 5.1 5.1 5.2 1.6 PROPOSED . STAIRWAY 5.0 5.0 5.0 5.0 5.1 5.1 5.2 5.3 to to to to to to to to 5.0 5.0 5.0 5.0 5.0 5.0 5.1 5.1 5.1 5.1 5.1 5.2 5.2 5.2 5.2 5.2 5.2 5.0 5.0 5.0 5.0 5.0 5.1 5.1 5.1 5.1 5.1 5.1

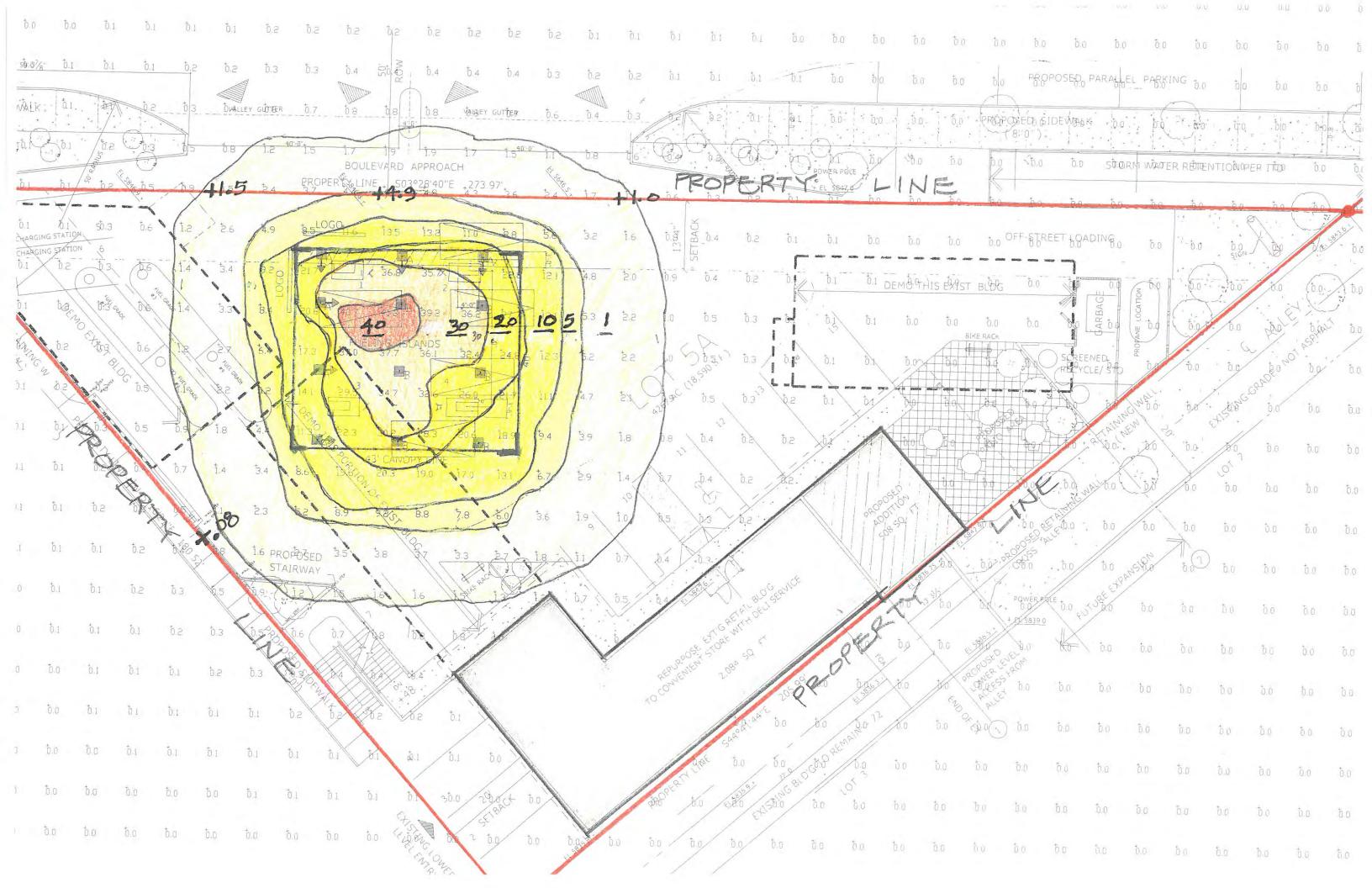
Luminaire Schedule

Qty

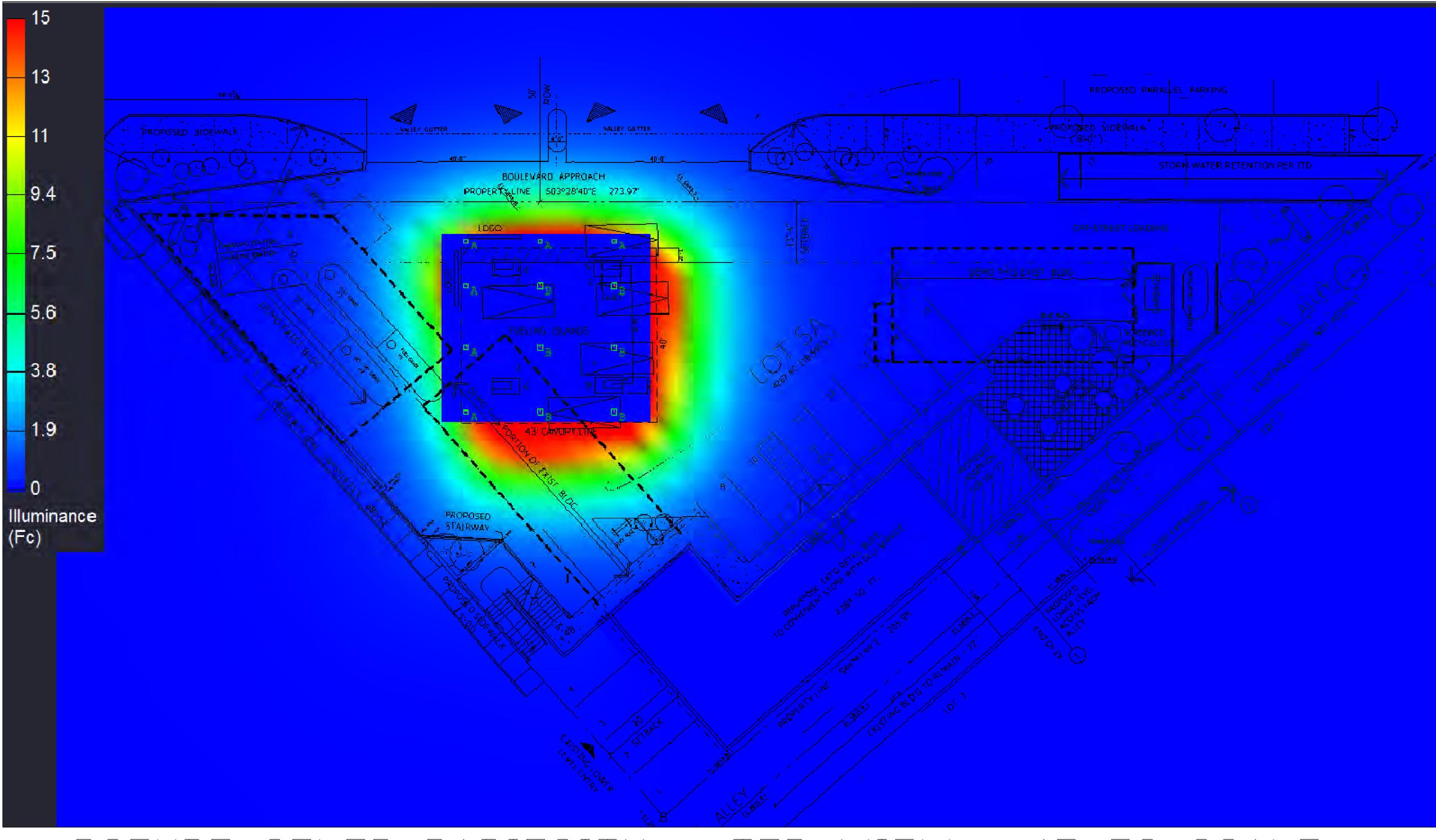
Label

Symbol

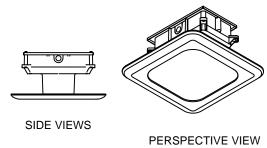
CANOPY

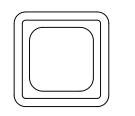






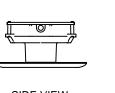
CRUS-SC-LED LED CANOPY LIGHT - LEGACY





BOTTOM VIEW

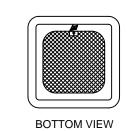
CRUS-AC-LED LED CANOPY LIGHT - LEGACY

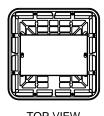


SIDE VIEW



PERSPECTIVE VIEW





PSEUDO COLOR RADIOSITY - TOP VIEW - 15 FC SCALE

Based on the information provided, all dimensions and luminaire locations shown represent recommended positions. The engineer and/or architect must determine the applicability of the layout to existing or future field conditions.

This lighting plan represents illumination levels calculated from laboratory data taken under controlled conditions in accordance with The Illuminating Engineering Society (IES) approved methods. Actual performance of any manufacturer's luminaires may vary due to changes in electrical voltage, tolerance in lamps/LED's and other variable field conditions. Calculations do not include obstructions such as buildings, curbs, landscaping, or any other architectural elements unless noted.

_uminaire Sched	dule							
Symbol	Qty	Label	Arrangement	Description	LLF	Lumens/Lamp	Arr. Lum. Lumens	Arr. Watts
	6	А	SINGLE	CRUS-AC-LED-LW-CW DIMMED 15% MTD @ 15'	0.850	N.A.	8746	82.9
•	6	В	SINGLE	CRUS-SC-LED-VLW-CW DIMMED 15% MTD @ 15'	0.850	N.A.	8842	78.7

Calculation Summary							
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
ALL CALC POINTS	Illuminance	Fc	1.62	41.3	0.0	N.A.	N.A.
CANDPY	Illuminance	Fc	28,51	41.3	11.3	2.52	3,65

Total Project Watts Total Watts = 969.6



SCALE: 1"=16'



LD-133509-2 LIGHTING PROPOSAL 43 X 40 CANDPY RADIOSITY STATE HWY 75 KETCHUM,ID



MFMORANDUM

125 West Main Street Bozeman, MT 59715 (406) 624-6117 www.altaplanning.com

To: Roy Bracken

North Town Partners Lot 5A Ketchum Idaho

From: Joe Gilpin, Principal

Date: June 29, 2016

Re: Motor Fueling Station Pedestrian Analysis

Introduction

This preliminary analysis of pedestrian access at the proposed Motor Fueling Station summarizes the site, pedestrian issues and design recommendations for the site as well as an approximately 3-block area study area.

To the Station Context and Recommendations

Located at the intersection of 10th Street and North Main Street, there are three major pedestrian catchment areas associated with the motor fueling station (illustrated in Figure 1). Pedestrians from these catchment areas will primarily access the site via North Main Street and 10th Street. Major pedestrian crossing points will include the intersections of:

- North Main Street and 9th Street
- North Main Street and 10th Street

Figure 1 illustrates catchment areas and major pedestrian access routes to the motor fueling station. The catchment areas and specific pedestrian issues and design recommendations areas are described below.

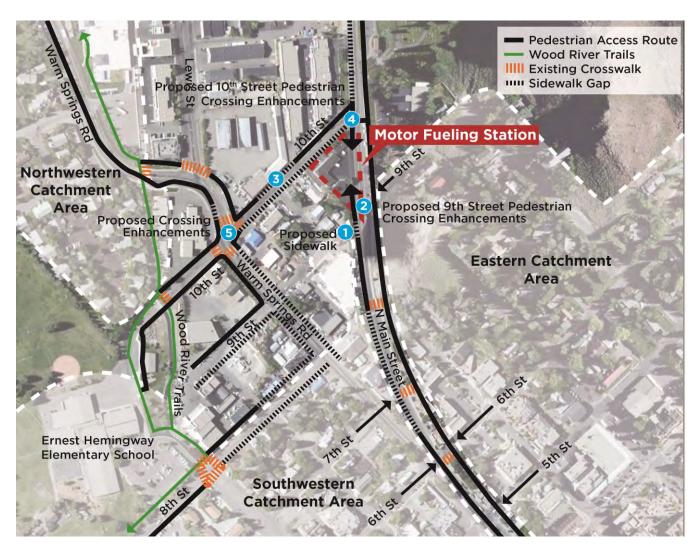


Figure 1: Pedestrian Catchment Areas and Circulation

Eastern Catchment Area Context and Recommendations

The eastern catchment area is comprised of a residential area and commercial district along North Main Street (State Highway 75). Pedestrians are likely to travel to the site along the eastern side of North Main Street and cross to the site at 9th Street. The sidewalk along the eastern side of North Main Street provides a connection from perpendicular streets to the site, with less g aps and driveway crossing than the western sidewalk. To address the existing gap in pedestrian facilities, a 5' concrete sidewalk (1) is proposed to connect pedestrians from Shum's Frenchman Place Condo to the motor fueling station.

A rectangular rapid flashing beacon (2), crosswalk and dedicated pedestrian ramps are proposed at the 9th Street crossing. The rectangular rapid flashing beacon (RRFB) would establish a high-visibility strobe-like warning to drivers when pedestrians are using the crosswalk, increasing motorist yielding compliance and pedestrian safety.

Southwestern Catchment Area Context and Recommendations

The southwestern catchment area is comprised of a residential area, commercial district along North Main Street, and the Ernest Hemingway Elementary School. Pedestrians are likely to travel to the motor fueling station along the western side of North Main Street or 10th Street. Driveways and parking along the length of 10th Street create large gaps in pedestrian facilities on both the north and south side of 10th Street. While the potential for pedestrian and vehicle conflicts are high along both sides of 10th, the north side is more desirable for pedestrian travel as only one large gap in sidewalk exists. There is no existing sidewalk on the south side of 10th, additionally the street is served with long banks of parallel parking, however there are two significant frontages where front-in perpendicular parking is present on both sides of the street. This is the least compatible parking type with pedestrians as the driver does not have any view of street conditions behind before backing up.

Options for clearly defining a pedestrian zone through this gap (3) are recommended. Converting the pull-in parking to angle parking bays would create space to establish a sidewalk between the business front and parking. If existing parking through this area prohibits a dedicated sidewalk facilities signage, changes in pavement material or color could help to define and increase visibility of pedestrian through this area.

Pedestrian crosswalks are recommended at the intersection of North Main Street and 10th Street (4) and Warm Springs Road and 10th Street (5). A RRFB should also be considered to increase pedestrian safety.

Northwestern Catchment Area Context and Recommendations

The northwestern catchment area is comprised of a residential area connected to the southwestern catchment area and motor fueling station via the Wood River Trail and existing sidewalks. Traveling along the trail or sidewalks, pedestrians are likely to travel to the motor fueling station along 10th Street.

Sidewalk and crossing improvement enhancements reflect recommendations along 10th Street outlined for the Southwestern Catchment Area.

Major Pedestrian Access Routes

Pull-in parking exists along many of the major pedestrian access routes and creates gaps in connectivity. While establishing continuous pedestrian facilities along these routes is outside of the scope of the Motor Fueling Station project, future initiatives should engage property and business owners to discuss converting pull-in spaces to angled parking bays. This would create space for the establishment of clear pedestrian zones between the angled parking and front of business, enhancing building fronts and connections to the surrounding area.

Another strategy for establishing continuous pedestrian facilities could include narrowing travel lanes and/or replacing pull-in parking with parallel parking. This would also allow for the establishment buffer area between the sidewalk and travel lanes, enhancing pedestrian comfort. The buffer area could be landscaped and act as snow storage in the winter. This strategy would result in significant loss of parking.

Motor Fueling Station Issues and Recommendations

Proposed plans (figure 2) for the Motor Fueling Station include pedestrian connections to and through the site. Existing proposals illustrate crosswalks across 10th Street and North Main Street, as described in previous catchment area recommendations. Proposed improvements also include ADA ramps at crosswalk sites and a sidewalk along North Main Street. A pedestrian crossing (1) should be considered south of the site in a location that it can be straight and moved away from the lane taper. A second pedestrian crossing should be considered in the illustrated location (2) unless moving to the north where the roadway is narrower could align with Knob Hill Inn Access. The northern crossing location would also require a pedestrian landing/sidewalk area.

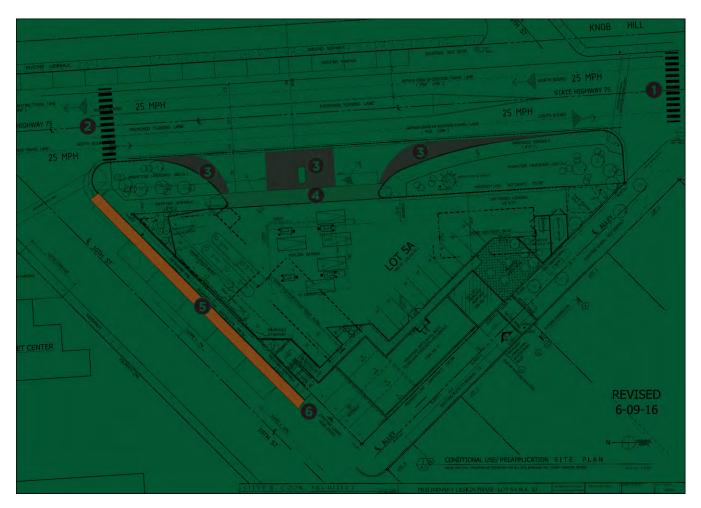


Figure 2: Proposed Site Plan

Pedestrian access to the site could be further enhanced by more clearly defining the pedestrian zone across the vehicle entrance through changes in the hardscape. One strategy is to better define the path for the most common vehicle to access the gas station (the passenger vehicle), while still allowing for the larger fueling trucks and other users to negotiate the entrance. The pictures below (figure 3) illustrate how the visibility of a pedestrian zone is enhanced through the use of colored/stamped pavement. Similar to the treatment below, the combination of rolled curbs and colored/stamped pavement (3) would maintain the wide turning radii required for large vehicles to access the site while lessening the gap in a dedicated pedestrian zone. Colored pedestrian areas (4) would also provide heightened awareness of walkers through primary vehicle access areas.





Figure 3: Stamped/colored pavement with rolled curb

Reducing the eastbound travel lane to 12' would allow for the addition of a 5' landscape area (5). The landscape area would serve as a year-round buffer between pedestrian and vehicle travel and in the winter serve as snow storage. West of this area (6), engineering solutions should be explored to continue the sidewalk beyond the retaining wall.



Steve Cook <steve@stevecookarchitect.com>

Connector Sidewalk from Bracken Station to Frenchmans

Dave Jensen < Dave.Jensen@itd.idaho.gov>

Mon, Jun 27, 2016 at 10:41 AM

To: Josh Gilder <josh@bma5b.com>

Cc: Steve Cook <steve@stevecookarchitect.com>, Sam Stahlnecker <sam@bma5b.com>

The sidewalk design connecting Bracken Station to businesses to the South has been approved by the ITD permit committee.

Thank you,

Dave Jensen TTS

Permit Coordinator

Idaho Transportation Department, D-4

216 South Date Street

Shoshone ID 83352 1521

208-886-7853 office

208-886-7895 fax

208-316-6449 cell

dave.jensen@itd.idaho.gov

From: Josh Gilder [mailto:josh@bma5b.com]

Sent: Monday, June 27, 2016 9:54 AM

To: Dave Jensen

Cc: Steve Cook; Sam Stahlnecker

Subject: Connector Sidewalk from Bracken Station to Frenchmans

[Quoted text hidden]

Retail S Analysis

- Hwy 75 & 10th Street
- Sun Valley ID
- January 2016
- Scenario- Store with Gas
- GmapUSA

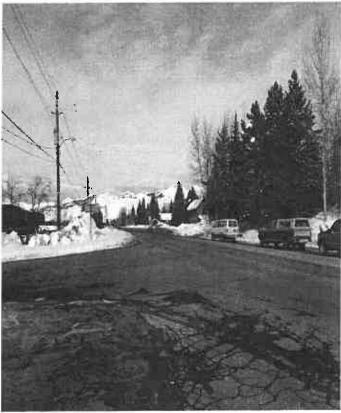


GmapUSA 1023 Hartland Dr Lawrence KS 66049 703 919 2430

Retail S Analysis

- Visibility and Access to Site
- Good Visibility Open view on corner
- Good Access Direct access to site from Hwy 75





GmapUSA 1023 Hartland Dr Lawrence KS 66049 703 919 2430

Retail S Analysis

Investment Parameters

Site-type: Total New Site

Brand: Gas = Sinclair Store = Sun Valley Mart

The scope of this analysis will include the potential of a Gas and C-store site with front parking in Ketchum Idaho.

The analysis was based on a location that will offer 8 gas fueling positions. The store will be 2200 square foot with 7 front parking spots.

Data Field Review

This study is based on survey data collected for competitive retail sites within 1.00 miles of the focus site, which has no direct competition at this time.

A full site survey was completed for each of these sites. Additional demographic, and income data, as well as traffic count information, were utilized in the completion of the study.

Trade Area

The site is located at the corner of Highway 75 and 10th Street in Ketchum Idaho. It is an intersection that does not have a stoplight. The Site will have good visibility and will have good access due to wide open curb cuts and pace of traffic.

The layout with the new store and gas pumps should focus on traffic that travels on Hwy 75, which should continue to carry the main traffic flow past the site.

The population is around 3,200 people within 2.0 miles and the median age is about 47 years old. The population is somewhat lighter than ideal for this type of site location and the median age is a little high for ideal C-store customer base population. However the focus for this site is the winter and especially the summer tourists that pass through this town.

The traffic count for Hwy 75 is estimated to be about 12,000 cars a day.

There are major seasonal variations affecting this trade area.

Competitive Environment- Fuel and C-store

There is no real competition in the local trade area for this location. There are some locations off the main street but offer no real competition to the main traffic flow of this location. They are small format sites and the trade area is completely different that this location's in terms of potential customers.

So overall there is no real competition in the trade area, the opportunity exists to capture some good business at this location.

Site Layout Options

The Layout should focus on Hwy 75 and if possible the store should face the highway. The key for this site is to have a unique design and graphic package outside and inside the store. We would highly recommend using the design firm that has the ability to create a statement store for this area.

It is important that all retail options fit on the lot without interference to each other Convenience of use becomes a high priority when you are creating a new market area.

Business Projections

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One of the keys for this site is to provide a good operation with a good offering that will bring in the commuter that passes by the intersection on a consistent basis. The store size is typically an important factor but in this area the offering and the access to the store are more important. However even more key will be the visibility and access to both the gas and store.

The focus on the merchandising should be having a quality offering that entices the commuter/tourist traffic that passes by the site on a regular basis. The site should have a large fountain and coffee offering to entice the commuters to use this site as their refreshment spot.

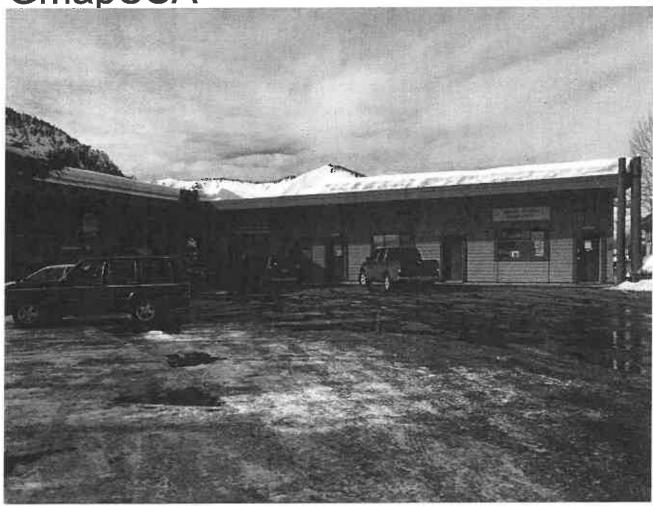
The site is on a key intersection in the local trade area, where there is no real competition within area this location, so care needs to be taken to make sure the car access points for the fuel islands and store are nice and easy with no negative impacts so this site can become a destination stop.

Overall the site is on a good corner is the area and has good potential. The traffic passing by the site is strong and along with the residential backup the location should do well. It might take a while to ramp up this location since the customer has not been conditioned to stop here, but an aggressive pricing posture would speed up the ramp up process.

Field Survey Section Sun Valley Idado

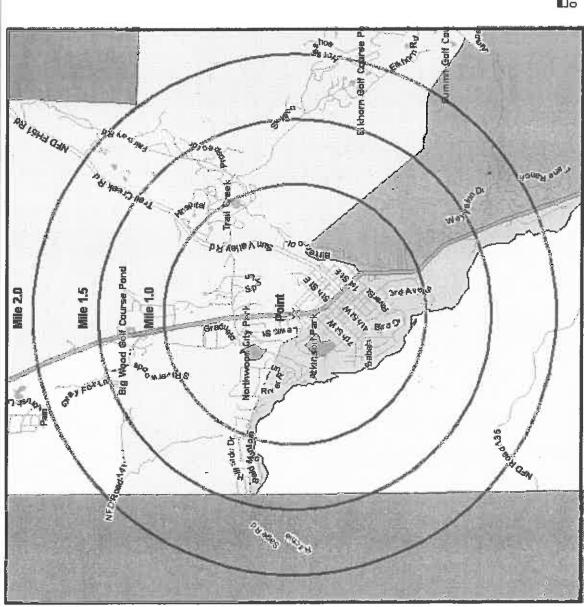
- Hwy 75 & 10th Street
- January 2016

GmapUSA

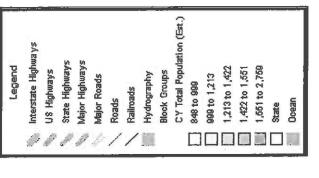


Population Current-Year

Sun Valley Site



Radius - See Appendix for Details Coord: 43.685806, -114.367549 N MAIN ST AT 10TH ST E KETCHUM,ID 83340







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Prepared By: GrnapUSA

Prepared For: Roy Bracken

Population Current-Year

Sun Valley Site

Appendix: Area Listing

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Radius 1 Type:

Radius Definition:

	-114.367549	1.00
	43.685806	00.00
	Center Point: 43.	Circle/Band: 0.0
	N MAIN ST AT 10TH ST E	KETCHUM, ID 83340

Area Name:

Type: Radius 2

Radius Definition:

	-114.367549	1.50
	43.685806	0.00
	Center Point:	Circle/Band:
	N MAIN ST AT 10TH ST E	KETCHUM, ID 83340

Area Name:

Radius 3 Type:

Kadius Definition:			
N MAIN ST AT 10TH ST E	Center Point:	43.685806	-114,367549
KETCHUM, ID 83340	Circle/Band:	0.00	2.00

Project Information:

Site: 2

Order Number: 975616310

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Sun Valley Site

Radius 1: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate Radius 2: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate Radius 3: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate

Description	0.00 - 1.00 mi Radius 1	iles	0.00 - 1.50 mi Radius 2		0.00 - 2.0 0 mi <i>Radius 3</i>	
opulation						
2020 Projection	1,771		2,390		2,989)
2015 Estimate	1,892		2,533		3,153	
2010 Census	2,099		2,782		3,442	
2000 Census	2,369		3,097		3,786	
Growth 2015-2020	-6.39%		-5.65%		-5.22%	
Growth 2010-2015	-9.84%		-8.95%		-8.38%	
Growth 2000-2010	-11.42%		-10.17%		-9.09%	
015 Est. Population by Single-Classification Race	1,892		2,533		3,153	<u> </u>
White Alone	1,694	89.53	2,290	90.41		90.39
Black or African American Alone	4	0.21	4	0.16	5	
Amer. Indian and Alaska Native Alone	5	0.26	7	0.28	10	
Asian Alone	30	1.59	38	1.50	46	
Native Hawaiian and Other Pac. Isl. Alone	0	0.00	1	0.04	2	
Some Other Race Alone	137	7.24	163	6.44	204	
Two or More Races	22	1.16	30	1.18	37	1.17
115 Est. Population by Hispanic or Latino Origin	1,892		2,533		3,153	
Not Hispanic or Latino	1,704	90.06	2,303	90.92		90.77
Hispanic or Latino:	188	9.94	230	9.08		9.23
Mexican	111	59.04	138		177	60.82
Puerto Rican	4	2.13	5	2.17	6	
Cuban	0	0.00	0	0.00	0	
All Other Hispanic or Latino	72	38.30	87	37.83	109	37.46
115 Est. Hisp. or Latino Pop by Single-Class. Race	188		230		291	
White Alone	38	20.21	51	22.17	67	23.02
Black or African American Alone	1	0.53	1	0.43	2	0.69
American Indian and Alaska Native Alone	4	2.13	5	2.17	7	2.41
Asian Alone	5	2.66	6	2.61	6	2.06
Native Hawaiian and Other Pacific Islander Alone	0	0.00	0	0.00	0	0.00
Some Other Race Alone	135	71.81	160	69.57		68.73
Two or More Races	5	2.66	7	3.04	9	3.09

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Prepared By: GmapUSA

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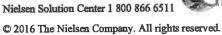
Radius 1: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate Radius 2: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate Radius 3: N MAIN ST AT 10TH ST E. KETCHUM, ID 83340, aggregate

Description		83340, aggregate 0.00 - 1.00 miles <i>Radius 1</i> %		0.00 - 1.50 miles Radius 2 %		0.00 - 2.00 miles <i>Radius 3</i> %	
S Est. Pop by Race, Asian Alone, by Category	30		38		46		
Chinese, except Taiwanese	0	0.00	0	0.00	0	0.0	
Filipino	14	46.67	18	47.37		47.8	
Japanese	6	20.00	8	21.05	10	21.7	
Asian Indian	0	0.00	0	0.00	0		
Korean	1	3.33	1	2.63	1		
Vietnamese	0	0.00	0	0.00	0		
Cambodian	0	0.00	0	0.00	0	•	
Hmong	0	0.00	0	0.00	0		
Laotian	0	0.00	0	0.00	0		
Thai	9	30.00	11.	28.95		28.2	
All Other Asian Races Including 2+ Category	0	0.00	0	0.00	0	0.0	
5 Est. Population by Ancestry	1,892		2,533		3,153		
Arab	0	0.00	0	0.00	0	0.0	
Czech	35	1.85	44	1.74	54	1.7	
Danish	10	0.53	16	0.63	22	0.7	
Dutch	25	1.32	32	1.26	40	1.2	
English	266	14.06	372	14.69	473	15.0	
French (except Basque)	48	2.54	62	2.45	76	2.4	
French Canadian	0	0.00	0	0.00	0	0.0	
German	208	10.99	283	11.17	355	11.2	
Greek	4	0.21	5	0.20	6	0.1	
Hungarian	1	0.05	2	0.08	4	0,1	
Irish	129	6.82	170	6.71	210	6.6	
Italian	55	2.91	72	2.84	88	2.7	
Lithuanian	0	0.00	0	0.00	0	0.0	
United States or American	82	4.33	111	4.38	139	4.4	
Norwegian	101	5.34	130	5.13	159	5.0	
Polish	18	0.95	22	0.87	26	0.8	
Portuguese	0	0.00	1	0.04	1	0.0	
Russian	4	0.21	6	0.24	7	0.2	
Scottish	41	2.17	57	2.25	71	2.2	
Scotch-Irish	47	2.48	58	2.29	70	2.2	
Slovak	0	0.00	1	0.04	2	0.0	
Subsaharan African	0	0.00	0	0.00	0	0.0	
Swedish	30	1.59	39	1.54	47	1.4	
Swiss	5	0.26	7	0.28	8	0.2	
Ukrainian	0	0.00	0	0.00	0	0.0	
Weish	0	0.00	0	0.00	0	0.0	
West Indian (except Hisp. groups)	0	0.00	0	0.00	0	0.0	
Other ancestries	698	36.89	933	36.83	1.161	36.8	

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Sun Valley Site

Radius 1: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate Radius 2: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate Radius 3: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate

Description	0.00 - 1.00 miles <i>Radius 1</i> %		0.00 - 1.50 miles <i>Radius 2</i> %		0.00 - 2.00 miles <i>Radius 3</i> %	
2015 Est. Population by Ancestry	類		-			
Ancestry Unclassified	85	4.49	110	4.34	135	4.28
2015 Est. Pop Age 5+ by Language Spoken at Home	1,825		2,448		3,048	
Speak Only English at Home	1,465	80.27	1,972	80.56	2,459	80.68
Speak Asian/Pac. Isl. Lang. at Home	5	0.27	12	0.49	19	0.62
Speak IndoEuropean Language at Home	48	2.63	61	2.49	74	2.43
Speak Spanish at Home	307	16.82	402	16.42	496	16.2
Speak Other Language at Home	0	0.00	0	0.00	0	0.00
2015 Est. Population by Sex	1,892	_	2,533		3,153	
Male	975	51.53	1,305	51.52	1,624	51.5
Female		48.47		48.48	1,529	
2015 Est. Population by Age	1,892		2,533		3,153	
Age 0 - 4	67	3.54	85	3.36	105	3.33
Age 5 - 9	85	4.49	107	4.22	131	4.15
Age 10 - 14	85	4.49	110	4.34	139	4.41
Age 15 - 17	47	2,48	64	2.53	81	2.57
Age 18 - 20	41	2.17	57	2.25	73	2.32
Age 21 - 24	48	2.54	71	2.80	93	2.95
Age 25 - 34	269	14.22	342	13.50	396	12.56
Age 35 - 44	270	14.27	344	13.58	410	13.00
Age 45 - 54	289	15.27	376	14.84	459	14.56
Age 55 - 64	304	16.07	419	16.54	539	17.09
Age 65 - 74	260	13.74	371	14.65	483	15.32
Age 75 - 84	101	5.34	150	5.92	198	6.28
Age 85 and over	24	1.27	36	1.42	46	1.46
Age 16 and over	1,638	86.58	2,209		2,750	
Age 18 and over	1,607	84.94	2,167	85.55	2,697	85.54
Age 21 and over	1,566	82.77	2.110	83.30	2,624	83.22
Age 65 and over	385	20.35	557	21.99	727	23.06
2015 Est. Median Age	46.1		47.3		48.2	
2015 Est. Average Age	44.6		45.5		46.0	

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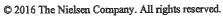
Radius 1: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate Radius 2: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate Radius 3: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate

Description	0.00 - 1.00 mi <u>Radius 1</u>	0.00 - 1.00 miles <i>Radius 1</i> %		les %	0.00 - 2.00 miles Radius 3 %	
015 Est. Male Population by Age	975	.,,	1,305		1,624	
Age 0 - 4	34	3.49	43	3.30	54	3.33
Age 5 - 9	45	4.62	55	4.21	66	
Age 10 - 14	38	3.90	51	3.91	67	4.13
Age 15 - 17	25	2.56	33	2.53	42	
Age 18 - 20	23	2.36	32	2.45	40	
Age 21 - 24	26	2.67	39	2.99	51	
Age 25 - 34	139	14.26		13.56		12.7
Age 35 - 44	155	15.90		15.10		14.29
Age 45 - 54	144	14.77	186	14.25	225	13.8:
Age 55 - 64	156	16.00	214	16.40		16.93
Age 65 - 74	128	13.13	184	14.10		14.8
Age 75 - 84	47	4.82	72	5.52		6.03
Age 85 and over	15	1.54	21	1.61	27	1.60
015 Est, Median Age, Male	45.2		46.4		47.4	
015 Est, Average Age, Male	44.4		45.2		45.8	
115 Est, Female Population by Age	917		1,228		1,529	ı
O15 Est. Female Population by Age	917	3.60	1,228	3.42		3.40
Age 0 - 4		3.60 4.47		3.42 4.23	52	3.40
Age 0 - 4 Age 5 - 9	33		42		52 65	3.40 4.25
Age 0 - 4 Age 5 - 9 Age 10 - 14	33 41	4.47	42 52	4.23	52 65	3.40 4.25 4.71
Age 0 - 4 Age 5 - 9 Age 10 - 14 Age 15 - 17	33 41 47	4.47 5.13	42 52 59	4.23 4.80	52 65 72	3.40 4.25 4.71 2.55
Age 0 - 4 Age 5 - 9 Age 10 - 14 Age 15 - 17 Age 18 - 20	33 41 47 22	4.47 5.13 2.40	42 52 59 31	4.23 4.80 2.52	52 65 72 39 33	3.40 4.25 4.71 2.55 2.10
Age 0 - 4 Age 5 - 9 Age 10 - 14 Age 15 - 17 Age 18 - 20 Age 21 - 24	33 41 47 22 18	4.47 5.13 2.40 1.96	42 52 59 31 25 33	4.23 4.80 2.52 2.04	52 65 72 39 33 42	3.40 4.23 4.73 2.53 2.16 2.73
Age 0 - 4 Age 5 - 9 Age 10 - 14 Age 15 - 17 Age 18 - 20 Age 21 - 24 Age 25 - 34	33 41 47 22 18 23 130	4.47 5.13 2.40 1.96 2.51	42 52 59 31 25 33 164	4.23 4.80 2.52 2.04 2.69	52 65 72 39 33 42	3.40 4.25 4.71 2.55 2.16 2.75 12.42
Age 0 - 4 Age 5 - 9 Age 10 - 14 Age 15 - 17 Age 18 - 20 Age 21 - 24 Age 25 - 34 Age 35 - 44	33 41 47 22 18 23 130	4.47 5.13 2.40 1.96 2.51 14.18	42 52 59 31 25 33 164 148	4.23 4.80 2.52 2.04 2.69 13.36	52 65 72 39 33 42 190 178	3.40 4.25 4.71 2.55 2.16 2.75 12.41 11.6
Age 0 - 4 Age 5 - 9 Age 10 - 14 Age 15 - 17 Age 18 - 20 Age 21 - 24 Age 25 - 34 Age 35 - 44 Age 45 - 54	33 41 47 22 18 23 130	4.47 5.13 2.40 1.96 2.51 14.18 12.54	42 52 59 31 25 33 164 148 190	4.23 4.80 2.52 2.04 2.69 13.36 12.05	52 65 72 39 33 42 190 178 233	3.40 4.25 4.71 2.55 2.16 2.75 12.41 11.64 15.24
Age 0 - 4 Age 5 - 9 Age 10 - 14 Age 15 - 17 Age 18 - 20 Age 21 - 24 Age 25 - 34 Age 35 - 44 Age 45 - 54 Age 55 - 64	33 41 47 22 18 23 130 115 145	4.47 5.13 2.40 1.96 2.51 14.18 12.54 15.81	42 52 59 31 25 33 164 148 190 204	4.23 4.80 2.52 2.04 2.69 13.36 12.05 15.47	52 65 72 39 33 42 190 178 233 264	3.40 4.25 4.71 2.55 2.16 2.75 12.41 11.64 15.24
Age 0 - 4 Age 5 - 9 Age 10 - 14 Age 15 - 17 Age 18 - 20 Age 21 - 24 Age 25 - 34 Age 35 - 44 Age 45 - 54 Age 55 - 64 Age 65 - 74	33 41 47 22 18 23 130 115 145	4.47 5.13 2.40 1.96 2.51 14.18 12.54 15.81 16.14	42 52 59 31 25 33 164 148 190 204	4.23 4.80 2.52 2.04 2.69 13.36 12.05 15.47 16.61	52 65 72 39 33 42 190 178 233 264 242	3.40 4.23 4.73 2.55 2.16 2.73 12.4 11.6 15.2 17.2 15.8
Age 0 - 4 Age 5 - 9 Age 10 - 14 Age 15 - 17 Age 18 - 20 Age 21 - 24 Age 25 - 34 Age 35 - 44 Age 45 - 54 Age 55 - 64	33 41 47 22 18 23 130 115 145 148	4.47 5.13 2.40 1.96 2.51 14.18 12.54 15.81 16.14 14.29	42 52 59 31 25 33 164 148 190 204	4.23 4.80 2.52 2.04 2.69 13.36 12.05 15.47 16.61 15.23	52 65 72 39 33 42 190 178 233 264 242	3.40 4.25 4.71 2.55 2.16 2.75 12.43 11.66 15.24 17.2 15.83 6.54
Age 0 - 4 Age 5 - 9 Age 10 - 14 Age 15 - 17 Age 18 - 20 Age 21 - 24 Age 25 - 34 Age 35 - 44 Age 45 - 54 Age 55 - 64 Age 65 - 74 Age 75 - 84	33 41 47 22 18 23 130 115 145 148 131	4.47 5.13 2.40 1.96 2.51 14.18 12.54 15.81 16.14 14.29 5.89	42 52 59 31 25 33 164 148 190 204 187	4.23 4.80 2.52 2.04 2.69 13.36 12.05 15.47 16.61 15.23 6.35	52 65 72 39 33 42 190 178 233 264 242	3.40 4.25 4.71 2.55 2.16 2.75 12.43 11.64 15.24 17.2 15.83 6.54 1.31

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Sun Valley Site

Radius 1: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate Radius 2: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate Radius 3: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate

Description	0.00 - 1.00 mi <i>Radius 1</i>	iles	0.00 - 1.50 mi Radius 2	les %	0.00 - 2.00 mi <i>Radius 3</i>	les 9
15 Est. Pop Age 15+ by Marital Status	1,654		2,230		2,778	3
Total, Never Married	584	35.31	730	32.74	849	30.5
Males, Never Married	368	22.25	462	20.72	539	19.4
Females, Never Married		13.00	268	12.02	310	11.1
Married, Spouse present	639	38.63	914	40.99	1,210	43.5
Married, Spouse absent	83	5.02	123	5.52	161	5.8
Widowed	52	3.14	87	3.90	116	4.1
Males Widowed	22	1.33	33	1.48	44	1.5
Females Widowed	30	1.81	53	2.38	71	2.5
Divorced	296	17.90	377	16.91	443	15.9
Males Divorced	111	6.71	154	6.91	192	6.9
Females Divorced	186	11.25	223	10.00	251	9.0
5 Est. Pop Age 25+ by Edu. Attainment	1,518		2,038		2,531	
Less than 9th grade	69	4.55	85	4.17	101	3.9
Some High School, no diploma	37	2.44	45	2.21	53	
High School Graduate (or GED)	181	11.92	231	11.33		10.9
Some College, no degree	281	18.51	385	18.89		19.4
Associate Degree	28	1.84	60	2.94		3.4
Bachelor's Degree	701	46.18	921	45.19	1,130	
Master's Degree	164	10.80	210	10.30	251	
Professional School Degree	38	2.50	64	3.14	85	
Doctorate Degree	H 17	1.12	37	1.82	55	2.1
5 Est. Pop Age 25+ by Edu. Attain., Hisp./Lat.	114		140		173	
No High School Diploma	80	70.18	98	70.00	120	69.3
High School Graduate	20	17.54		17.86		17.3
Some College or Associate's Degree	9	7.89	13	9.29	17	
Bachelor's Degree or Higher	4	3.51	5	3.57	6	3.4
usebolds						
2020 Projection	899		1,198		1,484	
2015 Estimate	962		1,272		1,566	1.5
2010 Census	1,071		1,401		1,712	
2000 Census	1,208		1,550		1,867	
Growth 2015-2020	-6.55%		-5.78%		-5.25%	
Growth 2010-2015	-10.17%		-9.24%		-8.55%	
Growth 2000-2010	-11,35%		-9.61%		-8.27%	

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Sun Valley Site

Radius 1: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate Radius 2: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate

Radius 3: N MAIN ST AT 10TH ST E, KETCHUM, ID Description	0.00 - 1.00 miles Radius 1 %		0.00 - 1.50 miles <i>Radius</i> 2 %		0.00 - 2.00 miles <i>Radius 3</i>	
2015 Est. Households by Household Type	962		1,272		1,566	
Family Households	416	43.24	571	44.89	734	46.8
Nonfamily Households		56.76	701	55.11	832	53.13
2015 Est. Group Quarters Population	8		49		76	
2015 HHs by Ethnicity, Hispanic/Latino	58	6.03	70	5.50	87	5.56
2015 Est. Households by HH Income	962		1,272		1,566	<u> </u>
Income < \$15,000	90	9.36	127	9.98	162	10.34
Income \$15,000 - \$24,999	83	8.63	106	8.33	132	8.43
Income \$25,000 - \$34,999	151	15.70	193	15.17		14.62
Income \$35,000 - \$49,999	113	11.75	150	11.79	189	12.07
Income \$50,000 - \$74,999	203	21.10	258	20.28	310	19.80
Income \$75,000 - \$99,999	125	12.99	157	12.34	184	11.75
Income \$100,000 - \$124,999	93	9.67	120	9.43	140	8.94
Income \$125,000 - \$149,999	25	2.60	37	2.91	52	3.32
Income \$150,000 - \$199,999	25	2.60	44	3.46	. 65	4.15
Income \$200,000 - \$249,999	13	1.35	20	1.57	27	
Income \$250,000 - \$499,999	27	2.81	39	3.07	51	
Income \$500,000+	15	1.56	21	1.65	25	1.60
2015 Est. Average Household Income	\$76,207		\$78,250		\$7 <u>9,</u> 592	
2015 Est. Median Household Income	\$55,476		\$55,759		\$55,671	
2015 Median HH Inc. by Single-Class. Race or Eth.						
White Alone	57,037		57,277		57,228	
Black or African American Alone	106,680		104,375		100,504	
American Indian and Alaska Native Alone	62,500		62,500		62,500	
Asian Alone	58,370		58,753		59,672	
Native Hawaiian and Other Pacific Islander Alone	0		56,250		56,250	
Some Other Race Alone	39,903		39,667		39,133	,
Two or More Races	14,999		14,999		14,999	
Hispanic or Latino	58,723		58,516		57,610	
Not Hispanic or Latino	54,989		55,369		55,388	
2015 Est. Family HH Type by Presence of Own Child.	416		571		734	
Married-Couple Family, own children	115	27.64	152	26.62	194	26.43

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Sun Valley Site

Radius 1: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate Radius 2: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate Radius 3: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate

Description	0.00 - 1.00 miles <i>Radius 1</i> %		0.00 - 1.50 miles <i>Radius 2</i> %		0.00 - 2.00 miles <i>Radius 3</i>		
Married-Couple Family, no own children	222	53.37		56.04	422	57.4	
Male Householder, own children	19	4.57	23	4.03	27	3.6	
Male Householder, no own children	10	2.40	13	2,28	16	2.1	
Female Householder, own children	32	7.69	40	7.01	47	6.4	
Female Householder, no own children	18	4.33	22	3.85	26	3.5	
015 Est. Households by Household Size	962		1,272	_	1,566	<u></u>	
1-person	396	41.16	519	40.80	629	40.1	
2-person	358	37.21	484	38.05	605	38.6	
3-person	110	11.43	141	11.08	170	10.	
4-person	66	6.86	85	6.68	108	6.9	
5-person	24	2.49	32	2.52	41	2.6	
6-person	5	0.52	7	0.55	9	0	
7-or-more-person	3	0.31	4	0.31	5	0.3	
115 Est. Average Household Size	1.96		1.95	<u> </u>	1.97		
015 Est. Households by Presence of People Under 18	962		1,272		1,566		
ouseholds with 1 or More People under Age 18:	173	17.98	-	17.61	279	17.8	
Married-Couple Family		67.05		68.75	197	70.0	
Other Family, Male Householder	21	12.14		11.61	30	10.	
Other Family, Female Householder		19.08		18.75	50	17.5	
Nonfamily, Male Householder	1	0.58	1	0.45	1	0.3	
Nonfamily, Female Householder		0,58	1	0.45	1	0.3	
ouseholds with No People under Age 18:	789	82.02	1,048	82.39	1;286	82.	
Married-Couple Family	221	28.01	318	30.34	420	32.	
Other Family, Male Householder	8	1.01	11	1.05	13	1.0	
Other Family, Female Householder	17	2.15	21	2.00	24	1.8	
Nonfamily, Male Householder	294	37.26	376	35.88	448	34.8	
Nonfamily, Female Householder	249	31.56	322	30.73	382	29.	
15 Est. Households by Number of Vehicles	962		1,272		1,566		
No Vehicles	68	7.07	80	6.29		5.5	
1 Vehicle	328	34.10	442	34.75	537	34.2	
2 Vehicles	380	39.50	502	39.47	627	40,0	
3 Vehicles	175	18.19	232	18.24	290	18.5	
4 Vehicles	8	0.83	11	0.86	16	1.0	
5 or more Vehicles	4	0.42	5	0.39	9	0.5	
15 Est. Average Number of Vehicles	1.7		1.7		1.8		

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Radius 3: N MAIN ST AT 10TH ST E. KETCHUM, ID 83340, aggregate

Radius 3: N MAIN ST AT 10TH ST E, KETCHUM, ID Description	0.00 - 1.00 mi Radius 1	les	0.00 - 1.50 mi <i>Radius 2</i>		0.00 - 2.00 mi <i>Radius 3</i>	les 9/
Family Households						
2020 Projection	391		541		699	
2015 Estimate	416		571		734	
2010 Census	460		624		797	
2000 Census	484		649		826	
Growth 2015-2020	-5.98%		-5.17%		-4.75%	
Growth 2010-2015	-9.67%		-8.60%		-7.94%	
Growth 2000-2010	-4.94%		-3.89%		-3.56%	
2015 Est. Families by Poverty Status	416		571		734	
2015 Families at or Above Poverty	410	98.56	563	98.60	723	98.50
2015 Families at or Above Poverty with Children	107	25.72	146	25.57	187	25.48
2015 Families Below Poverty	5	1.20	7	1.23	10	
2015 Families Below Poverty with Children	1	0.24	1	0.18	2	0.27
2015 Est. Pop Age 16+ by Employment Status	1,638		2,209		2,750	
In Armed Forces	0	0.00	0	0.00	0	0.00
Civilian - Employed	1,044	63.74	1,403	63.51	1,744	
Civilian - Unemployed	119	7.26	154	6.97		6.84
Not in Labor Force	474	28.94	652	29.52	819	29.78
2015 Est. Civ. Employed Pop 16+ by Class of Worker	1,061		1,423	_	1,769	
For-Profit Private Workers	648	61.07	888	62.40	1,104	62.4
Non-Profit Private Workers	53	5.00	64	4.50	79	
Local Government Workers	26	2.45	42	2.95	59	
State Government Workers	27	2.54	39	2.74	47	2.66
Federal Government Workers	2	0.19	4	0.28	6	
Self-Employed Workers	302	28.46	385	27.06		26.57
Unpaid Family Workers	1	0.09	2	0.14	4	0.23
2015 Est. Civ. Employed Pop 16+ by Occupation	1,061		1,423		1,769	
Architect/Engineer	44	4.15	57	4.01	66	
Arts/Entertainment/Sports	55	5.18	74	5.20	96	
Building Grounds Maintenance	70	6.60	91	6.39	110	6.22
Business/Financial Operations	13	1.23	21	1.48	30	
Community/Social Services	16	1.51	20	1.41	22	1.24
Computer/Mathematical	2	0.19	6	0.42	8	0.45
Construction/Extraction	115	10.84	142	9.98	169	9.55
Education/Training/Library	36	3.39	55	3.87	69	3.90

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Sun Valley Site

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Radius 3: N MAIN ST AT 10TH ST E, KETCHUM, 1 Description		0.00 - 1.00 miles <i>Radius 1</i> %		iles	0.00 - 2.00 miles <i>Radius 3</i> %	
Farming/Fishing/Forestry	2		Radius 2		11	
Food Prep/Serving	63	5,94	101	7.10	135	
Health Practitioner/Technician	18	1.70	27	1.90	38	
Healthcare Support	27	2.54	31	2.18	34	
Maintenance Repair	19	1.79	27	1.90	39	
Legal	16	1.51	19	1.34	22	
Life/Physical/Social Science	1	0.09	2	0.14	3	
Management	150	14.14	221	15.53	284	16.0
Office/Admin. Support	96	9.05	122	8.57	146	8.25
Production	20	1.89	27	1.90	38	2.15
Protective Services	15	1.41	21	1.48	27	1.53
Sales/Related	190	17.91	232	16.30	272	15.38
Personal Care/Service	47	4.43	67	4.71	85	4.80
Transportation/Moving	45	4,24	56	3.94	65	3.67
2015 Est. Pop 16+ by Occupation Classification	1,061		1,423		1,769)
Blue Collar	199	18.76	252	17.71	311	17.58
White Collar	637	60.04	855	60.08	1,057	59.75
Service and Farm	224	21.11	317			22.67
2015 Est. Workers Age 16+ by Transp. to Work	1,052		1,411		1,753	
Drove Alone	672	63.88	870	61.66	1,070	61.04
Car Pooled	97	9.22	127	9.00	157	8.96
Public Transportation	2	0.19	5	0.35	9	0.51
Walked	76	7.22	125	8.86	162	9.24
Bicycle	74	7.03	89	6.31	102	5.82
Other Means	11	1.05	16	1.13	21	1.20
Worked at Home	120	11.41	.178	12.62	233	13.29
2015 Est. Workers Age 16+ by Travel Time to Work *						
Less than 15 Minutes	753		984		1,188	
15 - 29 Minutes	177		252		333	
30 - 44 Minutes	6		12		22	
45 - 59 Minutes	3		4		6	
60 or more Minutes	6		9		14	
2015 Est. Avg. Travel Time to Work in Minutes	10.20		10.53	·- <u>·</u>	11.00	
015 Est. Occupied Housing Units by Tenure	962		1,272		1,566	
Owner Occupied	568	59.04	778	61 16	989	63.15
Renter Occupied		40.96		38.84		36.78
-						

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Sun Valley Site

Radius 1: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate
Radius 2: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate
Padius 2. N MAIN ST AT 10TH ST E. KETCHIIM, ID 83340, accrecate

Radius 3: N MAIN ST AT 10TH ST E, KETCHUM, ID		0.00 - 1.00 miles		0.00 - 1.50 miles		les
Description	Radius 1	%	Radius 2	%	Radius 3	%
2015 Owner Occ. HUs: Avg. Length of Residence	17.3		17.3		17.4	
2015 Renter Occ. HUs: Avg. Length of Residence	7.9		7.8		·· 7 <u>.7</u>	,
	568		778		989	,
2015 Est. Owner-Occupied Housing Units by Value				686		
Value Less than \$20,000	2		2	0.26		0.30
Value \$20,000 - \$39,999	7	1.23	8	1.03	-11	
Value \$40,000 - \$59,999	4		10	1.29		1.42
Value \$60,000 - \$79,999	2	0.35	3	0.39	6	
Value \$80,000 - \$99,999	9	1.58	12	1.54		1.42
Value \$100,000 - \$149,999	16	2.82	21	2.70	27	
Value \$150,000 - \$199,999	32	5.63	42	5.40	50	
Value \$200,000 - \$299,999	29	5.11	40	5.14	49	
Value \$300,000 - \$399,999	44	7.75	60	7.71	78	
Value \$400,000 - \$499,999	18	3.17	34	4.37	46	
Value \$500,000 - \$749,999	143	25.18	187	24.04		22.65
Value \$750,000 - \$999,999		17.96		17.61		17.39
Value \$1,000,000 or more	160	28.17	222	28,53	294	29.73
2015 Est. Median All Owner-Occupied Housing Value	\$711,172	_	\$710,229		\$718,271	
2015 Est. Housing Units by Units in Structure	2,322		3,297		4,211	
1 Unit Attached	72	3.10	110	3.34	142	3.37
1 Unit Detached	1,159	49.91		47.47	2,010	
2 Units	151	6.50	235	7.13		7.36
3 or 4 Units		17.83		18.87		18.36
5 to 19 Units			581	17.62		17.9
20 to 49 Units	122	5.25	148	4.49		3.97
50 or More Units	19	0.82	22	0.67	26	
Mobile Home or Trailer	10	0.43	13	0.39	29	
Boat, RV, Van, etc.	0	0.00	0	0.00	0	
2015 Est. Housing Units by Year Structure Built	2,322		3,297		4,211	
Housing Units Built 2010 or later	7	0.30	8	0.24	8	0.19
Housing Units Built 2010 of fater Housing Units Built 2000 to 2009		18.52		16.38		15.13
Housing Units Built 1990 to 1999		14:00		14.13		14.94
Housing Units Built 1980 to 1989		14.56		15.13		15.51
	782	33.68		35,55	1,498	
Housing Units Built 1970 to 1979 Housing Units Built 1960 to 1969	180	7.75	276	8.37	372	
	171	7.36	207.	6.28	238	
Housing Units Built 1950 to 1959	171	7.50	ZVI.	0.20	200	

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Sun Valley Site

Radius 1: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate

Radius 2: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate

Radius 3: N MAIN ST AT 10TH ST E, KETCHUM, ID 83340, aggregate

Description	0.00 - 1.00 mil <i>Radius I</i>	3177 31177		0.00 - 1.50 miles <i>Radius 2</i> %		0.00 - 2.00 miles <i>Radius 3</i> %		
Housing Units Built 1940 to 1949	29	1.25	43	1.30	63	1.50		
Housing Unit Built 1939 or Earlier	61	2.63	85	2.58	113	2.68		
2015 Est. Median Year Structure Built**	1979		1979		1979			

^{*}This row intentionally left blank. No total category data is available.



^{**1939} will appear when at least half of the Housing Units in this reports area were built in 1939 or earlier.

Sun Valley Site

Appendix: Area Listing

Area Name:

Type: Radius 1

Reporting Detail: Aggregate

Reporting Level: Block Group

Radius Definition:

N MAIN ST AT 10TH ST E

KETCHUM, ID 83340

Latitude/Longitude 43.685806 -114.367549

Radius

0.00 1.00

Area Name:

Type: Radius 2

Reporting Detail: Aggregate

Reporting Level: Block Group.

Radius Definition:

N MAIN ST AT 10TH ST E KETCHUM, ID 83340

Latitude/Longitude 43.685806 -114.367549

0.00 - 1.50 Radius

Area Name:

Type: Radius 3

Reporting Detail: Aggregate

Reporting Level: Block Group

Radius Definition:

N MAIN ST AT 10TH ST E

KETCHUM, ID 83340

Latitude/Longitude 43.685806 -114.367549

0.00 - 2.00 Radius

Project Information:

Site: 1

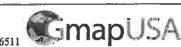
Order Number: 975616310

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Monument Signs

The modular primary ID sign system is also available in a monument structure. The system comprises a Hallmark, price sign and an APC sign panel. Monument signs are installed where local ordinances prohibit pole signs or require smaller signs.

- Mount monument signs in masonry, stucco, metal or wood.
- · Paint the masonry, stucco, metal or wood Chevron GY-210 Gray (Dark) (Dark Gray) unless prohibited by local architectural controls.
- Install the vertical column adjacent to the Hallmark and closest to the street.
- · Monument signs are available in the three sizes noted.
- Only 3-product price signs are available.
- Both numeral and product grade panels are changeable.
- · When using monument signs with Grand Entrance architecture, paint the base Dark Gray or Havana Cream.

≪ Street Side



The monument primary ID includes the Hallmark and price sign.

Туре	Width	Height	Area Sq. Ft.
C-30M (Hallmark and price sign)	5'-1 3/4"	2'-10 3/4"	14.9
C-30M APC	5'-1 3/4"	0'-8 1/2"	3.6
C-32M (Hallmark and price sign)	6'-11 1/2"	3'-10"	26.2
C-32M APC	6'-11 1/2"	0'-93/4"	5.7
C-45M (Hallmark and price sign)	8'- 5 1/2"	4'-8"	38.8
C-45M APC	8'-5 1/2"	0'-93/4"	6.9

Primary ID Sign - Monument Retrofit

Before



After



Legacy Monument Sign (Retrofit)

- Replace legacy Hallmark faces with new Hallmark face panels across all levels.
- The new Hallmark logo and 'with TECHRON' logo panel are required.
- Replace APC faces with new design faces. Short height/ Long APCs should be refaced and converted to 2 individual faces side by side, separated by an "H" bar divider.
- Replace legacy LPS faces with new Illuminated Price Sign faces.
- In cases where Chevron branded Diesel is available at the facility, install a new Green Pricer panel below the LPS, or reface the existing diesel pricer.
- Paint monument bases Dark Gray or to match building wainscot color if the C-Store is done in the Chevron approved color scheme.

Introduction

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Decision Trees

Signage

Canopy

Fueling Dispensers and Fixtures

Backcourt Building Treatments

Highway Signs

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IDAPA 58 TITLE 01 CHAPTER 07

58.01.07 - RULES REGULATING UNDERGROUND STORAGE TANK SYSTEMS

000. LEGAL AUTHORITY.

Chapters 1 and 88, Title 39, Idaho Code, grant authority to the Board of Environmental Quality to promulgate rules for the regulation of underground storage tank systems within the state of Idaho. (4-2-08)

001. TITLE AND SCOPE.

- **01.** Title. These rules shall be cited as IDAPA 58.01.07, "Rules Regulating Underground Storage Tank Systems." (4-2-08)
- **802. Scope**. These rules establish standards and procedures necessary for the regulation of underground storage tank systems. Compliance with these rules shall not relieve persons from the obligation to comply with other applicable state or federal laws. (4-2-08)

002. WRITTEN INTERPRETATIONS.

As described in Section 67-5201(19)(b)(iv), Idaho Code, the Department of Environmental Quality may have written statements which pertain to the interpretation of these rules. If available, such written statements can be inspected and copied at cost at the Department of Environmental Quality, 1410 N. Hilton, Boise, Idaho 83706-1255. (4-2-08)

003. ADMINISTRATIVE PROVISIONS.

Persons may be entitled to appeal agency actions authorized under these rules pursuant to IDAPA 58.01.23, "Rules of Administrative Procedure Before the Board of Environmental Quality." (4-2-08)

004. INCORPORATION BY REFERENCE.

Any reference to any document identified in Subsection 004.01 shall constitute the full adoption by reference into IDAPA 58.01.07. (4-2-08)

01. Documents Incorporated by Reference. Technical Standards and Corrective Action Requirements for Owners and Operators of Underground Storage Tanks, 40 CFR Part 280, revised as of July 1, 2007. (4-2-08)

02. Hazardous Substance Underground Storage Tank Systems.

- a. The following items only apply to hazardous substance underground storage tank systems and do not apply to petroleum underground storage tank systems: (4-2-08)
- i. The definition of "Hazardous substance UST system" in 40 CFR 280.12 and use of this term or regulations regarding hazardous substance in 40 CFR Part 280; and (4-2-08)
 - ii. 40 CFR 280.42 and any reference to 40 CFR 280.42 in 40 CFR Part 280. (4-2-08)

(4-2-08)

- **b.** All other provisions of 40 CFR Part 280 and all provisions of IDAPA 58.01.07 shall apply to hazardous substance underground storage tank systems. (4-2-08)
- **03. Consistency**. In the event of conflict or inconsistency between the language in IDAPA 58.01.07 and that found in 40 CFR Part 280, IDAPA 58.01.07 shall prevail. (4-2-08)
- **04. Stringency**. IDAPA 58.01.07 shall be no more stringent than federal law or regulations governing underground storage tank systems. (4-2-08)
- **05. Availability of Referenced Material**. The federal regulations adopted by reference can be obtained at the following locations: (4-2-08)
 - a. U.S. Government Printing Office, www.ecfr.gov; and (4-2-08)

Section 000 Page 2

b. Department of Environmental Quality, Hearing Coordinator, 1410 N. Hilton, Boise, ID 83706-1255, (208)373-0502. (4-2-08)

005. OFFICE HOURS -- MAILING ADDRESS AND STREET ADDRESS.

The state office of the Department of Environmental Quality and the office of the Board of Environmental Quality are located at 1410 N. Hilton, Boise, Idaho 83706-1255, (208) 373-0502, www.deq.idaho.gov. The office hours are 8 a.m. to 5 p.m. Monday through Friday. (4-2-08)

006. CONFIDENTIALITY OF RECORDS.

Information obtained by the Department under these rules is subject to public disclosure pursuant to the provisions of Title 74, Chapter 1, Idaho Code, and IDAPA 58.01.21, "Rules Governing the Protection and Disclosure of Records in the Possession of the Idaho Department of Environmental Quality." (4-2-08)

007. -- 009. (RESERVED)

010. **DEFINITIONS.**

For the purpose of the rules contained in IDAPA 58.01.07, "Rules Regulating Underground Storage Tank Systems," the following definitions apply: (4-2-08)

- **01. Board**. The Idaho Board of Environmental Quality. (4-2-08)
- **02. Community Water System.** A public water system that serves at least fifteen (15) service connections used by year-round residents of the area served by the system or regularly serves at least twenty-five (25) year-round residents. (4-2-08)
 - **O3. Department**. The Idaho Department of Environmental Quality. (4-2-08)
 - **O4. Director**. The Director of the Idaho Department of Environmental Quality or his authorized agent. (4-2-08)
- **05. Existing.** Solely for purposes of determining when secondary containment is required, existing is when a petroleum underground storage tank, piping, motor fuel dispensing system, facility, public water system or potable drinking water well is in place when a new installation or replacement of a tank, piping, or motor fuel dispensing system begins. (4-2-08)
 - **06. EPA.** The United States Environmental Protection Agency. (4-2-08)
- **O7. Installation of a New Motor Fuel Dispenser System**. The installation of a new motor fuel dispenser and the equipment necessary to connect the dispenser to the petroleum underground storage tank system. This equipment may include flexible connectors, risers, or other transitional components that are beneath the dispenser, below the shear valve, and connect the dispenser to the piping. It does not mean the installation of a motor fuel dispenser installed separately from the equipment needed to connect the dispenser to the petroleum underground storage tank system. (4-2-08)
- **08. Installer**. Any person who installs a new or replacement petroleum underground storage tank system. (4-2-08)
- **Motor Fuel.** Petroleum or a petroleum-based substance that is motor gasoline, aviation gasoline, No. 1 or No. 2 diesel fuel, or any grade of petroleum-blended gasohol, and is typically used in the operation of a motor engine. This includes blended petroleum motor fuels such as biodiesel and ethanol petroleum blends. (4-2-08)
- 10. New Underground Storage Tank. Has the same meaning as "underground storage tank or UST" in 40 CFR 280.12, except that such term includes tanks that have been previously used and meet the requirements of 40 CFR 280.20(a). (4-2-08)
 - 11. Non-Community Water System. A public water system that is not a community water system. A

Section 005 Page 3

non-community water system is either a transient non-community water system or a non-transient non-community water system. (4-2-08)

- **12. Person**. An individual, trust, firm, joint stock company, federal agency, corporation, state, municipality, commission, political subdivision of a state, or any interstate body. "Person" also includes a consortium, a joint venture, a commercial entity, and the United States government. (4-2-08)
- **13. Piping**. A hollow cylinder or a tubular conduit constructed of non-earthen materials that routinely contains and conveys regulated petroleum substances from the petroleum underground storage tank(s) to the dispenser(s) or other end-use equipment. It does not mean vent, vapor recovery, or fill lines that do not routinely contain regulated petroleum substances. (4-2-08)
- 14. Potable Drinking Water Well. Any hole (dug, driven, drilled, or bored) that extends into the earth until it meets ground water which supplies water for a non-community public water system or otherwise supplies water for household use (consisting of drinking, bathing, and cooking, or other similar uses). Such wells may provide water to entities such as a single-family residence, group of residences, businesses, schools, parks, campgrounds, and other permanent or seasonal communities. (4-2-08)
- 15. **Product Deliverer**. Any person who delivers or deposits product into a petroleum underground storage tank. This term may include major oil companies, jobbers, petroleum transportation companies, or other product delivery entities. (4-2-08)
- 16. Public Water System. A system for the provision to the public of water for human consumption through pipes or, after August 5, 1998, other constructed conveyances, if such system has at least fifteen (15) service connections or regularly serves an average of at least twenty-five (25) individuals daily at least sixty (60) days out of the year. Such term includes: any collection, treatment, storage, and distribution facilities under control of the operator of such system and used primarily in connection with such system; and, any collection or pretreatment storage facilities not under such control which are used primarily in connection with such system. Such term does not include any "special irrigation district." A public water system is either a "community water system" or a "non-community water system."
- 17. Red Tag. A tamper-resistant tag, device, or mechanism attached to the tank's fill pipes that clearly identifies a petroleum underground storage tank as ineligible for product delivery. The tag or device shall be visible to the product deliverer and shall clearly state that it is unlawful to deliver to, deposit into, or accept product into the ineligible petroleum underground storage tank.

 (4-2-08)
- **18. Repair.** Solely for purposes of determining when secondary containment is required, as it applies to petroleum underground storage tanks, piping, and motor fuel dispensers systems, repair means any activity that does not meet the definition of replace. (4-2-08)
- **Replace**. As it applies to petroleum underground storage tanks and piping, replace is defined as follows: (4-2-08)
- **a.** Petroleum Underground Storage Tank. Replace means to remove an existing tank and install a new tank. (4-2-08)
- **b.** Piping. Replace means to remove and put back in one hundred (100) percent of the piping, excluding connectors, connected to a single petroleum underground storage tank system. This definition does not alter the requirement in 40 CFR 280.33(c) to replace metal pipe sections and fittings that have released product as a result of corrosion or other damage. A replacement of metal pipe section and fittings pursuant to 40 CFR 280.33(c) shall be considered a replacement under this definition only if one hundred (100) percent of the metal piping, excluding connectors, is replaced. (4-2-08)
- **20. Secondary Containment**. A release detection and prevention system that meets the requirements of 40 CFR 280.43(g). The piping shall have an inner and outer barrier and a method of monitoring the space between the inner and outer barriers for a leak or release. (4-2-08)

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- **21. Under-Dispenser Spill Containment**. Containment underneath a dispenser that will prevent leaks from the dispenser from reaching soil or ground water. Such containment must: (4-2-08)
 - **a.** At installation or modification, be liquid-tight on its sides, bottom, and at any penetrations; and (4-2-08)
 - **b.** Be compatible with the substance conveyed by the piping; and either (4-2-08)
 - c. Allow for visual inspection and access to the components in the containment system; or (4-2-08)
- **d.** Be monitored for releases using a release detection method that meets the requirements of 40 CFR 280.43(g). (4-2-08)
- 011. 099. (RESERVED)

100. ADDITIONAL MEASURES TO PROTECT GROUND WATER FROM CONTAMINATION.

01. Notification. An owner, operator or designee must:

- (4-2-08)
- a. Provide written notice to the Department thirty (30) days prior to the installation of a new piping system or a new or replacement petroleum underground storage tank. (4-2-08)
- **b.** Provide notice to the Department twenty-four (24) hours prior to the installation of a replacement piping system. (4-2-08)
- **02. Notification Forms**. The written notice required in Subsection 100.01.a. shall be made upon forms provided by the Department. (4-2-08)
- **03.** Requirements for Petroleum UST Systems. Owners, operators, and installers of a new or replacement petroleum underground storage tank or piping system shall comply with the following requirements.

 (4-2-08)
- a. Each new petroleum underground storage tank, or piping connected to any such new tank, installed after February 23, 2007, or any existing petroleum underground storage tank, or existing piping connected to such existing tank, that is replaced after February 23, 2007, shall have secondary containment and be monitored for leaks if the new or replaced petroleum underground storage tank or piping is within one thousand (1,000) feet of any existing public water system or any existing potable drinking water well. At a minimum, secondary containment systems must be designed, constructed, and installed to contain regulated substances released from the tank system until they are detected and removed, prevent the release of regulated substances to the environment at any time during the operational life of the petroleum underground storage tank system, and be checked for evidence of a release at least every thirty (30) days. The following conditions are excluded:

 (4-2-08)
 - i. Suction piping that meets the requirements of 40 CFR 280.41(b)(2)(i) through (v); (4-2-08)
 - ii. Piping that manifolds two (2) or more petroleum underground storage tanks together; (4-2-08)
 - iii. Existing piping to which new piping is connected to install a dispenser; and (4-2-08)
 - iv. Tanks identified in 40 CFR 280.10(b). (4-2-08)
- **b.** If the owner installs, within one (1) year, a potable drinking water well at the new facility that is within one thousand (1,000) feet of the petroleum underground tanks, piping, or motor fuel dispenser system as part of the new underground storage tank facility installation, secondary containment and under-dispenser containment are required, regardless of whether the well is installed before or after the petroleum underground tanks, piping, and motor fuel dispenser system are installed. (4-2-08)
 - **c.** The notice required in Subsection 100.01 shall indicate whether the new or replacement installation

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is within one thousand (1,000) feet of an existing public water system or any existing potable drinking water well. If the owner and installer certify that the installation is not within one thousand (1,000) feet of an existing public water system or any existing potable drinking water well, the owner, operator or designee shall provide and maintain documentation showing that a reasonable investigation of water systems and drinking water wells was undertaken. A reasonable investigation includes, but is not limited to, a search of the records of:

(4-2-08)

- i. The public or private water service provider in the area which the new or replacement installation is located (if any); (4-2-08)
 - ii. The city or county in which the new or replacement installation is located; (4-2-08)
 - iii. The Idaho Department of Water Resources; and (4-2-08)
 - iv. The Idaho Department of Environmental Quality. (4-2-08)
- **d.** In the case of a replacement of an existing petroleum underground storage tank or existing piping connected to the petroleum underground storage tank, Section 100 shall apply only to the specific petroleum underground storage tank or piping being replaced, not to other petroleum underground storage tanks and connected pipes comprising such system. (4-2-08)
- e. Each installation of a new motor fuel dispenser system shall include under-dispenser spill containment if the new dispenser is within one thousand (1,000) feet of any existing public water system or any existing potable drinking water well. (4-2-08)
- **04.** Requirements for Hazardous Substance UST Systems. Owners, operators, and installers of a new or replacement hazardous substance underground storage tank or piping system shall have secondary containment as required in 40 CFR 280.42. (4-2-08)
- **05. Certification**. Owners and operators shall also comply with the certification requirements of 40 CFR 280.22(f) as incorporated by reference into these rules. (4-2-08)

101. -- 199. (RESERVED)

200. RELEASE REPORTING REQUIREMENTS.

01. Information to be Reported.

(4-2-08)

- **a.** In addition to the requirements in IDAPA 58.01.02, "Water Quality Standards," Subsection 851.01, owners or operators shall report the following information regarding confirmed petroleum underground storage tank releases to the Department on forms provided by the Department: (4-2-08)
 - i. The release source; and (4-2-08)
 - ii. The release cause. (4-2-08)
- **b.** Releases less than twenty-five (25) gallons that are cleaned up within twenty-four (24) hours, and which do not cause a sheen on nearby surface water, do not need to be reported. (4-2-08)
 - **02.** Release Sources. Release sources may include, but are not limited to the following: (4-2-08)
 - **a.** Petroleum Underground Storage Tanks; (4-2-08)
 - **b.** Piping; (4-2-08)
- **c.** Dispensers, which include the dispenser and equipment used to connect the dispenser to the piping. A release from a suction pump or components located above the shear valve would be an example of a release from the dispenser; (4-2-08)

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- **d.** Submersible turbine pump area, which includes the submersible turbine pump head (typically located in the tank sump), the line leak detector, and the piping that connects the submersible turbine pump to the petroleum underground storage tank; and (4-2-08)
- e. Delivery problem, which identifies releases that occurred during product delivery to the petroleum underground storage tank. Typical causes associated with this source are spills and overfills. (4-2-08)
 - **03.** Release Causes. Release causes may include, but are not limited to the following: (4-2-08)
- a. Spills which may occur when the delivery hose is disconnected from the fill pipe of the petroleum underground storage tank or when the nozzle is removed from the vehicle at the dispenser; (4-2-08)
- **b.** Overfills which may occur from the fill pipe at the petroleum underground storage tank or when the nozzle fails to shut off at the dispenser; (4-2-08)
- **c.** Physical or mechanical damage of all types except corrosion. Examples include a puncture of the petroleum underground storage tank or piping, loose fittings, broken components, and components that have changed dimension like elongation or swelling; (4-2-08)
 - **d.** Corrosion of a metal tank, piping, flex connector, or other component; and (4-2-08)
- **e.** Installation problem that occurs specifically because the underground storage tank system was not installed properly. (4-2-08)
- **Q4.** Requirements. The reporting required in Section 200 shall be reported to the Department within ninety (90) days of a confirmed release. The reporting requirement in Section 200 shall not relieve owners or operators from the obligation to comply with IDAPA 58.01.02, "Water Quality Standards," Section 851, "Petroleum Release Reporting, Investigation, and Confirmation," and IDAPA 58.01.02, "Water Quality Standards," Section 852, "Petroleum Release Response and Corrective Action." (4-2-08)

201. -- 299. (RESERVED)

300. TRAINING REQUIREMENTS.

- **01. Requirements**. The Department shall adopt a training program to help owners and operators comply with the requirements of these rules. The training program requirements shall: (4-2-08)
- - **b.** Be developed in cooperation with petroleum underground storage tank owners and tank operators; (4-2-08)
- **c.** Take into consideration training programs implemented by petroleum underground storage tank owners and operators as of August 8, 2005; (4-2-08)
 - **d.** Provide for training to be conducted on site or at another mutually convenient location; and (4-2-08)
 - **e.** Be appropriately communicated to petroleum underground storage tank owners and operators. (4-2-08)
- **02. Operator Designation**. For each petroleum underground storage tank system regulated under these rules, the owner or operator shall: (4-2-08)
 - **a.** Designate: (4-2-08)
 - i. The class A operator, who is the individual(s) having primary responsibility for on-site operation

Section 300 Page 7

and maintenance of the petroleum underground storage tank system. This does not require that the class A operator be on site; (4-2-08)

- ii. The class B operator, who is the individual(s) having daily on-site responsibility for the operation and maintenance of the petroleum underground storage tank system. This does not require that the class B operator be on site at all times; and (4-2-08)
- iii. The class C operator, who is the daily, on-site individual(s) having primary responsibility for addressing emergencies presented by a spill or release from the petroleum underground storage tank system. The class C operator can be designated by the class A or B operator. (4-2-08)
- **b.** Maintain a record at the facility where the petroleum underground storage tank is located listing each person designated in Subsections 300.02.a.i., 300.02.a.ii., and 300.02.a.iii. (4-2-08)
- **c.** Notify the Department in writing of the individual(s) designated in Subsections 300.02.a.i. and 300.02.a.ii. within thirty (30) days of the designation. (4-2-08)
- **O3. Training.** The owner or operator of each petroleum underground storage tank system regulated under these rules shall ensure that the individual(s) identified in Subsections 300.02.a.i. and 300.02.a.ii. participate in the training conducted by the Department or a state of Idaho approved third party. (4-2-08)
- a. The individual(s) identified in Subsections 300.02.a.i. or 300.02.a.ii. shall provide training to the persons identified in Subsection 300.02.a.iii. (4-2-08)
- **b.** The individual(s) identified in Subsection 300.02.a.iii. must be trained before assuming responsibility for responding to emergencies. (4-2-08)
- **c.** The individual(s) identified in Subsections 300.02.a.i. and 300.02.a.ii. shall repeat the training within thirty (30) days if the petroleum underground storage tank system for which they have responsibility is determined to be out of compliance with these rules. (4-2-08)
- **04. Unattended Sites**. In the case of unattended sites, a sign must be posted in a location visible from the dispensers indicating emergency shut-off procedures and emergency contact phone numbers. (4-2-08)

301. -- 399. (RESERVED)

400. INSPECTIONS.

O1. Department Authority. In order to fulfill the statutory requirements of Chapter 88, Title 39, Idaho Code, officers, employees or representatives of the Department, or third-party inspectors as described in Subsection 400.02, are authorized to inspect petroleum underground storage tanks, contents of the tanks, and associated equipment and records relating to such tanks, contents, and associated equipment. (4-2-08)

02. Third-Party Inspections.

(4-2-08)

- a. Third-party inspectors must be certified, licensed, or registered by an approved state program to perform on-site inspections. At a minimum, third-party inspectors must meet the requirements listed in Subsections 400.02.a.i. through 400.02.a.v.: (4-2-08)
- i. Be trained in the state-specific inspection protocols and procedures, and perform inspections pursuant to such protocols and procedures; (4-2-08)
- ii. Successfully complete the state's required training program. The training program for third-party inspectors must be comparable to the training program for Department inspectors; (4-2-08)
- iii. Not be the owner or operator of the petroleum underground storage tank, an employee of the owner or operator of the petroleum underground storage tank, or a person having daily on-site responsibility for the operation and maintenance of the petroleum underground storage tank; (4-2-08)

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- Use an inspection report form developed by the Department. Review of applicable records and other activities that can be accomplished off-site may be combined with activities conducted at the site to fulfill the on-site inspection requirement; and (4-2-08)
- Complete and submit the inspection report to the Department in the manner and time frame established by the Department. All third-party inspection reports must be submitted electronically to the Department for review and for the Department to make a compliance determination for each site. If requested by the Department, third-party inspectors shall provide all supporting documentation for its inspection reports.
- Third-party inspection procedures must contain an audit program, developed by the Department, to monitor third-party inspectors on a routine basis. The audit program must include a sufficient number of on-site inspections to effectively assess inspector performance.
- If a third-party inspector fails to demonstrate to the approved state program adequate competence and proficiency to perform petroleum underground storage tank inspections, or the approved state program otherwise determines it is not appropriate for the third-party inspector to conduct on-site inspections as part of a third-party inspection program, the approved state program must take appropriate action against the third-party inspector as provided by law. (4-2-08)
- 03. **Inspections.** All inspections shall be done in accordance with the provisions of Section 39-108, Idaho Code. At a minimum, an on-site inspection must assess compliance with the following: (4-2-08)

a.	Notification;	(4-2-08)
b.	Corrosion protection;	(4-2-08)
c.	Overfill prevention in place and operational;	(4-2-08)
d.	Spill prevention in place and operational;	(4-2-08)
e.	Tank and piping release detection;	(4-2-08)
f.	Reporting suspected releases;	(4-2-08)
g.	Records of tank and piping repairs;	(4-2-08)
h.	Secondary containment where required;	(4-2-08)
i.	Financial responsibility; and	(4-2-08)
j.	Temporary closure.	(4-2-08)

401. -- 499. (RESERVED)

500. DELIVERY PROHIBITION.

- **Prohibition**. Effective August 8, 2007, it shall be unlawful for any person to deliver to, deposit into, or accept a regulated petroleum substance into a petroleum underground storage tank at a facility which has been identified by the Department to be ineligible for such delivery, deposit, or acceptance. (4-2-08)
- Classification as Ineligible. The Department shall classify a petroleum underground storage tank as ineligible for delivery, deposit, or acceptance of a regulated petroleum substance as soon as practicable after the Department determines one or more of the following conditions exists: (4-2-08)
 - Required spill prevention equipment is not installed; (4-2-08)

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- **b.** Required overfill protection equipment is not installed; (4-2-08)
- c. Required leak detection equipment is not installed; or (4-2-08)
- **d.** Required corrosion protection equipment is not installed. (4-2-08)
- **03. Warning of Violations**. The Department may classify a petroleum underground storage tank as ineligible for delivery, deposit, or acceptance of a regulated petroleum substance if the owner or operator of the tank has been issued a written warning for any of the following violations, and the owner or operator fails to initiate corrective action within thirty (30) days of the issuance of the written warning, unless the deadline is extended by the Department:

 (4-2-08)
 - **a.** Failure to properly operate or maintain leak detection equipment; (4-2-08)
 - **b.** Failure to properly operate or maintain spill, overfill, or corrosion protection equipment; or (4-2-08)
 - c. Failure to maintain financial responsibility. (4-2-08)
- **O4. Service of Notice**. If the Department classifies a petroleum underground storage tank as ineligible for delivery, deposit, or acceptance of a regulated petroleum substance pursuant to Subsections 500.02 or 500.03, the Department shall provide a written notice of the determination to the owner or operator prior to prohibiting the delivery, deposit, or acceptance of a regulated petroleum substance. Notice is considered properly served by the Department in any of the following ways:

 (4-2-08)
 - **a.** The notice is personally delivered to the owner or operator; or (4-2-08)
- **b.** The notice is clearly posted at a public entrance to the facility where the petroleum underground storage tank is located and a copy of the notice is also sent by **certified** mail to the last known address of the owner or operator. (4-2-08)
- **05. Red-Tagging**. Once service of the written notice of the ineligible determination is complete, the Department shall then attach a red tag to each fill pipe of the ineligible petroleum underground storage tank clearly identifying the tank as ineligible. The Department shall also maintain a list of all petroleum underground storage tanks that are classified as ineligible for delivery, deposit, or acceptance of a regulated petroleum substance. The Department shall make the list available to the public by posting the list on the Department's website at www.deq.idaho.gov. (4-2-08)
 - **06. Written Notice**. The written notice required by Subsection 500.04 must include: (4-2-08)
 - **a.** The specific reasons or violations that led to the ineligible classification; (4-2-08)
- **b.** A statement notifying the owner and operator that the petroleum underground storage tank is ineligible for delivery and it is unlawful for any person to deliver to, deposit into, or accept a regulated petroleum substance into the petroleum underground storage tank; (4-2-08)
 - c. The effective date the petroleum underground storage tank is deemed ineligible for delivery; (4-2-08)
- **d.** The name and address of the department representative to whom a written request for re-inspection can be made, if a re-inspection is necessary; (4-2-08)
- e. A statement regarding the right to appeal the Department's action regarding ineligible classification pursuant to IDAPA 58.01.23, "Rules of Administrative Procedure Before the Board of Environmental Quality"; and (4-2-08)
 - **f.** The option to request a compliance conference pursuant to Subsection 500.07. (4-2-08)

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- **O7. Compliance Conference.** The owner or operator may request a compliance conference with the Department within fifteen (15) days of receipt of the notice. A compliance conference shall be scheduled within twenty (20) days and conducted in an informal manner by the Department. At the compliance conference, the owner or operator may explain why he believes the petroleum underground storage tank should not be classified as ineligible. During the compliance conference, the owner or operator and the Department will identify and establish appropriate acts and a time schedule for compliance as necessary. (4-2-08)
- **O8. Duration of Ineligible Classification**. The classification of a petroleum underground storage tank as ineligible shall remain in effect until the conditions cited in the notice no longer exist. If the Department determines that an ineligible storage tank has returned to compliance and is now eligible for delivery, deposit, or acceptance of a regulated petroleum substance, the Department or an authorized designee shall, as soon as practicable, remove the red tag from the petroleum underground storage tank and also remove the petroleum underground storage tank from the ineligible list posted on its website. The Department will also send a written notice to the owner and operator that an ineligible storage tank has returned to compliance and is now eligible for delivery, deposit, or acceptance of a regulated petroleum substance. (4-2-08)
- **09. Declining Classification**. The Director may decline to classify a petroleum underground storage tank as ineligible if the Director decides that classifying the petroleum underground storage tank as ineligible for delivery, deposit, or acceptance is not in the best interest of the public. (4-2-08)
- a. The Director may only defer application of delivery prohibition for up to one hundred eighty (180) days after determining a petroleum underground storage tank is ineligible for delivery, deposit, or acceptance of a regulated petroleum substance. (4-2-08)
- **b.** The Director may authorize the delivery, deposit, or acceptance of product into an ineligible petroleum underground storage tank if such activity is necessary to test or calibrate the underground storage tank or dispenser system. (4-2-08)
- **10. Department Authority.** Nothing in Section 500 shall affect or preempt the authority of the Department to prohibit the delivery, deposit, or acceptance of a regulated petroleum substance to a petroleum underground storage tank under other existing authorities. (4-2-08)
- 11. **Proper Notice**. A person shall not be in violation of Subsection 500.01 if the Department fails to provide the notice required by Subsections 500.04 and 500.05. (4-2-08)
- **12. Unlawful to Tamper with Red Tag.** It shall be unlawful for any person to tamper with and/or remove the red tag without the Department's approval. (4-2-08)

501. -- 599. (RESERVED)

600. PETROLEUM UNDERGROUND STORAGE TANK DATABASE.

- **01. Maintenance**. The Department shall maintain a database which provides details on the status of all petroleum underground storage tanks in the state of Idaho which are subject to regulation. The database shall be updated no less than the end of each calendar quarter. (4-2-08)
 - **02. Identification**. The database shall identify any tanks subject to delivery prohibition. (4-2-08)
- **Q3. Petition**. Petroleum underground storage tank owners or operators may petition the Department to correct any inaccurate information for their tanks and the Department shall correct any such inaccurate information within thirty (30) days after verification. (4-2-08)
- **04. Availability**. The database shall be available to the public on the Department's website at www.deq.idaho.gov. (4-2-08)

601. -- 999. (RESERVED)

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MEMORANDUM

TO:

Jim Anderson

Xerxes Corporation 1210 N. Tustin Ave. Anaheim, CA 92807

FROM:

J.M. Plecnik

Consultant

1880 San Anseline Long Beach, CA 90815

(562) 985-4406

DATE:

September 11, 2007

SUBJECT:

Seismic Behavior of Xerxes Underground Tanks

The behavior of Xerxes underground tanks under seismic loading can best be summarized in two different categories. The first category would consider seismic loads occurring at some distance away from the tank, so that the rupture of the earth's crust is not in the immediate vicinity of the underground tank. That is, any rupture in the earth's crust is not occurring directly at the site of the tank. In this case, the behavior of the seismic tank is similar to that of a ball in the ocean that is subjected to an oncoming wave. Since the size of the ball is much smaller than the wave, the ball will simply move up and down with the wave motion. Likewise, in the case of an earthquake which also emits energy waves (ground waves similar to that of water in the ocean), the underground tank simply rides this energy wave without causing large localized forces on the tank itself. The second category is more critical and it consists of the rupture (fault lines) of the earth's crust occurring at or very near the location of the tank. In this case, the tank is incapable of surviving the rupture of the earth's crust and, like the soil, will also rupture.

In mid 1990's, Xerxes Corporation commissioned the CSULB Structures Lab, under the direction of J.M. Plecnik, to analyze 8 ft. diameter Xerxes underground tanks subjected to

seismic loading. This research effort culminated in several lengthy reports, including the May 28, 1997 report titled *Evaluation of 8 ft. Diameter Xerxes Underground Tanks Under Seismic Loading*. This report was utilized to obtain a design US Patent, number US 6,397,168 B1, dated May 28, 2002. The basic conclusions of this research effort are summarized in the above two categories.

The underground fiberglass storage tanks usually come with additional components, including straps and deadmen. The straps and the deadmen are intended only for uplift forces caused by buoyancy when the underground tanks are located below the water table. Hence, the straps and the deadmen are not intended to mitigate or reduce the seismic loads imposed on the underground gasoline storage tanks.

In conclusion, the seismic loads in underground tanks, located away from the fault, consist of added lateral and vertical pressures on the tank wall, which are not significant in relation to stresses produced by external soil and external hydrostatic loads, as well as internal vacuum or internal hydrostatic loads. Seismic loads are relatively short term and these added seismic loads on the tank do increase the stress levels in the tank shell. However, the increase is not critical, due to the fact that the safety factors for fiberglass underground tanks under non-seismic load conditions exceed 5.0.



Fiberglass Underground Storage Tanks for Petroleum Applications



Xerxes[®] Corporation – A trusted brand for more than 30 years



Xerxes History

Xerxes Corporation is widely viewed today as the leading manufacturer of underground storage tanks in the United States. Established in 1979, Xerxes has forged strong brand loyalty built on a reputation for innovation and the highest quality products and services.

Like most market leaders, we have a long history of design innovation including development of the first UL-listed double-wall fiberglass tank. We followed that with the introduction of a second-generation double-wall design, which for the first time incorporated a factory-installed hydrostatic monitoring system. This method of leak detection has become the most popular form of monitoring fiberglass underground tanks. More recently, we further improved our tank design by incorporating Parabeam®, a unique and proprietary three-dimensional glass fabric. Parabeam bonds the primary and secondary walls of our double-wall tank together for greater structural integrity, while also allowing for a free-flowing, clearly defined interstice between the two walls. Industry-leading innovations such as these, plus many others, are why petroleum equipment distributors, fuel marketers and commercial accounts rely on Xerxes for safe underground storage tank products.

One Company – Two Trusted Brands

Today, Xerxes is part of the ZCL[®] Composites group of companies manufacturing underground and aboveground fiberglass tanks for a wide range of applications, primarily petroleum products. ZCL Composites (ZCL) is a publicly traded company on the Toronto Stock Exchange (TSX: ZCL). Established in 1987, ZCL began manufacturing fiberglass tanks in Canada. Like Xerxes in the United States, ZCL's growth and the popularity of fiberglass tanks in Canada has been steady. Combined, the Xerxes brand in the United States and the ZCL brand in Canada make us North America's largest manufacturer of underground storage tanks. We service our underground storage tank customers from six strategically located North American manufacturing plants, four in the United States and two in Canada. Our extensive geographic coverage gives us unmatched ability to cost-effectively deliver tanks anywhere in North America. With more than 200,000 tanks installed, our position as the industry's leading manufacturer of underground storage tanks strengthens each year.

Benefits of Xerxes fiberglass underground storage tanks





Why choose a fiberglass tank?

Since their introduction in the 1960s, fiberglass underground tanks have rapidly grown in popularity. It was becoming clear that rusting steel tanks were leaking and creating serious environmental damage. Therefore, the initial focus of fiberglass manufacturers was to design storage vessels that weren't vulnerable to the effects of external corrosion.

Throughout the 1980s, major oil companies and other large fuel marketers quickly began to realize the benefits of fiberglass over steel underground tanks. Today the preference for fiberglass tanks reaches across all segments of the market and includes those who specify, install and own underground storage tanks. Further, the recognized benefits of fiberglass extend well beyond external corrosion protection. Today, with a greater industry-wide understanding of the increased regulatory burden and risks associated with storage tanks, tank buyers are much more educated and sophisticated in their product selection.

Consider the following features and benefits:

Corrosion Resistance – External corrosion protection will always be a concern, but, with the widespread use of ethanol-blended gasoline (E10, E15, E85), biodiesel fuels and ultra-low sulfer diesel (ULSD), the focus has shifted to internal corrosion protection. These new biofuels are creating increasing incidents of aggressive microbial-induced corrosion (MIC) of metal components in fueling systems. Fiberglass tanks are not vulnerable to internal corrosion caused by MIC. Neither do they rust externally due to corrosive soil environments.

Fuel Compatibility – In addition to creating corrosive conditions in tanks, new ethanol-blended fuels today also raise questions regarding compatibility of the stored fuel with tank materials. Xerxes double-wall fiberglass tanks are not only warranted for the full range of ethanol-blended gasoline, they are also UL-tested and UL-listed as compatible with 0-100 percent ethanol storage. This is a very clear and distinct difference from steel storage tanks.

Track Record – With hundreds of thousands of tanks installed thoughout North America during the last three decades, fiberglass tanks have an outstanding record of both protecting the environment and minimizing tank owners' risk. The great majority of new underground tanks installed today for North America's largest fuel retailers and commercial fleet facilities are fiberglass tanks. After exploring their options and evaluating years of product performance, these tank owners overwhelmingly continue to choose fiberglass.

Why choose a Xerxes tank?

During the last three decades, Xerxes has gained a worldwide reputation as a leader in underground storage tank technology. Since its inception in 1979, Xerxes has steadily grown from a tank manufacturer with a small market share to its role today as the market leader. This recognition can be attributed to the many experienced Xerxes employees who strive to not only meet but to exceed our customers' requirements. Equally significant is the quality of the tanks and related products that we manufacture.

Underground storage tanks are not commodity products. Xerxes storage tanks offer customers a number of unique and significant design and performance differences superior to both competitive fiberglass tanks and steel tanks.

Consider the following:

Rib Design – Circumferential ribs are an important design element of any fiberglass underground vessel. Therefore, the rib geometry and how it's incorporated into the cylinder, or tank itself, is an important consideration for designers and customers as they compare products. In the Xerxes design, with its consistent, high-profile rib structure, ribs are fabricated directly into the tank cylinder – not as a secondary step in the process. This increases the overall strength of the tank and results in a structurally superior product.





Parabeam[®] Construction – As part of our history of continuous improvement, Xerxes introduced Parabeam, a unique and proprietary three-dimensional glass fabric, into its underground tank design. Parabeam enhances overall structural integrity by creating a bond between the primary and secondary cylinder walls, while providing a free-flowing interstitial space for monitoring capabilities. Another important benefit is the elimination of false alarms created by fluctuating reservoir levels that can be a recurring problem in other manufacturers' hydrostatically monitored tanks.

Maintenance-Free – Many manufacturers of steel tanks have reduced their warranty duration from 30 years to 10 years, and have incorporated language that requires ongoing maintenance and removal of water bottoms as a condition of warranty coverage. The presence of water in the bottom of fuel tanks is a common condition. Maintenance to frequently remove it can be expensive over both the short-term and long-term life of a tank, and can also leave an owner vulnerable to denied warranty claims should a steel tank corrode internally. Xerxes offers a 30-year limited warranty with no restrictions regarding water-bottom monitoring and removal.

Company Stability – Over the last 30 years, tank manufacturers have gone out of business or filed for bankruptcy and no longer provide warranty coverage. Customers who purchase underground tanks do so with the expectation that their tank will provide many years of trouble-free service, and that the manufacturer will be around to suppport its products and its warranties. Xerxes has a three-decade record of doing just that.

TRUCHEK® – State-of-the-art continuous monitoring



TRUCHEK[®] hydrostatic tank monitoring for double-wall tanks is an easy, precise and reliable method for continuous leak detection and for tank-tightness testing. For two decades, TRUCHEK has been successfully monitoring thousands of tanks in many different types of installations.

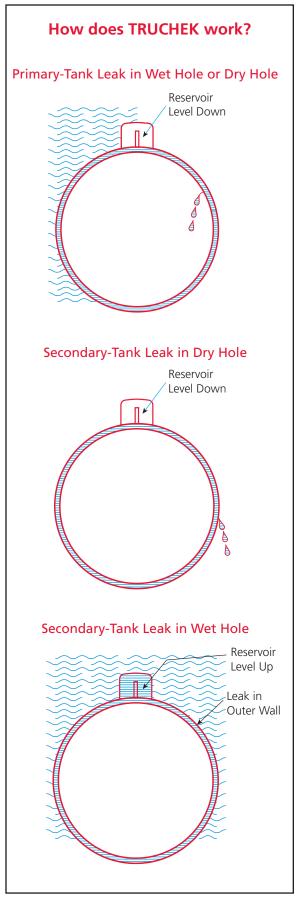
Continuous Monitoring

When you order a Xerxes double-wall tank with the TRUCHEK option, the interstice between the two tank walls is filled at the factory with a calcium-chloride fluid that also partially fills a reservoir, creating hydrostatic pressure throughout the interstice. An electronic probe placed in the tank's reservoir alarms when the fluid level either falls below or rises above the acceptable level. This increasingly popular method of leak monitoring gives tank owners greater peace of mind than the alternative method of using a simple liquid sensor, which often never detects an outer-wall breach. TRUCHEK has become the industry standard as a state-of-the-art technique for continuous monitoring.

Changing regulations in some markets now require that new doublewall tanks have continuous leak detection using a constant vacuum, air pressure or hydrostatic pressure in the interstice. TRUCHEK is the ideal solution to this growing regulatory requirement.

Tank Tightness

TRUCHEK also provides a simple, precise and reliable method to perform a tank-tightness test. The 10-hour tightness-test procedure meets the strict NFPA329 criteria. A shorter 4-hour test (while product is dispensing) exceeds EPA's criteria for a tank-tightness test.



Additional underground storage tank solutions

When a customer's needs go beyond the standard double-wall tank, Xerxes offers products that address a wide range of requirements. With a full line of tank accessories, we offer customers the most comprehensive range of solutions found in the petroleum equipment industry today. Please visit www.xerxes.com for additional information on each of these products.

Multicompartment Tanks – These Xerxes tanks are a popular choice among retail gasoline marketers and fleet fueling owners. The ability to store two or three grades of fuel, or gasoline and diesel, in a single tank is particularly appealing when the amount of onsite space needed for multiple tanks is either not available or difficult to obtain. Customers may also find installation and insurance cost savings when using multicompartment tanks. The Xerxes double-wall multicompartment tank comes standard with a double-wall bulkhead, while other tank manufacturers require an upgrade to a double-wall bulkhead. Xerxes offers a wide range of capacity options in 6-, 8- and 10-foot-diameter models.





Triple-Wall Tanks – Some customers and regulatory agencies now require even more enhanced protection than double-wall tanks provide. Conditions that lend themselves to considering a triple-wall tank are sensitive groundwater aquifers, or nearby lakes or streams. The Xerxes UL-listed triple-wall tank, with an additional Parabeam interstice, is the innovative and cost-effective answer when this level of containment is required.

The ZCL Phoenix System® – In some situations, single-wall tanks that need to be upgraded to double-wall tanks offer site challenges that make removal of existing tanks either cost-prohibitive or extremely difficult. For instance, tanks are sometimes covered or surrounded by buildings, roads or rail lines. In such cases, converting a single-wall tank (either fiberglass or steel) into a double-wall tank might be done most efficiently with ZCL's Phoenix System. This ULC-listed system consists of two corrosion-resistant laminates with the proprietary Parabeam glass fabric between the laminates creating an interstitial space. The interstice can be either dry or hydrostatically monitored. The Phoenix System, applied onsite by trained installers, is biofuels compatible, including ethanol-blended fuels and biodiesels.



Additional underground storage tank solutions



Diesel Exhaust Fluid Tanks – Demand for diesel exhaust fluid (DEF) is growing significantly as increasing numbers of commercial, passenger, rail and marine diesel engines that require the use of DEF enter the market. A Xerxes underground tank is the ideal solution for the very unique storage requirements that DEF presents. Unlike carbon steel tanks, a Xerxes fiberglass tank does not require special coatings or linings to protect the purity of the DEF product. Extensive testing with third-party laboratories was conducted to verify the suitability of long-term storage while maintaining product quality.

Xerxes uses stainless steel fittings, manway covers and striker plates on all tanks designed for DEF storage. A UL label is attached to all tanks that meet listing criteria. Each tank interior is thoroughly cleaned and then sealed to prevent contamination during shipping and installation.

In the relatively brief period of time that DEF has been used in North America, Xerxes has established a leadership role in introducing fiberglass tanks as the bulk storage vessel of choice. With more than 1,000 DEF tanks in service, customers are clearly putting their trust in Xerxes' design innovation capabilities.





Oil/Water Separators – With a fiberglass underground tank at the heart of the design, a Xerxes oil/water separator incorporates unique refinements within the vessel to create a separator that removes free-floating oils and settleable sands from oil/water mixtures. A properly sized polypropylene vertical-tube coalescer is designed to produce effluent quality of 10 ppm free-floating oil. A Xerxes oil/water separator is an excellent choice for managing water runoff from parking lots or equipment washdown stations. This product is also available with a UL 2215 listing.

Storage tank accessories

Today's retail and commercial fueling facilities are sophisticated systems that are installed in a highly regulated environment. While the storage tank is the critical component in an underground fuel system, other important accessories are necessary in order to provide spill containment, tank anchoring, tank-top corrosion protection, leak detection and other important functions. Xerxes engineers have designed innovative, complimentary products that provide system designers and installers with cost-effective, easy-to-install accessories. Not all tank manufacturers provide the wide range of accessories that Xerxes offers. This is another example of how Xerxes' innovative spirit benefits customers.

As with many products, Xerxes tanks and accessories require proper installation to ensure that the customer receives the long-lasting, trouble-free performance that its products are designed for. To that end, Xerxes provides a comprehensive Installation Manual and Operating Guidelines document that outlines the easy, yet proper, steps necessary for a successful installation.



Storage tank accessories





Containment Sumps and Collars – Sumps and collars are common accessories found on virtually all double-wall tanks installed today. Xerxes supplies optional, factory-installed containment collars that provide secondary containment around tank fittings and manways. Designed to be a custom match to the collar, the Xerxes containment sump comes in a variety of models and sizes, all engineered to accommodate different customer preferences and needs. Xerxes sumps and collars are also available in double-wall models that can be monitored with the reliable TRUCHEK hydrostatic monitoring system.

Anchoring System – Site-specific installation conditions generally dictate whether a tank-anchoring system is necessary. Some customers choose to anchor all their tanks. Xerxes offers a complete tank-anchoring system, including reinforced precast concrete deadman (designed to American Concrete Institute standards), fiberglass anchoring straps and galvanized turnbuckles. Each component is engineered to specific tank sizes and for ease of installation. In most cases concrete deadmen can be delivered on the same trailer as the tank. This both minimizes the shipping cost and assures that deadmen are ready for use when the tank is set.

Hydrostatic Monitoring – The image on page 8 illustrates the functional design of the highly effective TRUCHEK hydrostatic monitoring system. A "jacket" of calcium-chloride solution is factory-installed in the tank interstice and connected to a tank-top reservoir where the fluid level is monitored with a simple level sensor. The unique Parabeam construction of a Xerxes double-wall tank eliminates false leak alarms that can occur with other tank designs. In addition to its simple, yet highly effective, monitoring capabilities, TRUCHEK provides true continuous monitoring of both tank walls regardless of site conditions. This continuous-monitoring feature is increasingly attractive to state and federal regulators, and may become a requirement for all new double-wall tanks in the future.

Guide Specifications for Xerxes Underground Petroleum Storage Tanks

Short form:

The contractor shall provide a double-wall or triple-wall fiberglass reinforced plastic (FRP) UL-listed underground storage tank as shown on the drawings. The tank size, fittings and accessories shall be as shown on the drawings. The fiberglass tank shall be manufactured by Xerxes Corporation.

The tank shall be tested and installed according to the Xerxes Installation Manual and Operating Guidelines for Fiberglass Underground Storage Tanks in effect at time of installation.

Long form:

Part I: General

1.01 Quality Assurance

A. Acceptable Manufacturer: Xerxes Corporation

B. Governing Standards, as applicable:

- Underwriters Laboratories (UL) Standard for Safety 1316 Glass-Fiber-Reinforced Plastic Underground Storage Tanks for Petroleum Products, Alcohols, and Alcohol-Gasoline Mixtures. A UL label shall be attached to each tank.
- National Fire Protection Association (NFPA) Standards: NFPA 30: Flammable and Combustible Liquids Code, NFPA 30A: Code for Motor Fuel Dispensing Facilities and Repair Garages, NFPA 31: Standard for the Installation of Oil-Burning Equipment.
- 3. City of New York Department of Buildings M.E.A., #161-89-M.
- 4. American Concrete Institute (ACI) standard ACI 318-11, Building Code Requirements for Structural Concrete.

C. Submittals

 Contractor shall submit ___ copies of shop drawings, manufacturer's product brochures, and Installation Instructions.

Part II: Products

2.01 Double-Wall and Triple-Wall Fiberglass Reinforced Plastic (FRP) Underground Storage Tanks:

A. Loading Conditions – Tank shall meet these design criteria:

- 1. Interstitial Pressure The interstitial space of the tank shall withstand a minimum 20-psig pressure test.
- 2. **Internal Load** Tank shall withstand a 5-psig air-pressure test with a 5:1 safety factor.
- Surface Loads Tank shall withstand surface H-20 and HS-20 axle loads when properly installed according to Xerxes' current Installation Manual and Operating Guidelines.
- External Hydrostatic Pressure Tank shall be designed for 7'
 of overburden over the top of the tank, the hole fully flooded
 and a safety factor of 5:1 against general buckling.

B. Product Storage:

- 1. The primary compartment of double-wall and triple-wall tanks shall be vented and operated at atmospheric pressure only.
- 2. Tank shall be capable of storing liquids with a specific gravity up
- Tank shall be capable of storing products identified in the manufacturer's standard limited warranty in effect at the time of purchase.

C. Materials:

- The primary and secondary walls of the tank shall be manufactured with 100% premium resin and glass-fiber reinforcement. No sand or silica fillers shall be added to the resin.
- 2. The interstitial space between the primary and secondary walls shall be constructed with a glass reinforcement material such as Parabeam[®], which provides a structural bond between the two tank walls, while creating a defined interstice that allows for free flow of liquid.

D. Tank Dimensions (Refer to Xerxes literature on gallonage):

- 1. Tank shall have nominal capacity of _____ gallons.
- 2. Tank shall have nominal outside diameter of _____ feet
- 3. Tank shall have a nominal overall length of _____ feet/inches.

2.02 Tank Monitoring System

A. General

- Tank shall be continuously monitored with the TRUCHEK[®] hydrostatic leak monitoring system.
- The continuous monitoring system shall include monitoring fluid factory-installed in the interstitial space and within a fiberglass tank-top mounted reservoir.
- 3. The monitoring system shall be recognized by the National Work Group on Leak Detection Evaluations (NWGLDE) as continuous leak detection and as a precision tank test.
- 4. The monitoring system shall be independently tested by a qualified third party and verified to be capable of detecting leaks as small as .05 gallons per hour when TRUCHEK tank-tightness test procedures are followed.

B. Design

- The continuous monitoring system shall be designed to detect a leak in either the primary or secondary wall at all times, regardless of the water-table conditions at the installation site.
- 2. The interstice of the tank shall be designed for a 5:1 safety factor beyond normal hydrostatic operating pressure to ensure structural integrity and to prevent false leak alarms.

2.03 Accessories

A. Tank Anchoring

- 1. Anchor straps shall be as supplied by tank manufacturer and designed for a maximum load of 25,000 lbs.
- 2. Galvanized turnbuckles (two per anchor strap) shall be supplied by the tank manufacturer.
- Prefabricated concrete anchors shall be supplied by the tank manufacturer, designed to the ACI 318-11 standard, manufactured with 4,000 psi concrete, and shall have adjustable anchor points.

B. Manways

 The standard manway shall be flanged, 22" I.D. and complete with UL-listed gaskets, bolts and covers as shown on tank drawings.

C. Threaded Fittings

- All threaded fittings shall be NPT half or full couplings, in 2", 4" or 6" diameters.
- 2. Fittings shall be installed on the tank-top centerline or in the cover of the manway as shown on the tank drawings.

D. Containment Collars & Sumps

- 1. The tank shall have factory-installed 42"-or 48"-diameter containment collars as shown on the tank drawings.
- Containment sumps in 42"-or 48"-diameter, provided by the tank manufacturer and designed for mounting on the containment collars, shall be supplied as shown on the tank drawings.
- 3. Adhesive shall be provided by the tank manufacturer with each containment collar and sump.
- Containment collars and sumps shall be designed and supplied as a containment system. Only sumps provided by the manufacturer shall be allowed.

Part III: Testing and Installation

3.01 Testing

A. Testing – Tank shall be tested according to the Xerxes Installation Manual and Operating Guidelines in effect at time of installation.

3.02 Installation

A. Installation – Tank shall be installed according to the Xerxes Installation Manual and Operating Guidelines in effect at time of installation.

Part IV: Limited Warranty

4.01 Limited Warranty

A. Limited Warranty – Warranty shall be manufacturer's standard limited warranty in effect at time of purchase.

Xerxes Underground Double-Wall Tank Data

	Nominal Capacity (gallons)	Actual Capacity (gallons)	Tank Length (feet/inches)	Nominal Shipping Weights (lbs) (dry interstitial)	Nominal Shipping Weights (lbs) (wet interstitial)	Number of Anchor Straps Required
4-foot- diameter	600	602	7′-3 1/2″	900	1,100	2
diameter	1,000	1,009	11'-7 1/2"	1,400	1,700	2
	2,000	2,013	22' -3 5/8"	2,800	3,400	2
	2,500	2,324	13'-5 3/4"	2,200	2,800	2
	3,000	2,910	16'-4 1/4"	2,600	3,300	2
6-foot-	4,000	3,789	20′-8″	3,600	4,400	2
diameter	5,000	4,961	26′-5″	4,300	5,200	4
	6,000	5,840	30'-8 3/4"	5,000	6,100	4
		l				_
	4,000	4,190	15'- 1/2"	2,700	3,600	2
0.64	5,000	5,089	17'-8 1/2"	3,200	4,200	2
8-foot- diameter	6,000	6,044	20'-6 1/2"	3,700	4,900	2
	8,000	7,899	26'- 1/2"	4,800	6,200	4
	10,000	9,753	31'-6 1/2"	5,900	7,500	4
	12,000	11,608	37'- 1/2"	7,000	8,800	4
	15,000	14,881	46'- 9"	9,100	11,200	6
	10,000	10,420	21'-5 1/4"	4,900	6,400	4
	12,000	11,904	24'- 1/4"	5,600	7,200	4
10-foot-	15,000	15,041	29'-5 3/4"	7,000	8,900	4
diameter	20,000	19,782	37'-8 3/4"	9,000	11,300	6
	25,000	25,431	47′-6 3/4″	11,800	14,600	8
	30,000	30,172	55'-9 3/4"	14,000	17,200	10
	35,000	34,912	64'- 3/4"	16,500	20,100	12
	40,000	40,443	73′-8 1/4″	19,000	23,100	14
	20,000	20,638	29′ -4″	14,000	16,700	6
	25,000	25,381	35′ -7″	16,600	19,700	8
12-foot-	30,000	31,072	43′ -1″	19,900	23,500	10
diameter	35,000	35,815	49′ -4″	22,500	26,500	12
	40,000	39,609	54' -4"	24,600	28,900	12
	45,000	44,352	60′ -7″	27,400	32,100	16
	48,000	48,146	65′ -7″	29,500	34,500	18
	50,000	50,044	68′ -1″	30,500	35,700	18

Notes:

- 1. Tank data for single-wall and multicompartment tank models is available at www.xerxes.com.
- 2. Actual height of the tank may be greater than the actual diameter due to fittings and accessories. Load height during shipping may vary due to tank placement on the shipping trailer.
- 3. If an overfill-protection device is installed in the tank, the actual capacity will be reduced.

North American Manufacturing Facilities



ZCL Manufacturing Facilities

Edmonton, AB Drummondville, QC

Xerxes Manufacturing Facilities

Anaheim, CA Hagerstown, MD Seguin, TX Tipton, IA



ZCL Composites Inc. 1420 Parsons Road SW Edmonton, AB, T6X 1M5 Canada 780-466-6648 www.zcl.com



Xerxes Corporation 7901 Xerxes Avenue South Minneapolis, MN 55431 USA 952-887-1890 www.xerxes.com



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MEMORANDUM

Date: July 6, 2016

To: Brittany Skelton

City of Ketchum Department of Planning and Building

From: Hales Engineering

Subject: Ketchum – Bracken Station TIS, Additional Information

UT16-851

The purpose of this memorandum is to address requests for additional information from the City of Ketchum Planning Commission regarding the proposed Bracken Station in Ketchum, Idaho. This memo will address only requests regarding traffic related issues. Each request is stated as received in italics, followed by the response from Hales Engineering.

1. Obtain traffic counts at 10th Street/Main Street intersection in order to corroborate the 2008 data in the traffic study already conducted. If the traffic engineer wants to make the case that the need for new data is superfluous, and submits a narrative explaining why, that would be acceptable. However, the request for current data at the 10th Street/Main Street intersection is driven by public comment and providing this data also serves the purpose of addressing public concern, so obtaining the new counts is recommended.

Hales Engineering utilized peak-hour turning movement count data collected in February 2008 for a previous traffic impact study performed in the area. Using historical traffic data for SH-75 obtained from the Idaho Transportation Department (ITD), a growth rate of 1.1% per year was calculated based on recent trends. This 1.1% growth rate as well as a 30% seasonal adjustment, to reflect peak season traffic conditions, were used to estimate 2016 traffic conditions. These estimated traffic data were used for the traffic impact study.

In order to address concerns raised at the planning commission meeting held on June 13, 2016, additional peak hour turning movement counts were collected on June 29, 2016. When compared with the previously discussed estimated data, it was found that the traffic volumes used in the traffic impact study were <u>5% higher</u> than the volumes collected on June 29th.



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- 2. Address the projected makeup of vehicles that will be using the gas station.
 - a. What percentage will be oversized vehicles (RVs, construction trailers, et cetera)?
 - i. Address how the proportion of oversized vehicles impacts the amount of vehicles that can gueue in the turn lane.
 - b. Address potential back-up of northbound traffic lining up to make a left turn into the gas station and the implications of exceeding the length of the turn lane (e.g. traffic backed up further south than the turn lane extends).

Vehicle classification data were collected at a local gas station over two days. Only 7% of vehicles observed during data collection activities were larger vehicles (i.e. trucks pulling trailers or recreational vehicles). The remaining 93% of vehicles observed were passenger cars or pickup trucks. Using these data, we project that the vast majority of vehicles that will use the Bracken Station will be passenger cars and pickup trucks.

Standard practice for queuing analyses is to assume an average 20 feet of queuing length per vehicle. Obviously, larger vehicles (i.e. tractor trailers, RVs, etc.) will occupy more than 20 feet of queuing length. However, the projected vehicle classification does not suggest that it would be necessary to modify the 20 feet per vehicle assumption.

The proposed left-turn lane would serve vehicles turning left from Main Street (SH-75) into the gas station, as well as vehicles turning left onto 10th Street. The traffic impact study found that with future (2020) plus project traffic conditions, the 95th percentile queue at the intersection would extend for approximately 105 feet. The proposed left-turn lane is more than adequate to accommodate queues of this length.

Although it is unlikely that the left-turn queue would overflow into the thru lane, such an event would likely have minimal short-term impacts on thru traffic. Delay for northbound left-turning vehicles at the gas station access, as well as at 10th Street are anticipated to be quite short. When delays are short, queues tend to dissipate quickly. As soon as the queue is shortened to a length that can be accommodated by the left-turn lane, the flow of thru traffic is restored.

3. Address the potential for northbound (left) and southbound (right) turn lanes on 10th Street to facilitate left and right turns onto Main Street.

Separate right- and left-turn lanes at stop-controlled approaches to unsignalized intersections can help to mitigate delay on the approach by allowing right-turning vehicles to execute a right-turn movement while bypassing waiting left-turning vehicles, or vice versa.

A separate right-turn lane is not recommended at this location. Turning movement wheel path analyses show that with the current approach geometry, larger vehicles are able to execute right-turn movements with minimal encroachments into opposing



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traffic lanes. It is likely that the addition of a separate right-turn lane would constrain the right-turn movement such as to require significant encroachment into opposing traffic lanes. The traffic impact study found that delays at this intersection are anticipated to be relatively low, and therefore a separate right-turn lane would not provide significant benefit.

If you have any questions regarding this memo, please feel free to contact us.

Appendix A

"Bracken Station Traffic Impact Study Updated"

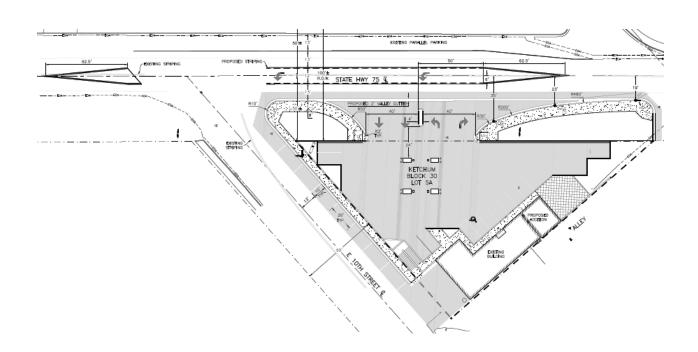
by Hales Engineering

dated October 3, 2016



Bracken Station

Traffic Impact Study UPDATED



Ketchum, Idaho

October 3, 2016

UT16-851



EXECUTIVE SUMMARY

This study addresses the traffic impacts associated with the proposed Bracken Station in Ketchum, Idaho. The proposed gas station will be located on the southwest corner of the 10th Street / Main Street (SH-75) intersection.

Included within the analyses for this study are the traffic operations and recommended mitigation measures for existing conditions and plus project conditions (conditions after development of the proposed project) at key intersections and roadways in the vicinity of the site. Future 2020 and 2026 conditions are also analyzed.

TRAFFIC ANALYSIS

The following is an outline of the traffic analysis performed by Hales Engineering for the traffic conditions of this project.

Existing (2016) Background Conditions Analysis

Hales Engineering performed weekday morning (7:00 to 9:00 a.m.) and afternoon (3:00 to 7:00 p.m.) peak period traffic counts at the following intersections:

- 10th Street / Main Street (SH-75)
- 5th Street / Main Street (SH-75)

These counts were performed on Thursday, September 1, 2016. The morning peak hour was determined to be between 8:00 and 9:00 a.m. and the evening peak hour was determined to be between 4:15 and 5:15 p.m. The evening peak hour volumes were found to be significantly higher than the morning peak hour volumes. Therefore, the evening peak hour volumes were used in the analysis to represent the worst-case conditions. Detailed count data are included in Appendix A. At the request of Ketchum City staff, anticipated traffic from the nearby Ketchum Community School was also added into the existing (2016) background traffic.

As shown in Table ES-1, both study intersections are currently operating at LOS B during the p.m. peak hour. The 95th percentile queues on the north- and eastbound approaches to the 10th Street / Main Street (SH-75) intersection were observed to extend for approximately 85 feet. The queues on the four approaches to the 5th Street / Main Street (SH-75) intersection were observed to extend between approximately 120 feet and 200 feet. No other significant queuing was observed.

i



Project Conditions Analysis

The proposed land use for the development has been identified as follows:

Gasoline/Service Station with Convenience Market 8 Vehicle Fueling Positions

As directed by Ketchum City staff, trip generation for the development was calculated using data collected at an existing gas station in the area that was determined to be characteristically similar to the proposed Bracken Station. Data was gathered in the morning (7:00 to 9:00 a.m.) and afternoon (3:00 to 7:00 p.m.) peak periods on Thursday, September 1, 2016. The number of entering and exiting vehicles, the vehicle classification, and the duration of time that each vehicle remained on-site was recorded. A summary of these data can be found in Appendix E, however for information purposes, the average dwell time for a fueling vehicles was 5 minutes and 05 seconds, average dwell time for someone using the C-store was 5 minutes and 51 seconds, and the average dwell time for someone fueling and using the C-store was 9 minutes and 37 seconds. These data were used to determine a trip generation rate using the number of fueling positions as the independent variable (similar to the method used in the Institute of Transportation Engineers (ITE) *Trip Generation (9th Edition, 2012)*).

Trip generation for the proposed project is as follows:

• p.m. Peak Hour Trips: 90

Existing (2016) Plus Project Conditions Analysis

As shown in Table ES-1 both study intersections, as well as the project access, are anticipated to operate at acceptable levels of service during the p.m. peak hour with project traffic added. During the p.m. peak hour, the 95th percentile queue lengths on the 10th Street / Main Street (SH-75) intersection are anticipated to extend for approximately 70 feet on the eastbound approach and approximately 80 feet on the northbound approach with project traffic added. The 95th percentile queue length on northbound Main Street (SH-75) at the proposed project access is anticipated to extend for approximately 50 feet. The queues on the four approaches to the 5th Street / Main Street (SH-75) intersection are anticipated to remain in the range of approximately 120 feet and 200 feet with project traffic added.

Future (2020) Background Conditions Analysis

As shown in Tables ES-1, both study intersections are anticipated to operate at LOS B during the p.m. peak hour with future (2020) background traffic conditions. No significant changes to the 95th percentile queues are anticipated with projected future (2020) background traffic conditions.



Future (2020) Plus Project Conditions Analysis

As shown in Tables ES-1, both study intersections, as well as the project access, are anticipated to operate at acceptable levels of service during the p.m. peak hour with project traffic added. During the p.m. peak hour, the 95th percentile queue length on the northbound approach to the Main Street (SH-75) / 10th Street intersection is anticipated to extend for approximately 50 feet, while the queue length on the eastbound approach is anticipated to extend for approximately 80 feet. The northbound queue length on Main Street (SH-75) at the proposed project access is anticipated to extend for approximately 45 feet. It is anticipated that the 95th percentile queues at the 5th Street / Main Street (SH-75) intersection will remain unchanged with project traffic added.

Future (2026) Background Conditions Analysis

As shown in Tables ES-1 both study intersections are anticipated to operate at LOS B during the p.m. peak hour with future (2020) background traffic conditions. The anticipated 95th percentile queue lengths at the 10th Street / Main Street (SH-75) intersection are anticipated to extend for approximately 110 feet on both the north- and eastbound approaches with projected future (2020) background traffic conditions. The 95th percentile queues on the northbound approach to the 5th Street / Main Street (SH-75) intersection are anticipated to extend for over 350 feet.

Future (2026) Plus Project Conditions Analysis

As shown in Tables ES-1, the 10th Street / Main Street (SH-75) intersection is anticipated to operate at LOS F during the p.m. peak hour with project traffic added. All other study intersections are anticipated to operate at acceptable levels of service. During the p.m. peak hour, the 95th percentile queue length on the northbound approach to the Main Street (SH-75) / 10th Street intersection is anticipated to extend for approximately 70 feet, while the queue length on the eastbound approach is anticipated to extend for approximately 250 feet. The northbound queue length on Main Street (SH-75) at the proposed project access is anticipated to extend for approximately 60 feet. It is anticipated that the 95th percentile queues on the northbound approach to the 5th Street / Main Street (SH-75) intersection will extend for approximately 450 feet with project traffic added.

<u>Hypothetical Future (2026) Plus Project Conditions Analysis</u>

The analysis of this scenario was requested by Ketchum City staff. Trip generation for this hypothetical scenario was determined using data collected by university students in February 2010 at a gas station on US-89 in Provo, Utah. As shown in Tables ES-1, the 10th Street / Main Street (SH-75) intersection would be anticipated to operate at LOS F during the p.m. peak hour with project traffic added. All other study intersections would be anticipated to



operate at acceptable levels of service. During the p.m. peak hour, the 95th percentile queue length on the northbound approach to the Main Street (SH-75) / 10th Street intersection would be anticipated to extend for approximately 70 feet, while the queue length on the eastbound approach is anticipated to extend for approximately 350 feet. The northbound queue length on Main Street (SH-75) at the proposed project access would be anticipated to extend for approximately 80 feet. It would be anticipated that the 95th percentile queues on the northbound approach to the 5th Street / Main Street (SH-10) intersection will extend for approximately 450 feet with project traffic added.

		- Diackell Sta	TABLE ES-1 P.M. Peak Hour ID Ketchum - Bracken Station TIS									
Projected 2016 Background	Projected 2016 Plus Project	Future 2020 Background	Future 2020 Plus Project	Future 2026 Background	Future 2026 Plus Project	Hypothetical Future 2026 Plus Project						
LOS (Sec/Veh ¹)	LOS (Sec/Veh ¹)	LOS (Sec/Veh ¹)	LOS (Sec/Veh ¹)	LOS (Sec/Veh ¹)	LOS (Sec/Veh ¹)	LOS (Sec/Veh ¹)						
B (10.7) / EB	B (11.5) / EB	B (10.2) / EB	B (10.8) / EB	B (13.7) / EB	F (>50) / EB	F (>50) / EB						
B (10.8)	B (10.7)	B (11.2)	B (11.1)	B (16.5)	B (18.9)	C (25.2)						
-	A (5.9) / EB	-	A (7.9) / EB	-	B (13.1) / EB	C (17.4) / EB						
1. Intersection LOS and delay (seconds/vehicle) values represent the overall intersection average for signalized and all-way stop controlled intersections and the worst approach for all other unsignalized ntersections. 2. This is a project intersection and is only analyzed in the plus project scenarios.												
L(Background OS (Sec/Veh¹) B (10.7) / EB B (10.8) - s represent the over	Background Plus Project OS (Sec/Veh¹) LOS (Sec/Veh¹) B (10.7) / EB B (11.5) / EB B (10.8) B (10.7) - A (5.9) / EB s represent the overall intersection average in the plus project scenarios.	Background Plus Project Background OS (Sec/Veh¹) LOS (Sec/Veh¹) LOS (Sec/Veh¹) B (10.7) / EB B (11.5) / EB B (10.2) / EB B (10.8) B (10.7) B (11.2) - A (5.9) / EB - s represent the overall intersection average for signalized and all in the plus project scenarios.	Background Plus Project Background Plus Project OS (Sec/Veh¹) LOS (Sec/Veh¹) LOS (Sec/Veh¹) LOS (Sec/Veh¹) B (10.7) / EB B (11.5) / EB B (10.2) / EB B (10.8) / EB B (10.8) B (10.7) B (11.2) B (11.1) - A (5.9) / EB - A (7.9) / EB s represent the overall intersection average for signalized and all-way stop controlled in the plus project scenarios.	Background Plus Project Background Plus Project Background OS (Sec/Veh¹) LOS (Sec/Veh¹) LOS (Sec/Veh¹) LOS (Sec/Veh¹) LOS (Sec/Veh¹) B (10.7) / EB B (11.5) / EB B (10.2) / EB B (10.8) / EB B (13.7) / EB B (10.8) B (10.7) B (11.2) B (11.1) B (16.5) - A (5.9) / EB - A (7.9) / EB - s represent the overall intersection average for signalized and all-way stop controlled intersections and the way the plus project scenarios.	Background Plus Project Background Plus Project Background Plus Project OS (Sec/Veh¹) LOS (Sec/Veh¹) LOS (Sec/Veh¹) LOS (Sec/Veh¹) LOS (Sec/Veh¹) LOS (Sec/Veh¹) B (10.7) / EB B (11.5) / EB B (10.2) / EB B (10.8) / EB B (13.7) / EB F (>50) / EB B (10.8) B (10.7) B (11.2) B (11.1) B (16.5) B (18.9) - A (5.9) / EB - A (7.9) / EB - B (13.1) / EB s represent the overall intersection average for signalized and all-way stop controlled intersections and the worst approach for all of the plus project scenarios.						

RECOMMENDATIONS

The following mitigation measures are recommended:

Existing (2016) Background Conditions Analysis

No mitigation measures are recommended.

Existing (2016) Plus Project Conditions Analysis

Although the delays at the study intersections are anticipated to be minimal, it is recommended that a two-way left-turn lane be constructed from a location north of 10th Street to a location south of the project. This will allow northbound left-turning vehicles to 10th Street or into the project access to decelerate and/or queue without blocking the flow of through



traffic. This will improve traffic safety in the area, as well as minimize delays. No other mitigation measures are recommended.

Future (2020) Background Conditions Analysis

No mitigation measures are recommended.

Future (2020) Plus Project Conditions Analysis

No additional mitigation measures are recommended.

Future (2026) Background Conditions Analysis

A mitigation that could be implemented at 10th Street / Main Street (SH-75) is to signalize the intersection when traffic volume warrants are met, as identified in the *Manual on Uniform Traffic Control Devices* (MUTCD), Federal Highway Administration, 2009. No additional mitigation measures are recommended.

Future (2026) Plus Project Conditions Analysis

Although significant delays are anticipated on the eastbound approach to the 10th Street / Main Street (SH-75) intersection, it is generally expected that executing a left-turn movement from a stop-controlled approach onto a busy highway during peak traffic periods. The addition of a separate right-turn lane on the eastbound approach to the 10th Street / Main Street (SH-75) intersection would likely mitigate the delay and queuing on the approach. However, the skewed geometry of the intersection may render this mitigation measure unworkable due to the constrained turning radius that would be created. No additional mitigation measures are recommended.



SUMMARY OF KEY FINDINGS/RECOMMENDATIONS

The following is a summary of key findings and recommendations:

- Data Collection
 - Turning movement count data were collected at the 10th Street / Main Street (SH-75) and 5th Street / Main Street (SH-75) intersections on Thursday, September 1, 2019 and Monday, September 5, 2016 (Labor Day).
 - At the direction of Ketchum City Staff, trip generation data was collected at an existing gas station that was determined to be characteristically similar to the proposed Bracken Station.
 - At the direction of Ketchum City staff, data from the Thursday, September 1, 2016 counts were used for these analyses, as it produced a "worst case" scenario.
- Project Characteristics
 - The proposed gas station will have eight fueling positions.
 - It is anticipated that the proposed gas station will generate approximately 90 vehicle trips, and six pedestrian/bicycle trips during the p.m. peak hour.
- 10th Street / Main Street (SH-75) Intersection
 - This intersection is currently operating at an acceptable level of service, and is anticipated to continue to do so through 2020. In year 2026 with all known projects (Ketchum Community School, Warm Springs Ranch Resort, redevelopment of the Stock property), the intersection will become constrained.
 - With future (2026) background conditions, the 95th percentile queue lengths on the northbound and eastbound approaches are anticipated to extend for approximately 110 feet.
- 5th Street / Main Street (SH-75) Intersection
 - This intersection is currently operating at an acceptable level of service, and is anticipated to continue to do so through 2026.
 - o With future (2026) background conditions, the 95th percentile queue length on the northbound approach is anticipated to extend for approximately 370 feet.
- Project Access
 - The proposed project access is anticipated to operate at an acceptable level of service through 2026.
 - The 95th percentile queue length for northbound left-turning vehicles at the proposed project access is anticipated to extend for approximately 60 feet with future (2026) plus project conditions.



- Recommended Mitigation Measures
 - o It is recommended that a two-way left-turn lane be constructed on Main Street (SH-75) along the project frontage. This will serve as a left-turn lane for northbound vehicles turning into the project access, and for northbound vehicles turning onto 10th Street.
 - O Although pedestrian volumes during the p.m. peak hour are anticipated to be relatively low, if pedestrian crosswalks are installed on Main Street (SH-75) at 9th Street, it should be done in accordance with the Idaho Transportation Department with their permit process. It is also recommended that pedestrian activated rectangular rapid flashing beacon signs be installed to increase visibility of the crossings.



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I. INTRODUCTION

A. Purpose

This study addresses the traffic impacts associated with the proposed Bracken Station in Ketchum, Idaho. The proposed gas station will be located on the southwest corner of the 10th Street / Main Street (SH-75) intersection. Figure 1 shows a vicinity map of the proposed development.

Included within the analyses for this study are the traffic operations and recommended mitigation measures for existing conditions and plus project conditions (conditions after development of the proposed project) at key intersections and roadways in the vicinity of the site. Future 2020 and 2026 conditions are also analyzed.



Figure 1 Vicinity map showing the project location in Ketchum, Idaho



B. Scope

The study area was defined based on conversations with the development team, following general guidelines for traffic impact studies. This study was scoped to evaluate the traffic operational performance impacts of the project on the following intersection:

- 10th Street / Main Street (SH-75)
- 5th Street / Main Street (SH-75)

C. Analysis Methodology

Level of service (LOS) is a term that describes the operating performance of an intersection or roadway. LOS is measured quantitatively and reported on a scale from A to F, with A representing the best performance and F the worst. Table 1 provides a brief description of each LOS letter designation and an accompanying average delay per vehicle for both signalized and unsignalized intersections.

The Highway Capacity Manual 2010 (HCM 2010) methodology was used in this study to remain consistent with "state-of-the-practice" professional standards. This methodology has different quantitative evaluations for signalized and unsignalized intersections. For signalized and all-way stop intersections, the LOS is provided for the overall intersection (weighted average of all approach delays). For all other unsignalized intersections LOS is reported based on the worst approach.

D. Level of Service Standards

For the purposes of this study, a minimum overall intersection performance for each of the study intersections was set at LOS D. However, if LOS E or F conditions exist, an explanation and/or mitigation measures will be presented. An LOS D threshold is consistent with "state-of-the-practice" traffic engineering principles for urbanized areas.



Table 1 Level of Service Descriptions

Level of Service	Description of Traffic Conditions	Average Delay (seconds/vehicle)
	Signalized Intersections	Overall Intersection
А	Extremely favorable progression and a very low level of control delay. Individual users are virtually unaffected by others in the traffic stream.	0 ≤ 10.0
В	Good progression and a low level of control delay. The presence of other users in the traffic stream becomes noticeable.	> 10.0 and ≤ 20.0
С	Fair progression and a moderate level of control delay. The operation of individual users becomes somewhat affected by interactions with others in the traffic stream.	>20.0 and ≤ 35.0
D	Marginal progression with relatively high levels of control delay. Operating conditions are noticeably more constrained.	> 35.0 and ≤ 55.0
E	Poor progression with unacceptably high levels of control delay. Operating conditions are at or near capacity.	> 55.0 and ≤ 80.0
F	Unacceptable progression with forced or breakdown operating conditions.	> 80.0
	Unsignalized Intersections	Worst Approach
A	Free Flow / Insignificant Delay	0 ≤ 10.0
В	Stable Operations / Minimum Delays	>10.0 and ≤ 15.0
С	Stable Operations / Acceptable Delays	>15.0 and ≤ 25.0
D	Approaching Unstable Flows / Tolerable Delays	>25.0 and ≤ 35.0
Е	Unstable Operations / Significant Delays	>35.0 and ≤ 50.0
F	Forced Flows / Unpredictable Flows / Excessive Delays	> 50.0

Source: Hales Engineering Descriptions, based on Highway Capacity Manual, 2010 Methodology (Transportation Research Board, 2010)



II. EXISTING (2016) BACKGROUND CONDITIONS

A. Purpose

The purpose of the existing (2016) background analysis is to study the intersections and roadways during the peak travel periods of the day with background traffic and geometric conditions. Through this analysis, background traffic operational deficiencies can be identified and potential mitigation measures recommended. This analysis will provide a baseline condition that may be compared to the build conditions to identify the impacts of the development.

B. Roadway System

The primary roadways that will provide access to the project site are described below:

Main Street (SH-75) – is a state-maintained roadway that is classified by the Idaho Transportation Department (ITD) as a "regional" route in the vicinity of the project. SH-75 is a north/south route connecting Ketchum, as well as other communities such as Sun Valley and Hailey, to US-20 to the south. As a regional route in an urban area with a speed limit less than 35 mph, this roadway has minimum signal spacing of 2,640 feet, and a minimum street spacing of 660 feet. The minimum driveway distance from an upstream intersection is 250 feet, the minimum distance from a downstream intersection is 660 feet, and the minimum distance between accesses is 250 feet. Main Street (SH-75) has one travel lane in each direction and the posted speed limit in the vicinity of the proposed project is 25 mph.

C. Traffic Volumes

Hales Engineering performed weekday morning (7:00 to 9:00 a.m.) and afternoon (3:00 to 7:00 p.m.) peak period traffic counts at the following intersections:

- 10th Street / Main Street (SH-75)
- 5th Street / Main Street (SH-75)

These counts were performed on Thursday, September 1, 2016. The morning peak hour was determined to be between 8:00 and 9:00 a.m. and the evening peak hour was determined to be between 4:15 and 5:15 p.m. The evening peak hour volumes were found to be significantly higher than the morning peak hour volumes. Therefore, the evening peak hour volumes were used in the analysis to represent the worst-case conditions. Detailed count data are included in Appendix A. At the request of Ketchum City staff, anticipated traffic from the nearby Ketchum Community School was also added into the existing (2016) background traffic.



Peak period pedestrian/bicycle volumes were collected along with the vehicular volumes at each of the study intersections. While significant pedestrian/bicycle volumes were observed at the 5th Street / Main Street (SH-75) intersection, no pedestrians or bicyclists were observed at the 10th Street / Main Street (SH-75) intersection. Data collected on an alternative day (Monday, September 5, 2016, Labor Day) showed a total of 17 pedestrians/bicyclists at the 10th Street / Main Street (SH-75) intersection, with only 3 of those being during the p.m. peak hour. (Pedestrian/bicycle data is included alongside the vehicle count data in Appendix A.)

Figure 2 shows the existing p.m. peak hour volume as well as intersection geometry at the study intersections.

D. Level of Service Analysis

Using Synchro/SimTraffic, which follow the Highway Capacity Manual (HCM) 2010 methodology introduced in Chapter I, the p.m. peak hour LOS was computed for the study intersection. The results of this analysis are reported in Table 2 (see Appendix B for the detailed LOS reports). Multiple runs of SimTraffic were used to provide a statistical evaluation of the intersection. These results serve as a baseline condition for the impact analysis of the proposed development during existing (2016) conditions. As shown in Table 2, both study intersections are currently operating at LOS B during the p.m. peak hour.

Table 2 Existing (2016) Background p.m. Peak Hour Level of Service

Intersection	Worst Approach			Overall Intersection		
Description Cont		Approach ^{1,3}	Aver. Delay (Sec/Veh) ¹	LOS ¹	Aver. Delay (Sec/Veh) ²	LOS ²
10 th Street / Main Street (SH-75)	EB Stop	EB	10.7	В	-	-
5 th Street / Main Street (SH-75)	Signal	-	-	-	10.8	В

- 1. This represents the worst approach LOS and delay (seconds / vehicle) and is only reported for non-all-way stop unsignalized intersections.
- 2. This represents the overall intersection LOS and delay (seconds / vehicle) and is reported for all-way stop, roundabout, and signalized intersections.
- 3. Southbound = Southbound approach, etc.

Source: Hales Engineering, September 2016

E. Queuing Analysis

Hales Engineering calculated the 95^{th} percentile queue lengths for each of the study intersections. The queue reports can be found in Appendix D. The 95^{th} percentile queues on the north- and eastbound approaches to the 10^{th} Street / Main Street (SH-75) intersection were observed to extend for approximately 85 feet. The queues on the four approaches to the 5^{th} Street / Main



Street (SH-75) intersection were observed to extend for between approximately 120 feet and 200 feet. No other significant queuing was observed.

F. Mitigation Measures

No mitigation measures are recommended.



Figure 2 Existing (2016) background p.m. peak hour traffic volumes.



III. PROJECT CONDITIONS

A. Purpose

The project conditions analysis explains the type and intensity of development. This provides the basis for trip generation, distribution, and assignment of project trips to the surrounding study intersections defined in the Introduction.

B. Project Description

This study addresses the traffic impacts associated with the gas station in Ketchum, Idaho. The proposed gas station will be located on the southwest corner of the Main Street (SH-75) / 10th Street intersection. A site plan for the proposed development can be found in Appendix C.

The proposed land use for the development has been identified as follows:

Gasoline/Service Station with Convenience Market 8 Vehicle Fueling Positions

C. Trip Generation

As directed by Ketchum City staff, trip generation for the development was calculated using data collected at an existing gas station in the area that was determined to be characteristically similar to the proposed Bracken Station. The gas station identified by Ketchum City staff is also located on SH-75 in the nearby town of Hailey, Idaho. This gas station has 14 fueling stations, a convenience store, and a reportedly popular food truck on site.

Data was gathered in the morning (7:00 to 9:00 a.m.) and afternoon (3:00 to 7:00 p.m.) peak periods on Thursday, September 1, 2016. The number of entering and exiting vehicles, the vehicle classification, and the duration of time that each vehicle remained on-site was recorded. A summary of these data can be found in Appendix E, however for information purposes, the average dwell time for a fueling vehicles was 5 minutes and 05 seconds, average dwell time for someone using the C-store was 5 minutes and 51 seconds, and the average dwell time for someone fueling and using the C-store was 9 minutes and 37 seconds. These data were used to determine a trip generation rate using the number of fueling positions as the independent variable (similar to the method used in the Institute of Transportation Engineers (ITE) *Trip Generation (9th Edition, 2012)*). Trip Generation for the proposed project is included in Table 3.

Data collected at the characteristically similar gas station showed that approximately six percent of all trips to the gas station were pedestrian/bicycle trips. This would equate to 6 pedestrian/bicycle trips during the p.m. peak hour.



D. Trip Distribution and Assignment

Project traffic is assigned to the roadway network based on the type of trip and the proximity of project access points to major streets, high population densities, and regional trip attractions. Existing travel patterns observed during data collection also provide helpful guidance to establishing these distribution percentages, especially in close proximity to the site. The resulting distribution of projected generated trips is as follows:

To/From Project:

- 40% North
- 60% South

These trip distribution assumptions and the prevailing movements at each intersection were used to assign the evening peak hour generated traffic at the study intersections to create trip assignment for the proposed development. Trip assignment for the development is shown in Figure 3.

		Table 3 ID Ketchum Gas St Trip Generat						
P.M. Peak Hour Land Use ¹	Number of Units	Unit	Trip Generation	%	% Fuiting	Trips	Trips	Total p.m.
Gasoline/Service Station with Convenience Marke		Type Vehicle Fueling Positions	90	Entering 50%	Exiting 50%	Entering 45	Exiting 45	Trips 90
Project Total p.m. Peak Hour Trips		J				45	45	90
1. Land Use Code from the Institute of Transportation Engineers Trip Generation Manual (9th Edition - 2012)								
SOURCE: Hales Engineering, September 2016								

E. Access

The proposed access for the site will be gained at the following locations (see also site plan in Appendix C):

Main Street (SH-75):

One full-movement "boulevard approach" accesses is proposed on Main Street (SH-75) approximately 77 feet south of 10th Street. A "boulevard approach" consists of two forty-foot wide openings in the curb separated by a small island. One opening is for ingress movements, and the other for egress movements.

E. Pedestrian Access and Circulation

Pedestrians and bicyclists will access the site from the sidewalk that is planned to be constructed along the frontage of the proposed project, as well as from a proposed stairway to be constructed down to 10th Street.



A pedestrian analysis was completed in June 2016 by Alta Planning and Design. (This report is included in Appendix F.) The Alta report identified three "catchment areas" from where pedestrians would travel to arrive at the proposed project site, likely travel routes to and from each area, as well as deficiencies in pedestrian facilities along each of these routes. This study recommends that gaps in sidewalk connectivity be filled along Main Street (SH-75) and 10th Street; that crosswalks be installed at the 10th Street / Main Street (SH-75) and 10th Street; and that pedestrian facility enhancements (such as rectangular rapid flashing beacons (RRFB)) be installed on Main Street (SH-75) to enhance safety.

A low number of pedestrians and bicyclists were observed during the a.m. and p.m. peak traffic periods. Alternate data collection on a holiday showed more pedestrian and bicycle activity, suggesting that this type of traffic is more prevalent at off-peak times or on weekends.

If it is determined by city staff that marked pedestrian crosswalks are to be installed at mid-block locations, or at approaches to intersections that are not stop controlled, pedestrian activated RRFBs should be installed to increase the visibility of the pedestrian facility to drivers on Main Street (SH-75).

It is also possible that by installing RRFBs, or other pedestrian crossing enhancements, at 9th Street and/or 10th Street, that the relative visibility of the existing mid-block pedestrian crossings at approximately 8th Street and 7th Street may be reduced. It is recommended that Ketchum City consider installing pedestrian activated RRFBs at these locations, or consider removing redundant mid-block pedestrian crossings on this segment of Main Street (SH-75).



Figure 3 Trip assignment for p.m. peak hour.



IV. EXISTING (2016) PLUS PROJECT CONDITIONS

A. Purpose

This section of the report examines the traffic impacts of the proposed project at each of the study intersections. The net trips generated by the proposed development were combined with the existing background traffic volumes to create the existing plus project conditions. This scenario provides valuable insight into the potential impacts of the proposed project on background traffic conditions.

B. Traffic Volumes

Project trips were assigned to the study intersections based on the trip distribution percentages discussed in Chapter III and permitted intersection turning movements. The existing (2016) plus project p.m. peak hour volumes were generated for the study intersections and are shown in Figure 4.

C. Level of Service Analysis

Using Synchro/SimTraffic, which follow the Highway Capacity Manual (HCM) 2010 methodology introduced in Chapter I, the p.m. peak hour LOS was computed for each study intersection. The results of this analysis are reported in Table 4 (see Appendix B for the detailed LOS reports). Multiple runs of SimTraffic were used to provide a statistical evaluation of the interaction between the intersections. As shown in Table 4, both study intersections, as well as the project access, are anticipated to operate at acceptable levels of service during the p.m. peak hour with project traffic added.

D. Queuing Analysis

Hales Engineering calculated the 95th percentile queue lengths for each of the study intersections. The queue reports can be found in Appendix D. During the p.m. peak hour, the 95th percentile queue lengths on the 10th Street / Main Street (SH-75) intersection are anticipated to extend for approximately 70 feet on the eastbound approach and approximately 80 feet on the northbound approach with project traffic added (note: this occurs with a single northbound lane). The 95th percentile queue length on northbound Main Street (SH-75) at the proposed project access is anticipated to extend for approximately 50 feet. The queues on the four approaches to the 5th Street / Main Street (SH-75) intersection are anticipated to remain in the range of approximately 120 feet and 200 feet with project traffic added.



E. Mitigation Measures

Although the delays at the study intersections are anticipated to be minimal, it is recommended that a two-way left-turn lane be constructed from a location north of 10th Street to a location south of the project. This will allow northbound vehicles turning left onto 10th Street or into the project access to decelerate and/or queue without blocking the flow of through traffic. This will improve traffic safety in the area, as well as minimize delays. No other mitigation measures are recommended.

Table 4 Existing (2016) Plus Project p.m. Peak Hour Level of Service

Intersection	Wor	st Approach	Overall Intersection			
Description	Control	Approach ^{1,3}	Aver. Delay (Sec/Veh)¹	LOS ¹	Aver. Delay (Sec/Veh) ²	LOS ²
10 th Street / Main Street (SH-75)	EB Stop	EB	11.5	В	-	-
5 th Street / Main Street (SH-75)	Signal	-	-	-	10.7	В
Project Access / Main Street (SH-75)	EB Stop	EB	5.9	Α	-	-

^{1.} This represents the worst approach LOS and delay (seconds / vehicle) and is only reported for non-all-way stop unsignalized intersections.

Source: Hales Engineering, September 2016

^{2.} This represents the overall intersection LOS and delay (seconds / vehicle) and is reported for all-way stop, roundabout, and signalized intersections.

^{3.} Southbound = Southbound approach, etc.



Figure 4 Existing (2016) plus project p.m. peak hour traffic volumes.



V. FUTURE (2020) BACKGROUND CONDITIONS

A. Purpose

The purpose of the future (2020) background analysis is to study the intersections and roadways during the peak travel periods of the day for future background traffic and geometric conditions. Through this analysis, future background traffic operational deficiencies can be identified and potential mitigation measures recommended.

B. Roadway Network

Based on information received from city staff, no improvements are planned for any of the roadways or intersections within the study area before 2020.

C. Traffic Volumes

Hales Engineering used data from a nearby automatic traffic recorder (ATR 68) to calculate an annual growth rate of 1.1% for traffic on Main Street (SH-75). This growth rate was used to project future (2020) traffic volumes for the study intersections. At the request of Ketchum City staff, anticipated traffic from the nearby Ketchum Community School was also added into the future (2020) background traffic volumes. Future 2020 p.m. peak hour turning movement volumes are shown in Figure 5.

D. Level of Service Analysis

Using Synchro/SimTraffic, which follow the Highway Capacity Manual (HCM) 2010 methodology introduced in Chapter I, the p.m. peak hour LOS was computed for each study intersection. The results of this analysis are reported in Table 5 (see Appendix B for the detailed LOS reports). Multiple runs of SimTraffic were used to provide a statistical evaluation of the interaction between the intersections. These results serve as a baseline condition for the impact analysis of the proposed development for future (2020) conditions. As shown in Table 5, both study intersections are anticipated to operate at LOS B during the p.m. peak hour with future (2020) background traffic conditions.

E. Queuing Analysis

Hales Engineering calculated the 95th percentile queue lengths for each of the study intersections. The queue reports can be found in Appendix D. No significant changes to the 95th percentile queues are anticipated with projected future (2020) background traffic conditions.



F. Mitigation Measures

No mitigation measures are recommended.

Table 5 Future (2020) Background p.m. Peak Hour Level of Service

Intersection	Worst Approach			Overall Intersection		
Description Cont		Approach ^{1,3}	Aver. Delay (Sec/Veh)¹	LOS ¹	Aver. Delay (Sec/Veh) ²	LOS ²
10 th Street / Main Street (SH-75)	EB Stop	EB	10.2	В	-	-
5 th Street / Main Street (SH-75)	Signal	-	-	-	11.2	В

^{1.} This represents the worst approach LOS and delay (seconds / vehicle) and is only reported for non-all-way stop unsignalized intersections.

Source: Hales Engineering, September 2016

^{2.} This represents the overall intersection LOS and delay (seconds / vehicle) and is reported for all-way stop, roundabout, and signalized intersections.

^{3.} Southbound = Southbound approach, etc.



Figure 5 Future (2020) background p.m. peak hour volumes.



VI. FUTURE (2020) PLUS PROJECT CONDITIONS

A. Purpose

The purpose of the future (2020) plus project analysis is to study the intersections and roadways during the peak travel periods of the day for future background traffic and geometric conditions plus the net trips generated by the proposed development. This scenario provides valuable insight into the potential impacts of the proposed project on future background traffic conditions.

B. Traffic Volumes

Trips were assigned to the study intersections based on the trip distribution percentages discussed in Chapter III and permitted intersection turning movements. It was also assumed that the previously recommended center two-way left-turn lane had been constructed along the project frontage.

The future (2020) plus project p.m. peak hour volumes were generated for the study intersections and are shown in Figure 6.

C. Level of Service Analysis

Using Synchro/SimTraffic, which follow the Highway Capacity Manual (HCM) 2010 methodology introduced in Chapter I, the p.m. peak hour LOS was computed for each study intersection. The results of this analysis are reported in Table 6 (see Appendix B for the detailed LOS reports). Multiple runs of SimTraffic were used to provide a statistical evaluation of the interaction between the intersections. As shown in Table 6, both study intersections, as well as the project access, are anticipated to operate at acceptable levels of service during the p.m. peak hour with project traffic added.

D. Queuing Analysis

Hales Engineering calculated the 95th percentile queue lengths for each of the study intersections. The queue reports can be found in Appendix D. During the p.m. peak hour, the 95th percentile queue length on the northbound approach to the Main Street (SH-75) / 10th Street intersection is anticipated to extend for approximately 50 feet, while the queue length on the eastbound approach is anticipated to extend for approximately 80 feet. The northbound queue length on Main Street (SH-75) at the proposed project access is anticipated to extend for approximately 45 feet. It is anticipated that the 95th percentile queues at the 5th Street / Main Street (SH-10) intersection will remain unchanged with project traffic added.



E. Mitigation Measures

No additional mitigation measures are recommended.

Table 6 Future (2020) Plus Project p.m. Peak Hour Level of Service

Intersection		Wor	st Approach		Overall Inters	ection
Description	Control	Approach ^{1,3}	Aver. Delay (Sec/Veh) ¹	LOS ¹	Aver. Delay (Sec/Veh) ²	LOS ²
10 th Street / Main Street (SH-75)	EB Stop	EB	10.8	В	-	-
5 th Street / Main Street (SH-75)	Signal	-	-	-	11.1	В
Project Access / Main Street (SH-75)	EB Stop	EB	7.9	Α	-	-

^{1.} This represents the worst approach LOS and delay (seconds / vehicle) and is only reported for non-all-way stop unsignalized intersections.

Source: Hales Engineering, September 2016

^{2.} This represents the overall intersection LOS and delay (seconds / vehicle) and is reported for all-way stop, roundabout, and signalized intersections.

^{3.} Southbound = Southbound approach, etc.



Figure 6 Future (2020) plus project p.m. peak hour volumes.



VII. FUTURE (2026) BACKGROUND CONDITIONS

A. Purpose

The purpose of the future (2026) background analysis is to study the intersections and roadways during the peak travel periods of the day for future background traffic and geometric conditions. Through this analysis, future background traffic operational deficiencies can be identified and potential mitigation measures recommended.

B. Roadway Network

Based on information received from city staff, no improvements are planned for any of the roadways or intersections within the study area before 2026. It was assumed that the signal timing plan at the 5th Street / Main Street (SH-75) intersection had been updated.

C. Traffic Volumes

Hales Engineering used data from a nearby automatic traffic recorder (ATR 68) to calculate an annual growth rate of 1.1% for traffic on Main Street (SH-75). This growth rate was used to project future (2026) traffic volumes for the study intersections. At the request of Ketchum City staff, anticipated traffic from the nearby Ketchum Community School, the proposed Warm Springs Ranch Resort, and the reported potential development of the Stock Lumber site were also added into the future (2026) background traffic volumes. Future 2026 p.m. peak hour turning movement volumes are shown in Figure 7.

D. Level of Service Analysis

Using Synchro/SimTraffic, which follow the Highway Capacity Manual (HCM) 2010 methodology introduced in Chapter I, the p.m. peak hour LOS was computed for each study intersection. The results of this analysis are reported in Table 7 (see Appendix B for the detailed LOS reports). Multiple runs of SimTraffic were used to provide a statistical evaluation of the interaction between the intersections. These results serve as a baseline condition for the impact analysis of the proposed development for future (2026) conditions. As shown in Table 7, both study intersections are anticipated to operate at LOS B during the p.m. peak hour with future (2026) background traffic conditions.

E. Queuing Analysis

Hales Engineering calculated the 95th percentile queue lengths for each of the study intersections. The queue reports can be found in Appendix D. The anticipated 95th percentile queue lengths at the 10th Street / Main Street (SH-75) intersection are anticipated to extend for approximately 110



feet on both the north- and eastbound approaches with projected future (2026) background traffic conditions. The 95th percentile queues on the northbound approach to the 5th Street / Main Street (SH-75) intersection are anticipated to extend for over 350 feet.

F. Mitigation Measures

A mitigation that could be implemented at 10th Street / Main Street (SH-75) is to signalize the intersection when traffic volume warrants are met, as identified in the *Manual on Uniform Traffic Control Devices* (MUTCD), Federal Highway Administration, 2009. No additional mitigation measures are recommended.

Table 7 Future (2026) Background p.m. Peak Hour Level of Service

Intersection		Wor	st Approach		Overall Inters	ection
Description	Control	Approach ^{1,3}	Aver. Delay (Sec/Veh) ¹	LOS ¹	Aver. Delay (Sec/Veh) ²	LOS ²
10 th Street / Main Street (SH-75)	EB Stop	EB	13.7	В	-	-
5 th Street / Main Street (SH-75)	Signal	-	-	-	16.5	В

^{1.} This represents the worst approach LOS and delay (seconds / vehicle) and is only reported for non-all-way stop unsignalized intersections.

Source: Hales Engineering, September 2016

^{2.} This represents the overall intersection LOS and delay (seconds / vehicle) and is reported for all-way stop, roundabout, and signalized intersections.

^{3.} Southbound = Southbound approach, etc



Figure 7 Future (2026) background p.m. peak hour volumes.



VIII. FUTURE (2026) PLUS PROJECT CONDITIONS

A. Purpose

The purpose of the future (2026) plus project analysis is to study the intersections and roadways during the peak travel periods of the day for future background traffic and geometric conditions plus the net trips generated by the proposed development. This scenario provides valuable insight into the potential impacts of the proposed project on future background traffic conditions.

B. Traffic Volumes

Trips were assigned to the study intersections based on the trip distribution percentages discussed in Chapter III and permitted intersection turning movements. It was also assumed that the previously recommended center TWLTL had been constructed along the project frontage.

The future (2026) plus project p.m. peak hour volumes were generated for the study intersections and are shown in Figure 8.

C. Level of Service Analysis

Using Synchro/SimTraffic, which follow the Highway Capacity Manual (HCM) 2010 methodology introduced in Chapter I, the p.m. peak hour LOS was computed for each study intersection. The results of this analysis are reported in Table 8 (see Appendix B for the detailed LOS reports). Multiple runs of SimTraffic were used to provide a statistical evaluation of the interaction between the intersections. As shown in Table 8, the 10th Street / Main Street (SH-75) intersection is anticipated to operate at LOS F during the p.m. peak hour with project traffic added. All other study intersections are anticipated to operate at acceptable levels of service.

D. Queuing Analysis

Hales Engineering calculated the 95th percentile queue lengths for each of the study intersections. The queue reports can be found in Appendix D. During the p.m. peak hour, the 95th percentile queue length on the northbound approach to the Main Street (SH-75) / 10th Street intersection is anticipated to extend for approximately 70 feet, while the queue length on the eastbound approach is anticipated to extend for approximately 250 feet. The northbound queue length on Main Street (SH-75) at the proposed project access is anticipated to extend for approximately 60 feet. It is anticipated that the 95th percentile queues on the northbound approach to the 5th Street / Main Street (SH-10) intersection will extend for approximately 450 feet with project traffic added.



E. Mitigation Measures

Although significant delays are anticipated on the eastbound approach to the 10th Street / Main Street (SH-75) intersection, it is generally expected that executing a left-turn movement from a stop-controlled approach onto a busy highway during peak traffic periods. The addition of a separate right-turn lane on the eastbound approach to the 10th Street / Main Street (SH-75) intersection would likely mitigate the delay and queuing on the approach. However, the skewed geometry of the intersection may render this mitigation measure unworkable due to the constrained turning radius that would be created. No additional mitigation measures are recommended.

Table 8 Future (2026) Plus Project p.m. Peak Hour Level of Service

Intersection		Wor	st Approach		Overall Inters	ection
Description	Control	Approach ^{1,3}	Aver. Delay (Sec/Veh) ¹	LOS ¹	Aver. Delay (Sec/Veh) ²	LOS ²
10 th Street / Main Street (SH-75)	EB Stop	EB	>50	F	-	-
5 th Street / Main Street (SH-75)	Signal	-	-	-	18.9	В
Project Access / Main Street (SH-75)	EB Stop	EB	13.1	В	-	-

^{1.} This represents the worst approach LOS and delay (seconds / vehicle) and is only reported for non-all-way stop unsignalized intersections.

Source: Hales Engineering, September 2016

^{2.} This represents the overall intersection LOS and delay (seconds / vehicle) and is reported for all-way stop, roundabout, and signalized intersections.

^{3.} Southbound = Southbound approach, etc.



Figure 8 Future (2026) plus project p.m. peak hour volumes.



IX. HYPOTHETICAL FUTURE (2026) PLUS PROJECT CONDITIONS

A. Purpose

The purpose of the hypothetical future (2026) plus project analysis is to study the intersections and roadways during the peak travel periods of the day for future background traffic and geometric conditions plus the net trips generated by a hypothetical land use. The analysis of this scenario was requested by Ketchum City staff.

B. Trip Generation

As directed by Ketchum City staff, trip generation for this hypothetical scenario was determined using data collected by university students in February 2010 at a gas station on US-89 in Provo, Utah. The data collected showed that approximately 185 total trips were generated at this gas station during the p.m. peak hour.

C. Traffic Volumes

Trips were assigned to the study intersections based on the trip distribution percentages discussed in Chapter III and permitted intersection turning movements. The hypothetical future (2026) plus project p.m. peak hour volumes were generated for the study intersections and are shown in Figure 9.

D. Level of Service Analysis

Using Synchro/SimTraffic, which follow the Highway Capacity Manual (HCM) 2010 methodology introduced in Chapter I, the p.m. peak hour LOS was computed for each study intersection. The results of this analysis are reported in Table 9 (see Appendix B for the detailed LOS reports). Multiple runs of SimTraffic were used to provide a statistical evaluation of the interaction between the intersections. As shown in Table 9, the 10th Street / Main Street (SH-75) intersection would be anticipated to operate at LOS F during the p.m. peak hour with project traffic added. All other study intersections would be anticipated to operate at acceptable levels of service.

E. Queuing Analysis

Hales Engineering calculated the 95th percentile queue lengths for each of the study intersections. The queue reports can be found in Appendix D. During the p.m. peak hour, the 95th percentile queue length on the northbound approach to the Main Street (SH-75) / 10th Street intersection would be anticipated to extend for approximately 70 feet, while the queue length on the eastbound approach is anticipated to extend for approximately 350 feet. The northbound queue length on Main Street (SH-75) at the proposed project access would be anticipated to extend for



approximately 80 feet. It would be anticipated that the 95th percentile queues on the northbound approach to the 5th Street / Main Street (SH-10) intersection will extend for approximately 450 feet with project traffic added.

Table 9 Hypothetical Future (2026) Plus Project p.m. Peak Hour Level of Service

Intersection		Wor	st Approach		Overall Intersection				
Description	Control	Approach ^{1,3}	Aver. Delay (Sec/Veh) ¹	LOS ¹	Aver. Delay (Sec/Veh) ²	LOS ²			
10 th Street / Main Street (SH-75)	EB Stop	EB	>50	F	-	-			
5 th Street / Main Street (SH-75)	Signal	-	-	-	25.2	С			
Project Access / Main Street (SH-75)	EB Stop	EB	17.4	С	-	-			

^{1.} This represents the worst approach LOS and delay (seconds / vehicle) and is only reported for non-all-way stop unsignalized intersections.

Source: Hales Engineering, September 2016

^{2.} This represents the overall intersection LOS and delay (seconds / vehicle) and is reported for all-way stop, roundabout, and signalized intersections.

^{3.} Southbound = Southbound approach, etc.



Figure 9 Hypothetical future (2026) plus project p.m. peak hour volumes.



APPENDIX A

Turning Movement Counts

L2DataCollection.com Idaho (208) 860-7554 Utah (801) 413-2993

Study: HALE0048

Intersection: SH-75 / 10th Street

City: Ketchum, Idaho Control: Stop Sign File Name: SH75 & 10th D1

Site Code : 00000000 Start Date : 9/1/2016

Page No : 1

Groups Printed- General Traffic - 3+ Axle Heavy Trucks

		SH	[-75	•		SH		Heavy 1ruc		10th S	Street		
		From				From				From So			
Start Time	Bear Right	Thru	Peds	App. Total	Thru	Hard Left	Peds	App. Total	Hard Right	Bear Left	Peds	App. Total	Int. Total
07:00 AM	4	13	0	17	34	4	0	38	3	3	0	6	61
07:15 AM	5	21	0	26	51	2	0	53	0	11	0	11	90
07:30 AM	14	26	0	40	52	10	0	62	3	14	0	17	119
07:45 AM	14	36	0	50	47	7	0	54	3	10	0	13	117
Total	37	96	0	133	184	23	0	207	9	38	0	47	387
08:00 AM	5	32	0	37	58	7	0	65	4	17	0	21	123
08:15 AM	3	31	0	34	60	8	0	68	12	12	0	24	126
08:30 AM	4	30	0	34	57	7	0	64	8	11	0	19	117
08:45 AM	8	28	0	36	60	8	0	68	7	13	0	20	124
Total	20	121	0	141	235	30	0	265	31	53	0	84	490
03:00 PM	9	70	0	79	59	12	0	71	12	15	0	27	177
03:15 PM	7	82	0	89	73	13	0	86	13	14	0	27	202
03:30 PM	8	81	0	89	50	10	0	60	14	6	0	20	169
03:45 PM	12	75	0	87	59	12	0	71	10	12	0	22	180
Total	36	308	0	344	241	47	0	288	49	47	0	96	728
04:00 PM	20	85	0	105	61	10	0	71	21	10	0	31	207
04:15 PM	15	87	0	102	65	11	0	76	10	10	0	20	198
04:30 PM	10	91	0	101	54	10	0	64	11	15	0	26	191
04:45 PM	14	112	0	126	69	13	0	82	21	8	0	29	237
Total	59	375	0	434	249	44	0	293	63	43	0	106	833
05:00 PM	15	81	0	96	70	14	0	84	19	18	0	37	217
05:15 PM	9	55	0	64	53	12	0	65	8	22	0	30	159
05:30 PM	8	61	0	69	63	5	0	68	15	10	0	25	162
05:45 PM	5	58	00	63	52	6	0	58	6	8	0	14	135
Total	37	255	0	292	238	37	0	275	48	58	0	106	673
06:00 PM	9	57	0	66	54	5	0	59	9	10	0	19	144
06:15 PM	8	37	0	45	62	10	0	72	6	5	0	11	128
06:30 PM	9	48	0	57	34	1	0	35	6	7	0	13	105
06:45 PM	1	36	0	37	39	6	0	45	3	5	0	8	90
Total	27	178	0	205	189	22	0	211	24	27	0	51	467
Grand Total	216	1333	0	1549	1336	203	0	1539	224	266	0	490	3578
Apprch %	13.9	86.1	0		86.8	13.2	0		45.7	54.3	0		
Total %	6	37.3	0	43.3	37.3	5.7	0	43	6.3	7.4	0	13.7	
General Traffic	216	1315	0	1531	1316	203	0	1519	224	265	0	489	3539
% General Traffic	100	98.6	0	98.8	98.5	100	0	98.7	100	99.6	0	99.8	98.9
3+ Axle Heavy Trucks	0	18	0	18	20	0	0	20	0	1	0	1	39
% 3+ Axle Heavy Trucks	0	1.4	0	1.2	1.5	0	0	1.3	0	0.4	0	0.2	1.1

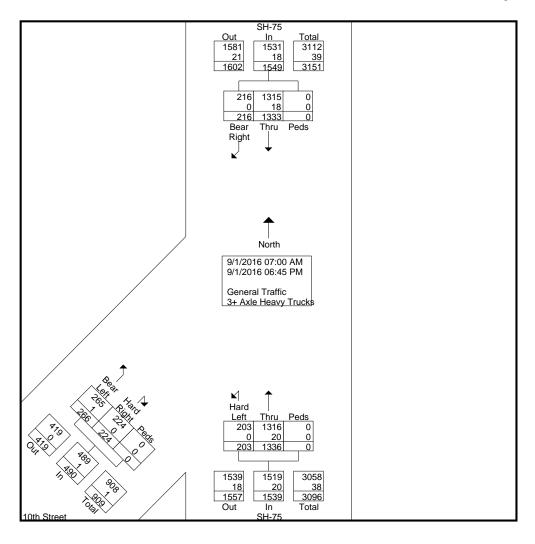
L2DataCollection.com Idaho (208) 860-7554 Utah (801) 413-2993

Study: HALE0048

Intersection: SH-75 / 10th Street

City: Ketchum, Idaho Control: Stop Sign File Name: SH75 & 10th D1

Site Code : 00000000 Start Date : 9/1/2016



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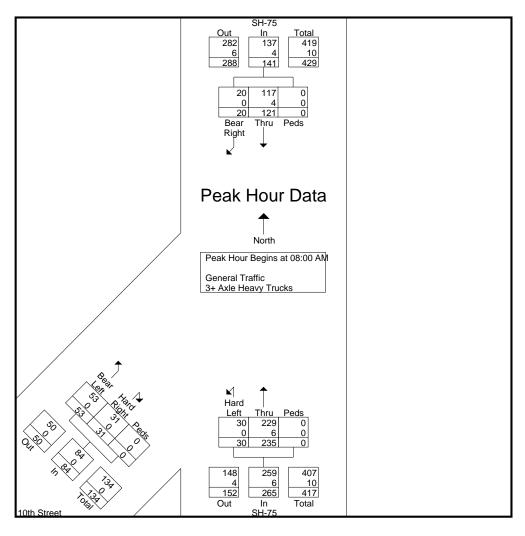
Study: HALE0048

Intersection: SH-75 / 10th Street

City: Ketchum, Idaho Control: Stop Sign File Name: SH75 & 10th D1

Site Code : 00000000 Start Date : 9/1/2016

		SH	-75			SH	I-75			10th	Street		
		From	North			From	South			From So	outhwest		
Start Time	Bear Right	Thru	Peds	App. Total	Thru	Hard Left	Peds	App. Total	Hard Right	Bear Left	Peds	App. Total	Int. Total
Peak Hour Analysis F	rom 07:00 A	M to 11:45	AM - Pea	k 1 of 1									
Peak Hour for Entire I	ntersection I	Begins at 08	8:00 AM										
08:00 AM	5	32	0	37	58	7	0	65	4	17	0	21	123
08:15 AM	3	31	0	34	60	8	0	68	12	12	0	24	126
08:30 AM	4	30	0	34	57	7	0	64	8	11	0	19	117
08:45 AM	8	28	0	36	60	8	0	68	7	13	0	20	124
Total Volume	20	121	0	141	235	30	0	265	31	53	0	84	490
— % App. Total	14.2	85.8	0		88.7	11.3	0		36.9	63.1	0		
PHF	.625	.945	.000	.953	.979	.938	.000	.974	.646	.779	.000	.875	.972
General Traffic	20	117	0	137	229	30	0	259	31	53	0	84	480
% General Traffic	100	96.7	0	97.2	97.4	100	0	97.7	100	100	0	100	98.0
3+ Axle Heavy Trucks	0	4	0	4	6	0	0	6	0	0	0	0	10
% 3+ Axle Heavy Trucks	0	3.3	0	2.8	2.6	0	0	2.3	0	0	0	0	2.0



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Study: HALE0048

Intersection: SH-75 / 10th Street

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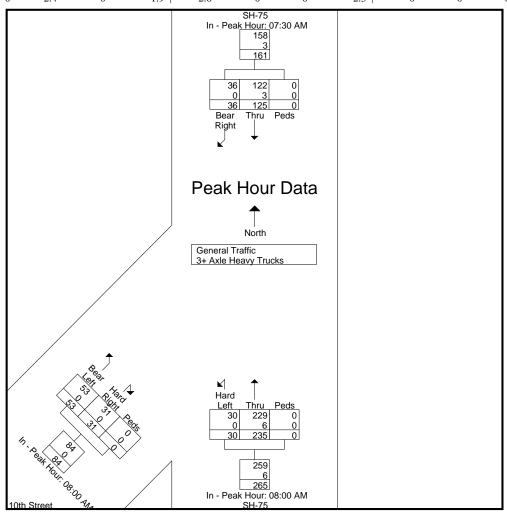
Page No : 4

		SI	I-75			SI	I-75			10th	Street		
		From	North			From	South			From So	outhwest		
Start Time	Bear Right	Thru	Peds	App. Total	Thru	Hard Left	Peds	App. Total	Hard Right	Bear Left	Peds	App. Total	Int. Total
Dook Hour Analysis E	rom 07:00	AM to 11.4	5 AM Dog	Jr 1 of 1									

Peak Hour Analysis From 07:00 AM to 11:45 AM - Peak 1 of 1

Peak Hour for Each Approach Begins at:

	07:30 AM				08:00 AM				08:00 AM			
+0 mins.	14	26	0	40	58	7	0	65	4	17	0	21
+15 mins.	14	36	0	50	60	8	0	68	12	12	0	24
+30 mins.	5	32	0	37	57	7	0	64	8	11	0	19
+45 mins.	3	31	0	34	60	8	0	68	7	13	0	20
Total Volume	36	125	0	161	235	30	0	265	31	53	0	84
% App. Total	22.4	77.6	0		88.7	11.3	0		36.9	63.1	0	
PHF	.643	.868	.000	.805	.979	.938	.000	.974	.646	.779	.000	.875
General Traffic	36	122	0	158	229	30	0	259	31	53	0	84
% General Traffic	100	97.6	0	98.1	97.4	100	0	97.7	100	100	0	100
3+ Axle Heavy Trucks	0	3	0	3	6	0	0	6	0	0	0	0
% 3+ Axle Heavy Trucks	0	2.4	0	1.9	2.6	0	0	2.3	0	0	0	0



L2DataCollection.com Idaho (208) 860-7554 Utah (801) 413-2993

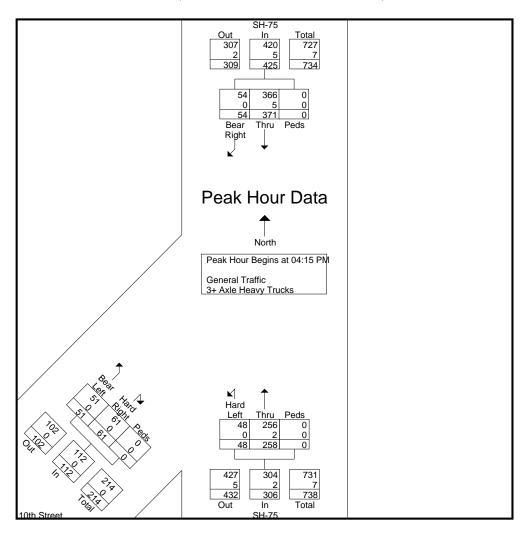
Study: HALE0048

Intersection: SH-75 / 10th Street

City: Ketchum, Idaho Control: Stop Sign File Name: SH75 & 10th D1

Site Code : 00000000 Start Date : 9/1/2016

		SE	[-75			SI	I-75			10th	Street		
		From	North			From	South			From So	outhwest		
Start Time	Bear Right	Thru	Peds	App. Total	Thru	Hard Left	Peds	App. Total	Hard Right	Bear Left	Peds	App. Total	Int. Total
Peak Hour Analysis F	rom 12:00 F	PM to 06:45	PM - Peak	1 of 1									
Peak Hour for Entire l	Intersection	ction Begins at 04:15 PM											
04:15 PM	15	87	0	102	65	11	0	76	10	10	0	20	198
04:30 PM	10	91	0	101	54	10	0	64	11	15	0	26	191
04:45 PM	14	112	0	126	69	13	0	82	21	8	0	29	237
05:00 PM	15	81	0	96	70	14	0	84	19	18	0	37	217
Total Volume	54	371	0	425	258	48	0	306	61	51	0	112	843
% App. Total	12.7	87.3	0		84.3	15.7	0		54.5	45.5	0		
PHF	.900	.828	.000	.843	.921	.857	.000	.911	.726	.708	.000	.757	.889
General Traffic	54	366	0	420	256	48	0	304	61	51	0	112	836
% General Traffic	100	98.7	0	98.8	99.2	100	0	99.3	100	100	0	100	99.2
3+ Axle Heavy Trucks	0	5	0	5	2	0	0	2	0	0	0	0	7
% 3+ Axle Heavy Trucks	0	1.3	0	1.2	0.8	0	0	0.7	0	0	0	0	0.8



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Study: HALE0048

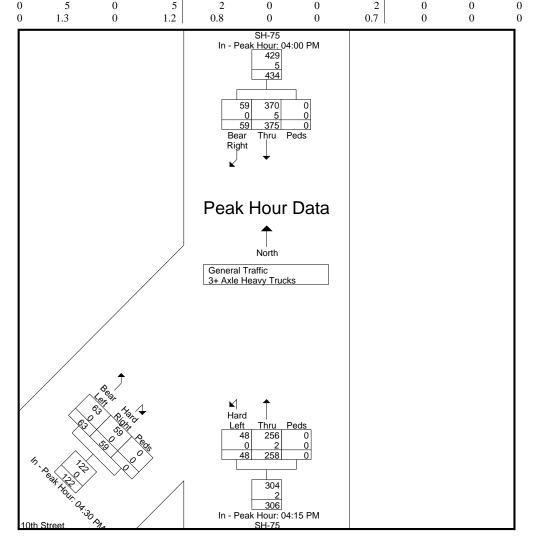
Intersection: SH-75 / 10th Street

City: Ketchum, Idaho Control: Stop Sign

File Name: SH75 & 10th D1

Site Code : 00000000 Start Date : 9/1/2016

		SH	[-75			SI	I-75			10th	Street	
		From	North			From	South			From So	uthwest	
Start Time	Bear Right	Thru	Peds	App. Total	Thru	Hard Left	Peds	App. Total	Hard Right	Bear Left	Peds	App. Total
Peak Hour Analysis F	rom 12:00 F	PM to 06:45	PM - Peak	1 of 1								
Peak Hour for Each A	pproach Be	gins at:										
	04:00 PM				04:15 PM				04:30 PM]		
+0 mins.	20	85	0	105	65	11	0	76	11	15	0	26
+15 mins.	15	87	0	102	54	10	0	64	21	8	0	29
+30 mins.	10	91	0	101	69	13	0	82	19	18	0	37
+45 mins.	14	112	0	126	70	14	0	84	8	22	0	30
Total Volume	59	375	0	434	258	48	0	306	59	63	0	122
% App. Total	13.6	86.4	0		84.3	15.7	0		48.4	51.6	0	
PHF	.738	.837	.000	.861	.921	.857	.000	.911	.702	.716	.000	.824
General Traffic	59	370	0	429	256	48	0	304	59	63	0	122
% General Traffic	100	98.7	0	98.8	99.2	100	0	99.3	100	100	0	100
3+ Axle Heavy Trucks	0	5	0	5	2	0	0	2	0	0	0	0
% 3+ Axle Heavy Trucks	0	1.3	0	1.2	0.8	0	0	0.7	0	0	0	0



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Study: HALE0048

Intersection: SH-75 / 10th Street

City: Ketchum, Idaho Control: Stop Sign File Name: SH75 & 10th D1 Site Code: 00000000

Start Date : 9/1/2016

Page No : 7

Image 1



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Study: HALE0048

Intersection: SH-75 / 5th Street

City: Ketchum, Idaho Control: Signalized

File Name: SH75 & 5th D1

Site Code : 00000000 Start Date : 9/1/2016

Page No : 1

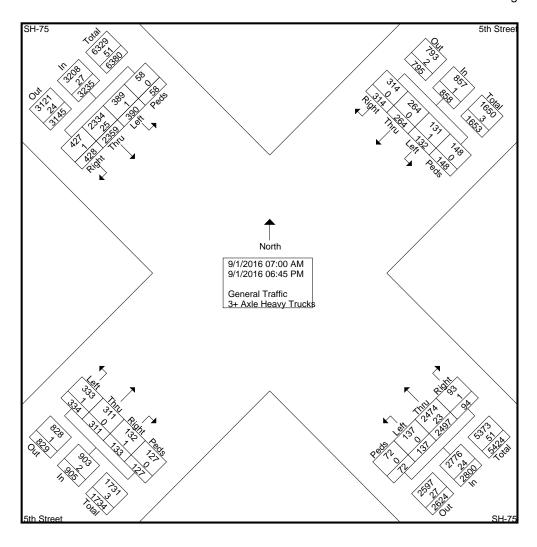
						Gro	_			al Traffi	c - 3+ A	Axle He									1
			SH-75	5			5	th Stre	et				SH-75	5			5	th Stre	eet		
		Fron	n North	ıwest			Fron	n Nortl	heast			Fron	n Soutl	heast			Fron	ı Soutl	nwest		
Start Time	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Int. Total
07:00 AM	7	39	9	1	56	4	4	1	2	11	4	91	7	1	103	4	3	4	5	16	186
07:15 AM	5	32	7	0	44	6	2	0	1	9	2	112	8	0	122	2	8	8	3	21	196
07:30 AM	5	61	2	1	69	6	2	2	1	11	2	146	5	0	153	2	7	11	1	21	254
07:45 AM	13	76	11	3	103	4	11	3	1	19	3	166	1	2	172	5	11	11	4	31	325
Total	30	208	29	5	272	20	19	6	5	50	11	515	21	3	550	13	29	34	13	89	961
08:00 AM	9	83	17	0	109	4	7	1	4	16	6	149	4	0	159	7	16	7	1	31	315
08:15 AM	13	72	13	0	98	4	7	3	0	14	5	117	6	0	128	1	11	15	1	28	268
08:30 AM	11	85	16	3	115	7	3	3	1	14	7	134	3	1	145	5	7	8	4	24	298
08:45 AM	10	67	13	4	94	8	4	3	7	22	7	127	5	1	140	3	8	14	2	27	283
Total	43	307	59	7	416	23	21	10	12	66	25	527	18	2	572	16	42	44	8	110	1164
03:00 PM	15	136	19	2	172	12	13	3	8	36	3	91	12	4	110	6	24	29	5	64	382
03:15 PM	24	129	13	8	174	19	12	9	11	51	4	104	6	5	119	9	20	15	11	55	399
03:30 PM	24	132	24	0	180	19	13	11	8	51	4	83	3	4	94	12	18	16	4	50	375
03:45 PM	30	111	23_	4	168	14	17	10	15	56	6	93	7	3	109	10	14	23	4	51	384
Total	93	508	79	14	694	64	55	33	42	194	17	371	28	16	432	37	76	83	24	220	1540
04:00 PM	42	126	15	0	183	21	25	7	3	56	5	102	7	2	116	6	23	19	5	53	408
04:15 PM	22	134	27	4	187	19	21	7	13	60	4	93	3	1	101	7	12	13	7	39	387
04:30 PM	28	129	23	1	181	15	16	8	16	55	3	101	7	5	116	9	17	13	8	47	399
04:45 PM	27	137	20	6	190	20	15	6	8	49	1	110	10	13	134	5	17	22	5	49	422
Total	119	526	85	11	741	75	77	28	40	220	13	406	27	21	467	27	69	67	25	188	1616
05:00 PM	37	137	20	1	195	22	18	7	7	54	5	105	4	3	117	3	24	22	3	52	418
05:15 PM	22	122	25	4	173	17	27	7	10	61	2	113	7	4	126	5	10	16	5	36	396
05:30 PM	11	122	15	0	148	27	6	4	0	37	2	98	3	12	115	2	12	14	9	37	337
05:45 PM	20	89	20	2	131	21	11	7	14	53	5	81	4	8	98	8	7	10	13	38	320
Total	90	470	80	7	647	87	62	25	31	205	14	397	18	27	456	18	53	62	30	163	1471
06:00 PM	19	91	16	3	129	17	9	9	6	41	4	80	5	1	90	9	16	12	8	45	305
06:15 PM	10	92	10	6	118	13	9	5	1	28	2	82	6	2	92	3	7	14	11	35	273
06:30 PM	15	86	13	3	117	10	9	11	7	37	5	59	7	0	71	3	9	9	6	27	252
06:45 PM	9	71	19	2	101	5	3	5	4	17	3	60	7	0	70	7	10	9	2	28	216
Total	53	340	58	14	465	45	30	30	18	123	14	281	25	3	323	22	42	44	27	135	1046
Grand Total	428	2359	390	58	3235	314	264	132	148	858	94	2497	137	72	2800	133	311	334	127	905	7798
Apprch %	13.2	72.9	12.1	1.8		36.6	30.8	15.4	17.2		3.4	89.2	4.9	2.6		14.7	34.4	36.9	14		
Total %	5.5	30.3	5	0.7	41.5	4	3.4	1.7	1.9	11	1.2	32	1.8	0.9	35.9	1.7	4	4.3	1.6	11.6	
General Traffic	427	2334	389	58	3208	314	264	131	148	857	93	2474	137	72	2776	132	311	333	127	903	7744
% General Traffic	99.8	98.9	99.7	100	99.2	100	100	99.2	100	99.9	98.9	99.1	100	100	99.1	99.2	100	99.7	100	99.8	99.3
3+ Axle Heavy Trucks	1	25	1	0	27	0	0	1	0	1	1	23	0	0	24	1	0	1	0	2	54
% 3+ Axle Heavy Trucks	0.2	1.1	0.3	0	0.8	0	0	0.8	0	0.1	1.1	0.9	0	0	0.9	0.8	0	0.3	0	0.2	0.7

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Study: HALE0048

Intersection: SH-75 / 5th Street

City: Ketchum, Idaho Control: Signalized File Name: SH75 & 5th D1 Site Code: 00000000 Start Date: 9/1/2016



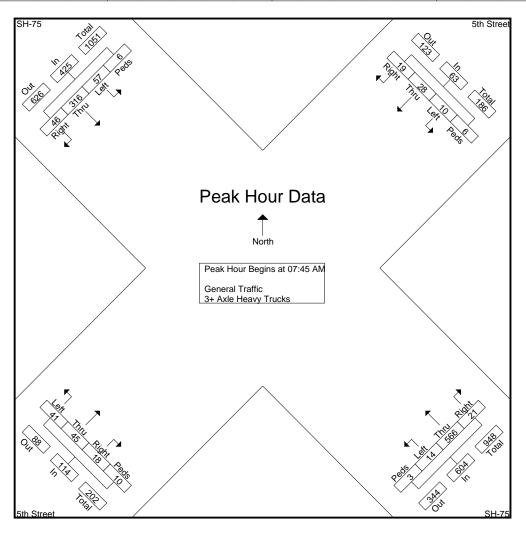
L2DataCollection.com Idaho (208) 860-7554 Utah (801) 413-2993

Study: HALE0048

Intersection: SH-75 / 5th Street

City: Ketchum, Idaho Control: Signalized File Name: SH75 & 5th D1 Site Code: 00000000 Start Date: 9/1/2016

			SH-75	5			5	th Stre	eet				SH-75	;			5	th Stre	eet		
		Fron	ı Nortl	nwest			Fron	n Nort	heast			Fron	1 South	ieast			Fron	a South	west		
Start Time	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Int. Total
Peak Hour An	alysis F	rom 07:	:00 AM	I to 11:4	45 AM - 1	Peak 1 o	of 1														
Peak Hour for	Entire 1	Intersec	tion Be	gins at	07:45 AN	Л															
07:45 AM	13	76	11	3	103	4	11	3	1	19	3	166	1	2	172	5	11	11	4	31	325
08:00 AM	9	83	17	0	109	4	7	1	4	16	6	149	4	0	159	7	16	7	1	31	315
08:15 AM	13	72	13	0	98	4	7	3	0	14	5	117	6	0	128	1	11	15	1	28	268
08:30 AM	11	85	16	3	115	7	3	3	1	14	7	134	3	1	145	5	7	8	4	24	298
Total Volume	46	316	57	6	425	19	28	10	6	63	21	566	14	3	604	18	45	41	10	114	1206
% App. Total	10.8	74.4	13.4	1.4		30.2	44.4	15.9	9.5		3.5	93.7	2.3	0.5		15.8	39.5	36	8.8		
PHF	.885	.929	.838	.500	.924	.679	.636	.833	.375	.829	.750	.852	.583	.375	.878	.643	.703	.683	.625	.919	.928



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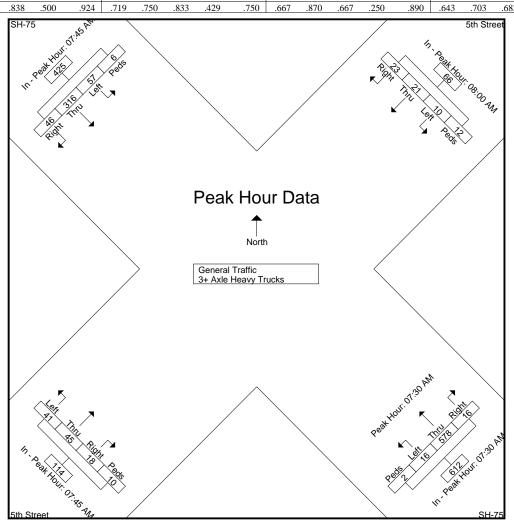
Study: HALE0048

Intersection: SH-75 / 5th Street

City: Ketchum, Idaho Control: Signalized File Name: SH75 & 5th D1 Site Code: 00000000

Start Date : 9/1/2016

			SH-75	;			5	th Stre	et				SH-75	;			5	th Stre	eet		
		Fron	n North	west			Fron	n Nort	heast			Fron	a South	ieast			Fron	1 South	ıwest		
Start Time	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Int.
Peak Hour An	alysis F	rom 07	:00 AM	to 11:4	45 AM - 1	Peak 1 c	of 1														
Peak Hour for	Each A	pproacl	n Begin	s at:																	_
	07:45 AM	I				08:00 AM					07:30 AM					07:45 AM	1				
+0 mins.	13	76	11	3	103	4	7	1	4	16	2	146	5	0	153	5	11	11	4	31	
+15 mins.	9	83	17	0	109	4	7	3	0	14	3	166	1	2	172	7	16	7	1	31	
+30 mins.	13	72	13	0	98	7	3	3	1	14	6	149	4	0	159	1	11	15	1	28	
+45 mins.	11	85	16	3	115	8	4	3	7	22	5	117	6	0	128	5	7	8	4	24	
Total Volume	46	316	57	6	425	23	21	10	12	66	16	578	16	2	612	18	45	41	10	114	
% App. Total	10.8	74.4	13.4	1.4		34.8	31.8	15.2	18.2		2.6	94.4	2.6	0.3		15.8	39.5	36	8.8		
PHF	.885	.929	.838	.500	.924	.719	.750	.833	.429	.750	.667	.870	.667	.250	.890	.643	.703	.683	.625	.919	



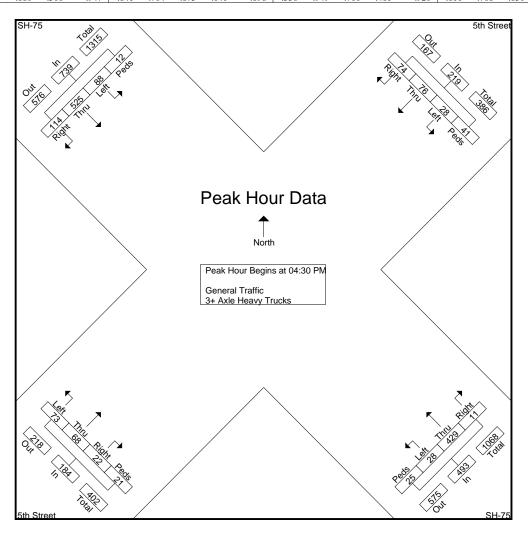
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Study: HALE0048

Intersection: SH-75 / 5th Street

City: Ketchum, Idaho Control: Signalized File Name: SH75 & 5th D1 Site Code: 00000000 Start Date: 9/1/2016

			SH-75	;			51	th Stre	et				SH-75	5			5	th Stre	et		
		Fron	n North	west			Fron	n Nortl	heast			Fron	1 South	neast			Fron	South	west		
Start Time	Rig ht	Thr u	Left	Ped s	App. Total	Rig ht	Thr u	Left	Ped s	App. Total	Right	Thr u	Left	Peds	App. Total	Right	Thr u	Left	Peds	App. Total	Int. Total
Peak Hour Ana	alysis F	rom 12:	:00 PM	to 06:4	15 PM - P	eak 1 of	1														
Peak Hour for	Entire I	ntersec	tion Be	gins at	04:30 PM	1															
04:30 PM	28	129	23	1	181	15	16	8	16	55	3	101	7	5	116	9	17	13	8	47	399
04:45 PM	27	137	20	6	190	20	15	6	8	49	1	110	10	13	134	5	17	22	5	49	422
05:00 PM	37	137	20	1	195	22	18	7	7	54	5	105	4	3	117	3	24	22	3	52	418
05:15 PM	22	122	25	4	173	17	27	7	10	61	2	113	7	4	126	5	10	16	5	36	396
Total Volume	114	525	88	12	739	74	76	28	41	219	11	429	28	25	493	22	68	73	21	184	1635
% App. Total	15.4	71	11.9	1.6		33.8	34.7	12.8	18.7		2.2	87	5.7	5.1		12	37	39.7	11.4		
PHF	.770	.958	.880	.500	.947	.841	.704	.875	.641	.898	.550	.949	.700	.481	.920	.611	.708	.830	.656	.885	.969



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Study: HALE0048

Intersection: SH-75 / 5th Street

City: Ketchum, Idaho Control: Signalized File Name: SH75 & 5th D1 Site Code: 00000000

Start Date : 9/1/2016

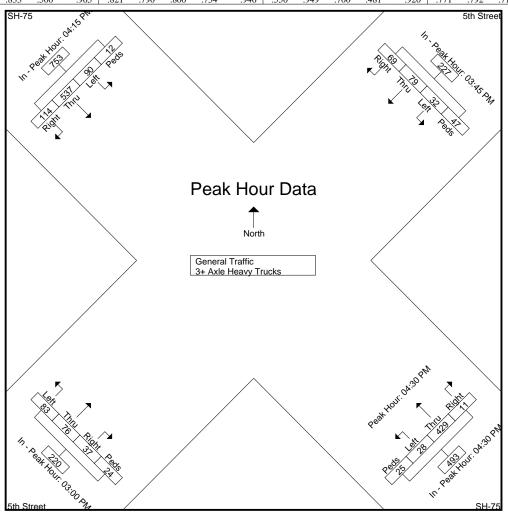
Page No : 6

			SH-75	;			5	th Stre	et				SH-75	5			5	th Stre	eet		
		Fron	n Nortl	ıwest			Fron	n Nort	heast			Fron	1 Soutl	neast			Fron	South	ıwest		
Start Time	Rig ht	Thr u	Left	Ped s	App. Total	Rig ht	Thr u	Left	Ped s	App. Total	Right	Thr u	Left	Peds	App. Total	Right	Thr u	Left	Peds	App. Total	Int. Total

Peak Hour Analysis From 12:00 PM to 06:45 PM - Peak 1 of 1

Peak Hour for Each Approach Begins at:

	04:15 PM					03:45 PM					04:30 PM					03:00 PM				
+0 mins.	22	134	27	4	187	14	17	10	15	56	3	101	7	5	116	6	24	29	5	64
+15 mins.	28	129	23	1	181	21	25	7	3	56	1	110	10	13	134	9	20	15	11	55
+30 mins.	27	137	20	6	190	19	21	7	13	60	5	105	4	3	117	12	18	16	4	50
+45 mins.	37	137	20	1	195	15	16	8	16	55	2	113	7	4	126	10	14	23	4	51
Total Volume	114	537	90	12	753	69	79	32	47	227	11	429	28	25	493	37	76	83	24	220
% App. Total	15.1	71.3	12	1.6		30.4	34.8	14.1	20.7		2.2	87	5.7	5.1		16.8	34.5	37.7	10.9	
PHF	.770	.980	.833	.500	.965	.821	.790	.800	.734	.946	.550	.949	.700	.481	.920	.771	.792	.716	.545	.859



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Study: HALE0048 Intersection: SH-75 / 5th Street

City: Ketchum, Idaho Control: Signalized

File Name: SH75 & 5th D1 Site Code : 00000000

Start Date : 9/1/2016 Page No : 7

Image 1





APPENDIX B

Level of Service Results



SimTraffic LOS Report

ID Ketchum - Bracken Station TIS Existing (2016) Background p.m. Peak Hour Project:

Analysis Period: Time Period:

Project #: *UT16-851*

10th Street & Main Street (SH-75) Unsignalized Intersection:

Type:

Annroach	Mayamant	Demand	Volume	e Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
	L	48	52	108	5.4	Α
NB	Т	530	533	101	0.9	Α
	Subtotal	578	585	101	1.3	Α
	Т	373	365	98	0.9	Α
SB	R	54	58	108	0.6	Α
	Subtotal	427	423	99	0.9	Α
	L	51	50	99	14.7	В
NE	R	61	65	107	7.7	Α
	Subtotal	112	115	103	10.7	В
Total		1,116	1,123	101	2.1	Α

5th Street & Main Street (SH-75) Signalized Intersection:

Type:

Annyoooh	Mayamant	Demand	Volume	e Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
	L	28	26	92	15.5	В
NW	Т	431	440	102	11.0	В
INVV	R	11	11	100	3.1	Α
	Subtotal	470	477	101	11.1	В
	L	88	89	101	15.7	В
SE	Т	527	522	99	8.1	Α
SE	R	114	119	104	4.7	Α
	Subtotal	729	730	100	8.5	Α
	L	73	70	96	18.2	В
NE	Т	68	63	93	19.2	В
INE	R	22	26	117	12.7	В
	Subtotal	163	159	98	17.7	В
	L	28	24	85	16.9	В
SW	Т	76	75	99	16.6	В
300	R	74	73	99	9.3	Α
	Subtotal	178	172	97	13.5	В
Total		1,540	1,538	100	10.8	В

1: 10th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.3	0.1	0.2	0.1
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.2
Total Del/Veh (s)	4.7	1.0	0.8	0.5	15.0	7.0	2.0
Vehicles Entered	13	129	90	15	12	16	275
Vehicles Exited	12	128	90	15	13	15	273
Hourly Exit Rate	48	512	360	60	52	60	1092
Input Volume	48	524	369	53	50	60	1104
% of Volume	100	98	98	113	104	100	99

1: 10th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.3	0.3	0.2	0.2	0.1
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.2
Total Del/Veh (s)	6.0	0.9	0.9	0.6	15.1	7.5	2.2
Vehicles Entered	12	133	91	16	13	19	284
Vehicles Exited	12	134	92	16	12	19	285
Hourly Exit Rate	48	536	368	64	48	76	1140
Input Volume	48	524	369	53	50	60	1104
% of Volume	100	102	100	121	96	127	103

1: 10th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.5	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.2
Total Del/Veh (s)	5.1	0.9	0.9	0.7	12.5	5.6	1.9
Vehicles Entered	13	137	94	13	12	15	284
Vehicles Exited	13	137	94	13	12	15	284
Hourly Exit Rate	52	548	376	52	48	60	1136
Input Volume	49	546	385	56	53	63	1152
% of Volume	106	100	98	93	91	95	99

1: 10th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.3	0.3	0.2	0.2	0.1
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.2
Total Del/Veh (s)	5.3	0.8	0.9	0.5	14.0	9.3	2.1
Vehicles Entered	14	134	90	14	12	15	279
Vehicles Exited	14	133	91	14	12	16	280
Hourly Exit Rate	56	532	364	56	48	64	1120
Input Volume	48	524	369	53	50	60	1104
% of Volume	117	102	99	106	96	107	101

1: 10th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.3	0.3	0.2	0.2	0.1
Total Delay (hr)	0.1	0.1	0.1	0.0	0.2	0.1	0.7
Total Del/Veh (s)	5.4	0.9	0.9	0.6	14.7	7.7	2.1
Vehicles Entered	52	533	366	58	49	65	1123
Vehicles Exited	52	533	365	58	50	65	1123
Hourly Exit Rate	52	533	365	58	50	65	1123
Input Volume	48	530	373	54	51	61	1116
% of Volume	108	101	98	108	99	107	101

2: 5th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.3	0.1	0.2	0.2	0.3	0.1	0.2	0.2	0.3	0.1	0.2	0.2
Total Delay (hr)	0.1	0.3	0.0	0.0	0.3	0.0	0.1	0.1	0.0	0.0	0.1	0.0
Total Del/Veh (s)	12.2	7.6	4.2	13.7	9.5	1.9	16.5	15.7	12.4	17.3	16.9	9.4
Vehicles Entered	22	126	31	6	107	3	16	16	7	6	20	18
Vehicles Exited	21	124	31	5	106	3	17	16	7	6	22	18
Hourly Exit Rate	84	496	124	20	424	12	68	64	28	24	88	72
Input Volume	87	522	113	28	427	11	72	67	22	28	75	73
% of Volume	97	95	110	71	99	109	94	96	127	86	117	99

2: 5th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.2
Total Delay (hr)	1.1
Total Del/Veh (s)	9.7
Vehicles Entered	378
Vehicles Exited	376
Hourly Exit Rate	1504
Input Volume	1525
% of Volume	99

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2: 5th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.2	0.1	0.4	0.3	0.3	0.1	0.2	0.2	0.2	0.2	0.2	0.2
Total Delay (hr)	0.1	0.3	0.0	0.0	0.3	0.0	0.1	0.1	0.0	0.0	0.1	0.0
Total Del/Veh (s)	14.8	7.4	4.4	14.9	10.2	2.5	18.0	18.8	11.4	18.7	14.7	8.2
Vehicles Entered	22	131	28	7	112	4	17	16	7	6	17	19
Vehicles Exited	22	133	29	7	113	4	16	16	6	6	17	18
Hourly Exit Rate	88	532	116	28	452	16	64	64	24	24	68	72
Input Volume	87	522	113	28	427	11	72	67	22	28	75	73
% of Volume	101	102	103	100	106	145	89	96	109	86	91	99

2: 5th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.2
Total Delay (hr)	1.1
Total Del/Veh (s)	10.1
Vehicles Entered	386
Vehicles Exited	387
Hourly Exit Rate	1548
Input Volume	1525
% of Volume	102

2: 5th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.7	0.3	0.4	0.3	0.3	0.1	0.2	0.2	0.3	0.1	0.2	0.2
Total Delay (hr)	0.1	0.3	0.0	0.0	0.4	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	18.6	8.8	5.1	14.6	12.1	3.4	19.4	20.9	15.2	17.9	14.7	9.9
Vehicles Entered	25	133	30	7	115	3	19	16	6	6	18	18
Vehicles Exited	26	133	30	7	114	3	19	17	6	6	18	18
Hourly Exit Rate	104	532	120	28	456	12	76	68	24	24	72	72
Input Volume	91	543	118	29	444	11	75	70	23	29	78	76
% of Volume	114	98	102	97	103	109	101	97	104	83	92	95

2: 5th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.3
Total Delay (hr)	1.3
Total Del/Veh (s)	11.8
Vehicles Entered	396
Vehicles Exited	397
Hourly Exit Rate	1588
Input Volume	1587
% of Volume	100

2: 5th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.7	0.2	0.3	0.2	0.3	0.1	0.2	0.3	0.2	0.3	0.2	0.2
Total Delay (hr)	0.1	0.3	0.0	0.0	0.3	0.0	0.1	0.1	0.0	0.0	0.1	0.0
Total Del/Veh (s)	15.2	8.3	4.8	14.2	10.8	3.9	16.0	19.1	10.2	14.1	15.6	8.7
Vehicles Entered	21	133	29	7	108	2	18	15	6	6	19	19
Vehicles Exited	20	132	29	7	108	2	18	14	6	6	18	19
Hourly Exit Rate	80	528	116	28	432	8	72	56	24	24	72	76
Input Volume	87	522	113	28	427	11	72	67	22	28	75	73
% of Volume	92	101	103	100	101	73	100	84	109	86	96	104

2: 5th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.3
Total Delay (hr)	1.2
Total Del/Veh (s)	10.6
Vehicles Entered	383
Vehicles Exited	379
Hourly Exit Rate	1516
Input Volume	1525
% of Volume	99

2: 5th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.5	0.2	0.3	0.3	0.3	0.1	0.2	0.2	0.3	0.2	0.2	0.2
Total Delay (hr)	0.4	1.2	0.2	0.1	1.4	0.0	0.4	0.3	0.1	0.1	0.3	0.2
Total Del/Veh (s)	15.7	8.1	4.7	15.5	11.0	3.1	18.2	19.2	12.7	16.9	16.6	9.3
Vehicles Entered	90	523	119	26	441	11	69	63	26	24	74	73
Vehicles Exited	89	522	119	26	440	11	70	63	26	24	75	73
Hourly Exit Rate	89	522	119	26	440	11	70	63	26	24	75	73
Input Volume	88	527	114	28	431	11	73	68	22	28	76	74
% of Volume	101	99	104	92	102	100	96	93	117	85	99	99

2: 5th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	All
Denied Delay (hr)	0.1
Denied Del/Veh (s)	0.2
Total Delay (hr)	4.7
Total Del/Veh (s)	10.8
Vehicles Entered	1539
Vehicles Exited	1538
Hourly Exit Rate	1538
Input Volume	1540
% of Volume	100

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Total Network Performance By Interval

Interval Start	4:15	4:30	4:45	5:00	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.1
Denied Del/Veh (s)	0.3	0.3	0.4	0.3	0.3
Total Delay (hr)	1.5	1.6	1.8	1.6	6.5
Total Del/Veh (s)	12.0	12.6	14.0	12.9	13.8
Vehicles Entered	404	416	422	409	1646
Vehicles Exited	404	410	423	410	1647
Hourly Exit Rate	1616	1640	1692	1640	1647
Input Volume	5992	5992	6243	5992	6055
% of Volume	27	27	27	27	27

Intersection: 1: 10th Street & Main Street (SH-75), Interval #1

Movement	NB	NE
Directions Served	LT	LR
Maximum Queue (ft)	83	79
Average Queue (ft)	30	42
95th Queue (ft)	85	87
Link Distance (ft)	263	1050
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1: 10th Street & Main Street (SH-75), Interval #2

Movement	NB	B9	B9	NE
Directions Served	LT	T		LR
Maximum Queue (ft)	95	4	3	77
Average Queue (ft)	31	1	0	44
95th Queue (ft)	91	7	6	82
Link Distance (ft)	263	194	194	1050
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 1: 10th Street & Main Street (SH-75), Interval #3

Movement	NB	NE
Directions Served	LT	LR
Maximum Queue (ft)	88	72
Average Queue (ft)	32	41
95th Queue (ft)	91	74
Link Distance (ft)	263	1050
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

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Intersection: 1: 10th Street & Main Street (SH-75), Interval #4

Movement	NB	B9	NE
Directions Served	LT	T	LR
Maximum Queue (ft)	75	2	86
Average Queue (ft)	28	0	43
95th Queue (ft)	78	4	87
Link Distance (ft)	263	194	1050
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), All Intervals

Movement	NB	В9	В9	NE
Directions Served	LT	T		LR
Maximum Queue (ft)	128	5	3	104
Average Queue (ft)	30	0	0	43
95th Queue (ft)	86	4	3	83
Link Distance (ft)	263	194	194	1050
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 2: 5th Street & Main Street (SH-75), Interval #1

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	124	112	171	84	118	120
Average Queue (ft)	85	75	102	21	69	70
95th Queue (ft)	130	117	180	92	123	118
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 2: 5th Street & Main Street (SH-75), Interval #2

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	119	114	188	133	111	115
Average Queue (ft)	87	74	110	33	69	66
95th Queue (ft)	126	122	194	139	114	121
Link Distance (ft)	194	194	872	872	838	1044
Haratara DII. Thank (0/)						

Upstream Blk Time (%)

Queuing Penalty (veh)

Storage Bay Dist (ft)

Storage Blk Time (%)

Queuing Penalty (veh)

Intersection: 2: 5th Street & Main Street (SH-75), Interval #3

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	126	130	206	115	132	118
Average Queue (ft)	98	84	136	29	78	67
95th Queue (ft)	136	139	222	115	150	124
Link Distance (ft)	194	194	872	872	838	1044
Unstroam RIV Timo (%)						

Upstream Blk Time (%)

Queuing Penalty (veh)

Storage Bay Dist (ft)

Storage Blk Time (%)

Queuing Penalty (veh)

Intersection: 2: 5th Street & Main Street (SH-75), Interval #4

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	114	124	181	120	131	116
Average Queue (ft)	86	83	116	31	73	66
95th Queue (ft)	130	130	192	120	142	121
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)						

Queuing Penalty (veh)

Storage Bay Dist (ft)

Storage Blk Time (%)

Queuing Penalty (veh)

Intersection: 2: 5th Street & Main Street (SH-75), All Intervals

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	134	141	237	181	173	154
Average Queue (ft)	89	79	116	29	72	67
95th Queue (ft)	131	128	200	118	134	121
Link Distance (ft)	194	194	872	872	838	1044
Unstream Blk Time (%)						

Queuing Penalty (veh)

Storage Bay Dist (ft)

Storage Blk Time (%)

Queuing Penalty (veh)

Network Summary

Network wide Queuing Penalty, Interval #1: 0

Network wide Queuing Penalty, Interval #2: 0

Network wide Queuing Penalty, Interval #3: 0

Network wide Queuing Penalty, Interval #4: 0

Network wide Queuing Penalty, All Intervals: 0



SimTraffic LOS Report

ID Ketchum - Bracken Station TIS Existing (2016) Plus Project p.m. Peak Hour Project:

Analysis Period: Time Period:

Project #: *UT16-851*

10th Street & Main Street (SH-75) Unsignalized Intersection:

Type:

Annroach	Mayamant	Demand	Volume	e Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
	L	48	47	97	4.4	Α
NB	Т	279	277	99	0.9	Α
IND						
	Subtotal	327	324	99	1.4	Α
	Т	391	380	97	0.9	Α
SB	R	54	54	100	0.5	Α
OB						
	Subtotal	445	434	98	0.9	Α
	L	51	54	106	16.0	С
NE	R	61	63	104	7.6	Α
	Subtotal	112	117	104	11.5	В
Total		884	875	99	2.5	Α

5th Street & Main Street (SH-75) Signalized Intersection:

Type:

Annracah	Mayamant	Demand	Volume	e Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
	L	28	26	92	17.1	В
NW	Т	451	447	99	10.9	В
INVV	R	11	11	100	2.6	Α
	Subtotal	490	484	99	11.0	В
	L	91	93	102	14.4	В
SE	Т	547	546	100	8.2	Α
SE	R	118	116	98	4.5	Α
	Subtotal	756	755	100	8.4	Α
	L	76	75	99	19.5	В
NE	Т	68	66	97	18.0	В
INE	R	22	22	99	13.4	В
	Subtotal	166	163	98	18.1	В
	L	28	29	103	16.9	В
SW	Т	76	74	98	15.7	В
300	R	78	77	99	8.7	Α
	Subtotal	182	180	99	12.9	В
Total		1,594	1,582	99	10.7	В



SimTraffic LOS Report

ID Ketchum - Bracken Station TIS Existing (2016) Plus Project p.m. Peak Hour Project:

Analysis Period: Time Period:

Project #: *UT16-851*

Main Street (SH-75) & Project Access Unsignalized Intersection:

Type:

. 7						
Approach	Movement	Demand	Volume	Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
	L	27	24	88	4.7	Α
NB	Т	577	578	100	1.4	Α
IND						
	Subtotal	604	602	100	1.5	Α
	Т	434	426	98	0.4	Α
SB	R	18	18	99	0.2	Α
OB						
	Subtotal	452	444	98	0.4	Α
	L	18	16	88	8.9	Α
EB	R	27	27	99	4.2	Α
						_
	Subtotal	45	43	96	5.9	Α
Total		1,102	1,089	99	1.3	Α

Intersection:

Type:

Mayamant	Demand	Volume	e Served	Delay/Vel	h (sec)
Movement	Volume	Avg	%	Avg	LOS
	Jovennent	Volume	Volume Avg	Volume Avg %	Volume Avg % Avg

1: 10th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.5	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.2
Total Del/Veh (s)	4.4	0.9	0.9	0.5	14.3	9.4	2.5
Vehicles Entered	11	73	99	13	13	17	226
Vehicles Exited	11	73	98	14	13	16	225
Hourly Exit Rate	44	292	392	56	52	64	900
Input Volume	48	276	387	53	50	60	874
% of Volume	92	106	101	106	104	107	103

1: 10th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.3	0.3	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.1
Total Del/Veh (s)	4.5	0.9	0.8	0.4	13.0	5.8	2.2
Vehicles Entered	11	64	90	12	12	15	204
Vehicles Exited	11	64	91	12	13	15	206
Hourly Exit Rate	44	256	364	48	52	60	824
Input Volume	48	276	387	53	50	60	874
% of Volume	92	93	94	91	104	100	94

1: 10th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.3	0.3	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.2
Total Del/Veh (s)	4.6	0.9	0.8	0.5	18.4	6.4	2.6
Vehicles Entered	12	72	96	15	14	14	223
Vehicles Exited	12	72	96	15	14	14	223
Hourly Exit Rate	48	288	384	60	56	56	892
Input Volume	49	288	403	56	53	63	912
% of Volume	98	100	95	107	106	89	98

1: 10th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.3	0.4	0.1	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.2
Total Del/Veh (s)	4.0	0.9	0.9	0.6	14.6	7.4	2.5
Vehicles Entered	13	68	95	13	14	18	221
Vehicles Exited	13	68	95	13	14	18	221
Hourly Exit Rate	52	272	380	52	56	72	884
Input Volume	48	276	387	53	50	60	874
% of Volume	108	99	98	98	112	120	101

1: 10th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.2	0.2	0.2
Total Delay (hr)	0.1	0.1	0.1	0.0	0.2	0.1	0.6
Total Del/Veh (s)	4.4	0.9	0.9	0.5	16.0	7.6	2.5
Vehicles Entered	47	277	380	54	53	64	875
Vehicles Exited	47	277	380	54	54	63	875
Hourly Exit Rate	47	277	380	54	54	63	875
Input Volume	48	279	391	54	51	61	884
% of Volume	97	99	97	100	106	104	99

2: 5th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.3	0.1	0.3	0.2	0.3	0.2	0.2	0.2	0.1	0.1	0.2	0.2
Total Delay (hr)	0.1	0.3	0.0	0.0	0.3	0.0	0.1	0.1	0.0	0.0	0.1	0.0
Total Del/Veh (s)	14.9	8.2	5.1	15.8	10.8	2.2	19.0	15.4	12.0	17.7	16.1	8.5
Vehicles Entered	24	136	31	6	110	3	20	16	6	8	17	19
Vehicles Exited	23	133	30	6	109	3	21	16	6	8	19	19
Hourly Exit Rate	92	532	120	24	436	12	84	64	24	32	76	76
Input Volume	90	541	117	28	446	11	75	67	22	28	75	77
% of Volume	102	98	103	86	98	109	112	96	109	114	101	99

2: 5th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.2
Total Delay (hr)	1.2
Total Del/Veh (s)	10.7
Vehicles Entered	396
Vehicles Exited	393
Hourly Exit Rate	1572
Input Volume	1577
% of Volume	100

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2: 5th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.3	0.1	0.3	0.2	0.3	0.1	0.2	0.2	0.1	0.2	0.2	0.2
Total Delay (hr)	0.1	0.3	0.0	0.0	0.3	0.0	0.1	0.1	0.0	0.0	0.1	0.0
Total Del/Veh (s)	13.4	7.5	3.9	15.3	10.2	2.6	18.9	17.0	13.1	12.2	14.7	7.0
Vehicles Entered	25	130	27	7	108	3	16	15	6	8	17	20
Vehicles Exited	26	135	27	7	109	3	16	15	6	7	16	19
Hourly Exit Rate	104	540	108	28	436	12	64	60	24	28	64	76
Input Volume	90	541	117	28	446	11	75	67	22	28	75	77
% of Volume	116	100	92	100	98	109	85	90	109	100	85	99

2: 5th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.2
Total Delay (hr)	1.1
Total Del/Veh (s)	9.8
Vehicles Entered	382
Vehicles Exited	386
Hourly Exit Rate	1544
Input Volume	1577
% of Volume	98

2: 5th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.4	0.2	0.3	0.3	0.3	0.2	0.3	0.2	0.2	0.2	0.2	0.2
Total Delay (hr)	0.1	0.3	0.0	0.0	0.4	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	15.6	8.6	4.1	17.9	11.3	2.5	20.6	18.8	14.9	17.2	14.3	9.7
Vehicles Entered	22	139	32	7	120	2	20	17	6	7	21	19
Vehicles Exited	22	138	31	6	120	2	20	17	6	7	21	20
Hourly Exit Rate	88	552	124	24	480	8	80	68	24	28	84	80
Input Volume	94	564	122	29	465	11	78	70	23	29	78	80
% of Volume	94	98	102	83	103	73	103	97	104	97	108	100

2: 5th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.2
Total Delay (hr)	1.3
Total Del/Veh (s)	11.3
Vehicles Entered	412
Vehicles Exited	410
Hourly Exit Rate	1640
Input Volume	1643
% of Volume	100

2: 5th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.3	0.1	0.4	0.3	0.3	0.1	0.2	0.2	0.1	0.2	0.2	0.2
Total Delay (hr)	0.1	0.3	0.0	0.0	0.3	0.0	0.1	0.1	0.0	0.0	0.1	0.0
Total Del/Veh (s)	13.3	7.9	4.3	16.9	9.9	3.1	17.3	17.7	10.3	16.3	16.3	8.1
Vehicles Entered	23	141	28	7	108	3	18	17	4	7	18	19
Vehicles Exited	22	140	28	6	108	3	19	17	4	7	18	19
Hourly Exit Rate	88	560	112	24	432	12	76	68	16	28	72	76
Input Volume	90	541	117	28	446	11	75	67	22	28	75	77
% of Volume	98	104	96	86	97	109	101	101	73	100	96	99

2: 5th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.2
Total Delay (hr)	1.1
Total Del/Veh (s)	10.1
Vehicles Entered	393
Vehicles Exited	391
Hourly Exit Rate	1564
Input Volume	1577
% of Volume	99

2: 5th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.3	0.1	0.3	0.3	0.3	0.2	0.2	0.2	0.1	0.2	0.2	0.2
Total Delay (hr)	0.4	1.2	0.1	0.1	1.4	0.0	0.4	0.3	0.1	0.1	0.3	0.2
Total Del/Veh (s)	14.4	8.2	4.5	17.1	10.9	2.6	19.5	18.0	13.4	16.9	15.7	8.7
Vehicles Entered	94	547	117	27	447	11	74	64	22	28	73	77
Vehicles Exited	93	546	116	26	447	11	75	66	22	29	74	77
Hourly Exit Rate	93	546	116	26	447	11	75	66	22	29	74	77
Input Volume	91	547	118	28	451	11	76	68	22	28	76	78
% of Volume	102	100	98	92	99	100	99	97	99	103	98	99

2: 5th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	All
Denied Delay (hr)	0.1
Denied Del/Veh (s)	0.2
Total Delay (hr)	4.7
Total Del/Veh (s)	10.7
Vehicles Entered	1581
Vehicles Exited	1582
Hourly Exit Rate	1582
Input Volume	1594
% of Volume	99

3: Main Street (SH-75) & Project Access Performance by movement Interval #1 4:15

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.1	0.0	0.0	0.1
Total Del/Veh (s)	8.6	4.1	4.1	1.5	0.4	0.2	1.3
Vehicles Entered	5	7	6	144	110	5	277
Vehicles Exited	4	7	6	146	109	5	277
Hourly Exit Rate	16	28	24	584	436	20	1108
Input Volume	18	27	27	571	430	18	1091
% of Volume	89	104	89	102	101	111	102

3: Main Street (SH-75) & Project Access Performance by movement Interval #2 4:30

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.1	0.0	0.0	0.1
Total Del/Veh (s)	8.0	3.9	4.4	1.3	0.4	0.2	1.2
Vehicles Entered	3	6	6	138	102	4	259
Vehicles Exited	4	6	5	137	101	4	257
Hourly Exit Rate	16	24	20	548	404	16	1028
Input Volume	18	27	27	571	430	18	1091
% of Volume	89	89	74	96	94	89	94

3: Main Street (SH-75) & Project Access Performance by movement Interval #3 4:45

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All	
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0	
Total Delay (hr)	0.0	0.0	0.0	0.1	0.0	0.0	0.1	
Total Del/Veh (s)	6.7	4.4	4.3	1.4	0.4	0.1	1.2	
Vehicles Entered	4	8	5	155	106	4	282	
Vehicles Exited	4	8	6	153	107	4	282	
Hourly Exit Rate	16	32	24	612	428	16	1128	
Input Volume	19	28	28	595	447	19	1136	
% of Volume	84	114	86	103	96	84	99	

3: Main Street (SH-75) & Project Access Performance by movement Interval #4 5:00

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.1	0.0	0.0	0.1
Total Del/Veh (s)	10.1	4.8	5.2	1.4	0.4	0.2	1.3
Vehicles Entered	4	6	7	138	109	4	268
Vehicles Exited	4	6	7	142	109	4	272
Hourly Exit Rate	16	24	28	568	436	16	1088
Input Volume	18	27	27	571	430	18	1091
% of Volume	89	89	104	99	101	89	100

3: Main Street (SH-75) & Project Access Performance by movement Entire Run

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.2	0.0	0.0	0.4
Total Del/Veh (s)	8.9	4.2	4.7	1.4	0.4	0.2	1.3
Vehicles Entered	16	28	24	575	426	18	1087
Vehicles Exited	16	27	24	578	426	18	1089
Hourly Exit Rate	16	27	24	578	426	18	1089
Input Volume	18	27	27	577	434	18	1102
% of Volume	88	99	88	100	98	99	99

Total Network Performance By Interval

Interval Start	4:15	4:30	4:45	5:00	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.1
Denied Del/Veh (s)	0.3	0.3	0.3	0.3	0.3
Total Delay (hr)	1.7	1.5	1.8	1.6	6.6
Total Del/Veh (s)	12.9	11.8	13.3	12.4	13.5
Vehicles Entered	435	413	450	426	1724
Vehicles Exited	436	410	454	429	1729
Hourly Exit Rate	1744	1640	1816	1716	1729
Input Volume	6047	6047	6301	6047	6110
% of Volume	29	27	29	28	28

Intersection: 1: 10th Street & Main Street (SH-75), Interval #1

Movement	NB	NE
Directions Served	LT	LR
Maximum Queue (ft)	73	81
Average Queue (ft)	25	43
95th Queue (ft)	70	83
Link Distance (ft)	84	1050
Upstream Blk Time (%)	1	
Queuing Penalty (veh)	2	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1: 10th Street & Main Street (SH-75), Interval #2

Movement	NB	NE
Directions Served	LT	LR
Maximum Queue (ft)	67	70
Average Queue (ft)	23	38
95th Queue (ft)	69	72
Link Distance (ft)	84	1050
Upstream Blk Time (%)	0	
Queuing Penalty (veh)	1	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1: 10th Street & Main Street (SH-75), Interval #3

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	78	2	77
Average Queue (ft)	27	0	45
95th Queue (ft)	76	4	87
Link Distance (ft)	84	604	1050
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	1		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), Interval #4

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	69	2	95
Average Queue (ft)	25	0	42
95th Queue (ft)	70	5	82
Link Distance (ft)	84	604	1050
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	1		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), All Intervals

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	88	4	117
Average Queue (ft)	25	0	42
95th Queue (ft)	71	3	81
Link Distance (ft)	84	604	1050
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	1		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 2: 5th Street & Main Street (SH-75), Interval #1

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	126	128	198	117	149	109
Average Queue (ft)	92	80	118	30	79	71
95th Queue (ft)	131	138	203	118	146	122
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 2: 5th Street & Main Street (SH-75), Interval #2

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	118	106	188	119	120	106
Average Queue (ft)	93	72	120	30	71	61
95th Queue (ft)	132	116	200	126	126	107
Link Distance (ft)	194	194	872	872	838	1044
LL L DIL T! (0/)						

Upstream Blk Time (%)

Queuing Penalty (veh)

Storage Bay Dist (ft)

Storage Blk Time (%)

Queuing Penalty (veh)

Intersection: 2: 5th Street & Main Street (SH-75), Interval #3

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	131	128	210	116	134	117
Average Queue (ft)	98	79	126	30	79	75
95th Queue (ft)	146	131	226	141	139	129
Link Distance (ft)	194	194	872	872	838	1044
Unetroom DIL Time (0/)						

Upstream Blk Time (%)

Queuing Penalty (veh)

Storage Bay Dist (ft)

Storage Blk Time (%)

Queuing Penalty (veh)

Intersection: 2: 5th Street & Main Street (SH-75), Interval #4

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	122	126	174	89	127	125
Average Queue (ft)	92	84	112	26	73	70
95th Queue (ft)	134	132	187	98	134	125
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)						

Queuing Penalty (veh)

Storage Bay Dist (ft)

Storage Blk Time (%)

Queuing Penalty (veh)

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Intersection: 2: 5th Street & Main Street (SH-75), All Intervals

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	142	143	233	188	176	148
Average Queue (ft)	94	79	119	29	75	69
95th Queue (ft)	136	130	205	122	137	121
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)						
Ouguing Donalty (yoh)						

Queuing Penalty (veh)

Storage Bay Dist (ft)

Storage Blk Time (%)

Queuing Penalty (veh)

Intersection: 3: Main Street (SH-75) & Project Access, Interval #1

Movement	EB	EB	NB
Directions Served	L	R	LT
Maximum Queue (ft)	32	46	50
Average Queue (ft)	15	22	12
95th Queue (ft)	41	53	44
Link Distance (ft)	92	92	1131
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 3: Main Street (SH-75) & Project Access, Interval #2

Movement	EB	EB	NB
Directions Served	L	R	LT
Maximum Queue (ft)	27	38	50
Average Queue (ft)	11	17	15
95th Queue (ft)	35	45	54
Link Distance (ft)	92	92	1131
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 3: Main Street (SH-75) & Project Access, Interval #3

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	40	41	56	3
Average Queue (ft)	15	23	16	0
95th Queue (ft)	45	50	58	6
Link Distance (ft)	92	92	1131	84
Upstream Blk Time (%)	0			
Queuing Penalty (veh)	0			
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 3: Main Street (SH-75) & Project Access, Interval #4

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	39	46	61	12
Average Queue (ft)	14	20	19	2
95th Queue (ft)	43	51	67	16
Link Distance (ft)	92	92	1131	84
Upstream Blk Time (%)		0		
Queuing Penalty (veh)		0		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 3: Main Street (SH-75) & Project Access, All Intervals

Movement	EB	EB	NB	SB	
Directions Served	L	R	LT	TR	
Maximum Queue (ft)	48	56	82	15	
Average Queue (ft)	14	20	15	1	
95th Queue (ft)	41	50	56	8	
Link Distance (ft)	92	92	1131	84	
Upstream Blk Time (%)	0	0			
Queuing Penalty (veh)	0	0			
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Network Summary

Network wide Queuing Penalty, Interval #1: 2
Network wide Queuing Penalty, Interval #2: 1
Network wide Queuing Penalty, Interval #3: 1
Network wide Queuing Penalty, Interval #4: 1
Network wide Queuing Penalty, All Intervals: 1

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SimTraffic LOS Report

ID Ketchum - Bracken Station TIS Project:

Future (2020) Background p.m. Peak Hour Analysis Period: Time Period:

Project #: *UT16-851*

10th Street & Main Street (SH-75) Unsignalized Intersection:

Type:

A		Demand	Volume	Served	Delay/Ve	h (sec)
Approacn	Movement	Volume	Avg	%	Avg	LOS
	L	50	46	92	5.1	Α
NB	Т	552	560	101	0.9	Α
	Subtotal	602	606	101	1.2	Α
	Т	389	398	102	0.9	Α
SB	R	56	54	97	0.5	Α
	Subtotal	445	452	102	0.9	Α
	L	53	50	95	13.6	В
NE	R	64	68	107	7.7	Α
	Subtotal	117	118	101	10.2	В
Total		1,164	1,176	101	2.0	Α

5th Street & Main Street (SH-75) Signalized Intersection:

Type:

Annroach	Mayamant	Demand	Volum	e Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
	L	29	28	96	19.3	В
NW	Т	450	453	101	11.2	В
INVV	R	12	11	92	3.3	Α
	Subtotal	491	492	100	11.5	В
	L	92	95	103	15.5	В
SE	Т	550	560	102	8.4	Α
SE	R	119	122	102	4.5	Α
	Subtotal	761	777	102	8.7	Α
	L	76	78	103	19.7	В
NE	Т	71	68	96	19.4	В
INE	R	23	25	108	11.9	В
	Subtotal	170	171	101	18.4	В
	L	29	28	96	19.0	В
SW	Т	79	76	97	18.1	В
SVV	R	77	74	96	10.1	В
	Subtotal	185	178	96	14.9	В
Total		1,606	1,618	101	11.2	В

1: 10th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.3	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.2
Total Del/Veh (s)	4.8	0.9	0.9	0.4	14.0	8.8	2.2
Vehicles Entered	13	137	102	13	14	18	297
Vehicles Exited	12	136	101	13	15	17	294
Hourly Exit Rate	48	544	404	52	60	68	1176
Input Volume	49	547	385	55	52	63	1151
% of Volume	98	99	105	95	115	108	102

1: 10th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.1	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	4.5	0.9	0.9	0.6	11.6	5.7	1.7
Vehicles Entered	11	139	98	14	11	16	289
Vehicles Exited	12	139	98	14	11	16	290
Hourly Exit Rate	48	556	392	56	44	64	1160
Input Volume	49	547	385	55	52	63	1151
% of Volume	98	102	102	102	85	102	101

1: 10th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	NBL	NBT	SBT	SBR	NEL	NER	All	
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.2	0.2	0.2	
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.2	
Total Del/Veh (s)	5.4	0.8	0.9	0.5	14.8	7.5	2.0	
Vehicles Entered	11	142	99	14	13	17	296	
Vehicles Exited	11	142	100	14	13	17	297	
Hourly Exit Rate	44	568	400	56	52	68	1188	
Input Volume	52	569	401	58	55	66	1201	
% of Volume	85	100	100	97	95	103	99	

1: 10th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.3	0.3	0.1	0.2	0.1
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.2
Total Del/Veh (s)	5.2	0.8	0.9	0.5	12.7	7.5	1.9
Vehicles Entered	11	143	99	13	12	18	296
Vehicles Exited	11	142	99	13	12	18	295
Hourly Exit Rate	44	568	396	52	48	72	1180
Input Volume	49	547	385	55	52	63	1151
% of Volume	90	104	103	95	92	114	103

1: 10th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Denied Del/Veh (s)	0.0	0.0	0.4	0.3	0.2	0.2	0.2
Total Delay (hr)	0.1	0.1	0.1	0.0	0.2	0.1	0.6
Total Del/Veh (s)	5.1	0.9	0.9	0.5	13.6	7.7	2.0
Vehicles Entered	46	560	399	54	50	68	1177
Vehicles Exited	46	560	398	54	50	68	1176
Hourly Exit Rate	46	560	398	54	50	68	1176
Input Volume	50	552	389	56	53	64	1164
% of Volume	92	101	102	97	95	107	101

2: 5th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.6	0.2	0.3	0.2	0.3	0.1	0.2	0.2	0.2	0.2	0.2	0.1
Total Delay (hr)	0.1	0.4	0.0	0.0	0.4	0.0	0.1	0.1	0.0	0.0	0.1	0.0
Total Del/Veh (s)	15.5	8.8	4.6	22.4	11.3	4.6	18.8	15.6	11.5	17.1	17.7	8.5
Vehicles Entered	22	143	30	6	113	3	18	16	7	7	19	17
Vehicles Exited	21	139	30	6	110	3	19	16	7	7	20	18
Hourly Exit Rate	84	556	120	24	440	12	76	64	28	28	80	72
Input Volume	91	544	118	29	445	12	75	70	23	29	78	76
% of Volume	92	102	102	83	99	100	101	91	122	97	103	95

2: 5th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.3
Total Delay (hr)	1.3
Total Del/Veh (s)	11.2
Vehicles Entered	401
Vehicles Exited	396
Hourly Exit Rate	1584
Input Volume	1590
% of Volume	100

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2: 5th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.3	0.1	0.2	0.2	0.3	0.2	0.2	0.3	0.3	0.2	0.2	0.2
Total Delay (hr)	0.1	0.3	0.0	0.0	0.3	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	14.3	7.8	4.6	14.6	9.9	1.9	20.3	20.2	11.4	17.0	17.6	10.3
Vehicles Entered	23	138	28	7	111	3	19	14	6	9	20	18
Vehicles Exited	24	142	28	7	115	3	19	14	6	9	19	18
Hourly Exit Rate	96	568	112	28	460	12	76	56	24	36	76	72
Input Volume	91	544	118	29	445	12	75	70	23	29	78	76
% of Volume	105	104	95	97	103	100	101	80	104	124	97	95

2: 5th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.2
Total Delay (hr)	1.2
Total Del/Veh (s)	10.5
Vehicles Entered	396
Vehicles Exited	404
Hourly Exit Rate	1616
Input Volume	1590
% of Volume	102

2: 5th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.5	0.1	0.3	0.2	0.3	0.2	0.2	0.2	0.2	0.3	0.3	0.2
Total Delay (hr)	0.1	0.3	0.0	0.0	0.4	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	15.6	8.1	4.4	17.0	10.8	3.5	20.3	20.2	13.2	22.7	18.7	9.9
Vehicles Entered	25	141	32	7	120	3	19	20	6	6	19	20
Vehicles Exited	25	141	32	7	117	3	18	20	6	6	20	20
Hourly Exit Rate	100	564	128	28	468	12	72	80	24	24	80	80
Input Volume	95	567	123	30	464	12	78	73	24	30	81	79
% of Volume	105	99	104	93	101	100	92	110	100	80	99	101

2: 5th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.2
Total Delay (hr)	1.3
Total Del/Veh (s)	11.3
Vehicles Entered	418
Vehicles Exited	415
Hourly Exit Rate	1660
Input Volume	1656
% of Volume	100

2: 5th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.5	0.1	0.3	0.3	0.2	0.1	0.2	0.2	0.2	0.2	0.2	0.2
Total Delay (hr)	0.1	0.3	0.0	0.0	0.4	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	16.0	8.6	4.4	20.6	11.5	3.1	16.8	18.2	11.6	17.5	17.2	9.9
Vehicles Entered	25	138	32	8	111	2	20	17	6	6	18	18
Vehicles Exited	24	138	31	8	111	2	21	17	6	6	17	19
Hourly Exit Rate	96	552	124	32	444	8	84	68	24	24	68	76
Input Volume	91	544	118	29	445	12	75	70	23	29	78	76
% of Volume	105	101	105	110	100	67	112	97	104	83	87	100

2: 5th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.2
Total Delay (hr)	1.3
Total Del/Veh (s)	11.1
Vehicles Entered	401
Vehicles Exited	400
Hourly Exit Rate	1600
Input Volume	1590
% of Volume	101

2: 5th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.5	0.1	0.3	0.2	0.3	0.1	0.2	0.2	0.2	0.2	0.2	0.2
Total Delay (hr)	0.4	1.3	0.2	0.2	1.4	0.0	0.4	0.4	0.1	0.1	0.4	0.2
Total Del/Veh (s)	15.5	8.4	4.5	19.3	11.2	3.3	19.7	19.4	11.9	19.0	18.1	10.1
Vehicles Entered	96	560	122	29	455	11	77	67	25	27	76	74
Vehicles Exited	95	560	122	28	453	11	78	68	25	28	76	74
Hourly Exit Rate	95	560	122	28	453	11	78	68	25	28	76	74
Input Volume	92	550	119	29	450	12	76	71	23	29	79	77
% of Volume	103	102	102	96	101	92	103	96	108	96	97	96

2: 5th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	All
Denied Delay (hr)	0.1
Denied Del/Veh (s)	0.2
Total Delay (hr)	5.1
Total Del/Veh (s)	11.2
Vehicles Entered	1619
Vehicles Exited	1618
Hourly Exit Rate	1618
Input Volume	1606
% of Volume	101

Total Network Performance By Interval

Interval Start	4:15	4:30	4:45	5:00	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.2
Denied Del/Veh (s)	0.3	0.3	0.3	0.3	0.3
Total Delay (hr)	1.8	1.6	1.8	1.8	7.0
Total Del/Veh (s)	13.7	12.7	13.6	13.4	14.3
Vehicles Entered	431	422	444	426	1725
Vehicles Exited	429	426	437	430	1722
Hourly Exit Rate	1716	1704	1748	1720	1722
Input Volume	6247	6247	6512	6247	6313
% of Volume	27	27	27	28	27

Intersection: 1: 10th Street & Main Street (SH-75), Interval #1

Movement	NB	NE
Directions Served	LT	LR
Maximum Queue (ft)	72	78
Average Queue (ft)	26	45
95th Queue (ft)	74	80
Link Distance (ft)	263	1050
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1: 10th Street & Main Street (SH-75), Interval #2

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	81	2	64
Average Queue (ft)	30	0	37
95th Queue (ft)	85	4	66
Link Distance (ft)	263	604	1050
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), Interval #3

Movement	NB	NE
Directions Served	LT	LR
Maximum Queue (ft)	90	70
Average Queue (ft)	28	43
95th Queue (ft)	88	77
Link Distance (ft)	263	1050
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1: 10th Street & Main Street (SH-75), Interval #4

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	72	2	76
Average Queue (ft)	25	0	45
95th Queue (ft)	72	0	85
Link Distance (ft)	263	604	1050
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), All Intervals

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	119	4	90
Average Queue (ft)	27	0	43
95th Queue (ft)	80	2	77
Link Distance (ft)	263	604	1050
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 2: 5th Street & Main Street (SH-75), Interval #1

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	129	129	202	124	124	120
Average Queue (ft)	97	88	122	34	71	69
95th Queue (ft)	137	141	211	129	134	119
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 2: 5th Street & Main Street (SH-75), Interval #2

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	122	121	172	99	121	130
Average Queue (ft)	87	78	121	19	73	74
95th Queue (ft)	135	128	185	81	126	137
Link Distance (ft)	194	194	872	872	838	1044
111 DIL Thur (0/)						

Upstream Blk Time (%)

Queuing Penalty (veh)

Storage Bay Dist (ft)

Storage Blk Time (%)

Queuing Penalty (veh)

Intersection: 2: 5th Street & Main Street (SH-75), Interval #3

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	127	128	197	118	133	140
Average Queue (ft)	99	85	120	33	84	76
95th Queue (ft)	141	142	212	127	148	152
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)		0				
Queuing Penalty (veh)		0				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 2: 5th Street & Main Street (SH-75), Interval #4

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	135	122	195	137	126	118
Average Queue (ft)	96	84	123	39	75	71
95th Queue (ft)	140	127	226	147	133	126
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

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Intersection: 2: 5th Street & Main Street (SH-75), All Intervals

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	143	151	254	202	169	169
Average Queue (ft)	95	84	122	31	76	72
95th Queue (ft)	139	135	210	123	136	135
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)		0				
Queuing Penalty (veh)		0				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Network Summary

Network wide Queuing Penalty, Interval #1: 0	
Network wide Queuing Penalty, Interval #2: 0	
Network wide Queuing Penalty, Interval #3: 0	
Network wide Queuing Penalty, Interval #4: 0	
Network wide Queuing Penalty, All Intervals: 0	



SimTraffic LOS Report

ID Ketchum - Bracken Station TIS Project:

Future (2020) Plus Project p.m. Peak Hour Analysis Period: Time Period:

Project #: *UT16-851*

10th Street & Main Street (SH-75) Unsignalized Intersection:

Type:

Approach	Movement	Demand	Volum	e Served	Delay/Veh (sec)			
Approach	Movement	Volume	Avg	%	Avg	LOS		
	L	50	45	90	5.0	Α		
NB	Т	290	293	101	0.2	Α		
	Subtotal	340	338	99	0.8	Α		
	Т	407	405	99	0.9	Α		
SB	R	56	58	104	0.7	Α		
	Subtotal	463	463	100	0.9	Α		
	L	53	52	99	14.9	В		
NE	R	64	67	105	7.6	Α		
	Subtotal	117	119	102	10.8	В		
Total		919	920	100	2.2	Α		

5th Street & Main Street (SH-75) Signalized Intersection:

Type:

Annyoooh	Mayamant	Demand	Volume	e Served	Delay/Veh (sec)		
Approach	Movement	Volume	Avg	%	Avg	LOS	
	L	29	31	106	17.0	В	
NW	Т	470	468	100	11.1	В	
INVV	R	12	13	108	2.7	Α	
	Subtotal	511	512	100	11.2	В	
	L	95	95	100	15.8	В	
SE	Т	570	567	99	8.3	Α	
SE	R	123	126	102	4.9	Α	
	Subtotal	788	788	100	8.7	Α	
	L	79	79	100	18.8	В	
NE	Т	71	68	96	19.8	В	
INE	R	23	23	99	12.0	В	
	Subtotal	173	170	98	18.3	В	
	Ĺ	29	30	103	18.2	В	
SW	Т	79	77	98	17.2	В	
300	R	81	83	102	9.1	Α	
	Subtotal	189	190	101	13.8	В	
Total		1,661	1,660	100	11.1	В	



SimTraffic LOS Report

ID Ketchum - Bracken Station TIS Project:

Future (2020) Plus Project p.m. Peak Hour Analysis Period: Time Period:

Project #: *UT16-851*

Main Street (SH-75) & Project Access Unsignalized Intersection:

Type:

. , po.		• norginanizoa				
Annyosoh	Mayamant	Demand	Volume	e Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
	L	27	26	95	4.0	Α
NB	Т	602	605	100	0.2	Α
IND						
	Subtotal	629	631	100	0.4	Α
	Т	453	454	100	0.4	Α
SB	R	18	18	99	0.2	Α
OB						
	Subtotal	471	472	100	0.4	Α
	L	18	19	104	13.0	В
EB	R	27	31	114	4.8	Α
						_
	Subtotal	45	50	111	7.9	Α
Total		1,146	1,153	101	0.7	Α

Intersection:

Type:

Annroach	Movement	Demand	Volum	e Served	Delay/Veh (sec)			
Approach	Movement	Volume	Avg	%	Avg	LOS		
Total								

1: 10th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.1
Total Del/Veh (s)	4.2	0.2	0.9	0.6	13.0	6.7	2.0
Vehicles Entered	11	72	96	16	13	15	223
Vehicles Exited	11	72	96	17	13	15	224
Hourly Exit Rate	44	288	384	68	52	60	896
Input Volume	49	287	403	55	52	63	909
% of Volume	90	100	95	124	100	95	99

1: 10th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	5.2	0.2	1.0	0.6	12.7	6.5	2.1
Vehicles Entered	14	71	99	14	12	18	228
Vehicles Exited	14	72	98	14	12	17	227
Hourly Exit Rate	56	288	392	56	48	68	908
Input Volume	49	287	403	55	52	63	909
% of Volume	114	100	97	102	92	108	100

1: 10th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	NBL	NBT	SBT	SBR	NEL	NER	All	
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.2	0.1	0.2	
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.2	
Total Del/Veh (s)	4.8	0.2	0.9	0.8	15.4	8.5	2.2	
Vehicles Entered	10	78	103	14	13	18	236	
Vehicles Exited	10	78	103	14	14	17	236	
Hourly Exit Rate	40	312	412	56	56	68	944	
Input Volume	52	298	420	58	55	66	949	
% of Volume	77	105	98	97	102	103	99	

1: 10th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.1	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.2
Total Del/Veh (s)	5.1	0.2	0.9	0.5	15.0	7.9	2.3
Vehicles Entered	11	72	107	13	14	17	234
Vehicles Exited	11	72	108	14	14	17	236
Hourly Exit Rate	44	288	432	56	56	68	944
Input Volume	49	287	403	55	52	63	909
% of Volume	90	100	107	102	108	108	104

1: 10th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.2	0.2	0.2
Total Delay (hr)	0.1	0.0	0.1	0.0	0.2	0.1	0.6
Total Del/Veh (s)	5.0	0.2	0.9	0.7	14.9	7.6	2.2
Vehicles Entered	45	294	405	58	52	67	921
Vehicles Exited	45	293	405	58	52	67	920
Hourly Exit Rate	45	293	405	58	52	67	920
Input Volume	50	290	407	56	53	64	919
% of Volume	90	101	99	104	99	105	100

2: 5th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.4	0.2	0.3	0.2	0.3	0.1	0.2	0.3	0.1	0.2	0.2	0.2
Total Delay (hr)	0.1	0.3	0.0	0.0	0.3	0.0	0.1	0.1	0.0	0.0	0.1	0.0
Total Del/Veh (s)	14.7	7.8	4.9	15.8	10.6	2.5	19.3	19.2	12.5	15.3	14.8	8.1
Vehicles Entered	24	136	30	8	113	3	21	17	6	8	21	19
Vehicles Exited	23	134	29	8	109	3	22	18	6	9	22	20
Hourly Exit Rate	92	536	116	32	436	12	88	72	24	36	88	80
Input Volume	94	564	122	29	465	12	78	70	23	29	78	80
% of Volume	98	95	95	110	94	100	113	103	104	124	113	100

2: 5th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	All	
Denied Delay (hr)	0.0	
Denied Del/Veh (s)	0.2	
Total Delay (hr)	1.2	
Total Del/Veh (s)	10.6	
Vehicles Entered	406	
Vehicles Exited	403	
Hourly Exit Rate	1612	
Input Volume	1644	
% of Volume	98	

2: 5th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.4	0.1	0.3	0.3	0.3	0.2	0.2	0.2	0.2	0.2	0.2	0.2
Total Delay (hr)	0.1	0.3	0.0	0.0	0.4	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	15.5	8.4	4.4	15.1	10.7	1.7	18.1	18.7	9.5	15.8	17.9	8.8
Vehicles Entered	23	145	32	8	115	3	21	17	7	6	19	21
Vehicles Exited	23	147	32	8	118	3	20	17	6	7	18	20
Hourly Exit Rate	92	588	128	32	472	12	80	68	24	28	72	80
Input Volume	94	564	122	29	465	12	78	70	23	29	78	80
% of Volume	98	104	105	110	102	100	103	97	104	97	92	100

2: 5th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.2
Total Delay (hr)	1.3
Total Del/Veh (s)	10.7
Vehicles Entered	417
Vehicles Exited	419
Hourly Exit Rate	1676
Input Volume	1644
% of Volume	102

2: 5th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.4	0.1	0.3	0.4	0.3	0.2	0.2	0.2	0.2	0.2	0.2	0.2
Total Delay (hr)	0.1	0.3	0.0	0.0	0.4	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	16.5	7.8	4.9	17.5	11.1	2.0	17.0	18.5	11.7	21.1	16.4	9.9
Vehicles Entered	24	140	34	8	125	4	19	17	6	7	18	22
Vehicles Exited	24	139	34	8	124	3	19	18	6	7	18	22
Hourly Exit Rate	96	556	136	32	496	12	76	72	24	28	72	88
Input Volume	98	588	127	30	485	12	81	73	24	30	81	84
% of Volume	98	95	107	107	102	100	94	99	100	93	89	105

2: 5th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.2
Total Delay (hr)	1.3
Total Del/Veh (s)	10.8
Vehicles Entered	424
Vehicles Exited	422
Hourly Exit Rate	1688
Input Volume	1713
% of Volume	99

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2: 5th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.7	0.2	0.4	0.3	0.3	0.2	0.2	0.2	0.2	0.2	0.2	0.2
Total Delay (hr)	0.1	0.4	0.0	0.0	0.4	0.0	0.1	0.1	0.0	0.0	0.1	0.0
Total Del/Veh (s)	14.5	8.6	5.2	19.7	10.6	3.6	18.9	20.8	10.8	18.9	16.7	8.2
Vehicles Entered	26	148	32	7	118	3	18	17	6	8	18	20
Vehicles Exited	24	146	32	7	118	4	19	16	5	8	18	20
Hourly Exit Rate	96	584	128	28	472	16	76	64	20	32	72	80
Input Volume	94	564	122	29	465	12	78	70	23	29	78	80
% of Volume	102	104	105	97	102	133	97	91	87	110	92	100

2: 5th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	All
Denied Delay (hr)	0.0
Denied Del/Veh (s)	0.3
Total Delay (hr)	1.3
Total Del/Veh (s)	11.0
Vehicles Entered	421
Vehicles Exited	417
Hourly Exit Rate	1668
Input Volume	1644
% of Volume	101

2: 5th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.5	0.2	0.3	0.3	0.3	0.2	0.2	0.2	0.2	0.2	0.2	0.2
Total Delay (hr)	0.4	1.3	0.2	0.1	1.5	0.0	0.4	0.4	0.1	0.2	0.4	0.2
Total Del/Veh (s)	15.8	8.3	4.9	17.0	11.1	2.7	18.8	19.8	12.0	18.2	17.2	9.1
Vehicles Entered	96	569	126	31	472	13	78	68	24	29	77	82
Vehicles Exited	95	567	126	31	468	13	79	68	23	30	77	83
Hourly Exit Rate	95	567	126	31	468	13	79	68	23	30	77	83
Input Volume	95	570	123	29	470	12	79	71	23	29	79	81
% of Volume	100	99	102	106	100	108	100	96	99	103	98	102

2: 5th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	All
Denied Delay (hr)	0.1
Denied Del/Veh (s)	0.2
Total Delay (hr)	5.1
Total Del/Veh (s)	11.1
Vehicles Entered	1665
Vehicles Exited	1660
Hourly Exit Rate	1660
Input Volume	1661
% of Volume	100

3: Main Street (SH-75) & Project Access Performance by movement Interval #1 4:15

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Del/Veh (s)	9.1	4.4	3.8	0.2	0.4	0.1	0.6
Vehicles Entered	6	7	7	147	107	4	278
Vehicles Exited	5	7	7	146	107	4	276
Hourly Exit Rate	20	28	28	584	428	16	1104
Input Volume	18	27	27	596	448	18	1134
% of Volume	111	104	104	98	96	89	97

3: Main Street (SH-75) & Project Access Performance by movement Interval #2 4:30

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.3	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	10.4	4.7	4.1	0.2	0.5	0.2	0.7
Vehicles Entered	4	9	7	150	111	5	286
Vehicles Exited	4	9	7	150	112	5	287
Hourly Exit Rate	16	36	28	600	448	20	1148
Input Volume	18	27	27	596	448	18	1134
% of Volume	89	133	104	101	100	111	101

3: Main Street (SH-75) & Project Access Performance by movement Interval #3 4:45

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All	
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0	
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	
Total Del/Veh (s)	13.5	5.2	4.6	0.2	0.4	0.3	0.7	
Vehicles Entered	4	7	6	158	115	4	294	
Vehicles Exited	5	7	6	158	115	4	295	
Hourly Exit Rate	20	28	24	632	460	16	1180	
Input Volume	19	28	28	622	467	19	1183	
% of Volume	105	100	86	102	99	84	100	

3: Main Street (SH-75) & Project Access Performance by movement Interval #4 5:00

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	14.5	4.3	3.6	0.2	0.4	0.2	0.7
Vehicles Entered	5	8	7	151	120	5	296
Vehicles Exited	5	8	6	150	120	5	294
Hourly Exit Rate	20	32	24	600	480	20	1176
Input Volume	18	27	27	596	448	18	1134
% of Volume	111	119	89	101	107	111	104

3: Main Street (SH-75) & Project Access Performance by movement Entire Run

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.2	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.1	0.0	0.0	0.0	0.1	0.0	0.2
Total Del/Veh (s)	13.0	4.8	4.0	0.2	0.4	0.2	0.7
Vehicles Entered	19	31	27	606	453	18	1154
Vehicles Exited	19	31	26	605	454	18	1153
Hourly Exit Rate	19	31	26	605	454	18	1153
Input Volume	18	27	27	602	453	18	1146
% of Volume	104	114	95	100	100	99	101

Total Network Performance By Interval

Interval Start	4:15	4:30	4:45	5:00	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.2
Denied Del/Veh (s)	0.3	0.3	0.3	0.4	0.3
Total Delay (hr)	1.7	1.8	1.8	1.8	7.2
Total Del/Veh (s)	12.7	13.2	13.2	13.3	14.0
Vehicles Entered	446	452	460	458	1815
Vehicles Exited	444	449	461	455	1808
Hourly Exit Rate	1776	1796	1844	1820	1808
Input Volume	7391	7391	7708	7391	7470
% of Volume	24	24	24	25	24

Intersection: 1: 10th Street & Main Street (SH-75), Interval #1

Movement	NB	NE
Directions Served	L	LR
Maximum Queue (ft)	36	66
Average Queue (ft)	16	41
95th Queue (ft)	44	74
Link Distance (ft)	79	1043
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1: 10th Street & Main Street (SH-75), Interval #2

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	48	13	77
Average Queue (ft)	24	3	44
95th Queue (ft)	52	22	79
Link Distance (ft)	79	609	1043
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	0		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), Interval #3

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	38	6	78
Average Queue (ft)	16	1	45
95th Queue (ft)	44	9	88
Link Distance (ft)	79	609	1043
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), Interval #4

Movement	NB	NE
Directions Served	L	LR
Maximum Queue (ft)	44	76
Average Queue (ft)	17	42
95th Queue (ft)	49	77
Link Distance (ft)	79	1043
Upstream Blk Time (%)	0	
Queuing Penalty (veh)	0	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1: 10th Street & Main Street (SH-75), All Intervals

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	54	16	97
Average Queue (ft)	18	1	43
95th Queue (ft)	48	12	80
Link Distance (ft)	79	609	1043
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	0		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 2: 5th Street & Main Street (SH-75), Interval #1

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	124	123	188	121	124	122
Average Queue (ft)	92	86	113	31	81	71
95th Queue (ft)	130	137	201	126	135	126
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 2: 5th Street & Main Street (SH-75), Interval #2

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	136	129	200	78	125	114
Average Queue (ft)	99	88	128	17	76	68
95th Queue (ft)	142	139	197	59	124	119
Link Distance (ft)	194	194	872	872	838	1044
LL L DIL T' (0/)						

Upstream Blk Time (%)

Queuing Penalty (veh)

Storage Bay Dist (ft)

Storage Blk Time (%)

Queuing Penalty (veh)

Intersection: 2: 5th Street & Main Street (SH-75), Interval #3

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	118	132	226	132	125	144
Average Queue (ft)	95	83	128	28	76	77
95th Queue (ft)	128	138	222	110	128	130
Link Distance (ft)	194	194	872	872	838	1044

Upstream Blk Time (%)

Queuing Penalty (veh)

Storage Bay Dist (ft)

Storage Blk Time (%)

Queuing Penalty (veh)

Intersection: 2: 5th Street & Main Street (SH-75), Interval #4

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	136	147	199	123	120	122
Average Queue (ft)	102	96	121	31	70	68
95th Queue (ft)	143	154	207	109	118	121
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)		0				
Queuing Penalty (veh)		0				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

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Intersection: 2: 5th Street & Main Street (SH-75), All Intervals

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	146	168	258	194	144	166
Average Queue (ft)	97	88	123	27	76	71
95th Queue (ft)	136	143	208	104	127	124
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)		0				
Queuing Penalty (veh)		0				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 3: Main Street (SH-75) & Project Access, Interval #1

Movement	EB	EB	NB
Directions Served	L	R	LT
Maximum Queue (ft)	37	42	38
Average Queue (ft)	17	21	12
95th Queue (ft)	44	48	40
Link Distance (ft)	92	92	102
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 3: Main Street (SH-75) & Project Access, Interval #2

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	38	53	46	5
Average Queue (ft)	16	25	14	1
95th Queue (ft)	44	59	48	11
Link Distance (ft)	92	92	102	79
Upstream Blk Time (%)		0		
Queuing Penalty (veh)		0		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

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Intersection: 3: Main Street (SH-75) & Project Access, Interval #3

Movement	EB	EB	NB
Directions Served	L	R	LT
Maximum Queue (ft)	44	48	45
Average Queue (ft)	18	22	12
95th Queue (ft)	50	55	42
Link Distance (ft)	92	92	102
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	0		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 3: Main Street (SH-75) & Project Access, Interval #4

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	42	44	43	2
Average Queue (ft)	17	24	14	0
95th Queue (ft)	47	53	44	4
Link Distance (ft)	92	92	102	79
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Oueuing Penalty (veh)				

Intersection: 3: Main Street (SH-75) & Project Access, All Intervals

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	56	65	64	7
Average Queue (ft)	17	23	13	0
95th Queue (ft)	46	54	44	6
Link Distance (ft)	92	92	102	79
Upstream Blk Time (%)	0	0		
Queuing Penalty (veh)	0	0		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

,
etwork wide Queuing Penalty, Interval #1: 0
etwork wide Queuing Penalty, Interval #2: 0
etwork wide Queuing Penalty, Interval #3: 0
etwork wide Queuing Penalty, Interval #4: 0
etwork wide Queuing Penalty, All Intervals: 0

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SimTraffic LOS Report

ID Ketchum - Bracken Station TIS Project:

Future (2026) Background p.m. Peak Hour Analysis Period: Time Period:

Project #: *UT16-851*

10th Street & Main Street (SH-75) Unsignalized Intersection:

Type:

Approach	Mayramant	Demand	Volume	Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
	L	69	65	95	5.9	Α
NB	Т	687	676	98	1.1	Α
	Subtotal	756	741	98	1.5	Α
	Т	414	413	100	1.1	Α
SB	R	64	64	100	0.7	Α
_	Subtotal	478	477	100	1.0	Α
	L	68	65	96	18.3	С
NE	R	95	98	103	10.7	В
	Subtotal	163	163	100	13.7	В
Total		1,397	1,381	99	2.8	Α

5th Street & Main Street (SH-75) Signalized Intersection:

Type:

Annyoooh	Mayamant	Demand	Volum	e Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
	L	31	31	99	34.8	С
NW	Т	593	580	98	22.4	С
INVV	R	12	12	100	6.2	Α
	Subtotal	636	623	98	22.7	С
	L	98	98	100	25.8	С
SE	Т	752	746	99	11.4	В
SE	R	127	132	104	6.3	Α
	Subtotal	977	976	100	12.2	В
	L	81	82	101	20.2	С
NE	Т	75	71	95	20.8	С
INE	R	25	27	107	13.9	В
	Subtotal	181	180	99	19.5	В
	Ĺ	31	29	93	18.4	В
SW	Т	84	83	99	18.1	В
300	R	82	79	96	10.9	В
	Subtotal	197	191	97	15.2	В
Total		1,992	1,970	99	16.5	В

1: 10th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.2	0.2	0.2
Total Delay (hr)	0.0	0.1	0.0	0.0	0.1	0.1	0.3
Total Del/Veh (s)	6.1	1.1	1.0	0.6	17.2	12.1	3.0
Vehicles Entered	17	168	103	15	18	25	346
Vehicles Exited	17	167	104	15	18	25	346
Hourly Exit Rate	68	668	416	60	72	100	1384
Input Volume	68	680	410	63	67	94	1382
% of Volume	100	98	101	95	107	106	100

1: 10th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
, , ,							
Denied Del/Veh (s)	0.0	0.0	0.5	0.4	0.2	0.2	0.2
Total Delay (hr)	0.0	0.1	0.0	0.0	0.1	0.1	0.3
Total Del/Veh (s)	6.1	1.2	1.1	0.8	17.9	8.5	2.7
Vehicles Entered	17	163	105	18	15	24	342
Vehicles Exited	17	164	105	18	15	24	343
Hourly Exit Rate	68	656	420	72	60	96	1372
Input Volume	68	680	410	63	67	94	1382
% of Volume	100	96	102	114	90	102	99

1: 10th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.5	0.2	0.2	0.2
Total Delay (hr)	0.0	0.1	0.0	0.0	0.1	0.1	0.3
Total Del/Veh (s)	6.3	1.3	1.0	0.7	17.1	11.1	2.9
Vehicles Entered	16	173	107	16	17	23	352
Vehicles Exited	16	173	107	16	17	24	353
Hourly Exit Rate	64	692	428	64	68	96	1412
Input Volume	71	709	427	66	70	98	1441
% of Volume	90	98	100	97	97	98	98

1: 10th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.3	0.2	0.2	0.1
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.1	0.2
Total Del/Veh (s)	4.6	0.9	1.0	0.5	17.7	9.7	2.5
Vehicles Entered	15	174	97	16	15	25	342
Vehicles Exited	15	173	97	16	15	25	341
Hourly Exit Rate	60	692	388	64	60	100	1364
Input Volume	68	680	410	63	67	94	1382
% of Volume	88	102	95	102	90	106	99

1: 10th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.2	0.2	0.2
Total Delay (hr)	0.1	0.2	0.1	0.0	0.3	0.3	1.1
Total Del/Veh (s)	5.9	1.1	1.1	0.7	18.3	10.7	2.8
Vehicles Entered	65	678	412	64	65	98	1382
Vehicles Exited	65	676	413	64	65	98	1381
Hourly Exit Rate	65	676	413	64	65	98	1381
Input Volume	69	687	414	64	68	95	1397
% of Volume	95	98	100	100	96	103	99

2: 5th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	1.4	0.5	0.8	0.4	0.3	0.1	0.2	0.2	0.2	0.1	0.2	0.2
Total Delay (hr)	0.2	0.5	0.1	0.1	0.7	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	25.1	10.5	5.9	23.0	17.0	2.8	22.7	22.3	18.9	17.0	18.6	10.7
Vehicles Entered	23	180	34	8	148	3	20	19	7	7	20	18
Vehicles Exited	23	177	33	8	143	3	20	19	7	7	20	18
Hourly Exit Rate	92	708	132	32	572	12	80	76	28	28	80	72
Input Volume	97	744	126	31	587	12	80	74	25	31	83	81
% of Volume	95	95	105	103	97	100	100	103	112	90	96	89

2: 5th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	All
Denied Delay (hr)	0.1
Denied Del/Veh (s)	0.4
Total Delay (hr)	2.0
Total Del/Veh (s)	14.6
Vehicles Entered	487
Vehicles Exited	478
Hourly Exit Rate	1912
Input Volume	1971
% of Volume	97

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2: 5th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	4.7	2.5	3.6	0.3	0.3	0.1	0.2	0.2	0.2	0.2	0.3	0.2
Total Delay (hr)	0.2	0.6	0.1	0.1	0.7	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	23.1	11.1	6.0	25.8	16.8	4.3	20.5	18.0	10.6	17.1	16.2	9.3
Vehicles Entered	27	183	31	8	142	2	19	15	8	8	18	21
Vehicles Exited	27	186	32	8	143	3	19	15	7	8	19	21
Hourly Exit Rate	108	744	128	32	572	12	76	60	28	32	76	84
Input Volume	97	744	126	31	587	12	80	74	25	31	83	81
% of Volume	111	100	102	103	97	100	95	81	112	103	92	104

2: 5th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	All
Denied Delay (hr)	0.2
Denied Del/Veh (s)	1.6
Total Delay (hr)	2.0
Total Del/Veh (s)	14.1
Vehicles Entered	482
Vehicles Exited	488
Hourly Exit Rate	1952
Input Volume	1971
% of Volume	99

2: 5th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	3.3	1.8	1.8	0.3	0.3	0.2	0.2	0.2	0.1	0.4	0.2	0.2
Total Delay (hr)	0.2	0.7	0.1	0.1	1.3	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	28.3	12.2	6.5	42.6	30.0	8.6	17.8	20.2	12.5	18.9	17.4	10.6
Vehicles Entered	28	196	35	7	149	4	21	18	8	6	22	21
Vehicles Exited	28	194	34	7	150	3	20	18	7	6	22	21
Hourly Exit Rate	112	776	136	28	600	12	80	72	28	24	88	84
Input Volume	101	775	131	32	611	12	84	77	26	32	87	85
% of Volume	111	100	104	88	98	100	95	94	108	75	101	99

2: 5th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	All
Denied Delay (hr)	0.2
Denied Del/Veh (s)	1.1
Total Delay (hr)	2.8
Total Del/Veh (s)	19.2
Vehicles Entered	515
Vehicles Exited	510
Hourly Exit Rate	2040
Input Volume	2053
% of Volume	99

2: 5th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	2.3	1.2	1.7	0.2	0.3	0.1	0.2	0.2	0.2	0.2	0.2	0.3
Total Delay (hr)	0.1	0.6	0.1	0.1	0.9	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	24.4	11.0	6.3	39.9	22.3	6.1	17.3	19.6	12.1	20.7	18.4	11.5
Vehicles Entered	20	188	32	9	146	2	21	17	5	8	21	19
Vehicles Exited	20	188	32	8	144	3	22	18	5	8	22	19
Hourly Exit Rate	80	752	128	32	576	12	88	72	20	32	88	76
Input Volume	97	744	126	31	587	12	80	74	25	31	83	81
% of Volume	82	101	102	103	98	100	110	97	80	103	106	94

2: 5th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	All
Denied Delay (hr)	0.1
Denied Del/Veh (s)	0.8
Total Delay (hr)	2.3
Total Del/Veh (s)	16.2
Vehicles Entered	488
Vehicles Exited	489
Hourly Exit Rate	1956
Input Volume	1971
% of Volume	99

2: 5th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.1	0.3	0.1	0.0	0.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	3.0	1.5	1.9	0.3	0.3	0.1	0.2	0.2	0.2	0.2	0.2	0.2
Total Delay (hr)	0.7	2.4	0.2	0.3	3.7	0.0	0.5	0.4	0.1	0.2	0.4	0.2
Total Del/Veh (s)	25.8	11.4	6.3	34.8	22.4	6.2	20.2	20.8	13.9	18.4	18.1	10.9
Vehicles Entered	99	747	132	32	584	12	81	70	27	29	82	78
Vehicles Exited	98	746	132	31	580	12	82	71	27	29	83	79
Hourly Exit Rate	98	746	132	31	580	12	82	71	27	29	83	79
Input Volume	98	752	127	31	593	12	81	75	25	31	84	82
% of Volume	100	99	104	99	98	100	101	95	107	93	99	96

2: 5th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	All
Denied Delay (hr)	0.5
Denied Del/Veh (s)	1.0
Total Delay (hr)	9.1
Total Del/Veh (s)	16.5
Vehicles Entered	1973
Vehicles Exited	1970
Hourly Exit Rate	1970
Input Volume	1992
% of Volume	99

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Total Network Performance By Interval

Interval Start	4:15	4:30	4:45	5:00	All
Denied Delay (hr)	0.1	0.2	0.2	0.1	0.6
Denied Del/Veh (s)	0.5	1.6	1.2	0.8	1.0
Total Delay (hr)	2.7	2.7	3.6	2.9	11.9
Total Del/Veh (s)	17.4	16.8	21.6	18.6	20.0
Vehicles Entered	519	517	545	519	2100
Vehicles Exited	516	516	541	525	2096
Hourly Exit Rate	2064	2064	2164	2100	2096
Input Volume	7563	7563	7881	7563	7642
% of Volume	27	27	27	28	27

Intersection: 1: 10th Street & Main Street (SH-75), Interval #1

Movement	NB	NE
Directions Served	LT	LR
Maximum Queue (ft)	110	107
Average Queue (ft)	44	61
95th Queue (ft)	111	118
Link Distance (ft)	263	1050
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 1: 10th Street & Main Street (SH-75), Interval #2

Movement	NB	B9	В9	SB	NE
Directions Served	LT	T		TR	LR
Maximum Queue (ft)	91	3	4	2	105
Average Queue (ft)	39	0	1	0	56
95th Queue (ft)	96	7	8	4	102
Link Distance (ft)	263	194	194	604	1050
Upstream Blk Time (%)					
Queuing Penalty (veh)					
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 1: 10th Street & Main Street (SH-75), Interval #3

Movement	NB	В9	В9	SB	NE
Directions Served	LT	Т		TR	LR
Maximum Queue (ft)	117	12	12	4	112
Average Queue (ft)	44	0	0	1	58
95th Queue (ft)	126	4	4	7	115
Link Distance (ft)	263	194	194	604	1050
Upstream Blk Time (%)	0				
Queuing Penalty (veh)	1				
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

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Intersection: 1: 10th Street & Main Street (SH-75), Interval #4

Movement	NB	В9	SB	NE
Directions Served	LT	Т	TR	LR
Maximum Queue (ft)	78	11	2	107
Average Queue (ft)	30	2	0	57
95th Queue (ft)	72	20	5	107
Link Distance (ft)	263	194	604	1050
Upstream Blk Time (%)				

Queuing Penalty (veh)

Storage Bay Dist (ft)

Storage Blk Time (%)

Queuing Penalty (veh)

Intersection: 1: 10th Street & Main Street (SH-75), All Intervals

Movement	NB	В9	B9	SB	NE
Directions Served	LT	T		TR	LR
Maximum Queue (ft)	158	15	14	9	151
Average Queue (ft)	39	1	0	0	58
95th Queue (ft)	104	10	5	5	111
Link Distance (ft)	263	194	194	604	1050
Upstream Blk Time (%)	0				
Queuing Penalty (veh)	0				
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 2: 5th Street & Main Street (SH-75), Interval #1

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	148	159	294	260	156	117
Average Queue (ft)	110	107	183	94	93	72
95th Queue (ft)	152	165	313	276	160	121
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	0	1				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

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Intersection: 2: 5th Street & Main Street (SH-75), Interval #2

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	152	174	305	268	133	133
Average Queue (ft)	121	114	179	86	73	74
95th Queue (ft)	164	176	327	287	131	137
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	0	1				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 2: 5th Street & Main Street (SH-75), Interval #3

Movement	SE	SE	В9	NW	NW	NE	SW
Directions Served	LT	TR	Т	LT	TR	LTR	LTR
Maximum Queue (ft)	169	175	3	330	280	132	138
Average Queue (ft)	126	124	0	236	159	80	74
95th Queue (ft)	175	185	6	451	421	141	144
Link Distance (ft)	194	194	986	872	872	838	1044
Upstream Blk Time (%)	0	1					
Queuing Penalty (veh)	1	1					
Storage Bay Dist (ft)							
Storage Blk Time (%)							
Queuing Penalty (veh)							

Intersection: 2: 5th Street & Main Street (SH-75), Interval #4

Movement	SE	SE	NW	NW	NE	SW
)E	SE	INVV	INVV	IVE	311
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	153	152	297	271	126	136
Average Queue (ft)	117	113	200	99	74	83
95th Queue (ft)	162	167	361	313	126	138
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)	0	0				
Queuing Penalty (veh)	0	1				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

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Intersection: 2: 5th Street & Main Street (SH-75), All Intervals

Movement	SE	SE	B9	NW	NW	NE	SW
Movement	3L	3L	D9	INVV	INVV	IVL	311
Directions Served	LT	TR	T	LT	TR	LTR	LTR
Maximum Queue (ft)	190	205	3	415	371	175	167
Average Queue (ft)	119	115	0	199	109	80	76
95th Queue (ft)	164	174	3	370	331	141	136
Link Distance (ft)	194	194	986	872	872	838	1044
Upstream Blk Time (%)	0	0					
Queuing Penalty (veh)	0	1					
Storage Bay Dist (ft)							
Storage Blk Time (%)							

Queuing Penalty (veh)

Network Summary

Network wide Queuing Penalty, Interval #1: 1

Network wide Queuing Penalty, Interval #2: 1

Network wide Queuing Penalty, Interval #3: 3

Network wide Queuing Penalty, Interval #4: 1

Network wide Queuing Penalty, All Intervals: 2



SimTraffic LOS Report

ID Ketchum - Bracken Station TIS Project:

Future (2026) Plus Project p.m. Peak Hour Analysis Period: Time Period:

Project #: *UT16-851*

10th Street & Main Street (SH-75) Unsignalized Intersection:

Type:

Approach	Marramant	Demand	Volume	e Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
	L	69	70	102	12.3	В
NB	T	307	304	99	0.2	Α
	Subtotal	376	374	99	2.5	Α
	Т	732	733	100	1.7	Α
SB	R	64	63	99	1.0	Α
	Subtotal	796	796	100	1.6	Α
	L	68	67	99	75.3	F
NE	R	95	93	98	54.7	F
	Subtotal	163	160	98	63.3	F
Total		1,334	1,330	100	9.4	Α

Intersection: 5th Street & Main Street (SH-75)

Signalized Type:

rype.		Signalized				
Annyagah	Mayamant	Demand	Volume	Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
	L	31	28	90	44.9	D
NW	Т	616	606	98	28.3	С
INVV	R	12	12	100	8.4	Α
	Subtotal	659	646	98	28.6	С
	L	101	101	100	32.7	С
SE	Т	772	778	101	11.6	В
SE	R	131	127	97	7.5	Α
	Subtotal	1,004	1,006	100	13.2	В
	L	84	89	106	20.5	С
NE	Т	75	77	103	19.4	В
INE	R	25	24	95	15.4	В
	Subtotal	184	190	103	19.4	В
	L	31	30	96	18.6	В
SW	Т	84	85	101	20.4	С
300	R	86	87	101	10.8	В
	Subtotal	201	202	100	16.0	В
Total		2,049	2,044	100	18.9	В



SimTraffic LOS Report

ID Ketchum - Bracken Station TIS Project:

Future (2026) Plus Project p.m. Peak Hour Analysis Period: Time Period:

Project #: *UT16-851*

Main Street (SH-75) & Project Access Unsignalized Intersection:

Type:

. , po.		• norginanizou				
Annyosoh	Mayamant	Demand	Volume	e Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
	L	27	27	99	6.7	Α
NB	Т	759	759	100	0.2	Α
IND						
	Subtotal	786	786	100	0.4	Α
	Т	809	807	100	0.7	Α
SB	R	18	21	115	0.3	Α
OB						
	Subtotal	827	828	100	0.7	Α
	L	18	17	93	21.7	С
EB	R	27	29	106	8.1	Α
		4-	40	400	40.4	_
	Subtotal	45	46	102	13.1	В
Total		1,659	1,660	100	0.9	Α
iolai		1,009	1,000	100	0.9	А

Intersection:

Type:

Approach	Mayamant	Demand	Volume	e Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
					1	
Total						

1: 10th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.7	0.6	0.2	0.2	0.5
Total Delay (hr)	0.1	0.0	0.1	0.0	0.2	0.2	0.6
Total Del/Veh (s)	11.4	0.2	1.6	1.0	49.8	34.2	6.4
Vehicles Entered	16	77	180	16	15	23	327
Vehicles Exited	16	77	180	16	15	22	326
Hourly Exit Rate	64	308	720	64	60	88	1304
Input Volume	68	304	724	63	67	94	1320
% of Volume	94	101	99	102	90	94	99

1: 10th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.8	0.7	0.2	0.3	0.5
Total Delay (hr)	0.1	0.0	0.1	0.0	0.4	0.4	0.9
Total Del/Veh (s)	10.0	0.2	1.6	1.1	73.3	48.0	9.3
Vehicles Entered	19	73	180	14	17	25	328
Vehicles Exited	19	73	182	14	16	24	328
Hourly Exit Rate	76	292	728	56	64	96	1312
Input Volume	68	304	724	63	67	94	1320
% of Volume	112	96	101	89	96	102	99

1: 10th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	NBL	NBT	SBT	SBR	NEL	NER	All	
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Denied Del/Veh (s)	0.0	0.0	8.0	0.7	0.2	0.2	0.5	
Total Delay (hr)	0.1	0.0	0.1	0.0	0.4	0.4	0.9	
Total Del/Veh (s)	15.2	0.2	1.9	1.1	62.3	56.8	9.7	
Vehicles Entered	18	75	190	18	19	23	343	
Vehicles Exited	18	75	189	17	17	22	338	
Hourly Exit Rate	72	300	756	68	68	88	1352	
Input Volume	71	316	755	66	70	98	1376	
% of Volume	101	95	100	103	97	90	98	

1: 10th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.7	0.6	0.2	0.2	0.4
Total Delay (hr)	0.1	0.0	0.1	0.0	0.5	0.5	1.1
Total Del/Veh (s)	12.1	0.2	1.6	0.8	80.6	64.1	11.2
Vehicles Entered	17	78	184	16	17	23	335
Vehicles Exited	17	79	182	16	19	24	337
Hourly Exit Rate	68	316	728	64	76	96	1348
Input Volume	68	304	724	63	67	94	1320
% of Volume	100	104	101	102	113	102	102

1: 10th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.2	0.0	0.0	0.0	0.2
Denied Del/Veh (s)	0.0	0.0	0.8	0.7	0.2	0.2	0.5
Total Delay (hr)	0.2	0.0	0.3	0.0	1.4	1.4	3.5
Total Del/Veh (s)	12.3	0.2	1.7	1.0	75.3	54.7	9.4
Vehicles Entered	70	304	734	63	67	94	1332
Vehicles Exited	70	304	733	63	67	93	1330
Hourly Exit Rate	70	304	733	63	67	93	1330
Input Volume	69	307	732	64	68	95	1334
% of Volume	102	99	100	99	99	98	100

2: 5th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	2.2	1.1	0.5	0.4	0.3	0.1	0.2	0.2	0.2	0.3	0.2	0.2
Total Delay (hr)	0.2	0.6	0.1	0.1	1.3	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	32.6	11.7	7.7	44.2	29.6	4.3	19.5	20.7	12.9	18.6	19.2	12.9
Vehicles Entered	24	191	32	6	156	3	22	19	7	6	22	24
Vehicles Exited	23	187	32	6	146	2	25	19	7	7	22	24
Hourly Exit Rate	92	748	128	24	584	8	100	76	28	28	88	96
Input Volume	100	764	130	31	610	12	83	74	25	31	83	85
% of Volume	92	98	98	77	96	67	120	103	112	90	106	113

2: 5th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	All
Denied Delay (hr)	0.1
Denied Del/Veh (s)	0.7
Total Delay (hr)	2.8
Total Del/Veh (s)	19.5
Vehicles Entered	512
Vehicles Exited	500
Hourly Exit Rate	2000
Input Volume	2028
% of Volume	99

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2: 5th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	3.3	1.7	0.8	0.3	0.3	0.2	0.2	0.2	0.2	0.4	0.2	0.2
Total Delay (hr)	0.3	0.6	0.1	0.1	1.0	0.0	0.1	0.1	0.0	0.0	0.1	0.0
Total Del/Veh (s)	37.5	11.2	7.9	31.7	22.6	4.9	18.4	16.0	13.2	15.2	16.7	7.2
Vehicles Entered	25	195	30	6	149	3	18	22	5	7	20	21
Vehicles Exited	26	200	30	6	156	3	17	22	5	7	19	21
Hourly Exit Rate	104	800	120	24	624	12	68	88	20	28	76	84
Input Volume	100	764	130	31	610	12	83	74	25	31	83	85
% of Volume	104	105	92	77	102	100	82	119	80	90	92	99

2: 5th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	All
Denied Delay (hr)	0.1
Denied Del/Veh (s)	1.0
Total Delay (hr)	2.4
Total Del/Veh (s)	16.7
Vehicles Entered	501
Vehicles Exited	512
Hourly Exit Rate	2048
Input Volume	2028
% of Volume	101

2: 5th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	1.0	0.4	0.4	0.4	0.3	0.1	0.2	0.2	0.3	0.3	0.2	0.2
Total Delay (hr)	0.2	0.6	0.1	0.1	1.2	0.0	0.1	0.1	0.0	0.0	0.2	0.1
Total Del/Veh (s)	28.4	11.1	6.6	44.9	27.1	13.2	21.6	18.8	14.4	20.2	22.6	12.9
Vehicles Entered	26	201	30	8	154	3	24	18	7	7	22	21
Vehicles Exited	27	199	30	7	150	2	23	18	7	8	23	20
Hourly Exit Rate	108	796	120	28	600	8	92	72	28	32	92	80
Input Volume	104	796	135	32	635	12	87	77	26	32	87	89
% of Volume	104	100	89	88	94	67	106	94	108	100	106	90

2: 5th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

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10 min of Dol(Vala (a)
enied Del/Veh (s) 0.4
otal Delay (hr) 2.8
otal Del/Veh (s) 18.6
ehicles Entered 521
ehicles Exited 514
ourly Exit Rate 2056
nput Volume 2112
of Volume 97

2: 5th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	2.1	0.5	0.3	0.4	0.3	0.1	0.2	0.2	0.2	0.2	0.2	0.2
Total Delay (hr)	0.2	0.6	0.1	0.1	1.3	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	28.6	11.6	7.6	45.9	28.7	9.7	20.3	18.8	16.6	14.0	18.9	8.6
Vehicles Entered	25	194	36	7	155	3	23	18	5	8	21	22
Vehicles Exited	25	192	36	8	154	3	24	18	5	8	21	22
Hourly Exit Rate	100	768	144	32	616	12	96	72	20	32	84	88
Input Volume	100	764	130	31	610	12	83	74	25	31	83	85
% of Volume	100	101	111	103	101	100	116	97	80	103	101	104

2: 5th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	All
Denied Delay (hr)	0.1
Denied Del/Veh (s)	0.5
Total Delay (hr)	2.8
Total Del/Veh (s)	18.8
Vehicles Entered	517
Vehicles Exited	516
Hourly Exit Rate	2064
Input Volume	2028
% of Volume	102

2: 5th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.1	0.2	0.0	0.0	0.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	2.1	0.9	0.5	0.4	0.3	0.1	0.2	0.2	0.2	0.3	0.2	0.2
Total Delay (hr)	0.9	2.5	0.3	0.3	4.8	0.0	0.5	0.4	0.1	0.2	0.5	0.3
Total Del/Veh (s)	32.7	11.6	7.5	44.9	28.3	8.4	20.5	19.4	15.4	18.6	20.4	10.8
Vehicles Entered	101	780	128	27	614	12	87	77	24	29	85	88
Vehicles Exited	101	778	127	28	606	12	89	77	24	30	85	87
Hourly Exit Rate	101	778	127	28	606	12	89	77	24	30	85	87
Input Volume	101	772	131	31	616	12	84	75	25	31	84	86
% of Volume	100	101	97	90	98	100	106	103	95	96	101	101

2: 5th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	All
Denied Delay (hr)	0.4
Denied Del/Veh (s)	0.6
Total Delay (hr)	10.9
Total Del/Veh (s)	18.9
Vehicles Entered	2052
Vehicles Exited	2044
Hourly Exit Rate	2044
Input Volume	2049
% of Volume	100

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3: Main Street (SH-75) & Project Access Performance by movement Interval #1 4:15

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	18.8	6.8	6.1	0.2	0.6	0.3	8.0
Vehicles Entered	4	6	7	190	198	4	409
Vehicles Exited	4	7	7	189	198	4	409
Hourly Exit Rate	16	28	28	756	792	16	1636
Input Volume	18	27	27	751	801	18	1642
% of Volume	89	104	104	101	99	89	100

3: Main Street (SH-75) & Project Access Performance by movement Interval #2 4:30

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	18.1	8.4	6.8	0.2	0.7	0.4	0.9
Vehicles Entered	5	8	7	186	200	6	412
Vehicles Exited	5	8	7	187	201	6	414
Hourly Exit Rate	20	32	28	748	804	24	1656
Input Volume	18	27	27	751	801	18	1642
% of Volume	111	119	104	100	100	133	101

3: Main Street (SH-75) & Project Access Performance by movement Interval #3 4:45

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	16.9	8.8	7.2	0.2	0.7	0.3	0.9
Vehicles Entered	4	8	6	185	206	5	414
Vehicles Exited	4	8	6	184	205	6	413
Hourly Exit Rate	16	32	24	736	820	24	1652
Input Volume	19	28	28	783	834	19	1711
% of Volume	84	114	86	94	98	126	97

3: Main Street (SH-75) & Project Access Performance by movement Interval #4 5:00

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	34.1	8.4	7.0	0.3	0.6	0.3	1.0
Vehicles Entered	4	6	7	198	202	5	422
Vehicles Exited	4	6	7	198	203	5	423
Hourly Exit Rate	16	24	28	792	812	20	1692
Input Volume	18	27	27	751	801	18	1642
% of Volume	89	89	104	105	101	111	103

3: Main Street (SH-75) & Project Access Performance by movement Entire Run

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.1	0.1	0.1	0.0	0.1	0.0	0.4
Total Del/Veh (s)	21.7	8.1	6.7	0.2	0.7	0.3	0.9
Vehicles Entered	17	29	27	759	806	21	1659
Vehicles Exited	17	29	27	759	807	21	1660
Hourly Exit Rate	17	29	27	759	807	21	1660
Input Volume	18	27	27	759	809	18	1659
% of Volume	93	106	99	100	100	115	100

Total Network Performance By Interval

Interval Start	4:15	4:30	4:45	5:00	All
Denied Delay (hr)	0.1	0.2	0.1	0.1	0.5
Denied Del/Veh (s)	0.9	1.2	0.6	0.7	0.9
Total Delay (hr)	4.1	4.0	4.4	4.6	17.0
Total Del/Veh (s)	24.4	23.7	25.2	26.3	26.9
Vehicles Entered	552	543	568	557	2221
Vehicles Exited	542	548	557	571	2219
Hourly Exit Rate	2168	2192	2228	2284	2219
Input Volume	9998	9998	10415	9998	10102
% of Volume	22	22	21	23	22

Intersection: 1: 10th Street & Main Street (SH-75), Interval #1

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	53	8	158
Average Queue (ft)	28	1	90
95th Queue (ft)	63	9	182
Link Distance (ft)	79	609	1043
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	0		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), Interval #2

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	64	17	180
Average Queue (ft)	35	3	120
95th Queue (ft)	71	21	248
Link Distance (ft)	79	609	1043
Upstream Blk Time (%)	1		
Queuing Penalty (veh)	1		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), Interval #3

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	86	23	214
Average Queue (ft)	39	5	113
95th Queue (ft)	81	25	226
Link Distance (ft)	79	609	1043
Upstream Blk Time (%)	2		
Queuing Penalty (veh)	5		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), Interval #4

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	70	42	218
Average Queue (ft)	34	7	138
95th Queue (ft)	73	44	310
Link Distance (ft)	79	609	1043
Upstream Blk Time (%)	2		
Queuing Penalty (veh)	3		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), All Intervals

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	96	57	271
Average Queue (ft)	34	4	116
95th Queue (ft)	72	28	248
Link Distance (ft)	79	609	1043
Upstream Blk Time (%)	1		
Queuing Penalty (veh)	2		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 2: 5th Street & Main Street (SH-75), Interval #1

Movement	SE	SE	В9	NW	NW	NE	SW
Directions Served	LT	TR	Т	LT	TR	LTR	LTR
Maximum Queue (ft)	192	192	22	379	341	130	147
Average Queue (ft)	124	129	3	237	146	88	90
95th Queue (ft)	198	198	37	463	424	141	168
Link Distance (ft)	194	194	980	872	872	838	1044
Upstream Blk Time (%)	1	1					
Queuing Penalty (veh)	5	4					
Storage Bay Dist (ft)							
Storage Blk Time (%)							
Queuing Penalty (veh)							

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Intersection: 2: 5th Street & Main Street (SH-75), Interval #2

Movement	SE	SE	В9	NW	NW	NE	SW
Directions Served	LT	TR	T	LT	TR	LTR	LTR
Maximum Queue (ft)	194	188	2	354	303	126	113
Average Queue (ft)	127	134	0	209	122	77	69
95th Queue (ft)	200	206	5	387	357	130	113
Link Distance (ft)	194	194	980	872	872	838	1044
Upstream Blk Time (%)	1	1					
Queuing Penalty (veh)	2	2					
Storage Bay Dist (ft)							
Storage Blk Time (%)							
Queuing Penalty (veh)							

Intersection: 2: 5th Street & Main Street (SH-75), Interval #3

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	189	174	356	297	151	168
Average Queue (ft)	132	126	211	127	89	89
95th Queue (ft)	202	199	423	371	148	167
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)	1	1				
Queuing Penalty (veh)	3	5				
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 2: 5th Street & Main Street (SH-75), Interval #4

Mayamant	CE	CE	NI\A/	NIVA/	NE	CM
Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	170	179	378	327	130	120
Average Queue (ft)	129	131	240	152	83	75
95th Queue (ft)	183	192	499	436	147	131
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)	0	0	0			
Queuing Penalty (veh)	1	1	0			
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

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Intersection: 2: 5th Street & Main Street (SH-75), All Intervals

Movement	SE	SE	В9	NW	NW	NE	SW
Directions Served	LT	TR	T	LT	TR	LTR	LTR
Maximum Queue (ft)	228	212	22	467	426	165	188
Average Queue (ft)	128	130	1	224	137	84	81
95th Queue (ft)	196	199	18	446	399	143	148
Link Distance (ft)	194	194	980	872	872	838	1044
Upstream Blk Time (%)	1	1		0			
Queuing Penalty (veh)	3	3		0			
Storage Bay Dist (ft)							
Storage Blk Time (%)							
Queuing Penalty (veh)							

Intersection: 3: Main Street (SH-75) & Project Access, Interval #1

Movement	EB	EB	NB	В9	В9	SB
Directions Served	L	R	LT	T		TR
Maximum Queue (ft)	42	37	56	5	4	22
Average Queue (ft)	16	20	18	1	1	3
95th Queue (ft)	45	45	55	8	8	20
Link Distance (ft)	92	92	102	194	194	79
Upstream Blk Time (%)			0			0
Queuing Penalty (veh)			0			0
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 3: Main Street (SH-75) & Project Access, Interval #2

Movement	EB	EB	NB	SB
	ED		IND	
Directions Served	L	R	LT	TR
Maximum Queue (ft)	35	45	65	11
Average Queue (ft)	16	25	18	2
95th Queue (ft)	42	55	64	13
Link Distance (ft)	92	92	102	79
Upstream Blk Time (%)			0	
Queuing Penalty (veh)			1	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 3: Main Street (SH-75) & Project Access, Interval #3

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	42	49	40	18
Average Queue (ft)	15	27	17	3
95th Queue (ft)	44	56	46	18
Link Distance (ft)	92	92	102	79
Upstream Blk Time (%)	0	0		
Queuing Penalty (veh)	0	0		
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 3: Main Street (SH-75) & Project Access, Interval #4

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	46	40	56	10
Average Queue (ft)	17	16	20	2
95th Queue (ft)	52	44	55	12
Link Distance (ft)	92	92	102	79
Upstream Blk Time (%)	1	0	0	
Queuing Penalty (veh)	0	0	0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 3: Main Street (SH-75) & Project Access, All Intervals

Movement	EB	EB	NB	B9	В9	SB
Directions Served	L	R	LT	T		TR
Maximum Queue (ft)	56	55	76	5	4	35
Average Queue (ft)	16	22	18	0	0	2
95th Queue (ft)	46	51	56	4	4	16
Link Distance (ft)	92	92	102	194	194	79
Upstream Blk Time (%)	0	0	0			0
Queuing Penalty (veh)	0	0	0			0
Storage Bay Dist (ft)						
Storage Blk Time (%)						

Queuing Penalty (veh)

Network Summary

Network wide Queuing Penalty, Interval #1: 9 Network wide Queuing Penalty, Interval #2: 6 Network wide Queuing Penalty, Interval #3: 12 Network wide Queuing Penalty, Interval #4: 5 Network wide Queuing Penalty, All Intervals: 8

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SimTraffic LOS Report

Project: **ID Ketchum - Bracken Station TIS** Hypothetical Future (2026) Plus Project p.m. Peak Hour Analysis Period: Time Period:

Project #: UT16-851

10th Street & Main Street (SH-75) Unsignalized Intersection:

Type:

A		Demand	Volume	Served	Delay/Ve	h (sec)
Approacn	Movement	Volume	Avg	%	Avg	LOS
	L	69	64	93	12.0	В
NB	T	325	317	97	0.2	Α
	Subtotal	394	381	97	2.2	Α
	Т	751	740	99	1.8	Α
SB	R	64	62	97	1.1	Α
	Subtotal	815	802	98	1.7	Α
	L	68	65	96	89.6	F
NE	R	95	94	99	74.4	F
	Subtotal	163	159	98	80.6	F
Total		1,371	1,342	98	11.4	В

Intersection: 5th Street & Main Street (SH-75)

Signalized Type:

Ammussah	Mayramant	Demand	Volume	Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
	L	31	31	99	60.8	Ε
NW	Т	645	643	100	44.8	D
INVV	R	12	14	117	17.3	В
	Subtotal	688	688	100	45.0	D
	L	101	102	101	41.8	D
SE	Т	801	779	97	12.5	В
SL	R	131	128	98	7.7	Α
	Subtotal	1,033	1,009	98	14.9	В
	L	84	88	105	19.0	В
NE	Т	75	73	98	19.3	В
INL	R	25	26	103	13.9	В
	Subtotal	184	187	102	18.4	В
	L	31	28	90	17.7	В
SW	Т	84	84	100	17.5	В
300	R	86	90	105	10.1	В
	Subtotal	201	202	100	14.2	В
Total		2,107	2,086	99	25.2	С



SimTraffic LOS Report

ID Ketchum - Bracken Station TIS Hypothetical Future (2026) Plus Project p.m. Peak Hour Project: Analysis Period: Time Period:

Project #: UT16-851

Main Street (SH-75) & Project Access Unsignalized Intersection:

Type:

Ammussch	Mayamant	Demand	Volume	Served	Delay/Ve	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
	L	56	58	104	8.0	Α
NB	Т	759	765	101	0.4	Α
110						
	Subtotal	815	823	101	0.9	Α
	Т	809	797	98	0.8	Α
SB	R	37	37	99	0.3	Α
0.5						
	Subtotal	846	834	99	0.8	Α
	L	37	35	94	29.3	D
EB	R	56	50	90	9.1	Α
	Subtotal	93	85	91	17.4	С
Total		1,754	1,742	99	1.7	Α

Intersection:

Type:

Approach	Movement	Demand	Volum	e Served	Delay/Vel	h (sec)
Approach	Movement	Volume	Avg	%	Avg	LOS
Total						

1: 10th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.8	0.7	0.2	0.3	0.5
Total Delay (hr)	0.0	0.0	0.1	0.0	0.3	0.3	8.0
Total Del/Veh (s)	8.5	0.2	1.8	1.0	62.3	47.7	8.1
Vehicles Entered	16	78	181	16	16	24	331
Vehicles Exited	16	78	182	15	16	23	330
Hourly Exit Rate	64	312	728	60	64	92	1320
Input Volume	68	322	743	63	67	94	1357
% of Volume	94	97	98	95	96	98	97

1: 10th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.7	0.7	0.2	0.2	0.5
Total Delay (hr)	0.0	0.0	0.1	0.0	0.4	0.4	1.0
Total Del/Veh (s)	9.6	0.2	1.7	1.2	78.6	63.7	10.3
Vehicles Entered	16	80	183	16	17	22	334
Vehicles Exited	16	81	182	16	16	21	332
Hourly Exit Rate	64	324	728	64	64	84	1328
Input Volume	68	322	743	63	67	94	1357
% of Volume	94	101	98	102	96	89	98

1: 10th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	NBL	NBT	SBT	SBR	NEL	NER	All	
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	
Denied Del/Veh (s)	0.0	0.0	8.0	0.6	0.2	0.2	0.5	
Total Delay (hr)	0.1	0.0	0.1	0.0	0.5	0.7	1.4	
Total Del/Veh (s)	15.6	0.3	1.9	1.0	91.6	81.9	14.3	
Vehicles Entered	15	78	192	16	17	28	346	
Vehicles Exited	15	78	192	16	16	27	344	
Hourly Exit Rate	60	312	768	64	64	108	1376	
Input Volume	71	335	774	66	70	98	1414	
% of Volume	85	93	99	97	91	110	97	

1: 10th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.8	0.7	0.2	0.2	0.5
Total Delay (hr)	0.1	0.0	0.1	0.0	0.4	0.5	1.1
Total Del/Veh (s)	13.5	0.2	1.7	1.1	85.1	72.0	11.7
Vehicles Entered	17	81	186	16	15	21	336
Vehicles Exited	18	81	184	16	17	23	339
Hourly Exit Rate	72	324	736	64	68	92	1356
Input Volume	68	322	743	63	67	94	1357
% of Volume	106	101	99	102	101	98	100

1: 10th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.2	0.0	0.0	0.0	0.2
Denied Del/Veh (s)	0.0	0.0	8.0	0.7	0.2	0.2	0.5
Total Delay (hr)	0.2	0.0	0.4	0.0	1.7	2.0	4.3
Total Del/Veh (s)	12.0	0.2	1.8	1.1	89.6	74.4	11.4
Vehicles Entered	65	318	742	63	66	96	1350
Vehicles Exited	64	317	740	62	65	94	1342
Hourly Exit Rate	64	317	740	62	65	94	1342
Input Volume	69	325	751	64	68	95	1371
% of Volume	93	97	99	97	96	99	98

2: 5th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	2.9	1.0	1.7	0.4	0.4	0.2	0.2	0.2	0.2	0.2	0.2	0.2
Total Delay (hr)	0.3	0.7	0.1	0.1	1.9	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	39.9	12.6	7.5	56.1	40.7	13.9	17.1	18.8	11.2	16.0	18.0	10.9
Vehicles Entered	26	195	31	9	165	3	22	15	6	8	22	21
Vehicles Exited	25	187	30	8	157	4	22	16	7	8	22	22
Hourly Exit Rate	100	748	120	32	628	16	88	64	28	32	88	88
Input Volume	100	793	130	31	638	12	83	74	25	31	83	85
% of Volume	100	94	92	103	98	133	106	86	112	103	106	104

2: 5th Street & Main Street (SH-75) Performance by movement Interval #1 4:15

Movement	All
Denied Delay (hr)	0.1
Denied Del/Veh (s)	0.8
Total Delay (hr)	3.6
Total Del/Veh (s)	23.8
Vehicles Entered	523
Vehicles Exited	508
Hourly Exit Rate	2032
Input Volume	2085
% of Volume	97

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2: 5th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	2.2	0.9	0.6	0.4	0.4	0.2	0.2	0.1	0.2	0.1	0.2	0.2
Total Delay (hr)	0.2	0.7	0.1	0.2	2.3	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	34.5	11.6	7.2	62.7	46.7	19.8	17.0	16.2	11.1	18.8	15.1	9.3
Vehicles Entered	25	195	31	8	161	3	21	17	6	6	21	23
Vehicles Exited	25	201	31	8	162	3	20	17	5	6	21	23
Hourly Exit Rate	100	804	124	32	648	12	80	68	20	24	84	92
Input Volume	100	793	130	31	638	12	83	74	25	31	83	85
% of Volume	100	101	95	103	102	100	96	92	80	77	101	108

2: 5th Street & Main Street (SH-75) Performance by movement Interval #2 4:30

Movement	All
Denied Delay (hr)	0.1
Denied Del/Veh (s)	0.6
Total Delay (hr)	3.8
Total Del/Veh (s)	25.1
Vehicles Entered	517
Vehicles Exited	522
Hourly Exit Rate	2088
Input Volume	2085
% of Volume	100

2: 5th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.1	0.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	8.2	3.7	1.3	0.3	0.4	0.2	0.2	0.2	0.2	0.2	0.2	0.2
Total Delay (hr)	0.4	0.7	0.1	0.1	2.3	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	52.5	13.0	7.5	56.3	46.5	17.4	19.7	21.3	16.7	15.7	19.3	10.4
Vehicles Entered	26	200	33	7	164	3	21	21	7	8	21	21
Vehicles Exited	25	198	33	7	168	4	21	21	7	7	20	20
Hourly Exit Rate	100	792	132	28	672	16	84	84	28	28	80	80
Input Volume	104	826	135	32	665	12	87	77	26	32	87	89
% of Volume	96	96	98	88	101	133	97	109	108	88	92	90

2: 5th Street & Main Street (SH-75) Performance by movement Interval #3 4:45

Movement	All
Denied Delay (hr)	0.3
Denied Del/Veh (s)	2.0
Total Delay (hr)	4.2
Total Del/Veh (s)	26.9
Vehicles Entered	532
Vehicles Exited	531
Hourly Exit Rate	2124
Input Volume	2172
% of Volume	98

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2: 5th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.0	0.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	4.9	1.5	1.5	0.3	0.3	0.1	0.2	0.2	0.2	0.2	0.2	0.2
Total Delay (hr)	0.3	0.6	0.1	0.1	1.6	0.0	0.1	0.1	0.0	0.0	0.1	0.1
Total Del/Veh (s)	34.1	11.8	8.0	53.3	34.8	14.3	18.7	18.2	14.2	18.3	15.3	8.3
Vehicles Entered	26	193	33	8	159	4	23	19	7	7	21	25
Vehicles Exited	26	192	34	7	157	4	25	19	7	7	21	26
Hourly Exit Rate	104	768	136	28	628	16	100	76	28	28	84	104
Input Volume	100	793	130	31	638	12	83	74	25	31	83	85
% of Volume	104	97	105	90	98	133	120	103	112	90	101	122

2: 5th Street & Main Street (SH-75) Performance by movement Interval #4 5:00

Movement	All
Denied Delay (hr)	0.1
Denied Del/Veh (s)	1.0
Total Delay (hr)	3.2
Total Del/Veh (s)	21.1
Vehicles Entered	525
Vehicles Exited	525
Hourly Exit Rate	2100
Input Volume	2085
% of Volume	101

2: 5th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Denied Delay (hr)	0.1	0.4	0.0	0.0	0.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	4.6	1.8	1.3	0.4	0.4	0.2	0.2	0.2	0.2	0.2	0.2	0.2
Total Delay (hr)	1.2	2.7	0.3	0.5	8.2	0.1	0.5	0.4	0.1	0.1	0.4	0.3
Total Del/Veh (s)	41.8	12.5	7.7	60.8	44.8	17.3	19.0	19.3	13.9	17.7	17.5	10.1
Vehicles Entered	102	783	128	31	650	14	87	73	26	28	84	90
Vehicles Exited	102	779	128	31	643	14	88	73	26	28	84	90
Hourly Exit Rate	102	779	128	31	643	14	88	73	26	28	84	90
Input Volume	101	801	131	31	645	12	84	75	25	31	84	86
% of Volume	101	97	98	99	100	117	105	98	103	90	100	105

2: 5th Street & Main Street (SH-75) Performance by movement Entire Run

Movement	All
Denied Delay (hr)	0.7
Denied Del/Veh (s)	1.1
Total Delay (hr)	14.8
Total Del/Veh (s)	25.2
Vehicles Entered	2096
Vehicles Exited	2086
Hourly Exit Rate	2086
Input Volume	2107
% of Volume	99

3: Main Street (SH-75) & Project Access Performance by movement Interval #1 4:15

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.1	0.0	0.0	0.0	0.0	0.0	0.2
Total Del/Veh (s)	25.8	8.5	9.3	0.4	0.8	0.3	1.7
Vehicles Entered	10	12	14	188	198	8	430
Vehicles Exited	9	12	14	187	198	8	428
Hourly Exit Rate	36	48	56	748	792	32	1712
Input Volume	37	55	55	751	801	37	1736
% of Volume	97	87	102	100	99	86	99

3: Main Street (SH-75) & Project Access Performance by movement Interval #2 4:30

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.2
Total Del/Veh (s)	21.5	8.8	6.9	0.4	8.0	0.4	1.4
Vehicles Entered	8	13	14	191	195	8	429
Vehicles Exited	8	13	14	191	195	8	429
Hourly Exit Rate	32	52	56	764	780	32	1716
Input Volume	37	55	55	751	801	37	1736
% of Volume	86	95	102	102	97	86	99

3: Main Street (SH-75) & Project Access Performance by movement Interval #3 4:45

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.1	0.0	0.0	0.0	0.1	0.0	0.2
Total Del/Veh (s)	32.9	10.1	9.5	0.4	0.9	0.3	1.9
Vehicles Entered	10	12	14	192	207	11	446
Vehicles Exited	9	12	14	192	207	11	445
Hourly Exit Rate	36	48	56	768	828	44	1780
Input Volume	38	58	58	783	834	38	1809
% of Volume	95	83	97	98	99	116	98

3: Main Street (SH-75) & Project Access Performance by movement Interval #4 5:00

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.1	0.0	0.0	0.0	0.0	0.0	0.2
Total Del/Veh (s)	29.9	8.8	6.7	0.3	8.0	0.3	1.6
Vehicles Entered	8	12	16	196	198	10	440
Vehicles Exited	9	12	15	195	197	10	438
Hourly Exit Rate	36	48	60	780	788	40	1752
Input Volume	37	55	55	751	801	37	1736
% of Volume	97	87	109	104	98	108	101

3: Main Street (SH-75) & Project Access Performance by movement Entire Run

	:						
Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.3	0.1	0.1	0.1	0.2	0.0	8.0
Total Del/Veh (s)	29.3	9.1	8.0	0.4	0.8	0.3	1.7
Vehicles Entered	35	50	59	766	798	37	1745
Vehicles Exited	35	50	58	765	797	37	1742
Hourly Exit Rate	35	50	58	765	797	37	1742
Input Volume	37	56	56	759	809	37	1754
% of Volume	94	90	104	101	98	99	99

Total Network Performance By Interval

Interval Start	4:15	4:30	4:45	5:00	All
Denied Delay (hr)	0.2	0.1	0.4	0.2	0.8
Denied Del/Veh (s)	1.0	0.8	2.2	1.2	1.3
Total Delay (hr)	5.1	5.6	6.5	5.2	22.3
Total Del/Veh (s)	29.6	31.5	35.1	28.8	34.1
Vehicles Entered	570	568	590	573	2302
Vehicles Exited	557	565	586	581	2290
Hourly Exit Rate	2228	2260	2344	2324	2290
Input Volume	10393	10393	10830	10393	10502
% of Volume	21	22	22	22	22

Intersection: 1: 10th Street & Main Street (SH-75), Interval #1

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	49	41	207
Average Queue (ft)	29	8	115
95th Queue (ft)	57	48	249
Link Distance (ft)	79	609	1043
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	0		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), Interval #2

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	55	21	228
Average Queue (ft)	28	5	134
95th Queue (ft)	61	22	319
Link Distance (ft)	79	609	1043
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	1		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), Interval #3

N. 4	ND	CD	NE
Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	86	25	297
Average Queue (ft)	38	5	175
95th Queue (ft)	81	28	443
Link Distance (ft)	79	609	1043
Upstream Blk Time (%)	3		
Queuing Penalty (veh)	6		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), Interval #4

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	75	11	247
Average Queue (ft)	36	2	144
95th Queue (ft)	80	16	339
Link Distance (ft)	79	609	1043
Upstream Blk Time (%)	2		
Queuing Penalty (veh)	4		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (SH-75), All Intervals

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	99	57	389
Average Queue (ft)	33	5	142
95th Queue (ft)	71	31	346
Link Distance (ft)	79	609	1043
Upstream Blk Time (%)	1		
Queuing Penalty (veh)	3		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 2: 5th Street & Main Street (SH-75), Interval #1

Movement	SE	SE	В9	NW	NW	NE	SW
	JL			11111			
Directions Served	LI	TR		LI	TR	LTR	LTR
Maximum Queue (ft)	199	212	48	444	395	138	140
Average Queue (ft)	130	136	7	308	221	78	80
95th Queue (ft)	207	213	60	574	546	138	138
Link Distance (ft)	194	194	980	872	872	838	1044
Upstream Blk Time (%)	2	2					
Queuing Penalty (veh)	10	8					
Storage Bay Dist (ft)							
Storage Blk Time (%)							
Queuing Penalty (veh)							

Intersection: 2: 5th Street & Main Street (SH-75), Interval #2

Movement	SE	SE	NW	NW	NE	SW
Directions Served	LT	TR	LT	TR	LTR	LTR
Maximum Queue (ft)	180	185	459	417	115	111
Average Queue (ft)	131	132	314	229	75	71
95th Queue (ft)	190	204	659	616	124	121
Link Distance (ft)	194	194	872	872	838	1044
Upstream Blk Time (%)	0	1	2	0		
Queuing Penalty (veh)	1	3	0	0		
Storage Bay Dist (ft)						
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 2: 5th Street & Main Street (SH-75), Interval #3

Movement	SE	SE	В9	NW	NW	NE	SW
Directions Served	LT	TR	T	LT	TR	LTR	LTR
Maximum Queue (ft)	200	206	47	515	457	129	143
Average Queue (ft)	146	143	6	342	280	86	75
95th Queue (ft)	222	223	76	678	630	139	139
Link Distance (ft)	194	194	980	872	872	838	1044
Upstream Blk Time (%)	4	2					
Queuing Penalty (veh)	18	9					
Storage Bay Dist (ft)							
Storage Blk Time (%)							
Queuing Penalty (veh)							

Intersection: 2: 5th Street & Main Street (SH-75), Interval #4

Movement	SE	SE	В9	NW	NW	NE	SW
Directions Served	LT	TR	T	LT	TR	LTR	LTR
Maximum Queue (ft)	192	203	42	418	383	125	117
Average Queue (ft)	132	135	6	266	212	86	71
95th Queue (ft)	208	215	61	507	481	147	121
Link Distance (ft)	194	194	980	872	872	838	1044
Upstream Blk Time (%)	1	1					
Queuing Penalty (veh)	6	4					
Storage Bay Dist (ft)							
Storage Blk Time (%)							
Queuing Penalty (veh)							

Intersection: 2: 5th Street & Main Street (SH-75), All Intervals

Movement	SE	SE	В9	NW	NW	NE	SW
Directions Served	LT	TR	T	LT	TR	LTR	LTR
Maximum Queue (ft)	236	241	96	614	567	160	168
Average Queue (ft)	135	137	5	308	236	81	75
95th Queue (ft)	208	214	57	611	574	138	131
Link Distance (ft)	194	194	980	872	872	838	1044
Upstream Blk Time (%)	2	1		0	0		
Queuing Penalty (veh)	9	6		0	0		
Storage Bay Dist (ft)							
Storage Blk Time (%)							

Queuing Penalty (veh)

Intersection: 3: Main Street (SH-75) & Project Access, Interval #1

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	60	54	76	34
Average Queue (ft)	29	30	33	6
95th Queue (ft)	62	62	81	34
Link Distance (ft)	92	92	102	79
Upstream Blk Time (%)	0	0	1	0
Queuing Penalty (veh)	0	0	3	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 3: Main Street (SH-75) & Project Access, Interval #2

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	44	70	74	34
Average Queue (ft)	24	33	33	6
95th Queue (ft)	52	68	77	35
Link Distance (ft)	92	92	102	79
Upstream Blk Time (%)		0	0	0
Queuing Penalty (veh)		0	1	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Hales Engineering 801.766.4343 Page 10

Intersection: 3: Main Street (SH-75) & Project Access, Interval #3

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	63	55	80	27
Average Queue (ft)	34	30	37	5
95th Queue (ft)	66	63	80	26
Link Distance (ft)	92	92	102	79
Upstream Blk Time (%)	1	0	0	
Queuing Penalty (veh)	0	0	0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 3: Main Street (SH-75) & Project Access, Interval #4

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	56	64	67	26
Average Queue (ft)	29	32	33	5
95th Queue (ft)	61	64	71	26
Link Distance (ft)	92	92	102	79
Upstream Blk Time (%)	0	0	0	
Queuing Penalty (veh)	0	0	0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 3: Main Street (SH-75) & Project Access, All Intervals

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	74	79	101	57
Average Queue (ft)	29	31	34	6
95th Queue (ft)	61	64	78	31
Link Distance (ft)	92	92	102	79
Upstream Blk Time (%)	0	0	0	0
Queuing Penalty (veh)	0	0	1	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				

Storage Blk Time (%)

Queuing Penalty (veh)

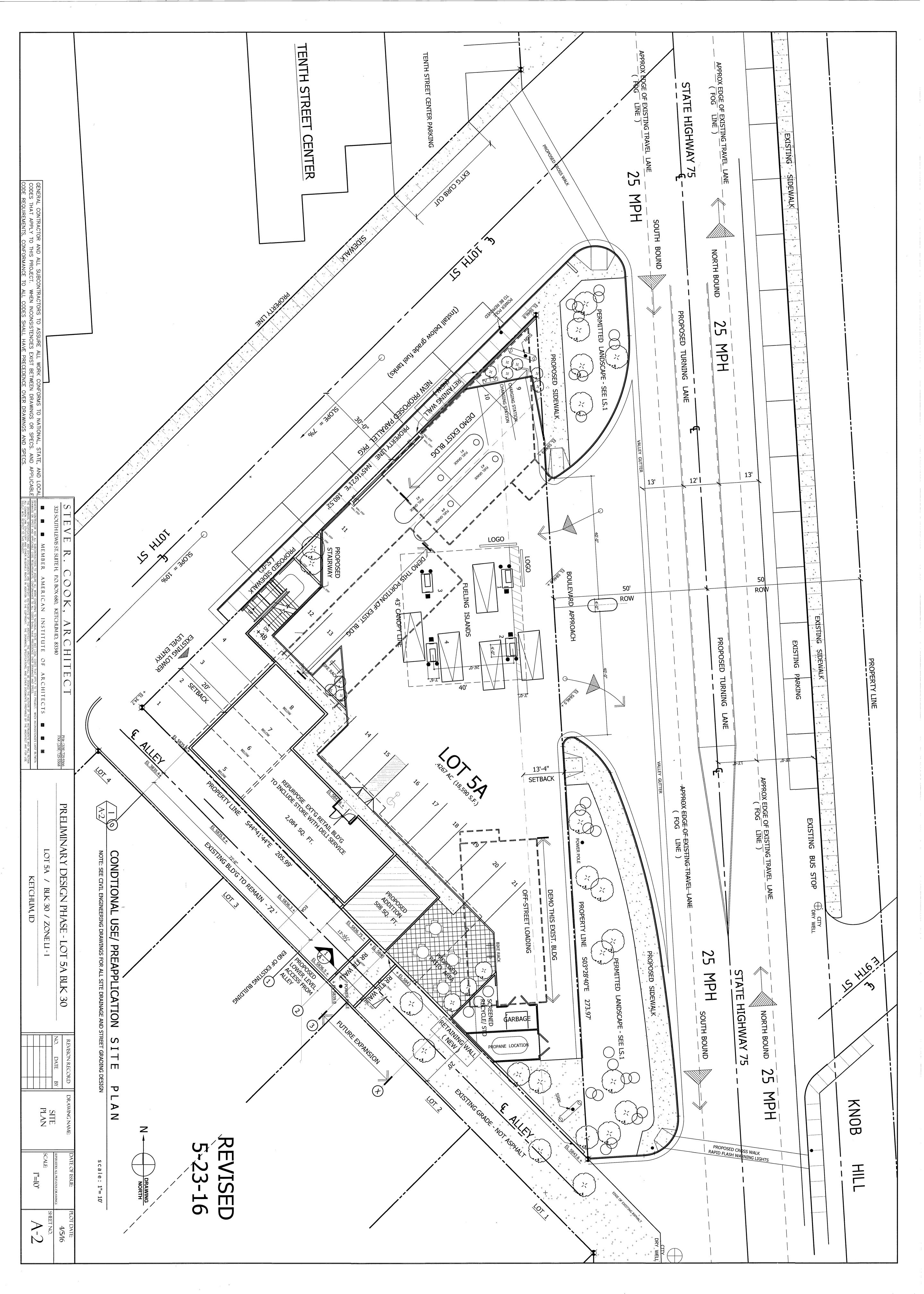
Network Summary

Network v	wide (Queuing	Penalty,	Interval	#1:	21
Network v	wide (Queuing	Penalty,	Interval	#2:	6
Network v	wide (Queuing	Penalty,	Interval	#3:	34
Network v	wide (Queuing	Penalty,	Interval	#4:	14
Network v	vide (Queuina	Penalty.	All Inter	vals	: 10



APPENDIX C

Site Plan





APPENDIX D

95th Percentile Queue Length Reports

Time Period: p.m. Peak Hour 95th Percentile Queue Length (feet)



			B9	NB		NE		W	;	SE	SW
Intersection	Time Period	۲	(blan k)	LT	LR	LTR	LT	TR	LT	TR	LTR
10th Street & Main Street (SH-75)	Existing (2016) Background	4	3	86	83						
5th Street & Main Street (SH-75)	Existing (2016) Background					134	200	118	131	128	121

Time Period: p.m. Peak Hour 95th Percentile Queue Length (feet)



			EB		NB NE		NW		SB	SE		SW
Intersection	Time Period	L	R	LT	LR	LTR	LT	TR	TR	LT	TR	LTR
10th Street & Main Street (SH-75)	Existing (2016) Plus Project			71	81				3			
5th Street & Main Street (SH-75)	Existing (2016) Plus Project					137	205	122		136	130	121
Main Street (SH-75) & Project Access	Existing (2016) Plus Project	41	50	56					8			

Time Period: p.m. Peak Hour 95th Percentile Queue Length (feet)



		NB		NE	1	NW SB		SE		SW
Intersection	Time Period	LT	LR	LTR	LT	TR	TR	LT	TR	LTR
10th Street & Main Street (SH-75)	Future (2020) Background	80	77				2			
5th Street & Main Street (SH-75)	Future (2020) Background			136	210	123		139	135	135

Time Period: p.m. Peak Hour 95th Percentile Queue Length (feet)



		EB		NB		NE		NW		SB	SE		SW
Intersection	Time Period	L	R	L	LT	LR	LTR	LT	TR	TR	LT	TR	LTR
10th Street & Main Street (SH-75)	Future (2020) Plus Project			48		80				12			
5th Street & Main Street (SH-75)	Future (2020) Plus Project						127	208	104		136	143	124
Main Street (SH-75) & Project Access	Future (2020) Plus Project	46	54		44					6			

Time Period: p.m. Peak Hour 95th Percentile Queue Length (feet)



			B9			NE	NW		SB	SE		SW
Intersection	Time Period	Т	(blan k)	LT	LR	LTR	LT	TR	TR	LT	TR	LTR
10th Street & Main Street (SH-75)	Future (2026) Background	10	5	104	111				5			
5th Street & Main Street (SH-75)	Future (2026) Background	3				141	370	331		164	174	136

Time Period: p.m. Peak Hour 95th Percentile Queue Length (feet)



			B9		EB		NB		NE	1	1W	SB	•	SE	SW
Intersection Time Period		Т	(blan k)	L	R	L	LT	LR	LTR	LT	TR	TR	LT	TR	LTR
10th Street & Main Street (SH-75) Future (2026) Plus P	roject					72		248				28			-
5th Street & Main Street (SH-75) Future (2026) Plus P	roject	18							143	446	399		196	199	148
Main Street (SH-75) & Project Acce Future (2026) Plus P	roject	4	4	46	51		56					16			

SimTraffic Queueing Report Project: ID Ketchum - Bracken Station TIS Time Period: p.m. Peak Hour 95th Percentile Queue Length (feet)



		В9		EB		NB		NE		W	SB	:	SE	SW
Intersection	Time Period	Т	L	R	L	LT	LR	LTR	LT	TR	TR	LT	TR	LTR
10th Street & Main Street (SH-75)	Hypothetical Future (2026) Plus Project				71		346				31			
5th Street & Main Street (SH-75)	Hypothetical Future (2026) Plus Project	57						138	611	574		208	214	131
Main Street (SH-75) & Project Access	Hypothetical Future (2026) Plus Project		61	64		78					31			



APPENDIX E

Surrogate Lane Use Data Summary

Hailey Chevron

9/1/2016

	AM East		East	AM	West	ΑN	l Combi	ned
		In	Out	In	Out	In	Out	Total
7:00	7:15	4	3	1	1	5	4	9
7:15	7:30	10	8	7	5	17	13	30
7:30	7:45	5	7	9	7	14	14	28
7:45	8:00	10	9	9	13	19	22	41
8:00	8:15	7	7	10	7	17	14	31
8:15	8:30	10	9	3	5	13	14	27
8:30	8:45	4	7	6	5	10	12	22
8:45	9:00	4	2	5	5	9	7	16

	Hourly Summary									
In Out Total										
	8:00	55	53	108						
7:15	8:15	67	63	130						
7:30	8:30	63	64	127						
7:45	8:45	59	62	121						
8:00	9:00	49	47	96						

Vehicle Composition										
Psngr RV Cmrcl Moto Ped										
AM East	47	0	5	0	2					
AM West	23	0	23	0	4					
AM Total	70	0	28	0	6					
	67.3%	67.3% 0.0% 26.9% 0.0% 5.8%								

		PM	East	PM West		PM Combined		
		In	Out	In	Out	In	Out	Total
3:00	3:15	10	6	3	2	13	8	21
3:15	3:30	7	9	5	4	12	13	25
3:30	3:45	15	12	7	6	22	18	40
3:45	4:00	13	14	4	7	17	21	38
4:00	4:15	11	11	11	6	22	17	39
4:15	4:30	8	9	10	13	18	22	40
4:30	4:45	9	10	12	9	21	19	40
4:45	5:00	8	10	6	11	14	21	35
5:00	5:15	14	12	8	6	22	18	40
5:15	5:30	7	8	7	5	14	13	27
5:30	5:45	9	8	5	8	14	16	30
5:45	6:00	9	9	8	6	17	15	32
6:00	6:15	8	8	6	7	14	15	29
6:15	6:30	11	11	3	2	14	13	27
6:30	6:45	8	7	3	5	11	12	23
6:45	7:00	8	10	4	5	12	15	27

	Hour	ly Su	mmar	У
		In	Out	Total
3:00	4:00	64	60	124
3:15	4:15	73	69	142
3:30	4:30	79	78	157
3:45	4:45	78	79	157
4:00	5:00	75	79	154
4:15	5:15	75	80	155
4:30	5:30	71	71	142
4:45	5:45	64	68	132
5:00	6:00	67	62	129
5:15	6:15	59	59	118
5:30	6:30	59	59	118
5:45	6:45	56	55	111
6:00	7:00	51	55	106

Vehicle Composition								
Psngr RV Cmrcl Moto Ped								
PM East	138	0	9	1	7			
PM West	76	0	18	0	9			
PM Total	214	0	27	1	16			
82.9% 0.0% 10.5% 0.4% 6.2%								



APPENDIX F

Motor Fueling Station Pedestrian Analysis



MFMORANDUM

125 West Main Street Bozeman, MT 59715 (406) 624-6117 www.altaplanning.com

To: Roy Bracken

North Town Partners Lot 5A Ketchum Idaho

From: Joe Gilpin, Principal

Date: June 29, 2016

Re: Motor Fueling Station Pedestrian Analysis

Introduction

This preliminary analysis of pedestrian access at the proposed Motor Fueling Station summarizes the site, pedestrian issues and design recommendations for the site as well as an approximately 3-block area study area.

To the Station Context and Recommendations

Located at the intersection of 10th Street and North Main Street, there are three major pedestrian catchment areas associated with the motor fueling station (illustrated in Figure 1). Pedestrians from these catchment areas will primarily access the site via North Main Street and 10th Street. Major pedestrian crossing points will include the intersections of:

- North Main Street and 9th Street
- North Main Street and 10th Street

Figure 1 illustrates catchment areas and major pedestrian access routes to the motor fueling station. The catchment areas and specific pedestrian issues and design recommendations areas are described below.

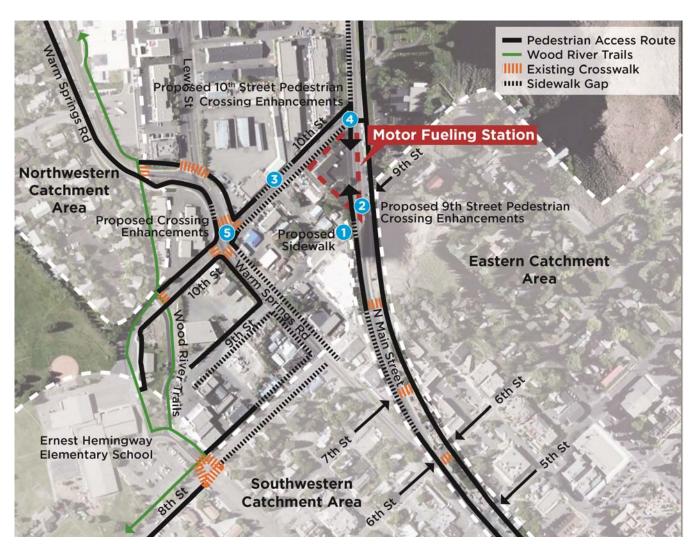


Figure 1: Pedestrian Catchment Areas and Circulation

Eastern Catchment Area Context and Recommendations

The eastern catchment area is comprised of a residential area and commercial district along North Main Street (State Highway 75). Pedestrians are likely to travel to the site along the eastern side of North Main Street and cross to the site at 9th Street. The sidewalk along the eastern side of North Main Street provides a connection from perpendicular streets to the site, with less g aps and driveway crossing than the western sidewalk. To address the existing gap in pedestrian facilities, a 5' concrete sidewalk (1) is proposed to connect pedestrians from Shum's Frenchman Place Condo to the motor fueling station.

A rectangular rapid flashing beacon (2), crosswalk and dedicated pedestrian ramps are proposed at the 9th Street crossing. The rectangular rapid flashing beacon (RRFB) would establish a high-visibility strobe-like warning to drivers when pedestrians are using the crosswalk, increasing motorist yielding compliance and pedestrian safety.

Southwestern Catchment Area Context and Recommendations

The southwestern catchment area is comprised of a residential area, commercial district along North Main Street, and the Ernest Hemingway Elementary School. Pedestrians are likely to travel to the motor fueling station along the western side of North Main Street or 10th Street. Driveways and parking along the length of 10th Street create large gaps in pedestrian facilities on both the north and south side of 10th Street. While the potential for pedestrian and vehicle conflicts are high along both sides of 10th, the north side is more desirable for pedestrian travel as only one large gap in sidewalk exists. There is no existing sidewalk on the south side of 10th, additionally long banks of front-in perpendicular parking exist on both sides of the street. This is the least compatible parking type with pedestrians as the driver does not have any view of street conditions behind before backing up.

Options for clearly defining a pedestrian zone through this gap (3) are recommended. Converting the pull-in parking to angle parking bays would create space to establish a sidewalk between the business front and parking. If existing parking through this area prohibits a dedicated sidewalk facilities signage, changes in pavement material or color could help to define and increase visibility of pedestrian through this area.

Pedestrian crosswalks are recommended at the intersection of North Main Street and 10th Street (4) and Warm Springs Road and 10th Street (5). A RRFB should also be considered to increase pedestrian safety.

Northwestern Catchment Area Context and Recommendations

The northwestern catchment area is comprised of a residential area connected to the southwestern catchment area and motor fueling station via the Wood River Trail and existing sidewalks. Traveling along the trail or sidewalks, pedestrians are likely to travel to the motor fueling station along 10th Street.

Sidewalk and crossing improvement enhancements reflect recommendations along 10th Street outlined for the Southwestern Catchment Area.

Major Pedestrian Access Routes

Pull-in parking exists along many of the major pedestrian access routes and creates gaps in connectivity. While establishing continuous pedestrian facilities along these routes is outside of the scope of the Motor Fueling Station project, future initiatives should engage property and business owners to discuss converting pull-in spaces to angled parking bays. This would create space for the establishment of clear pedestrian zones between the angled parking and front of business, enhancing building fronts and connections to the surrounding area.

Another strategy for establishing continuous pedestrian facilities could include narrowing travel lanes and/or replacing pull-in parking with parallel parking. This would also allow for the establishment buffer area between the sidewalk and travel lanes, enhancing pedestrian comfort. The buffer area could be landscaped and act as snow storage in the winter. This strategy would result in significant loss of parking.

Motor Fueling Station Issues and Recommendations

Proposed plans (figure 2) for the Motor Fueling Station include pedestrian connections to and through the site. Existing proposals illustrate crosswalks across 10th Street and North Main Street, as described in previous catchment area recommendations. Proposed improvements also include ADA ramps at crosswalk sites and a sidewalk along North Main Street. A pedestrian crossing (1) should be considered south of the site in a location that it can be straight and moved away from the lane taper. A second pedestrian crossing should be considered in the illustrated location (2) unless moving to the north where the roadway is narrower could align with Knob Hill Inn Access. The northern crossing location would also require a pedestrian landing/sidewalk area.

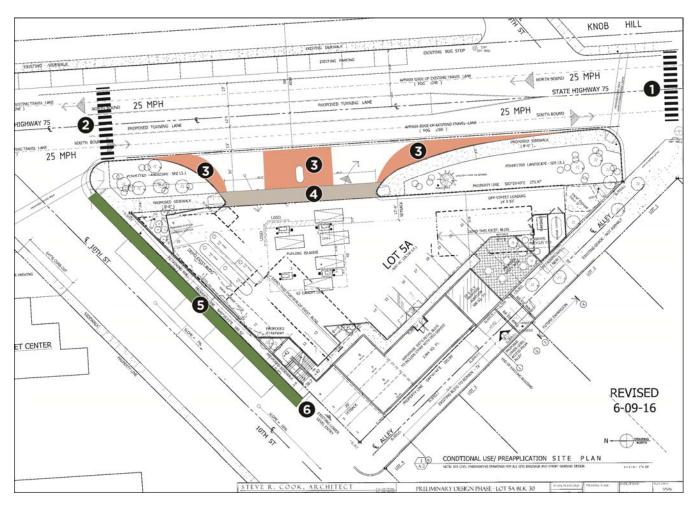


Figure 2: Proposed Site Plan

Pedestrian access to the site could be further enhanced by more clearly defining the pedestrian zone across the vehicle entrance through changes in the hardscape. One strategy is to better define the path for the most common vehicle to access the gas station (the passenger vehicle), while still allowing for the larger fueling trucks and other users to negotiate the entrance. The pictures below (figure 3) illustrate how the visibility of a pedestrian zone is enhanced through the use of colored/stamped pavement. Similar to the treatment below, the combination of rolled curbs and colored/stamped pavement (3) would maintain the wide turning radii required for large vehicles to access the site while lessening the gap in a dedicated pedestrian zone. Colored pedestrian areas (4) would also provide heightened awareness of walkers through primary vehicle access areas.



Figure 3: Stamped/colored pavement with rolled curb

Reducing the eastbound travel lane to 12' would allow for the addition of a 5' landscape area (5). The landscape area would serve as a year-round buffer between pedestrian and vehicle travel and in the winter serve as snow storage. West of this area (6), engineering solutions should be explored to continue the sidewalk beyond the retaining wall.



APPENDIX G

Background Land Use Trip Generation



Page 1 of 1

MEMORANDUM

Subject: Ketchum – Bracken Station TIS, Appendix G

UT16-851

This memorandum discusses the trip generation characteristics of the Ketchum Community School, the Warm Springs Ranch Resort, and the Stock Lumberyard development.

Ketchum City Staff requested that traffic from these three projects be included in projected 2020 and 2026 traffic volumes that were used for the background conditions analyses.

Ketchum Community School

Trip generation characteristics for the community school were developed based on information provided by the school for a pedestrian and bicycle study completed in January of 2016. Based on the unique characteristics of the school (when compared to more traditional schools), it was determined that trip generation during the p.m. peak hour of the Bracken Station study would be minimal. Trip generation and assignment for the Community School are shown in Figure G-1.

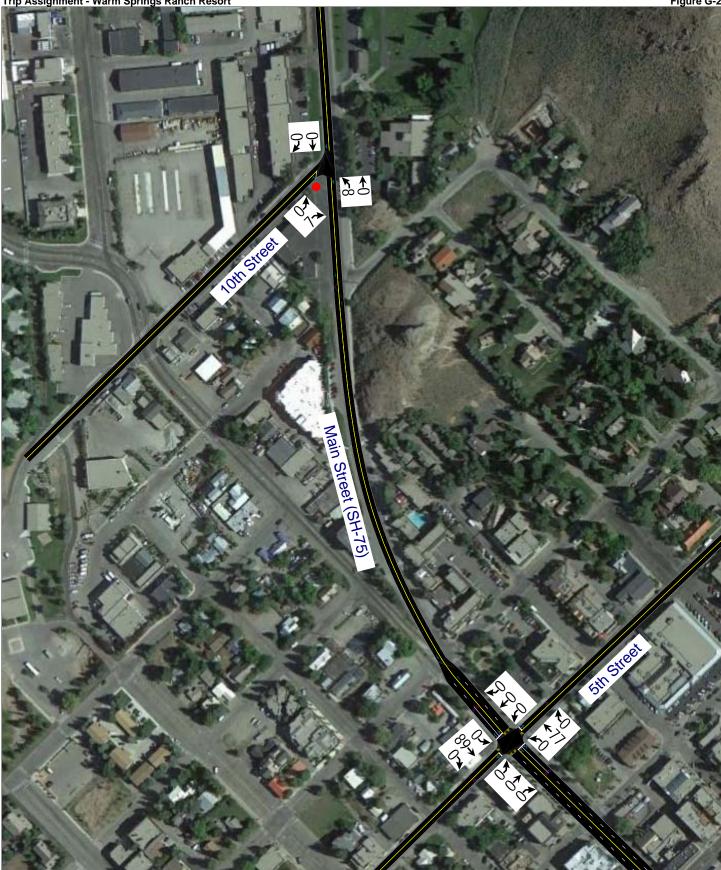
Warm Springs Ranch Resort

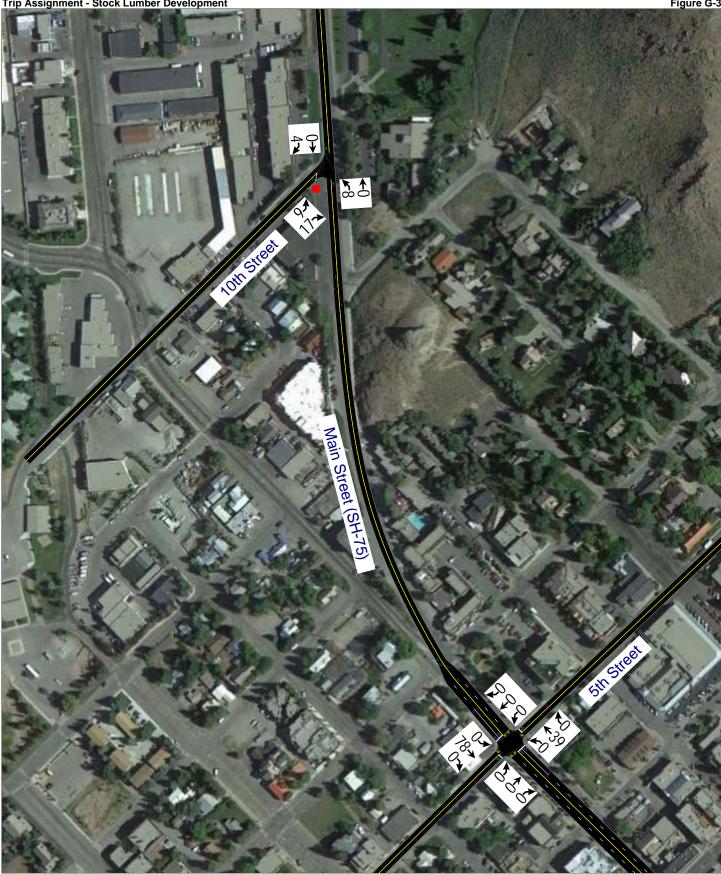
Trip generation for the Warm Springs Ranch Resort was taken from a traffic impact study (TIS) completed for the project in January of 2012. Trip generation and assignment for the Warm Springs Ranch Resort are shown in Figure G-2.

Stock Lumberyard Development

Plans for the redevelopment of the Stock Lumber yard in Ketchum were not readily available. Trip generation for the project were estimated using information from an article published in the Idaho Mountain Express on September 14, 2016. Trip generation and assignment for the Stock Lumberyard Development are shown in Figure G-3.







Appendix B

"Ketchum Gas Station Traffic Impact Study Updated"

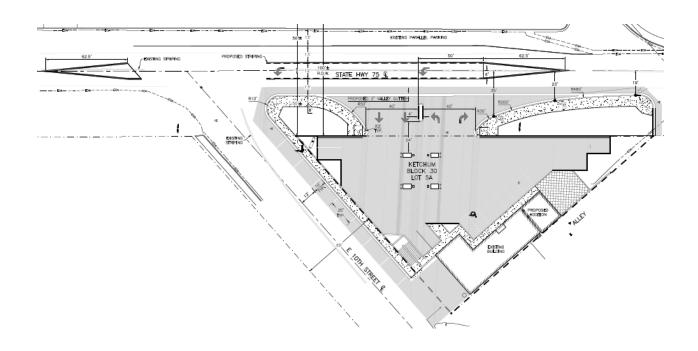
by Hales Engineering

dated May 2016



Ketchum Gas Station

Traffic Impact Study UPDATED



Ketchum, Idaho

May 2016

UT16-851



EXECUTIVE SUMMARY

This study addresses the traffic impacts associated with the proposed gas station in Ketchum, Idaho. The proposed gas station will be located on the southwest corner of the Main Street (SH-75) / 10th Street intersection.

Included within the analyses for this study are the traffic operations and recommended mitigation measures for existing conditions and plus project conditions (conditions after development of the proposed project) at key intersections and roadways in the vicinity of the site. Future 2020 conditions are also analyzed.

TRAFFIC ANALYSIS

The following is an outline of the traffic analysis performed by Hales Engineering for the traffic conditions of this project.

Existing (2016) Background Conditions Analysis

Hales Engineering used previous data for weekday morning (7:00 to 9:00 a.m.) and afternoon (4:00 to 6:00 p.m.) peak period traffic counts at the following intersections:

Main Street (SH-75) / 10th Street

These counts were performed for a previous project on Wednesday, February 13, 2008. Data from an automatic traffic recorder (ATR 68) was used to determine an annual growth rate of 1.1% and a seasonal adjustment of 30% for this segment of SH-75. Using these adjustments, peak period traffic volumes were calculated for the study intersection. The a.m. peak hour was determined to be between the hours of 8:00 and 9:00 a.m., and the p.m. peak hour was determined to be between the hours of 4:15 and 5:15 p.m. Detailed count data are included in Appendix A. The traffic volumes at this intersection was approximately 15% higher during the p.m. peak hour than during the a.m. peak hour. Therefore, the p.m. peak hour was chosen for detailed analysis as this represents the worst-case scenario.

As shown in Table ES-1, the Main Street (SH-75) / 10th Street intersection is currently operating at LOS A during the p.m. peak hour. The 95th percentile queues on the north- and eastbound approaches to the 10th Street / Main Street (SH-75) intersection was observed extend for approximately 80 feet. No other significant queuing was observed.

Project Conditions Analysis

The proposed land use for the development has been identified as follows:

Gasoline/Service Station with Convenience Market 8 Vehicle Fueling Positions

i



Trip generation for the development was calculated using trip generation rates published in the Institute of Transportation Engineers (ITE) *Trip Generation (9th Edition, 2012)*. Trip generation for the proposed project is as follows:

Weekday Daily Trips: 1,304
a.m. Peak Hour Trips: 82
p.m. Peak Hour Trips: 110

Existing (2016) Plus Project Conditions Analysis

As shown in Table ES-1, all study intersections are anticipated to operate at acceptable levels of service during the p.m. peak hour. During the p.m. peak hour, the 95th percentile queue length on the on the eastbound approach to the Main Street (SH-75) / 10th Street intersection is anticipated to extend for approximately 80 feet with project traffic added. Some queuing on northbound Main Street (SH-75) is also anticipated, which is likely attributed to left-turning vehicles blocking through traffic at the Main Street (SH-75) / 10th Street intersection as well as at the project access.

Future (2020) Background Conditions Analysis

As shown in Tables ES-1, the Main Street (SH-75) / 10th Street intersection is anticipated to operate at LOS C during the p.m. peak hour with future (2020) background traffic conditions. The 95th percentile queues on the north- and eastbound approaches to the Main Street (SH-75) / 10th Street intersection are anticipated to extend for approximately 110 feet. No other significant queuing is anticipated.

Future (2020) Plus Project Conditions Analysis

As shown in Tables ES-1, the Main Street (SH-75) / 10th Street intersection is anticipated to operate at LOS C with project traffic added, while the proposed access is anticipated to operate at LOS A during the p.m. peak hour. During the p.m. peak hour, the 95th percentile queue length on the northbound approach to the Main Street (SH-75) / 10th Street intersection is anticipated to extend for approximately 50 feet. All other queuing is anticipated to be nominal.



TABLE ES-1 P.M. Peak Hour ID Ketchum Gas Station TIS								
Intersection	Projected 2016 Background	Projected 2016 Plus Project	Future 2020 Background	Future 2020 Plus Project				
Description	LOS (Sec/Veh ¹)	LOS (Sec/Veh ¹)	LOS (Sec/Veh ¹)	LOS (Sec/Veh ¹)				
Main Street (ID-75) / 10th Street	A (9.7) / EB	B (10.9) / EB	C (15.9) / EB	C (17.8) / EB				
Main Street (ID-75) / Access 1	-	A (6.5) / EB	-	A (9.2) / EB				

^{1.} Intersection LOS and delay (seconds/vehicle) values represent the overall intersection average for signalized and all-way stop controlled intersections and the worst approach for all other unsignalized intersections.

Source: Hales Engineering, May 2016

RECOMMENDATIONS

The following mitigation measures are recommended:

Existing (2016) Background Conditions Analysis

No mitigation measures are recommended.

Existing (2016) Plus Project Conditions Analysis

It is recommend that a two-way left-turn lane be constructed from a location north of 10th Street to a location south of the project. No other mitigation measures are recommended.

Future (2020) Background Conditions Analysis

No additional mitigation measures are recommended.

Future (2020) Plus Project Conditions Analysis

No additional mitigation measures are recommended.

^{2.} This is a project intersection and is only analyzed in the plus project scenarios.



SUMMARY OF KEY FINDINGS/RECOMMENDATIONS

The following is a summary of key findings and recommendations:

- The Main Street (SH-75) / 10th Street intersection is currently operating at LOS A during the p.m. peak hour.
- With project traffic added, the Main Street (SH-75) / 10th Street intersection is anticipated to operate at LOS B, and the proposed project access is anticipated to operate at LOS A.
- It is recommended that a two-way left-turn lane be constructed on Main Street (SH-75) from a location north of 10th Street to a location south of the project.
- With future (2020) traffic conditions, the Main Street (SH-75) / 10th Street intersection is anticipated to operate at LOS C during the p.m. peak hour.
- With project traffic added, the Main Street (SH-75) / 10th Street intersection is anticipated to operate at an acceptable level of service, as well as the project access.



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I. INTRODUCTION

A. Purpose

This study addresses the traffic impacts associated with the proposed gas station in Ketchum, Idaho. The proposed gas station will be located on the southwest corner of the Main Street (SH-75) / 10th Street intersection. Figure 1 shows a vicinity map of the proposed development.

Included within the analyses for this study are the traffic operations and recommended mitigation measures for existing conditions and plus project conditions (conditions after development of the proposed project) at key intersections and roadways in the vicinity of the site. Future 2020 conditions are also analyzed.



Figure 1 Vicinity map showing the project location in Ketchum, Idaho



B. Scope

The study area was defined based on conversations with the development team, following general guidelines for traffic impact studies. This study was scoped to evaluate the traffic operational performance impacts of the project on the following intersection:

• Main Street (SH-75) / 10th Street

C. Analysis Methodology

Level of service (LOS) is a term that describes the operating performance of an intersection or roadway. LOS is measured quantitatively and reported on a scale from A to F, with A representing the best performance and F the worst. Table 1 provides a brief description of each LOS letter designation and an accompanying average delay per vehicle for both signalized and unsignalized intersections.

The Highway Capacity Manual 2010 (HCM 2010) methodology was used in this study to remain consistent with "state-of-the-practice" professional standards. This methodology has different quantitative evaluations for signalized and unsignalized intersections. For signalized and all-way stop intersections, the LOS is provided for the overall intersection (weighted average of all approach delays). For all other unsignalized intersections LOS is reported based on the worst approach.

D. Level of Service Standards

For the purposes of this study, a minimum overall intersection performance for each of the study intersections was set at LOS D. However, if LOS E or F conditions exist, an explanation and/or mitigation measures will be presented. An LOS D threshold is consistent with "state-of-the-practice" traffic engineering principles for urbanized areas.



Table 1 Level of Service Descriptions

Level of Service	Description of Traffic Conditions	Average Delay (seconds/vehicle)
	Signalized Intersections	Overall Intersection
А	Extremely favorable progression and a very low level of control delay. Individual users are virtually unaffected by others in the traffic stream.	0 ≤ 10.0
В	Good progression and a low level of control delay. The presence of other users in the traffic stream becomes noticeable.	> 10.0 and ≤ 20.0
С	Fair progression and a moderate level of control delay. The operation of individual users becomes somewhat affected by interactions with others in the traffic stream.	>20.0 and ≤ 35.0
D	Marginal progression with relatively high levels of control delay. Operating conditions are noticeably more constrained.	> 35.0 and ≤ 55.0
E	Poor progression with unacceptably high levels of control delay. Operating conditions are at or near capacity.	> 55.0 and ≤ 80.0
F	Unacceptable progression with forced or breakdown operating conditions.	> 80.0
	Unsignalized Intersections	Worst Approach
А	Free Flow / Insignificant Delay	0 ≤ 10.0
В	Stable Operations / Minimum Delays	>10.0 and ≤ 15.0
С	Stable Operations / Acceptable Delays	>15.0 and ≤ 25.0
D	Approaching Unstable Flows / Tolerable Delays	>25.0 and ≤ 35.0
Е	Unstable Operations / Significant Delays	>35.0 and ≤ 50.0
F	Forced Flows / Unpredictable Flows / Excessive Delays	> 50.0

Source: Hales Engineering Descriptions, based on Highway Capacity Manual, 2010 Methodology (Transportation Research Board, 2010)



II. EXISTING (2016) BACKGROUND CONDITIONS

A. Purpose

The purpose of the existing (2016) background analysis is to study the intersections and roadways during the peak travel periods of the day with background traffic and geometric conditions. Through this analysis, background traffic operational deficiencies can be identified and potential mitigation measures recommended. This analysis will provide a baseline condition that may be compared to the build conditions to identify the impacts of the development.

B. Roadway System

The primary roadways that will provide access to the project site are described below:

Main Street (SH-75) – is a state-maintained roadway that is classified by ITD as a "regional" route in the vicinity of the project. SH-75 is a north/south route connecting Ketchum, as well as other communities such as Sun Valley and Hailey, to US-20 to the south. As a regional route in an urban area with a speed limit less than 35 mph, this roadway has minimum signal spacing of 2,640 feet, and a minimum street spacing of 660 feet. The minimum driveway distance from an upstream intersection is 250 feet, the minimum distance from a downstream intersection is 660 feet, and the minimum distance between accesses is 250 feet. Main Street (SH-75) has one travel lane in each direction and the posted speed limit in the vicinity of the proposed project is 25 mph.

C. Traffic Volumes

Hales Engineering performed weekday morning (7:00 to 9:00 a.m.) and afternoon (4:00 to 6:00 p.m.) peak period traffic counts at the following intersections:

Main Street (SH-75) / 10th Street

These counts were performed for a previous project on Wednesday, February 13, 2008. Data from a nearby automatic traffic recorder (ATR 68) was used to determine an annual growth rate of 1.1% and a seasonal adjustment of 30% for this segment of SH-75. Using these adjustments, peak period traffic volumes were calculated for the study intersection. The a.m. peak hour was determined to be between the hours of 8:00 and 9:00 a.m., and the p.m. peak hour was determined to be between the hours of 4:15 and 5:15 p.m. Detailed count data are included in Appendix A. The traffic volumes at this intersection were approximately 15% higher during the p.m. peak hour than during the a.m. peak hour. Therefore, the p.m. peak hour was chosen for detailed analysis as this represents the worst-case scenario.



Figure 2 shows the existing p.m. peak hour volume as well as intersection geometry at the study intersection.

D. Level of Service Analysis

Using Synchro/SimTraffic, which follow the Highway Capacity Manual (HCM) 2010 methodology introduced in Chapter I, the p.m. peak hour LOS was computed for the study intersection. The results of this analysis are reported in Table 2 (see Appendix B for the detailed LOS reports). Multiple runs of SimTraffic were used to provide a statistical evaluation of the intersection. These results serve as a baseline condition for the impact analysis of the proposed development during existing (2016) conditions. As shown in Table 2, the Main Street (SH-75) / 10th Street intersection is currently operating at LOS A during the p.m. peak hour.

E. Queuing Analysis

Hales Engineering calculated the 95th percentile queue lengths for each of the study intersections. The queue reports can be found in Appendix D. The 95th percentile queues on the north- and eastbound approaches to the 10th Street / Main Street (SH-75) intersection was observed extend for approximately 80 feet. No other significant queuing was observed.

F. Mitigation Measures

No mitigation measures are recommended.

Table 2 Existing (2016) Background p.m. Peak Hour Level of Service

Intersection		Wor	st Approach		Overall Inters	ection
Description	Control	Approach ^{1,3}	Aver. Delay (Sec/Veh) ¹	LOS1	Aver. Delay (Sec/Veh) ²	LOS ²
Main Street (SH-75) / 10 th Street	EB Stop	EB	9.7	Α	-	-
This represents the worst approach This represents the overall intersecti Southbound = Southbound approach	on LOS and delay					ctions.

Source: Hales Engineering, May 2016

ID Ketchum Gas Station TIS Existing (2016) Background p.m. Peak Hour Figure 2 Join Street Main Street (ID-75)



III. PROJECT CONDITIONS

A. Purpose

The project conditions analysis explains the type and intensity of development. This provides the basis for trip generation, distribution, and assignment of project trips to the surrounding study intersections defined in the Introduction.

B. Project Description

This study addresses the traffic impacts associated with the gas station in Ketchum, Idaho. The proposed gas station will be located on the southwest corner of the Main Street (SH-75) / 10th Street intersection. A site plan for the proposed development can be found in Appendix C.

The proposed land use for the development has been identified as follows:

Gasoline/Service Station with Convenience Market 8 Vehicle Fueling Positions

C. Trip Generation

Trip generation for the development was calculated using trip generation rates published in the Institute of Transportation Engineers (ITE) *Trip Generation (9th Edition, 2012)*. Trip Generation for the proposed project is included in Table 3.

D. Trip Distribution and Assignment

Project traffic is assigned to the roadway network based on the type of trip and the proximity of project access points to major streets, high population densities, and regional trip attractions. Existing travel patterns observed during data collection also provide helpful guidance to establishing these distribution percentages, especially in close proximity to the site. The resulting distribution of projected generated trips is as follows:

To/From Project:

- 15% North
- 85% South

These trip distribution assumptions and the prevailing movements at each intersection were used to assign the evening peak hour generated traffic at the study intersections to create trip assignment for the proposed development. Trip assignment for the development is shown in Figure 3.



		ID Ketchum Gas St Trip Generat						
Veekday Daily Land Use ¹	Number of Units	Unit Type	Trip Generation	% Entering	% Exiting	Trips Entering	Trips Exiting	Total Daily Trips
Gasoline/Service Station with Convenience Market Project Total Daily Trips	8	Vehicle Fueling Positions	1,304	50%	50%	652 652	652 652	1,304 1,304
A.M. Peak Hour Land Use ¹	Number of Units	Unit Type	Trip Generation	% Entering	% Exiting	Trips Entering	Trips Exiting	Total a.m. Trips
Gasoline/Service Station with Convenience Market Project Total a.m. Peak Hour Trips	8	Vehicle Fueling Positions	82	50%	50%	41 41	41 41	82 82
P.M. Peak Hour Land Use ¹	Number of Units	Unit Type	Trip Generation	% Entering	% Exiting	Trips Entering	Trips Exiting	Total p.m. Trips
Gasoline/Service Station with Convenience Market Project Total p.m. Peak Hour Trips	8	Vehicle Fueling Positions	110	50%	50%	55 55	55 55	110 110

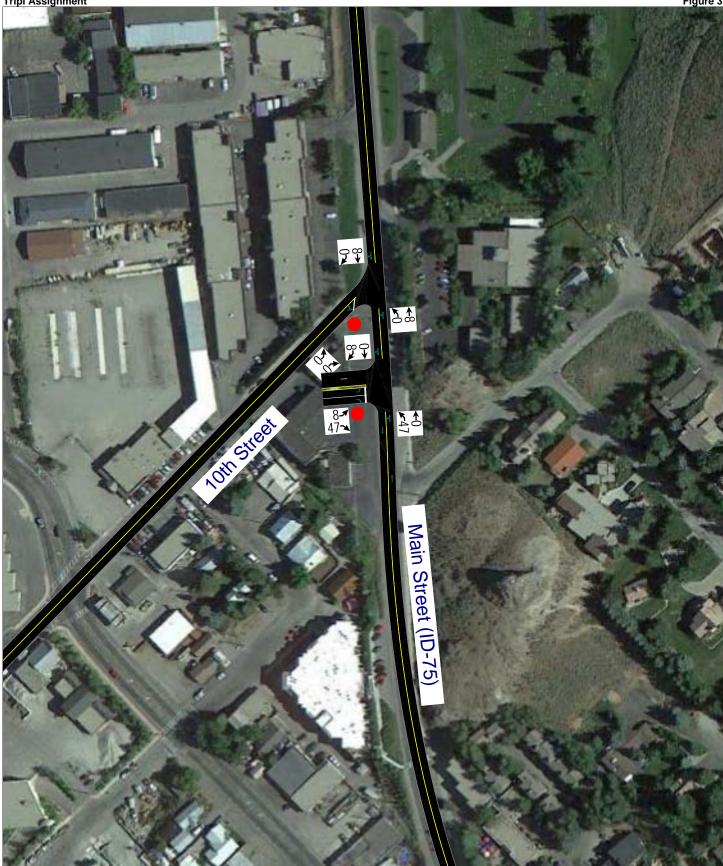
E. Access

The proposed access for the site will be gained at the following locations (see also site plan in Appendix C):

Main Street (SH-75):

One full-movement "boulevard approach" accesses is proposed on Main Street (SH-75), one approximately 60 feet south of 10th Street. A "boulevard approach" consists of two forty foot wide openings in the curb separated by a small island. One opening is for ingress movements, and the other for egress movements.

ID Ketchum Gas Station TIS
Tripi Assignment
p.m. Peak Hour
Figure 3



Hales Engineering 1220 North 500 West, Ste. 202 Lehi UT 84043 801.766.4343 5/3/2016



IV. EXISTING (2016) PLUS PROJECT CONDITIONS

A. Purpose

This section of the report examines the traffic impacts of the proposed project at each of the study intersections. The net trips generated by the proposed development were combined with the existing background traffic volumes to create the existing plus project conditions. This scenario provides valuable insight into the potential impacts of the proposed project on background traffic conditions.

B. Traffic Volumes

Project trips were assigned to the study intersections based on the trip distribution percentages discussed in Chapter III and permitted intersection turning movements. The existing (2016) plus project p.m. peak hour volumes were generated for the study intersections and are shown in Figure 4.

C. Level of Service Analysis

Using Synchro/SimTraffic, which follow the Highway Capacity Manual (HCM) 2010 methodology introduced in Chapter I, the p.m. peak hour LOS was computed for each study intersection. The results of this analysis are reported in Table 4 (see Appendix B for the detailed LOS reports). Multiple runs of SimTraffic were used to provide a statistical evaluation of the interaction between the intersections. As shown in Table 4, all study intersections are anticipated to operate at acceptable levels of service during the p.m. peak hour.

D. Queuing Analysis

Hales Engineering calculated the 95th percentile queue lengths for each of the study intersections. The queue reports can be found in Appendix D. During the p.m. peak hour, the 95th percentile queue length on the on the eastbound approach to the Main Street (SH-75) / 10th Street intersection is anticipated to extend for approximately 80 feet with project traffic added. Some queuing on northbound Main Street (SH-75) is also anticipated, which is likely attributed to left-turning vehicles blocking through traffic at the Main Street (SH-75) / 10th Street intersection as well as at the project access.

E. Mitigation Measures

It is recommend that a two-way left-turn lane be constructed from a location north of 10th Street to a location south of the project. No other mitigation measures are recommended.



Table 4 Existing (2016) Plus Project p.m. Peak Hour Level of Service

Intersection		Wor	st Approach		Overall Inters	ection
Description	Control	Approach ^{1,3}	Aver. Delay (Sec/Veh) ¹	LOS ¹	Aver. Delay (Sec/Veh) ²	LOS ²
Main Street (SH-75) / 10 th Street	EB Stop	EB	10.9	В	-	-
Main Street (SH-75) / Access 1	EB Stop	EB	6.5	Α	-	-

^{1.} This represents the worst approach LOS and delay (seconds / vehicle) and is only reported for non-all-way stop unsignalized intersections.

Source: Hales Engineering, May 2016

^{2.} This represents the overall intersection LOS and delay (seconds / vehicle) and is reported for all-way stop, roundabout, and signalized intersections.

^{3.} Southbound = Southbound approach, etc.

ID Ketchum Gas Station TIS Existing (2016) Plus Project p.m. Peak Hour Figure 4 Join Street Main Street

Hales Engineering 1220 North 500 West, Ste. 202 Lehi UT 84043 801.766.4343 5/12/2016



V. FUTURE (2020) BACKGROUND CONDITIONS

A. Purpose

The purpose of the future (2020) background analysis is to study the intersections and roadways during the peak travel periods of the day for future background traffic and geometric conditions. Through this analysis, future background traffic operational deficiencies can be identified and potential mitigation measures recommended.

B. Roadway Network

Based on information received, no improvements are planned for any of the roadways or intersections within the study area before 2020.

C. Traffic Volumes

Hales Engineering used the calculated annual growth rate discussed in Chapter II to project future (2020) traffic volumes for the study intersection. Future 2020 p.m. peak hour turning movement volumes are shown in Figure 5.

D. Level of Service Analysis

Using Synchro/SimTraffic, which follow the Highway Capacity Manual (HCM) 2010 methodology introduced in Chapter I, the p.m. peak hour LOS was computed for each study intersection. The results of this analysis are reported in Table 5 (see Appendix B for the detailed LOS reports). Multiple runs of SimTraffic were used to provide a statistical evaluation of the interaction between the intersections. These results serve as a baseline condition for the impact analysis of the proposed development for future (2020) conditions. As shown in Table 5, the Main Street (SH-75) / 10th Street intersection is anticipated to operate at LOS C during the p.m. peak hour with future (2020) background traffic conditions.

E. Queuing Analysis

Hales Engineering calculated the 95th percentile queue lengths for each of the study intersections. The queue reports can be found in Appendix D. The 95th percentile queues on the north- and eastbound approaches to the Main Street (SH-75) / 10th Street intersection are anticipated to extend for approximately 110 feet. No other significant queuing is anticipated.

F. Mitigation Measures

No additional mitigation measures are recommended.



Table 5 Future (2020) Background p.m. Peak Hour Level of Service

Intersection		Wor	st Approach		Overall Inters	ection
Description	Control	Approach ^{1,3}	Aver. Delay (Sec/Veh) ¹	LOS¹	Aver. Delay (Sec/Veh) ²	LOS ²
Main Street (SH-75) / 10 th Street	EB Stop	EB	15.9	С	-	-

^{1.} This represents the worst approach LOS and delay (seconds / vehicle) and is only reported for non-all-way stop unsignalized intersections.

Source: Hales Engineering, May 2016

^{2.} This represents the overall intersection LOS and delay (seconds / vehicle) and is reported for all-way stop, roundabout, and signalized intersections

^{3.} Southbound = Southbound approach, etc.

ID Ketchum Gas Station TIS Future (2020) Background p.m. Peak Hour Figure 5 Join Street Main Street (ID-75)

Hales Engineering 1220 North 500 West, Ste. 202 Lehi UT 84043

801.766.4343 5/12/2016



VI. FUTURE (2020) PLUS PROJECT CONDITIONS

A. Purpose

The purpose of the future (2020) plus project analysis is to study the intersections and roadways during the peak travel periods of the day for future background traffic and geometric conditions plus the net trips generated by the proposed development. This scenario provides valuable insight into the potential impacts of the proposed project on future background traffic conditions.

B. Traffic Volumes

Trips were assigned to the study intersections based on the trip distribution percentages discussed in Chapter III and permitted intersection turning movements. It was also assumed that the previously recommended center TWLTL had been constructed along the project frontage.

The future (2020) plus project p.m. peak hour volumes were generated for the study intersections and are shown in Figure 6.

C. Level of Service Analysis

Using Synchro/SimTraffic, which follow the Highway Capacity Manual (HCM) 2010 methodology introduced in Chapter I, the p.m. peak hour LOS was computed for each study intersection. The results of this analysis are reported in Table 6 (see Appendix B for the detailed LOS reports). Multiple runs of SimTraffic were used to provide a statistical evaluation of the interaction between the intersections. As shown in Table 6, the Main Street (SH-75) / 10th Street intersection is anticipated to operate at LOS C with project traffic added, while the proposed access is anticipated to operate at LOS A during the p.m. peak hour.

D. Queuing Analysis

Hales Engineering calculated the 95th percentile queue lengths for each of the study intersections. The queue reports can be found in Appendix D. During the p.m. peak hour, the 95th percentile queue length on the northbound approach to the Main Street (SH-75) / 10th Street intersection is anticipated to extend for approximately 50 feet. All other queuing is anticipated to be nominal.

E. Mitigation Measures

No additional mitigation measures are recommended.



Table 6 Future (2020) Plus Project p.m. Peak Hour Level of Service

Intersection		Wor	st Approach		Overall Inters	ection
Description	Control	Approach ^{1,3}	Aver. Delay (Sec/Veh) ¹	LOS ¹	Aver. Delay (Sec/Veh) ²	LOS ²
Main Street (SH-75) / 10 th Street	EB Stop	EB	17.8	С	-	-
Main Street (SH-75) / Access 1	EB Stop	EB	9.2	Α	-	-

^{1.} This represents the worst approach LOS and delay (seconds / vehicle) and is only reported for non-all-way stop unsignalized intersections.

Source: Hales Engineering, May 2016

^{2.} This represents the overall intersection LOS and delay (seconds / vehicle) and is reported for all-way stop, roundabout, and signalized intersections.

^{3.} Southbound = Southbound approach, etc.

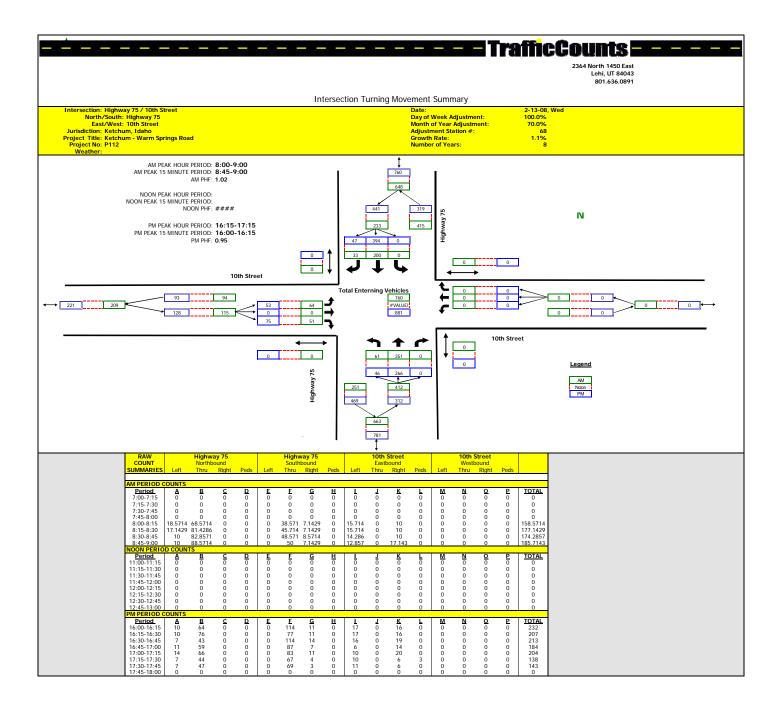
ID Ketchum Gas Station TIS Future (2020) Plus Project p.m. Peak Hour Figure 6 Main Street

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APPENDIX A

Turning Movement Counts





APPENDIX B

Level of Service Results



SimTraffic LOS Report

Project: Analysis Period: Time Period: **ID Ketchum Gas Station TIS**

Existing (2016) Background p.m. Peak Hour Project #: UT-16-851

10th Street & Main Street (ID-75) Unsignalized Intersection:

Type:

. , , ,	••	- ·				. , .
Approach	Movement	Demand	Volume	Served	Delay/Ve	eh (sec)
		Volume	Avg	%	Avg	LOS
	L	46	45	98	5.2	Α
NB	Т	266	263	99	1.0	Α
	Subtotal	312	308	99	1.6	Α
	Т	394	396	101	0.8	Α
SB	R	47	44	94	0.4	Α
	Subtotal	441	440	100	0.8	Α
	L	53	49	92	14.2	В
NE	R	75	76	101	6.8	Α
	Subtotal	128	125	98	9.7	Α
Total		880	873	99	2.4	Α

Intersection:

Type:

Approach	Movement	Demand	Volum	e Served	Delay/Ve	h (sec)
		Volume	Avg	%	Avg	LOS
Total		·				

3: 10th Street & Main Street (ID-75) Performance by movement Interval #1 4:15

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.3	0.3	0.2	0.1	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	5.7	1.0	0.8	0.5	12.0	6.0	2.2
Vehicles Entered	10	66	98	12	12	18	216
Vehicles Exited	10	66	97	12	12	19	216
Hourly Exit Rate	40	264	388	48	48	76	864
Input Volume	45	261	387	46	52	74	865
% of Volume	89	101	100	104	92	103	100

3: 10th Street & Main Street (ID-75) Performance by movement Interval #2 4:30

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.3	0.4	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	4.7	0.9	0.8	0.3	12.7	6.6	2.2
Vehicles Entered	11	66	96	11	13	20	217
Vehicles Exited	11	66	96	11	12	19	215
Hourly Exit Rate	44	264	384	44	48	76	860
Input Volume	45	261	387	46	52	74	865
% of Volume	98	101	99	96	92	103	99

3: 10th Street & Main Street (ID-75) Performance by movement Interval #3 4:45

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.2
Total Del/Veh (s)	5.8	1.4	0.9	0.5	18.7	7.8	2.9
Vehicles Entered	13	66	107	11	12	20	229
Vehicles Exited	13	66	108	12	13	20	232
Hourly Exit Rate	52	264	432	48	52	80	928
Input Volume	48	280	415	49	56	79	927
% of Volume	108	94	104	98	93	101	100

3: 10th Street & Main Street (ID-75) Performance by movement Interval #4 5:00

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.3	0.2	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	4.5	0.7	0.7	0.3	12.4	5.7	2.0
Vehicles Entered	11	65	96	10	13	18	213
Vehicles Exited	10	65	95	10	12	18	210
Hourly Exit Rate	40	260	380	40	48	72	840
Input Volume	45	261	387	46	52	74	865
% of Volume	89	100	98	87	92	97	97

3: 10th Street & Main Street (ID-75) Performance by movement Entire Run

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.3	0.2	0.2	0.2
Total Delay (hr)	0.1	0.1	0.1	0.0	0.2	0.1	0.6
Total Del/Veh (s)	5.2	1.0	0.8	0.4	14.2	6.8	2.4
Vehicles Entered	45	263	396	44	49	76	873
Vehicles Exited	45	263	396	44	49	76	873
Hourly Exit Rate	45	263	396	44	49	76	873
Input Volume	46	266	394	47	53	75	880
% of Volume	98	99	101	94	92	101	99

Total Network Performance By Interval

Interval Start	4:15	4:30	4:45	5:00	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.1
Denied Del/Veh (s)	0.3	0.3	0.3	0.3	0.3
Total Delay (hr)	0.2	0.2	0.3	0.2	0.9
Total Del/Veh (s)	3.4	3.4	4.3	3.3	3.8
Vehicles Entered	216	218	230	210	872
Vehicles Exited	216	217	231	209	872
Hourly Exit Rate	864	868	924	836	872
Input Volume	2497	2497	2676	2497	2542
% of Volume	35	35	35	33	34

Intersection: 3: 10th Street & Main Street (ID-75), Interval #1

Movement	NB	NE
Directions Served	LT	LR
Maximum Queue (ft)	82	73
Average Queue (ft)	27	40
95th Queue (ft)	80	72
Link Distance (ft)	274	1052
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 3: 10th Street & Main Street (ID-75), Interval #2

Movement	NB	NE
Directions Served	LT	LR
Maximum Queue (ft)	66	77
Average Queue (ft)	25	44
95th Queue (ft)	73	84
Link Distance (ft)	274	1052
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 3: 10th Street & Main Street (ID-75), Interval #3

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	85	2	88
Average Queue (ft)	35	0	46
95th Queue (ft)	93	5	91
Link Distance (ft)	274	610	1052
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

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Intersection: 3: 10th Street & Main Street (ID-75), Interval #4

Movement	NB	NE
Directions Served	LT	LR
Maximum Queue (ft)	64	67
Average Queue (ft)	24	39
95th Queue (ft)	66	70
Link Distance (ft)	274	1052
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 3: 10th Street & Main Street (ID-75), All Intervals

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	105	2	99
Average Queue (ft)	28	0	42
95th Queue (ft)	79	2	80
Link Distance (ft)	274	610	1052
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Network Summary

Network wide Queuing Penalty, Interval #1: 0
Network wide Queuing Penalty, Interval #2: 0
Network wide Queuing Penalty, Interval #3: 0
Network wide Queuing Penalty, Interval #4: 0
Network wide Queuing Penalty, All Intervals: 0



SimTraffic LOS Report

Project: **ID Ketchum Gas Station TIS**

Analysis Period: Time Period: Existing (2016) Plus Project p.m. Peak Hour

Project #: UT-16-851

10th Street & Main Street (ID-75) Unsignalized Intersection:

Type:

Approach	Movement	Demand	Volume	e Served	Delay/Ve	h (sec)
		Volume	Avg	%	Avg	LOS
	L	46	45	98	5.1	Α
NB	Т	274	271	99	1.0	Α
IND						
	Subtotal	320	316	99	1.6	Α
	Т	402	404	100	0.9	Α
SB	R	47	52	111	0.6	Α
						_
	Subtotal	449	456	102	0.9	Α
	L	53	52	98	15.2	С
NE	R	75	73	97	7.8	Α
	Subtotal	128	125	98	10.9	В
	Subtotal	120	125	30	10.9	
Total		897	897	100	2.5	Α

Main Street (ID-75) & Access 1 Unsignalized Intersection:

Type:

Approach	Movement	Demand	Volume	Served	Delay/Ve	h (sec)
		Volume	Avg	%	Avg	LOS
	L	47	44	94	3.5	Α
NB	Т	312	309	99	0.7	Α
	Subtotal	359	353	98	1.0	Α
	Т	469	470	100	0.4	Α
SB	R	8	8	100	0.2	Α
	Subtotal	477	478	100	0.4	Α
	L	8	7	88	11.8	В
EB	R	47	50	107	5.8	Α
	Subtotal	55	57	104	6.5	Α
Total		891	888	100	1.1	Α

1: 10th Street & Main Street (ID-75) Performance by movement Interval #1 4:15

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.2
Total Del/Veh (s)	4.8	0.9	8.0	0.5	16.1	7.5	2.4
Vehicles Entered	10	69	101	13	12	18	223
Vehicles Exited	10	70	100	13	13	18	224
Hourly Exit Rate	40	280	400	52	52	72	896
Input Volume	45	270	395	46	52	74	882
% of Volume	89	104	101	113	100	97	102

1: 10th Street & Main Street (ID-75) Performance by movement Interval #2 4:30

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.3	0.3	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	4.5	0.9	0.8	0.6	12.7	6.6	2.2
Vehicles Entered	12	64	96	13	12	17	214
Vehicles Exited	12	64	97	13	12	17	215
Hourly Exit Rate	48	256	388	52	48	68	860
Input Volume	45	270	395	46	52	74	882
% of Volume	107	95	98	113	92	92	98

1: 10th Street & Main Street (ID-75) Performance by movement Interval #3 4:45

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.1	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.2
Total Del/Veh (s)	6.4	1.2	1.0	0.5	14.5	8.3	2.8
Vehicles Entered	12	69	106	16	15	18	236
Vehicles Exited	12	69	104	16	14	18	233
Hourly Exit Rate	48	276	416	64	56	72	932
Input Volume	48	288	423	49	56	79	943
% of Volume	100	96	98	131	100	91	99

1: 10th Street & Main Street (ID-75) Performance by movement Interval #4 5:00

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.5	0.3	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.2
Total Del/Veh (s)	5.0	0.9	8.0	0.8	14.0	8.0	2.4
Vehicles Entered	10	68	101	11	12	20	222
Vehicles Exited	10	68	102	11	12	20	223
Hourly Exit Rate	40	272	408	44	48	80	892
Input Volume	45	270	395	46	52	74	882
% of Volume	89	101	103	96	92	108	101

1: 10th Street & Main Street (ID-75) Performance by movement Entire Run

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.2	0.2	0.2
Total Delay (hr)	0.1	0.1	0.1	0.0	0.2	0.2	0.6
Total Del/Veh (s)	5.1	1.0	0.9	0.6	15.2	7.8	2.5
Vehicles Entered	45	271	404	52	51	74	897
Vehicles Exited	45	271	404	52	52	73	897
Hourly Exit Rate	45	271	404	52	52	73	897
Input Volume	46	274	402	47	53	75	897
% of Volume	98	99	100	111	98	97	100

2: Main Street (ID-75) & Access 1 Performance by movement Interval #1 4:15

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All	
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0	
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	
Total Del/Veh (s)	8.7	6.3	3.4	0.7	0.4	0.3	1.0	
Vehicles Entered	2	11	11	78	118	1	221	
Vehicles Exited	2	11	11	78	117	1	220	
Hourly Exit Rate	8	44	44	312	468	4	880	
Input Volume	8	46	46	307	461	8	876	
% of Volume	100	96	96	102	102	50	100	

2: Main Street (ID-75) & Access 1 Performance by movement Interval #2 4:30

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	6.8	5.6	3.5	0.6	0.4	0.1	1.0
Vehicles Entered	2	13	12	75	113	2	217
Vehicles Exited	2	13	12	74	113	2	216
Hourly Exit Rate	8	52	48	296	452	8	864
Input Volume	8	46	46	307	461	8	876
% of Volume	100	113	104	96	98	100	99

2: Main Street (ID-75) & Access 1 Performance by movement Interval #3 4:45

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	14.6	5.9	3.7	0.9	0.5	0.2	1.2
Vehicles Entered	2	14	10	79	121	2	228
Vehicles Exited	2	14	10	80	120	2	228
Hourly Exit Rate	8	56	40	320	480	8	912
Input Volume	8	49	49	328	494	8	936
% of Volume	100	114	82	98	97	100	97

2: Main Street (ID-75) & Access 1 Performance by movement Interval #4 5:00

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	11.3	5.6	3.4	0.6	0.4	0.1	1.0
Vehicles Entered	2	11	11	77	118	3	222
Vehicles Exited	2	12	11	77	119	3	224
Hourly Exit Rate	8	48	44	308	476	12	896
Input Volume	8	46	46	307	461	8	876
% of Volume	100	104	96	100	103	150	102

2: Main Street (ID-75) & Access 1 Performance by movement Entire Run

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.1	0.0	0.1	0.1	0.0	0.3
Total Del/Veh (s)	11.8	5.8	3.5	0.7	0.4	0.2	1.1
Vehicles Entered	7	50	44	309	470	8	888
Vehicles Exited	7	50	44	309	470	8	888
Hourly Exit Rate	7	50	44	309	470	8	888
Input Volume	8	47	47	312	469	8	891
% of Volume	88	107	94	99	100	100	100

Total Network Performance By Interval

Interval Start	4:15	4:30	4:45	5:00	All	
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.1	
Denied Del/Veh (s)	0.3	0.3	0.3	0.3	0.3	
Total Delay (hr)	0.3	0.3	0.4	0.3	1.3	
Total Del/Veh (s)	4.4	4.2	4.8	4.4	4.7	
Vehicles Entered	245	239	261	244	989	
Vehicles Exited	245	241	257	248	989	
Hourly Exit Rate	980	964	1028	992	989	
Input Volume	3591	3591	3840	3591	3653	
% of Volume	27	27	27	28	27	

Intersection: 1: 10th Street & Main Street (ID-75), Interval #1

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	73	3	74
Average Queue (ft)	28	0	41
95th Queue (ft)	79	6	85
Link Distance (ft)	76	610	1051
Upstream Blk Time (%)	1		
Queuing Penalty (veh)	2		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (ID-75), Interval #2

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	63	2	78
Average Queue (ft)	28	0	39
95th Queue (ft)	72	5	75
Link Distance (ft)	76	610	1051
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	1		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (ID-75), Interval #3

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	74	2	86
Average Queue (ft)	29	0	50
95th Queue (ft)	79	4	88
Link Distance (ft)	76	610	1051
Upstream Blk Time (%)	2		
Queuing Penalty (veh)	7		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (ID-75), Interval #4

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	61	12	77
Average Queue (ft)	26	2	44
95th Queue (ft)	71	22	80
Link Distance (ft)	76	610	1051
Upstream Blk Time (%)	1		
Queuing Penalty (veh)	2		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (ID-75), All Intervals

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	81	19	102
Average Queue (ft)	28	1	43
95th Queue (ft)	75	12	83
Link Distance (ft)	76	610	1051
Upstream Blk Time (%)	1		
Queuing Penalty (veh)	3		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 2: Main Street (ID-75) & Access 1, Interval #1

Movement	EB	EB	NB	B3	SB
Directions Served	L	R	LT	T	TR
Maximum Queue (ft)	23	51	81	8	19
Average Queue (ft)	5	29	26	1	3
95th Queue (ft)	23	57	78	10	17
Link Distance (ft)	68	68	38	1119	76
Upstream Blk Time (%)		0	2		
Queuing Penalty (veh)		0	0		
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 2: Main Street (ID-75) & Access 1, Interval #2

Movement	EB	EB	NB	В3	SB
	LU.				
Directions Served	L	R	LT	T	TR
Maximum Queue (ft)	25	49	71	2	25
Average Queue (ft)	6	28	28	0	4
95th Queue (ft)	26	52	74	5	21
Link Distance (ft)	68	68	38	1119	76
Upstream Blk Time (%)		0	3		
Queuing Penalty (veh)		0	0		
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 2: Main Street (ID-75) & Access 1, Interval #3

Movement	EB	EB	NB	В3	SB
Directions Served	L	R	LT	T	TR
Maximum Queue (ft)	29	46	78	7	28
Average Queue (ft)	8	29	30	1	5
95th Queue (ft)	28	52	84	11	27
Link Distance (ft)	68	68	38	1119	76
Upstream Blk Time (%)		0	3		0
Queuing Penalty (veh)		0	0		0
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Intersection: 2: Main Street (ID-75) & Access 1, Interval #4

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	23	46	72	25
Average Queue (ft)	7	26	27	5
95th Queue (ft)	27	53	73	27
Link Distance (ft)	68	68	38	76
Upstream Blk Time (%)		0	2	
Queuing Penalty (veh)		0	0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 2: Main Street (ID-75) & Access 1, All Intervals

Movement	EB	EB	NB	В3	SB
Directions Served	L	R	LT	Т	TR
Maximum Queue (ft)	31	61	103	17	41
Average Queue (ft)	6	28	28	1	4
95th Queue (ft)	26	54	78	8	24
Link Distance (ft)	68	68	38	1119	76
Upstream Blk Time (%)		0	3		0
Queuing Penalty (veh)		0	0		0
Storage Bay Dist (ft)					
Storage Blk Time (%)					
Queuing Penalty (veh)					

Network Summary

Network wide Queuing Penalty, Interval #1: 2	
Network wide Queuing Penalty, Interval #2: 1	
Network wide Queuing Penalty, Interval #3: 7	
Network wide Queuing Penalty, Interval #4: 2	
Network wide Queuing Penalty, All Intervals: 3	



SimTraffic LOS Report

Project: Analysis Period: Time Period: **ID Ketchum Gas Station TIS**

Future (2020) Background p.m. Peak Hour Project #: UT-16-851

10th Street & Main Street (ID-75) Unsignalized Intersection:

Type:

Approach	Movement	Demand	Volume	Served	Delay/Ve	h (sec)
		Volume	Avg	%	Avg	LOS
	L	56	56	100	6.2	Α
NB	Т	323	331	103	1.6	Α
NB						
	Subtotal	379	387	102	2.3	Α
	Т	479	474	99	1.0	Α
SB	R	57	52	91	0.6	Α
0.5						_
	Subtotal	536	526	98	1.0	Α
	L	64	61	95	22.3	С
NE	R	91	90	99	11.5	В
	Subtotal	155	151	07	15.9	•
	Subtotai	199	151	97	15.9	С
Total		1,070	1,064	99	3.6	Α

Intersection:

Type:

Approach	Movement	Demand	Volum	e Served	Delay/Ve	h (sec)
		Volume	Avg	%	Avg	LOS
Total		·				

3: 10th Street & Main Street (ID-75) Performance by movement Interval #1 4:15

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.5	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.1	0.2
Total Del/Veh (s)	5.1	1.0	1.0	0.7	20.3	11.5	3.4
Vehicles Entered	13	80	114	14	16	23	260
Vehicles Exited	13	80	114	14	16	23	260
Hourly Exit Rate	52	320	456	56	64	92	1040
Input Volume	55	317	471	56	63	89	1051
% of Volume	95	101	97	100	102	103	99

3: 10th Street & Main Street (ID-75) Performance by movement Interval #2 4:30

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.1	0.3
Total Del/Veh (s)	5.3	1.5	1.0	0.5	23.6	13.4	3.7
Vehicles Entered	14	86	118	14	15	21	268
Vehicles Exited	14	87	119	13	16	21	270
Hourly Exit Rate	56	348	476	52	64	84	1080
Input Volume	55	317	471	56	63	89	1051
% of Volume	102	110	101	93	102	94	103

3: 10th Street & Main Street (ID-75) Performance by movement Interval #3 4:45

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.5	0.5	0.2	0.2	0.3
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.1	0.3
Total Del/Veh (s)	8.0	2.1	1.1	0.7	24.5	10.4	3.9
Vehicles Entered	16	83	125	14	15	24	277
Vehicles Exited	15	83	124	14	15	24	275
Hourly Exit Rate	60	332	496	56	60	96	1100
Input Volume	59	340	504	60	67	96	1126
% of Volume	102	98	98	93	90	100	98

3: 10th Street & Main Street (ID-75) Performance by movement Interval #4 5:00

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.1	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.1	0.2
Total Del/Veh (s)	5.5	1.6	1.0	0.4	18.2	9.0	3.2
Vehicles Entered	14	81	118	11	15	22	261
Vehicles Exited	14	82	116	11	15	21	259
Hourly Exit Rate	56	328	464	44	60	84	1036
Input Volume	55	317	471	56	63	89	1051
% of Volume	102	103	99	79	95	94	99

3: 10th Street & Main Street (ID-75) Performance by movement Entire Run

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.1	0.0	0.0	0.0	0.1
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.2	0.2	0.2
Total Delay (hr)	0.1	0.1	0.1	0.0	0.4	0.3	1.1
Total Del/Veh (s)	6.2	1.6	1.0	0.6	22.3	11.5	3.6
Vehicles Entered	56	331	474	52	61	91	1065
Vehicles Exited	56	331	474	52	61	90	1064
Hourly Exit Rate	56	331	474	52	61	90	1064
Input Volume	56	323	479	57	64	91	1070
% of Volume	100	103	99	91	95	99	99

Total Network Performance By Interval

Interval Start	4:15	4:30	4:45	5:00	All	
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.1	
Denied Del/Veh (s)	0.3	0.4	0.4	0.3	0.3	
Total Delay (hr)	0.4	0.4	0.5	0.4	1.6	
Total Del/Veh (s)	4.8	5.1	5.5	4.7	5.3	
Vehicles Entered	259	269	278	258	1064	
Vehicles Exited	260	272	275	260	1065	
Hourly Exit Rate	1040	1088	1100	1040	1065	
Input Volume	3034	3034	3251	3034	3088	
% of Volume	34	36	34	34	34	

Intersection: 3: 10th Street & Main Street (ID-75), Interval #1

Movement	NB	NE
Directions Served	LT	LR
Maximum Queue (ft)	70	103
Average Queue (ft)	25	60
95th Queue (ft)	73	112
Link Distance (ft)	274	1052
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 3: 10th Street & Main Street (ID-75), Interval #2

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	97	5	112
Average Queue (ft)	40	1	63
95th Queue (ft)	102	11	124
Link Distance (ft)	274	610	1052
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 3: 10th Street & Main Street (ID-75), Interval #3

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	113	2	102
Average Queue (ft)	53	0	60
95th Queue (ft)	130	5	107
Link Distance (ft)	274	610	1052
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

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Intersection: 3: 10th Street & Main Street (ID-75), Interval #4

Movement	NB	NE
Directions Served	LT	LR
Maximum Queue (ft)	97	105
Average Queue (ft)	37	53
95th Queue (ft)	102	98
Link Distance (ft)	274	1052
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 3: 10th Street & Main Street (ID-75), All Intervals

Movement	NB	SB	NE
Directions Served	LT	TR	LR
Maximum Queue (ft)	148	7	138
Average Queue (ft)	39	0	59
95th Queue (ft)	105	6	111
Link Distance (ft)	274	610	1052
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Network Summary

Network wide Queuing Penalty, Interval #1: 0	
Network wide Queuing Penalty, Interval #2: 0	
Network wide Queuing Penalty, Interval #3: 0	
Network wide Queuing Penalty, Interval #4: 0	
Network wide Queuing Penalty, All Intervals: 0	



SimTraffic LOS Report

Project: **ID Ketchum Gas Station TIS**

Analysis Period: Time Period: Future (2020) Plus Project p.m. Peak Hour

Project #: UT-16-851

10th Street & Main Street (ID-75) Unsignalized Intersection:

Type:

Approach	Movement	Demand	Volume	Served	Delay/Ve	h (sec)
		Volume	Avg	%	Avg	LOS
	L	56	55	98	6.0	Α
NB	Т	332	342	103	0.2	Α
	Subtotal	388	397	102	1.0	Α
	Т	487	478	98	1.1	Α
SB	R	57	58	102	0.7	Α
	Subtotal	544	536	99	1.1	Α
	L	64	64	100	24.2	С
NE	R	91	92	101	13.3	В
	Subtotal	155	156	101	17.8	С
Total		1,086	1,089	100	3.4	Α

Main Street (ID-75) & Access 1 Unsignalized Intersection:

Type:

1 1 1		Onorginanzoa		•	D 1 //	
Approach	Movement	Demand		Served	Delay/Ve	
		Volume	Avg	%	Avg	LOS
	L	47	44	94	3.8	Α
NB	Т	379	386	102	0.2	Α
	Subtotal	426	430	101	0.6	Α
	Т	570	564	99	0.5	Α
SB	R	8	7	88	0.2	Α
	Subtotal	578	571	99	0.5	Α
	L	8	10	125	15.9	С
EB	R	47	48	103	7.8	Α
	Subtotal	55	58	105	9.2	Α
Total		1,058	1,059	100	1.0	Α

1: 10th Street & Main Street (ID-75) Performance by movement Interval #1 4:15

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.4	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.1	0.3
Total Del/Veh (s)	5.2	0.2	1.1	0.6	24.1	14.9	3.8
Vehicles Entered	15	81	113	14	17	23	263
Vehicles Exited	15	81	113	14	15	24	262
Hourly Exit Rate	60	324	452	56	60	96	1048
Input Volume	55	326	478	56	63	89	1067
% of Volume	109	99	95	100	95	108	98

1: 10th Street & Main Street (ID-75) Performance by movement Interval #2 4:30

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.4	0.5	0.2	0.2	0.2
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.1	0.2
Total Del/Veh (s)	5.4	0.2	1.0	0.9	20.0	11.4	3.0
Vehicles Entered	14	87	120	15	15	22	273
Vehicles Exited	14	87	120	15	16	23	275
Hourly Exit Rate	56	348	480	60	64	92	1100
Input Volume	55	326	478	56	63	89	1067
% of Volume	102	107	100	107	102	103	103

1: 10th Street & Main Street (ID-75) Performance by movement Interval #3 4:45

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.5	0.4	0.1	0.2	0.3
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.1	0.3
Total Del/Veh (s)	7.2	0.2	1.2	0.6	24.4	14.1	3.8
Vehicles Entered	13	86	125	15	18	24	281
Vehicles Exited	13	86	126	15	17	23	280
Hourly Exit Rate	52	344	504	60	68	92	1120
Input Volume	59	348	513	60	67	96	1143
% of Volume	88	99	98	100	101	96	98

1: 10th Street & Main Street (ID-75) Performance by movement Interval #4 5:00

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.5	0.6	0.2	0.2	0.3
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.1	0.2
Total Del/Veh (s)	5.8	0.2	1.1	0.8	20.9	11.1	3.1
Vehicles Entered	12	87	119	14	14	22	268
Vehicles Exited	12	87	120	13	15	22	269
Hourly Exit Rate	48	348	480	52	60	88	1076
Input Volume	55	326	478	56	63	89	1067
% of Volume	87	107	100	93	95	99	101

1: 10th Street & Main Street (ID-75) Performance by movement Entire Run

Movement	NBL	NBT	SBT	SBR	NEL	NER	All
Denied Delay (hr)	0.0	0.0	0.1	0.0	0.0	0.0	0.1
Denied Del/Veh (s)	0.0	0.0	0.5	0.5	0.2	0.2	0.3
Total Delay (hr)	0.1	0.0	0.1	0.0	0.4	0.3	1.0
Total Del/Veh (s)	6.0	0.2	1.1	0.7	24.2	13.3	3.4
Vehicles Entered	55	342	477	58	64	92	1088
Vehicles Exited	55	342	478	58	64	92	1089
Hourly Exit Rate	55	342	478	58	64	92	1089
Input Volume	56	332	487	57	64	91	1086
% of Volume	98	103	98	102	100	101	100

2: Main Street (ID-75) & Access 1 Performance by movement Interval #1 4:15

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All	
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Denied Del/Veh (s)	0.1	0.2	0.0	0.0	0.0	0.0	0.0	
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	
Total Del/Veh (s)	17.6	7.2	3.4	0.2	0.5	0.1	1.0	
Vehicles Entered	2	12	11	93	135	2	255	
Vehicles Exited	2	11	12	93	135	2	255	
Hourly Exit Rate	8	44	48	372	540	8	1020	
Input Volume	8	46	46	372	560	8	1040	
% of Volume	100	96	104	100	96	100	98	

2: Main Street (ID-75) & Access 1 Performance by movement Interval #2 4:30

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.2	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	16.1	8.5	3.3	0.2	0.4	0.4	1.0
Vehicles Entered	3	12	11	98	142	1	267
Vehicles Exited	3	12	11	98	142	1	267
Hourly Exit Rate	12	48	44	392	568	4	1068
Input Volume	8	46	46	372	560	8	1040
% of Volume	150	104	96	105	101	50	103

2: Main Street (ID-75) & Access 1 Performance by movement Interval #3 4:45

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Total Del/Veh (s)	20.6	6.9	4.3	0.2	0.5	0.2	1.0
Vehicles Entered	2	12	12	97	147	2	272
Vehicles Exited	2	12	12	97	147	2	272
Hourly Exit Rate	8	48	48	388	588	8	1088
Input Volume	8	49	49	399	601	8	1114
% of Volume	100	98	98	97	98	100	98

2: Main Street (ID-75) & Access 1 Performance by movement Interval #4 5:00

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All	
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Denied Del/Veh (s)	0.1	0.2	0.0	0.0	0.0	0.0	0.0	
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	
Total Del/Veh (s)	17.4	8.5	3.7	0.2	0.5	0.3	1.0	
Vehicles Entered	2	12	10	97	140	2	263	
Vehicles Exited	2	12	10	97	140	2	263	
Hourly Exit Rate	8	48	40	388	560	8	1052	
Input Volume	8	46	46	372	560	8	1040	
% of Volume	100	104	87	104	100	100	101	

2: Main Street (ID-75) & Access 1 Performance by movement Entire Run

Movement	EBL	EBR	NBL	NBT	SBT	SBR	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.2	0.0	0.0	0.0	0.0	0.0
Total Delay (hr)	0.0	0.1	0.0	0.0	0.1	0.0	0.3
Total Del/Veh (s)	15.9	7.8	3.8	0.2	0.5	0.2	1.0
Vehicles Entered	10	48	44	385	564	7	1058
Vehicles Exited	10	48	44	386	564	7	1059
Hourly Exit Rate	10	48	44	386	564	7	1059
Input Volume	8	47	47	379	570	8	1058
% of Volume	125	103	94	102	99	88	100

Total Network Performance By Interval

Interval Start	4:15	4:30	4:45	5:00	All
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.1
Denied Del/Veh (s)	0.4	0.4	0.4	0.4	0.4
Total Delay (hr)	0.5	0.4	0.5	0.4	1.9
Total Del/Veh (s)	5.7	5.1	5.7	5.1	5.7
Vehicles Entered	285	295	305	291	1180
Vehicles Exited	285	297	305	294	1181
Hourly Exit Rate	1140	1188	1220	1176	1181
Input Volume	4290	4290	4594	4290	4366
% of Volume	27	28	27	27	27

Intersection: 1: 10th Street & Main Street (ID-75), Interval #1

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	47	16	129
Average Queue (ft)	26	2	64
95th Queue (ft)	53	16	130
Link Distance (ft)	71	616	1045
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	0		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (ID-75), Interval #2

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	49	6	114
Average Queue (ft)	24	1	61
95th Queue (ft)	54	9	121
Link Distance (ft)	71	616	1045
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	1		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (ID-75), Interval #3

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	51	8	122
Average Queue (ft)	25	1	66
95th Queue (ft)	57	11	127
Link Distance (ft)	71	616	1045
Upstream Blk Time (%)	1		
Queuing Penalty (veh)	1		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (ID-75), Interval #4

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	41	14	94
Average Queue (ft)	18	2	56
95th Queue (ft)	50	17	103
Link Distance (ft)	71	616	1045
Upstream Blk Time (%)	1		
Queuing Penalty (veh)	1		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 1: 10th Street & Main Street (ID-75), All Intervals

Movement	NB	SB	NE
Directions Served	L	TR	LR
Maximum Queue (ft)	64	27	158
Average Queue (ft)	23	2	62
95th Queue (ft)	54	14	121
Link Distance (ft)	71	616	1045
Upstream Blk Time (%)	0		
Queuing Penalty (veh)	1		
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 2: Main Street (ID-75) & Access 1, Interval #1

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	29	54	47	32
Average Queue (ft)	10	28	18	6
95th Queue (ft)	33	55	51	26
Link Distance (ft)	68	68	38	71
Upstream Blk Time (%)		0	2	
Queuing Penalty (veh)		0	0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 2: Main Street (ID-75) & Access 1, Interval #2

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	32	56	40	30
Average Queue (ft)	10	31	18	7
95th Queue (ft)	33	63	46	31
Link Distance (ft)	68	68	38	71
Upstream Blk Time (%)	0	1	1	0
Queuing Penalty (veh)	0	0	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 2: Main Street (ID-75) & Access 1, Interval #3

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	29	47	51	32
Average Queue (ft)	10	27	22	5
95th Queue (ft)	32	49	60	29
Link Distance (ft)	68	68	38	71
Upstream Blk Time (%)		0	2	0
Queuing Penalty (veh)		0	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 2: Main Street (ID-75) & Access 1, Interval #4

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	29	61	48	30
Average Queue (ft)	8	30	16	6
95th Queue (ft)	31	61	50	26
Link Distance (ft)	68	68	38	71
Upstream Blk Time (%)	0	1	2	
Queuing Penalty (veh)	0	0	0	
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Intersection: 2: Main Street (ID-75) & Access 1, All Intervals

Movement	EB	EB	NB	SB
Directions Served	L	R	LT	TR
Maximum Queue (ft)	36	72	63	46
Average Queue (ft)	9	29	19	6
95th Queue (ft)	32	58	52	28
Link Distance (ft)	68	68	38	71
Upstream Blk Time (%)	0	0	2	0
Queuing Penalty (veh)	0	0	0	0
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty, Interval #1: 0	
Network wide Queuing Penalty, Interval #2: 1	
Network wide Queuing Penalty, Interval #3: 1	
Network wide Queuing Penalty, Interval #4: 1	
Network wide Queuing Penalty, All Intervals: 1	
Network wide Queding Fenalty, All Intervals. 1	

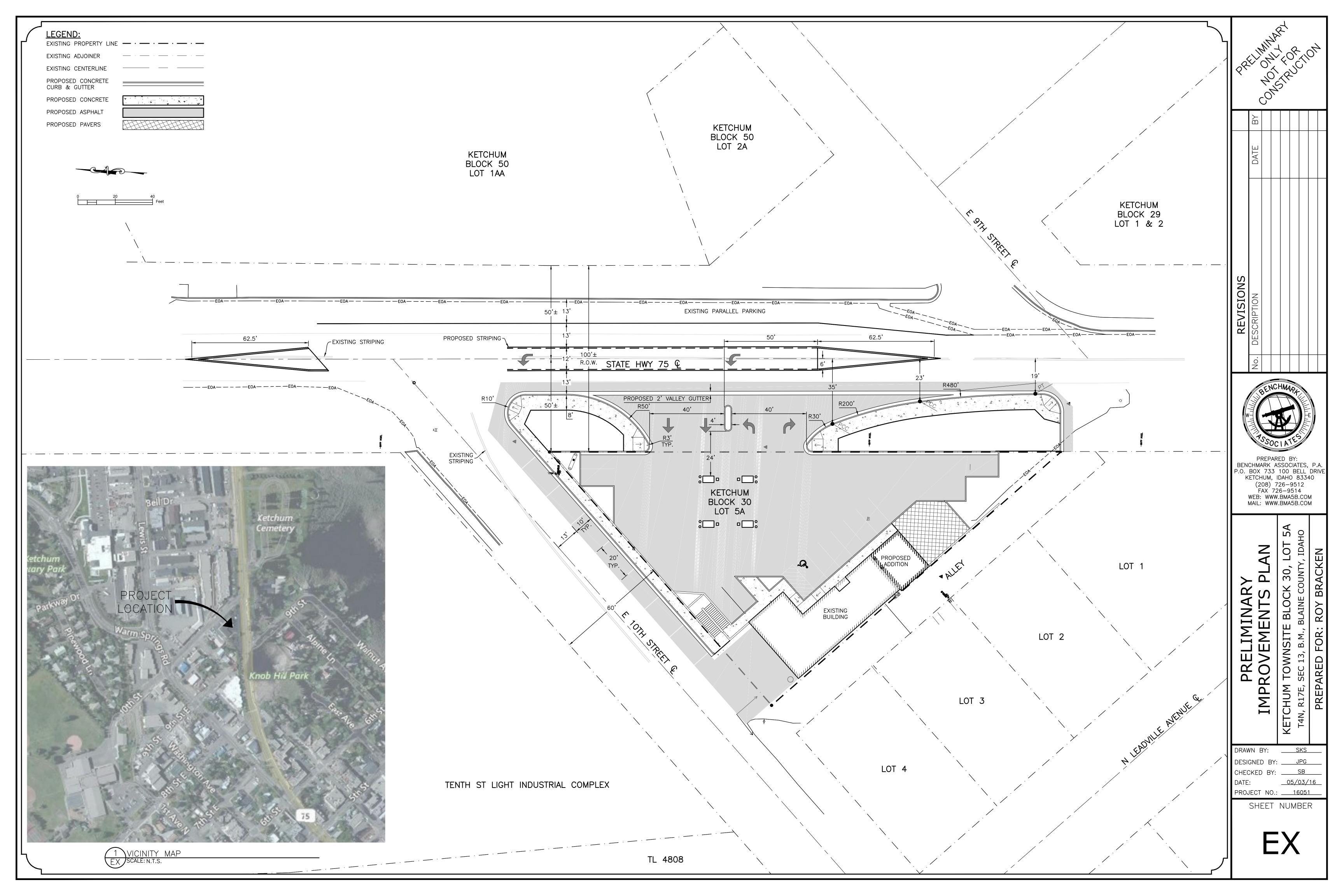
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APPENDIX C

Site Plan





APPENDIX D

95th Percentile Queue Length Reports

HALES | ENGINEERING | Innovative transportation solutions

Time Period: p.m. Peak Hour 95th Percentile Queue Length (feet)

			NE	SB
Intersection T	ime Period	LT	LR	TR
10th Street & Main Street (ID-75) Existing (2016) Background	79	80	2

Time Period: p.m. Peak Hour 95th Percentile Queue Length (feet)



	B3		EB	NB	NE	SB
Intersection Time Period	Т	L	R	LT	LR	TR
10th Street & Main Street (ID-75) Existing (2016) Plus Project				75	83	12
Main Street (ID-75) & Access 1 Existing (2016) Plus Project	8	26	54	78		24

HALES | ENGINEERING Innovative transportation solutions

Time Period: p.m. Peak Hour 95th Percentile Queue Length (feet)

		NB	NE	SB
Intersection	Time Period	LT	LR	TR
10th Street & Main Street (ID-75)	Future (2020) Background	105	111	6

Time Period: p.m. Peak Hour 95th Percentile Queue Length (feet)



		EB		NB		NE	SB
Intersection	Time Period	L	R	L	LT	LR	TR
10th Street & Main Street (ID-75)	Future (2020) Plus Project			54		121	14
Main Street (ID-75) & Access 1	Future (2020) Plus Project	32	58		52		28

Appendix C

Public Comment

Received June 25, 2016 through October 4, 2016

Brittany Skelton

From: Sent:

To:

Brittany Skelton; Micah Austin

Subject:

For the Public Record...concern about cut-off traffic intensifying as a result of Bracken

Station

Brittany and Micah,

I'm confident P&Z seriously considers "unintended consequences"* for all development proposals, but the consequences created by Bracken Station are so numerous it is easy to overlook some. (*BTW the phrase "unintended consequences" was used by Steve Cook, Chair of the P&Z Commission, when deliberating about city parking.)

As traffic on North Main Street backs up as a result of traffic dependent Bracken Station, there will be a dramatic increase in cut off traffic on 9th Street, Walnut Avenue, Alpine Lane.*

All three of these streets (one labelled as an "alley") are narrow and steep, servicing a residential neighborhood. Besides being narrow and steep in places, none has:

- 1. painted center lane lines
- 2. sidewalks
- 3. street lights

Currently: All three streets are used by those living in the neighborhood as well as dog walkers, pedestrians who access trail behind cemetery and use streets along with cyclists as a short cut between East Ketchum and Main Street North.)

The SPEED LIMIT posted on all three streets ranges from 15 mph to 10 mph

The following are signs and warnings on these streets (photos attached)

Alpine Ln Signs

SPEED LIMIT 10 / Stirctly Enforced

No Vehicles Over 10,000 Lbs

DANGER PLEASE DRIVE SLOW / 10 MPH / As Posted / Watch for Children and Pets

NO PARKING This Side of Alley (3 signs)

CHILD ZONE / DRIVE WITH CARE

Walnut Ave Signs

SPEED LIMIT 15 (2 signs)

SLOW/children at Play

No Vehicles over 10,000 lbs

13% grade (2 signs)

9th Street East Signs

Speed Limit 15

No Vehicles over 10,000 lbs

The presence of these multiple postings and greatly reduced speed limits indicates the City of Ketchum thought these streets potentially dangerous and laden with significant safety hazards.

*KETCHUM HISTORY: previously Alpine Lane was two way and left hand turns were permitted off 9th Street. Changing Alpine Lane to one way, decreasing the speed limits on East 9th Street, Alpine Lane and Walnut Avenue and limiting turn at 9th Street and Main Street to a right turn only were ALL initially implemented to discourage cut-off traffic.

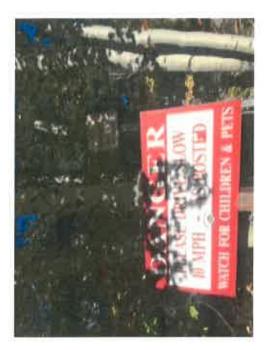




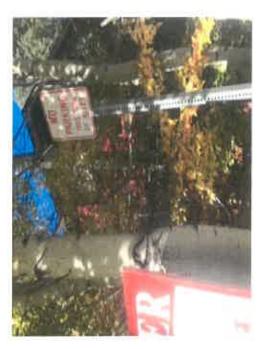
















Sent from my iPhone

B. (Barbi) Anne Reed ANNE REED GALLERY

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October 3, 2016

181 East 50th St. Garden City, ID 83714 (208) 484-4410 (Cell) thompsonengineers@cableone.net

Brittany Skelton City of Ketchum P.O. Box 2315 Ketchum, ID 83340

SENT VIA EMAIL

RE: Bracken Station Ketchum, ID

Dear Ms. Skelton,

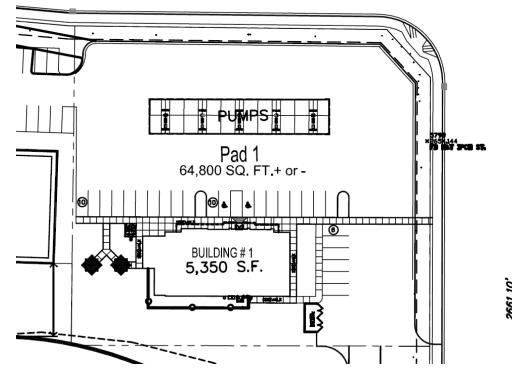
Thompson Engineers, Inc. has been retained by residents of Frenchman's Place condominiums to review the traffic impact study for the above referenced prepared by Hales Engineering and dated September 2016. This letter will summarize our findings.

The copy submitted to us did not contain a registered engineer's stamp, signature and date as required by the State of Idaho. The report contained no way to identify the individual responsible for the preparation of the report. There is no way to know if this report has been prepared by a professional engineer or under the direct supervision of a professional engineer.

Site Layout

The proposed site layout does not conform to the current practice for layout of convenience stores with gas pumps. The layout is similar to site plans for gas stations several decades ago where access was obtained from broad unrestricted approaches. A more modern layout is shown in Figure 1.

Figure 1 - Typical Modern Convenience Store Site Layout



Page 1 of 11



A more modern approach would be to separate the fueling positions from the entrance. This will allow cars to maneuver to find an available pump and queue up without interfering with vehicles entering and exiting the site. The preferred design would have the pumps in an array with clearly defined entrances and exits. The above site also has room for delivery trucks to circulate through the site without interfering with fueling vehicles.

The onsite circulation exhibits provided by Benchmark and the site plan provided by Steve Cook Architect indicate a very constrained site. The exhibits indicate that the operation was based upon observations of a local convenience store in Hailey. However, this was based on one observation and should not be considered proof that this is a typical situation. We have the following comments on each exhibit.

- The site layout shows a center to center separation of 24 feet between the fueling positions. To provide adequate room for maneuvering, gas stations typically provide at least 25 feet of clearance between pumps. The actual clearance provided in this site is probably less than 22 feet. Given that the vehicles will have to make a 90 degree turn to enter the fueling position, many vehicles will not be completely parallel to the fuel pumps and will impede flow through these lanes. The turning exhibits submitted in the original application showed that trucks and recreational vehicles could not access the fueling positions except at an angle. None of the exhibits show that even a passenger car can access westerly fueling positions 3 and 4 without extra movements.
- The site layout shows two lanes in and two lanes out. However, the lanes would only be 27 feet long measured from the fueling position to the curb of the island. This is not enough room to posture into the lane and provides stacking for only one vehicle.
- The turning exhibits assume that all vehicles will enter and exit via the assigned entrances and exits. In fact, the operator will have no control over vehicles entering and exiting the site. At a typical gas station, vehicles will enter the site, try to find an open fueling position or the shortest line, and orient the car so the gas cap is on the side with the fuel pump. With the fueling positions located immediately in front of the entrance and a constrained site, the congestion may end up on the highway.
- Exhibit #1 shows vehicles at the pumps. With even vehicles at the pumps, the left turn exit on to SH75 is completely blocked, as is the left entry lane from SH75. If even one vehicle is waiting to fuel at pump #3 or #4, the entire entrance or exit would be blocked.
- Exhibit #1 Recreational Vehicle Queuing and commercial Vehicle Delivery This exhibit indicates that the turning paths of the passenger car and truck will either hit the curb or need to pass extremely close to the curb. The exhibit does show vehicles at the pumps, but it would appear that if even one vehicle is waiting for a pump to become available, no vehicles could maneuver into or around the site.
- Exhibit #2. The plan shows a passenger car and trailer fueling at pump #2. This allows a second passenger car and trailer to maneuver around the first. However, if the first car stops at pump #1, for whatever reason, then it would appear that circulation around the site is not possible. It is not clear that the commercial vehicle can clear the first passenger car and trailer even at the shown location.
- Exhibit #3. It does not appear that the commercial vehicle shown can exit the site without backing up. If a vehicle is fueling at pump #3, this would not be possible. The passenger car without a trailer, shown in blue, barely fits between the truck and the curb. The passenger car



and trailer shown in green needs to hit the curb entering and exiting to get by the truck. It does not appear that the passenger car and trailer will be able to complete a right turn without moving into the left turn lane.

- Fourth Exhibit Recreational Vehicle Access and Circulation. It does not appear that a recreational vehicle traveling north on SH75 would be able to access any pumps but the westerly side of pumps #1 and #2. The central fueling positions are only accessible by a recreational vehicle if there are no other cars at the pumps, and that is only if they roll over the entrance curb. It does not appear that the vehicle shown at pump #2 in teal will be able to exit the site without backing up.
- Fifth Exhibit Box Truck Access and Circulation. It does not appear that a truck traveling north on SH75 would be able to access pumps#3 and #4, and maybe not the east side of pumps #1 and #2. The truck traveling south needs to jump the curb to Figure 2 Fuel Truck

no cars at any other position.

• Benchmark does not provide a turn exhibit for a fuel delivery truck. Fuel trucks are usually a tractor trailer combination which has different turning characteristics than box trucks. They could be as small as a WB-50, but could easily be as big as a WB-62 or even larger. Fueling trucks can often have double trailers. It is not clear that any of the above trucks can physically enter and exit the site. Certainly any fuel deliveries would be impossible with any other vehicles blocking the circulation aisles.

access the central fueling positions, and then needs to have

- The City staff letter to the applicant dated August 3, 2016, Item 10.a.i requests a circulation exhibit with large vehicles on site and a fuel delivery truck on site. This exhibit is not provided.
- The original application included turn templates for vehicles turning right from 10th St. on to SH75. The revised submittal modifies the site plan and roadway improvements. The left turn lane has been moved west by approximately 4 feet. In the original submittal, the 30 foot box truck was required to impede into the left turn lane to complete the right turn movement. This movement has not been confirmed with the revised site plan. Furthermore, 10th Street is on the city transit route. An appropriate bus turn movement exhibit has not been provided.

Site Access

Hales Engineering notes that SH75 is classified as a Regional Highway by the Idaho Transportation Department (ITD) for access policy purposes. ITD has the sole authority for granting the access. However, this project will require several exceptions to the ITD access policy. These exceptions can be considered by the City of Ketchum when reviewing the project.

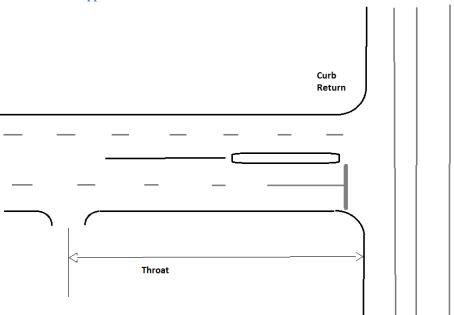
The location of the access as proposed will require an exception to the ITD Policy. IDAPA 39.03.42, the Idaho Code for governing highway right-of-way encroachments on State right-of-way, would require a minimum separation of 250 feet from the south edge of 10th Street and the north edge of the proposed approach. The site plan indicates a separation of about 70 feet which is a significant exception.



If SH75 and 10th Street is ever signalized as a possible mitigation, as discussed in the Hales report, the approach to the site would be limited to right-in and right-out movements. A full movement approach would need to be 500 feet from 10th St.

The width of the proposed approach would meet the ITD definition of a boulevard approach. A boulevard approach typically has at least two lanes entering and two lanes exiting which are separated by an island. Boulevard approaches are typically used to serve large subdivisions or retail centers, and almost never for access for a single retail approach. A typical boulevard approach is shown below.

Figure 3 - Typical Boulevard Approach



As discussed above, the proposed approach is restricted by the fueling positions. One entry lane and one exit lane are obstructed by the fueling stations, especially if the position is occupied with a vehicle. None of the access exhibits discussed above show any vehicles using these lanes as a lane. The inside lanes will not function as lanes.

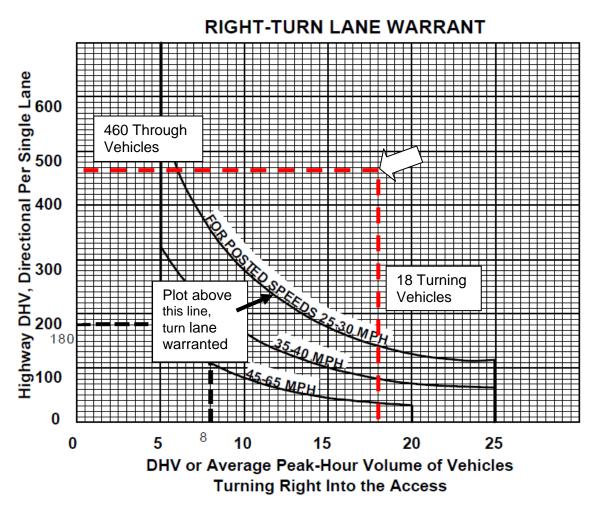
In a typical boulevard approach, no access would be allowed from the approach for a minimum of 50 feet. This is known as the throat of the approach. In the case of a development that generates more than 100 trips per day, the Ada County Highway District would require a traffic study to determine if the throat should be extended beyond 50 feet. In the case of this project, no throat is provided.

Boulevard approaches are usually designed with curb and gutter returns at the entrance. The minimum radius for a curb return is typically 30 feet. This project has a modified entrance without a circular radius at the entrance. The setback on the entrance is less than 30 feet.

In order to maintain safe movement of traffic, turn lanes are provided at intersections to safely remove turning traffic from the stream of through traffic. This project does provide a two way left turn lane, but it does not provide a right turn lane. ITD provides a graph to determine if a turn lane is warranted based on the volume of turning vehicles and the volume of through vehicles. In this case, a right turn lane is warranted for the right turn movement in the southbound direction of SH75. Not providing the right turn lane is an exception to ITD policy.



Figure 4 - ITD Right Turn Lane Warrant



IDAPA 39.03.42 notes that an exception to the separation requirement may be granted if it can be shown that turn lanes will not conflict with the proposed access location. In this case, a turn lane for the site would conflict with 10 St. Thus the exception to the separation policy should not have been granted.

Trip Generation

The Hales report estimates trip based on observed trip generation of a similar site in Hailey. No description of the site is provided. The Trip Generation Handbook, published by the Institute of Transportation Engineers, recommends that if a surrogate site is to be used to determine trip generation, several data sets should be collected, and preferably from multiple sites. The Hales study uses only one data set. Based on this observation, Hales estimates this site will generate 90 trips during the PM peak hour. We also note that the observation had no recreational vehicles so it may not be representative of traffic at this site during peak times.

The Recommended Practice for Preparation of Traffic Impact Studies, published by the Institute of Transportation Engineers, recommends that trip generation be estimated using the Trip Generation Manual. The Trip Generation Manual has data from hundreds of sites and provides average trip generation rates for a wide variety of land uses. Hales Engineering used the Trip Generation Manual to



estimate the number of trips from this site in the original report. Hales used Land Use 954, Service Station with Convenience Market. Trips were estimated using the number of fueling positions as the variable. With this land use, they determined that the site would generate 1304 trips per day and 110 during the PM peak hour.

The Trip Generation Manual also includes Land Use 853, Convenience Market with Gas Pumps. Using the number of fueling stations as the variable, the trip generation of the site would be 4,341 trips per day and 153 trips during the PM peak hour. Trips from a convenience market with gas pumps can also be estimated using the square footage of the building as the variable. In this case, the trip generation of the site would be 1776 vehicles per day and 107 during the PM peak hour.

The difference between a Service Station with a Convenience Market and a Convenience Market with Gas Pumps is basically the primary purpose of the business. If the primary purpose of the business is to sell gasoline, it would be a Service Station. If the primary purpose is to sell convenience foods and sundries, it would be a Convenience Market. Without a specific tenant, the worst case should be considered.

The difference in trip generation between number of fueling positions as the variable and square footage as the variable is probably due to a disproportion in the size of the building and number of fueling stations compared to more traditional convenience stores.

Hales did provide a supplemental study for trip generation at a higher rate. The supplemental study found a trip generation of 185 trips during the PM peak hour. This is double the rate used in the revised study. It is not clear which trip generation rate we are supposed to accept.

Background Traffic Volumes

Hales Engineering estimates growth in traffic volumes at 1.1% per year based on data from ITD automated traffic recorder number 68. This recorder is located north of Hailey, Idaho. ITD published data and reports from this recorder from the past 25 years. The last full year of data is 2015. The average daily traffic report from the ITD website is attached to this report. It is not clear what data was used to determine a growth rate of 1.1%. We used the annual average daily traffic from 2012 to 2015 and determined a growth rate of 2.7%. We do note that the growth rate has been relatively flat for the last 15 years.

A growth rate of 1.1% is typically below recommended growth rates. Given that the traffic recorder was located south of Ketchum, other factors, such as growth in building permits, should be used to justify a growth rate below 2%.

The data from counter 68 also suggests that the average traffic in July is 13% higher than the average traffic in September. However, since the data was collected on a holiday weekend, we reviewed the 2015 Labor Day weekend traffic to typical July traffic and found that it was comparable.

Level of Service

Hales Engineering uses the Synchro/Sim Traffic software of the Highway Capacity Manual to determine the level of service of the intersections. We use HCS 2010, which automates the same calculations as Synchro. However, there are several factors that can be set and modified that impact the level of service.



Both programs have default values, but not necessarily the same default values. As a result, minor differences in the software setting result in different level of service calculations.

For this review, we used the 2020 traffic volumes provided in the Hales report and recalculated the level of service. We also adjusted their traffic volumes to account for a 2.7% growth rate and for 110 trips generated by the site. Based on our analysis, the intersection of 10th and SH75 and the site entrance will operate at lower levels of service than stated in the Hales Report.

LOS Summary	2020 Plus Project						
PM Peak Hour Conditions	Hales V	olumes /	TEI Volumes				
	Delays/v	LOS	Delays/v	LOS			
SH 75 and 10th St Critical Movement	15.3	С	16.6	O			
SH75 and Site Entrance Critical Movement	16.4	С	17.1	С			

We also reviewed the operation of the signal at 5th St. and Main St. The lack of dedicated left turn lanes and other lane configuration makes timing of this intersection difficult. HCS 2010 advises that the overlapping left turn lanes may not accurately reflect the actual operation of the signal. However, we were unable to find a phasing or timing combination that resulted in any level of service other than LOS F. The Hales report indicated an overall intersection operation of LOS B. Without a detail input report, we cannot verify their results. Field observations of the operation of the signal would indicate that the signal is not operating at LOS B.

Queuing Analysis

The Hales report provided results from the queuing analysis, but did not provide the methods or actual calculations to determine the queuing.

The Hales report did not discuss queuing at the pumps. This is critical since any vehicle queuing at the pump will block any access to the site and result in congestion on the highway. They note a dwell time of 5.08 minutes for vehicles at the pumps. This could be considered the service rate but it will be a variable rate as some people will have bigger tanks to fill or other tasks. The arrival rate is somewhere between 45 and 90 vehicles per hour, depending on the trip generation method used. The arrival rate is also variable since vehicles will arrive randomly. With these variables, the average queue can be estimated using queuing theory. Assuming all 8 fueling stations are available and an arrival rate of 45 vehicles per hour, there would be minimal queuing at the fueling stations. However, if the arrival is 75 vehicles per hour, the queue exceeds three vehicles. Or, if a recreational vehicle blocks several fueling positions, leaving only five positions, the queue would exceed two vehicles. These vehicles would block any access or egress from the site.

Benchmark provided an exhibit of the queuing lengths provided and required. The on site queue length required is 50 feet +/-. One passenger car with a travel trailer could occupy all of that 50 feet. The queue length provided will conflict directly with any cars that may be queuing for the gas pumps. If more than one passenger car is queued up at the exit as discussed above, vehicles will not be able to exit the fueling stations.



In the 2020 Plus Project conditions, in section VI.D., Hales estimates that the queue on the northbound approach of the intersection of Main and 5th Sts will be 50 feet. They do not discuss the queue in the southbound direction which would seem to be an area of concern. As seen in the photographs below, traffic has already been observed to back up from 5th St. for several hundred feet. Traffic may not be stopped at 10th St., but it is delayed for a considerable distance. These backups can result in very few gaps for people to turn left or right from 10th St. and probably impact the level of service of that intersection.

Figure 5 - SH75 looking north from 6th St.



Figure 6 - SH75 looking south from 6th St.





Benchmark is showing striping for a two way left turn lane in SH75. This means that southbound traffic that wishes to turn left on 9th St. will also use this lane. Hales does not provide any information about traffic on 9th St. This is a potential conflict.

The two way left turn lane is provided instead of dedicated left turn lanes. Using the ITD guidelines for left turn lanes, both the site entrance and 10th St. would warrant left turn lanes. A left turn lane is different from a two way left turn lane. A left turn lane includes a storage length, a deceleration length, and a taper length as shown in Figure 8. Based on the ITD Traffic Manual, the storage length should be a minimum of 50 feet, the deceleration length should be 125 feet, and the taper should be a minimum of 100 feet. This provides space for cars to stack, slow down, and exit the traffic stream. The proximity of two left turn movements creates opportunities for congestion and endangers safety.

Figure 7 - Left Turn Lane Warrants

LEFT-TURN LANE WARRANT

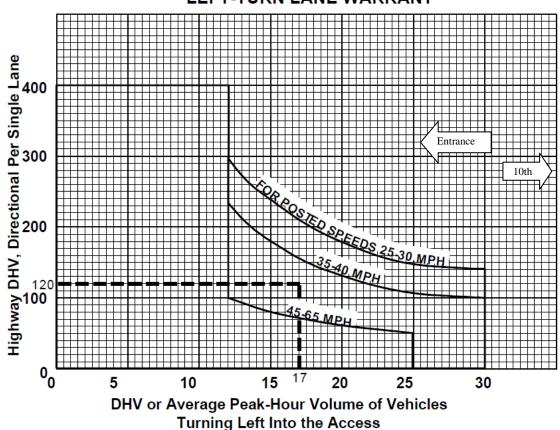
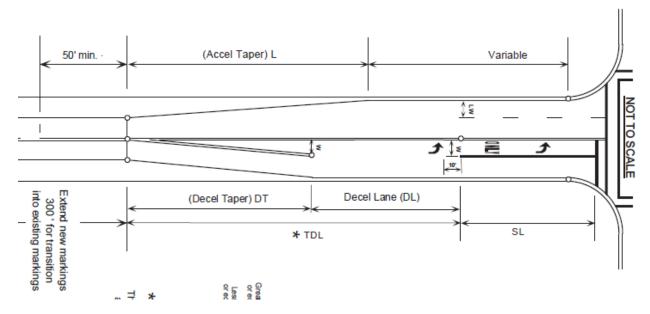




Figure 8- ITD Design Standard for Left Turn Lane



Pedestrian Access and Circulation

Convenience stores and gas stations are automobile oriented businesses by nature, and therefore tend not to be pedestrian friendly. Customers for these facilities arrive and depart almost completely by car.

The report by Alta Planning and Design included in the appendix of the Hales report makes several recommendations to improve pedestrian accessibility in and around the site. For the most part, we believe the recommendations made by Alta are good recommendations. Constructing the connecting sidewalk is obviously a benefit to pedestrians. The use of rectangular rapid flashing beacons is an effective and economical measure to improve pedestrian crossings.

One area of concern is the recommendation to move the cross walk at 9th Street away from the intersection. We are concerned this would conflict with the recommendations of the Americans with Disabilities Act, which recommends that crossings occur at the curb return.

The Hales report noted that there were very few pedestrians observed during the study hours. However, there is a transit bus stop located on SH75 directly opposite the proposed gas station. Clearly the transit service is accommodating pedestrians so pedestrians should be anticipated.

Gas stations are not generally pedestrian friendly because they create conflicts between vehicles and pedestrians. The most common problem location is at the entrances and exits. In the case of this project, pedestrians walking on the west side of SH75 must traverse 84 feet of driveway at the entrance to this site. The Highway Capacity Manual recommends a walking speed of 3-4 feet per second for designing pedestrian facilities. The different rate would depend on the study population. At these speeds, it would take a pedestrian 21 to 28 seconds to cross the driveway. With the site generating up to 180 trips per hour, or three per minute, conflicts are likely to occur.



Many agencies, such as the Ada County Highway District, limit the size of a commercial access to 36 feet. This allows one lane in and two lanes out. It allows pedestrians to cross the driveway in 9 or 10 seconds, thus reducing the likelihood of conflicts.

Conclusions

Based on the above review and analysis, we believe there are the following issues with the site. These issues will likely result in congestion both on and off the site.

- This site plan does not incorporate modern design practices for convenience stores with gas pumps. The proposed plan is more similar to gas station designs from several decades ago where the site access was unrestricted.
- The site is very constrained. The circulation exhibits do not demonstrate easy vehicular
 movement on the site. Several of the vehicles movements conflict with the proposed curbs.
 None of the exhibits show a vehicle entering and exiting the site without having to perform some
 sort of back up. It does not seem that larger vehicles can access some of the fueling stations at
 all
- The site circulation exhibits have not provided evidence that a fuel tank truck could access the site at all.
- The site plan does not provide for the orderly entrance and exit of vehicles. The proposed entrance and exit lanes are blocked by fueling positions within 27 feet of the entrance. It does not provide a logical method for vehicles to queue while waiting for fueling positions.
- The site plan will require an exception to ITD policy for the separation from 10th Street.
- The site plan will require an exception to ITD policy if they do not provide a southbound right turn lane at the entrance to the site.
- The site plan will require an exception to ITD policy if a left turn lane is not provided.
- The trip generation for the site is not clearly determined. The method used in the main report uses only one sample data. The ITE Trip Generation Manual would generate much higher volumes. The supplemental analysis estimates a higher trip generation for the site.
- The site plan does not provide enough queue storage on site. The proposed queue storage conflicts with the fueling positions and the queuing for the fueling positions.
- Delays from the 5th Street and Main Street signal have been observed to back up over 1000 feet. This will impact the access in and out of the site.
- The two way left turn lane provided does not meet design standards for a left turn lane which is warranted.
- The traffic report is not stamped, signed and dated as required by Idaho law.

For these reasons, it is our opinion that the proposed land use and proposed site plan could have a negative impact on the transportation system in the City of Ketchum. Should you have any questions, please call.

Sincerely,

Daniel A. Thompson, P.E.

Attachments

03-Oct-16

ITE Trip Generation Rates - 9th Edition Pass-by rates from ITE Trip Generation Handbook - 2nd Edition

(copyrights, Insitute of Transportation Engineers)

Description/ITE Code		ITE	ITE Vehicle Trip Generation Rates						Expected	Total Generated Trips			
	Units	(peak hours	s are for p	eak hou	r of adjace	ent stree	et traffic u	ınless hi	ghlighted	Units			
		Weekday	AM	PM	Pass-By	AM In	AM Out	PM In	PM Out		Daily	AM Hour	PM Hour
Convenience. Mkt w/ Gas Pumps 853	KSF ²	845.60	40.92	50.92	66%	50%	50%	50%	50%	2.6	2,199	106	132
Convenience. Mkt w/ Gas Pumps 853	Fuel Position	542.60	16.57	19.07	66%	50%	50%	50%	50%	8.0	4,341	133	153
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		VO-WAY STOP	CONTRO)L SU	JWWARY				
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Agency/Co.		n Engineers, Inc	Jurisdic			City of Ketchum			
Date Performed	9/23/2016		Analysi	s Year		2020 Tota	2020 Total Traffic		
Analysis Time Period	-								
Project Description Bra	acken Station wit	h Hales Conditions		" 0					
East/West Street: 10th Intersection Orientation:	North South				treet: Main (S	SH / 5)			
		4-	Study P	erioa (hrs): 0.25				
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Major Street Movement	1	Northbound 2	3		4	Southbou 5	na l	6	
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Volume (veh/h)	50	289	1			56		407	
Peak-Hour Factor, PHF	0.85	0.92	1.00		1.00	0.83		0.90	
Hourly Flow Rate, HFR	58	314	0		0	67		452	
veh/h)									
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Peak-Hour Factor, PHF	0.70	1.00	0.72		1.00	1.00		1.00	
Hourly Flow Rate, HFR									
veh/h)	75	0	88		0	0		0	
Percent Heavy Vehicles	0	0	0		0	0		0	
Percent Grade (%)		0				0			
Flared Approach		N				N			
Storage		0				0			
RT Channelized			0					0	
Lanes	0	0	0		0	0		0	
Configuration		LR							
Delay, Queue Length, a	nd Level of Serv	vice							
Approach	Northbound	Southbound		Vestbo	ound		Eastbound		
Movement	1	4	7	8	9	10	11	12	
ane Configuration	L						LR		
/ (veh/h)	58						163		
C (m) (veh/h)	1057						513	1	
//c	0.05				1		0.32		
95% queue length	0.17						1.36	<u> </u>	
Control Delay (s/veh)	8.6						15.3		
OS	A				 		75.3 C	+	
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Approach Delay (s/veh)							15.3		
Approach LOS							С		

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veh/h)	81	0	94		0	0		0		
Percent Heavy Vehicles	0	0	0		0	0		0		
Percent Grade (%)		0	•			0	•			
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Movement	1	4	7	8	9	10	11	12		
_ane Configuration	L						LR			
/ (veh/h)	62						175			
C (m) (veh/h)	1024						483			
//c	0.06						0.36			
95% queue length	0.19						1.64			
Control Delay (s/veh)	8.7					1	16.6			
OS	A.7					+	70.0			
		 								
Approach Delay (s/veh)							16.6			
Approach LOS						1	С			

General Information			Cita Inf	ormatio	n				
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Analyst	D. Thomp		Intersect			Site Acce			
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East/West Street: Entrar ntersection Orientation:			North/South Street: <i>Main (SH75)</i> Study Period (hrs): 0.25						
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Vehicle Volumes an	<u>d Adjustmen</u>								
Major Street	<u> </u>	Northbound				Southbou	und		
Movement	1	2	3		4	5		6	
/ a l	L	T 224	R	_	L	T 474		R	
Volume (veh/h) Peak-Hour Factor, PHF	27 0.85	321 0.92	1.00		1.00	471 0.83		18 0.90	
Hourly Flow Rate, HFR			1.00	_	1.00	1			
veh/h)	31	348	0		0	567		20	
Percent Heavy Vehicles	0			-	0				
Median Type		-		Undivided	1				
RT Channelized			0					0	
anes	1	1	0		0	1	 	0	
Configuration	† ;	† <i>†</i>	 	-		+ '		TR	
Jpstream Signal		1				0		773	
Minor Street		Eastbound				Westbou	ınd		
Movement	7	Eastbound 8	9	-+	10	vvestbot 11	iiiu	12	
NOVELLICIT	/	T	R R		L	<u> </u>		R	
/olume (veh/h)	18	 '	27			+ '		П	
Peak-Hour Factor, PHF	0.70	1.00	0.72	_	1.00	1.00		1.00	
Hourly Flow Rate, HFR									
(veh/h)	25	0	37		0	0		0	
Percent Heavy Vehicles	0	0	0		0	0		0	
Percent Grade (%)		0				0			
Flared Approach		T N	1			<u> </u>			
Storage		0	-			0			
		-				1 0			
RT Channelized			0			+		0	
Lanes	0	0	0		0	0		0	
Configuration		LR				1			
Delay, Queue Length, ar									
Approach	Northbound	Southbound	W	estbound/			Eastbound		
Movement	1	4	7	8	9	10	11	12	
_ane Configuration	L					1	LR		
/ (veh/h)	31						62		
C (m) (veh/h)	998					1	377		
//C	0.03	 	+		 	+	0.16	\vdash	
						+	}	-	
95% queue length	0.10					+	0.58	<u> </u>	
Control Delay (s/veh)	8.7	<u> </u>				1	16.4	<u> </u>	
_OS	Α						С		
Approach Delay (s/veh)							16.4		
Approach LOS							С		

	TV	VO-WAY STOP	CONTRO	DL SU	MMARY			
General Information)		Site In	form	ation			
Analyst	D. Thomp		Intersed	ction		Site Acces	ss	
Agency/Co.	Thompso 9/23/2016	n Engineers, Inc	Jurisdic			City of Ketchum		
Date Performed		Analysis Year			2020 Total Traffic			
Analysis Time Period	PM Peak							
Project Description Bra		h 2.7% growth and	l 110 trips g	enerate	ed			
East/West Street: Entrai					treet: Main (S	SH75)		
ntersection Orientation:			Study P	eriod (hrs): 0.25			
Vehicle Volumes an	<u>d Adjustmen</u>			ı		0		
Major Street Movement	1	Northbound	3		4	Southbou	na l	6
viovement	L	2 	R		4 	5 T		R
Volume (veh/h)	33	289	IN		<u>L</u>	483		22
Peak-Hour Factor, PHF	0.85	0.92	1.00		1.00	0.83		0.90
Hourly Flow Rate, HFR								
(veh/h)	38	314	0		0	581		24
Percent Heavy Vehicles	0				0			
Median Type				Undiv	rided	,		
RT Channelized			0					0
_anes	1	1	0		0	1		0
Configuration	L	T						TR
Jpstream Signal		1				0		
Minor Street				Westbou	nd			
Movement	7	8	9		10	11		12
	L	T	R		L	T		R
Volume (veh/h)	22	4.00	33			1.00		4.00
Peak-Hour Factor, PHF	0.70	1.00	0.72		1.00	1.00		1.00
Hourly Flow Rate, HFR (veh/h)	31	0	45		0	0		0
Percent Heavy Vehicles	0	0	0		0	0		0
Percent Grade (%)		0				0		
Flared Approach		N				N		
Storage		0				0		
RT Channelized			0					0
Lanes	0	0	0		0	0		0
Configuration		LR						
Delay, Queue Length, a	nd Level of Ser	vice						
Approach	Northbound	Southbound	1	Nestbo	ound		Eastbound	
Movement	1	4	7	8	9	10	11	12
ane Configuration	L						LR	
/ (veh/h)	38						76	
C (m) (veh/h)	983						372	1
//c	0.04						0.20	1
95% queue length	0.12						0.75	1
Control Delay (s/veh)	8.8					+	17.1	+
• • • • • • • • • • • • • • • • • • • •					-		C C	+
LOS	Α					+		
Approach Delay (s/veh)						_	17.1	
Approach LOS						_1	С	

Company large area of the		O-WAY STOP	0:4- 14	C					
General Information				formation	on	lau -			
Analyst	D. Thomp		Intersec			Site Acce			
Agency/Co.		n Engineers, Inc	Jurisdict			City of Ke			
Date Performed	9/23/2016 PM Peak		Analysis	s year		2026 Tota	аі і гатіс		
Analysis Time Period									
	trip generation i	rate	N - :41- 10 -		4. Main (0	1175)			
East/West Street: Entrai			North/South Street: <i>Main (SH75)</i> Study Period (hrs): 0.25						
ntersection Orientation:			Study Pe	erioa (nrs)): 0.25				
Vehicle Volumes an	<u>d Adjustmen</u>					0 11 1			
Major Street	1	Northbound	1 0		4	Southboo	und		
Movement	1	2	3		4	5		6	
/olumo (voh/h)	L 56	357	R		L	807		R 37	
/olume (veh/h) Peak-Hour Factor, PHF	0.85	0.92	1.00		1.00	0.85	- 	37 0.90	
Hourly Flow Rate, HFR									
veh/h)	65	388	0		0	949		41	
Percent Heavy Vehicles	0				0				
Median Type			Undivided			•	<u> </u>		
RT Channelized			0					0	
anes	1	1	0		0	1		0	
Configuration	L	T	<u> </u>		-	1	- 	TR	
Jpstream Signal	_	1				0		-	
/linor Street		Eastbound	-	- i		Westbou	ınd		
Movement	7	8	9		10	11	1	12	
	Ĺ	T	R		L	T T		R	
/olume (veh/h)	37	-	59			† 		••	
Peak-Hour Factor, PHF	0.70	1.00	0.72		1.00	1.00		1.00	
Hourly Flow Rate, HFR	52		81					0	
veh/h)	52	0	81		0	0		<u> </u>	
Percent Heavy Vehicles	0	0	0		0	0		0	
Percent Grade (%)		0				0			
lared Approach		N				N			
Storage		0				0			
RT Channelized			0			1		0	
_anes	0	0	0		0	0		0	
Configuration		LR			-	†		-	
Delay, Queue Length, a	nd Level of Sen		1						
Approach	Northbound	Southbound	۱۸	Vestbound		1	Eastbound		
Movement	1	4	7	8	9	10	11	12	
		+ +	'	O	+ 3	10	-	12	
ane Configuration	L				+	+	LR		
(veh/h)	65	ļļ				1	133		
C (m) (veh/h)	706						196		
r/c	0.09						0.68		
95% queue length	0.30						4.15		
Control Delay (s/veh)	10.6						55.2		
_OS	В						F		
Approach Delay (s/veh)					1	1	55.2		
						+	55.2 F		
Approach LOS						1	Г		

Counter #68 - Hailey Automatic traffic recorder

- Average Daily Traffic
- Published reports
- Combine Sites
- Your Cart

YEAR	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Annual 24-hr Avg.
1990	7297	7880	8250	7594	8635	10146	11445	11403	9780	9186	7866	7582	8931
1991	7506	7754	7698	7435	8271	9434	11071	11161	9323	9027	7419	7877	8675
1992	7645	8025	8374	8011	8666	9733	11388	11502	8983	8290	7324	7637	8830
1993	6589	6918	7553	7164	7299	8407	9195	8818	7990	7613	6699	6967	7607
1994	7992	8392	8665	7662	7806	8991	11244	11365	10449	9729	7848	8162	9034
1995	9149	9752	9706	9439	10283	12044	13752	12983	12165	11405	10057	10195	10911
1996	9516	10276	10548	9920	10695	12567	14041	14094	12417	11736	9942	10008	11313
1997	9874	10667	10669	10122	10804	12553	14485	14473	12494	11908	10328	10877	11605
1998	10090	10606	10858	10471	11239	13071	14957	14723	13412	11671	10543	11085	11894
1999	10539	10554	11261	10578	11479	13395	15467	15119	13876	12910	11253	11595	12336
2000	10726	11395	11769	11190	12109	14187	15753	15371	13678	13072	11122	11869	12687
2001	10999	11776	12245	11319	12343	14140	15880	15484	13450	12964	11476	11000	12756
2002	11458	11989	11794	12372	12663	14249	16067	15328	13228	13473	11448	12025	13008
2003	11973	12169	11887	12170	12857	14644	16545	16157	14203	14014	11582	12655	13405
2004	11608	12317	12657	12595	13147	15367	16732	16137	14708		11501	12711	
2005	11271	12269	12738	12260	13182	14993	16317	15842	14588	13389	12118	12521	13457
2006	11158	11694	12164	11828	13050	14755	15628	15493	13966	13412	11507	12435	13091
2007	11895	12192	12250	11979	12975	14543	16084	15692	13509	13667	11994	12319	13258
2008	10818	11876	11793	11636	12260	13750	15296	14898	13218	12765	10705	11152	12514
2009	10759	11195	10505	10723	11172	12957	14811	13672	12856		10291	11466	
2010	10224	10770	10539	10330	10487	12535	14571	14124	12497	11473	9877	11736	11597
2011	10456	10539	9974	9844	10128	12119	14347	13699	12091			9883	
2012	9684	10537	10023	9944	10371	12379	14441	13162	11984	11474	10005	11177	11264
2013	10411	10836	10556	10318	10873	12829	14561	13385	12334	11771	10055	11227	11596
2014	10514	10730	10490	10454	11017	12947	14887	14132	12538	12161	10378	11730	11831
2015	11157	11663	11290	11248	11806	13987	15198	14787	13389	12838	10864	12011	12520
2016	11682	12163	11581	11666	12302	14560	16017						

This Data is also available in the following forms: $\underline{\text{Comma Delimited}}$, $\underline{\text{Tab Delimited}}$ and $\underline{\text{Space Delimited}}$. Right-Click and 'Save Target As' to download a copy.

For a graph of June average daily traffic from year to year -- click here.

	TABLE 1 – ACCESS SPACING*										
HIGHWAY TYPE	AREA TYPE	Signalized Road Spacing	Public Road Spacing (A)	Driveway Distance Upstream From Public Road Intersection (B)	Driveway Distance Downstream From Unsignalized Public Road Intersection (C)	Distance Between Unsignalized Accesses Other Than Public Roads (D)					
	Rural	5,280 ft	5,280 ft	1,000 ft	650 ft	650 ft					
Statewide	Transitional	5,280 ft	2,640 ft	760 ft	500 ft	500 ft					
Route	Urban >35 mph	2,640 ft	1,320 ft	790 ft	500 ft	500 ft					
	Urban ≤35 mph	2,640 ft	1,320 ft	790 ft	250 ft**	250 ft**					
	Rural	5,280 ft	2,640 ft	1,000 ft	650 ft	650 ft					
Regional	Transitional	2,640 ft	1,320 ft	690 ft	360 ft**	360 ft**					
Route	Urban >35 mph	2,640 ft	660 ft	660 ft	360 ft**	360 ft**					
	Urban ≤35 mph	2,640 ft	660 ft	660 ft	250 ft**	250 ft**					
	Rural	2,640 ft	1,320 ft	760 ft	500 ft	500 ft					
District Route	Transitional	2,640 ft	660 ft	660 ft	360 ft**	360 ft**					
District Route	Urban >35 mph	1,320 ft	660 ft	660 ft	360 ft**	360 ft**					
	Urban ≤35 mph	1,320 ft	660 ft	660 ft	250 ft**	250 ft**					

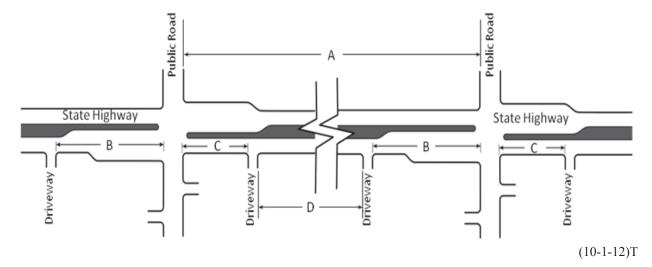
^{*}Distances in table are minimums based on optimal operational and safety conditions such as adequate sight distance and level grade. Definitions of spacing designated by (A), (B), (C), and (D) are represented on Figure 1.

(10-1-12)T

Section 400 Page 13

^{**} Where the public road intersection or private access intersection is signalized, the distances in the table are for driveways restricted to right-in/right-out movements only. For unrestricted driveways the minimum distance shall be 500 feet from a signalized intersection.

Figure 1:



- **d.** The District Engineer shall have the authority to deny an encroachment permit or require the applicant to provide a Traffic Impact Study when an on-site review indicates that the optimal conditions (such as sight distance and queue length) assumed in Table 1 do not exist, and that operational or safety problems may result from the encroachment spacing. (10-1-12)T
- e. The District Engineer shall have the authority to approve a decrease in the minimum access spacing distances set forth in Table 1, provided that the basis for any exception is justified and documented. The basis for the exception may include overriding economic opportunity considerations. For any exception that would result in a decrease in access spacing of more than ten percent (10%) of the distances set forth in Table 1, a Traffic Impact Study will be required in order to determine whether auxiliary lanes or other appropriate mitigation must be included in the permit's conditions. (10-1-12)T
- f. Unless the requirement is waived by the District Engineer, a Traffic Impact Study shall also be required when a new or expanded development seeks direct access to a state highway, and at full build out will generate one hundred (100) or more new trips during the peak hour, the new volume of trips will equal or exceed one thousand (1000) vehicles per day, or the new vehicle volume will result from development that equals or exceeds the threshold values in Table 2. If the District Engineer waives the requirement for a Traffic Impact Study, the basis for such waiver shall be justified and documented. (10-1-12)T
- g. When required, the Traffic Impact Study shall document access needs and impacts and whether any highway modifications are necessary to accommodate the new traffic volumes generated by the development. Such modifications could include, for example, turn lanes, additional through lanes, acceleration or deceleration lanes, medians, traffic signals, removal and/or consolidation of existing approaches, approaches limited to right-in/right-out access only, etc. (10-1-12)T
- h. If a District Engineer denies an encroachment permit application and the denial is appealed to the board, the board or its delegate shall have the authority to approve exceptions to the access and signal spacing distances in Table 1 if, in the judgment of the board, overriding economic considerations cause the exceptions to be in the best interests of the public. (10-1-12)T

Section 400 Page 14

From: Mayor"s Office

To: <u>Micah Austin; Keshia Owens; Brittany Skelton</u>

Subject: FW: Proposed Bracken Application of a CUP for a gas station

Date: Monday, October 03, 2016 4:00:32 PM

LISA ENOURATO | CITY OF KETCHUM

Assistant City Administrator

P.O. Box 2315 | 480 East Ave. N. | Ketchum, ID 83340

o: 208.726.7803 | f: 208.726.7812

lenourato@ketchumidaho.org | www.ketchumidaho.org

From: Richard Bartoccini [mailto:rbartoccini@gmail.com]

Sent: Monday, October 03, 2016 2:59 PM

To: Participate <participate@ketchumidaho.org>

Cc: Mayor's Office <mayorsoffice@ketchumidaho.org>

Subject: RE: Proposed Bracken Application of a CUP for a gas station

October 3, 2016

Dear: Planning and Zoning Commissioners:

We are strongly OPPOSED to this application. Our reasoning, as briefly as possible:

It will be dangerous. The code says: "The conditional use will not materially endanger the health, safety and welfare of the community".

We have lived at Frenchman's Place since 2005 and have turned on and off 10th and Main thousands of times. It is a very difficult and dangerous intersection. To turn on to Main Street going north or south it is necessary to focus on the speeding traffic, especially trucks and RV's, heading south. Then it is a quick glance to the right, pick an opening and go. Perhaps an even greater danger will be the back up on Main Street. Cars, RV's and delivery trucks in the station will block the ingress causing vehicles to stop on Main Street.. Having cars and trucks (and people trying to cross Main Street) darting in and out of a gas station/convenience store would GREATLY ENDANGER THE HEALTH, SAFETY AND WELFARE OF THE COMMUNITY". There is simply no way to mitigate these inherent dangers.

It will increase traffic. Per the code: "The conditional use is such that pedestrian and vehicular traffic associated with the use will not be hazardous or conflict with existing and anticipated traffic in the neighborhood".

Aside from what was stated above, by definition, a gas station/convenience store's primary purpose is to attract as much vehicular and pedestrian traffic as possible. Ingress and egress from all types of traffic into a gas station/convenience store would present a far greater negative impact on the health and safety of our residents and visitors than already exists. This would be true under "normal" conditions. Factor in the danger of rear end collisions from traffic coming south into vehicles trying to get into a gas station that is not big enough to accommodate cars, RV's, diners, and delivery trucks at the same time, and it really does become an accident waiting to happen.

<u>Gas stations are ugly</u>. We thought that the public officials have a stated objective of keeping and/or making Main Street as attractive as possible. Gas stations/convenience stores are very unattractive, to say the least.

Personal Harm: We live on the top floor, center unit of the Frenchman's Place Condos. We have

lived here since 2005 and purchased the unit a few years ago. We have co-existed with our commercial neighbors for many years. We get the fact that we are in the light industrial zone. Just so you know, some of the things we have had to endure over the years are: being awakened in the wee hours by semi's pulling in under our window on 9th Street and idling for hours; the back-up warning alarms on all types of delivery trucks; drunks coming out of Grumpy's and making a racket; the every night dumping of beer cans and bottles from Grumpy's into the alley dumpster; many of the same issues from the Base Camp gas station/convenience store; just to name a few. The proposed gas station/convenience store, which is not a permitted use, would be extremely harmful for us. The signage lights and the lights from vehicles would shine into our master bedroom window and onto our deck. The ambient light from the building, parking lot and signage would ruin our beautiful view to the north. The noise from delivery trucks, cars, RV's, etc., would be constant, day and night. Our personal safety, since the 10th & Main Street intersection so often, will be even more compromised. Our right to quiet enjoyment will greatly decrease and so will our property values. We are sure most of our neighbors feel the same. It really doesn't seem fair to permit such a use.

Please use your own good judgment and common sense and existing code guidelines to turn this application down.

Respectfully,

Richard and Judi Bartoccini

360 9th St. East Unit 24

From: Lisa Enourato on behalf of Participate

Micah Austin; Keshia Owens; Brittany Skelton To:

Cc: Suzanne Frick

Subject: FW: Bracken Gas Station/Convenience Store Cup application

Date: Wednesday, October 05, 2016 11:27:18 AM

LISA ENOURATO | CITY OF KETCHUM

Assistant City Administrator

P.O. Box 2315 | 480 East Ave. N. | Ketchum, ID 83340

o: 208.726.7803 | f: 208.726.7812

lenourato@ketchumidaho.org | www.ketchumidaho.org

From: Richard Bartoccini [mailto:rbartoccini@gmail.com]

Sent: Tuesday, October 04, 2016 12:39 PM

To: Participate <participate@ketchumidaho.org>

Subject: Bracken Gas Station/Convenience Store Cup application

Hi: It's me again. I sent you a letter yesterday but took the photo of the gas delivery truck this morning. The photo of the CocaCola delivery was take last month from our balcony at Frenchman's Place. I am sure you see the problem. (Base Camp has been a good neighbor since Mr. Wendland took over.) Thanks, Richard





From: <u>Micah Austin</u>
To: <u>Brittany Skelton</u>

Subject: FW: Application for Gas station

Date: Monday, October 03, 2016 11:02:50 AM

FYI: public comment on Bracken.

----Original Message-----

From: Lisa Enourato On Behalf Of Participate Sent: Monday, October 03, 2016 10:36 AM

To: Micah Austin <maustin@ketchumidaho.org>; Keshia Owens <kowens@ketchumidaho.org>

Subject: FW: Application for Gas station

LISA ENOURATO | CITY OF KETCHUM

Assistant City Administrator P.O. Box 2315 | 480 East Ave. N. | Ketchum, ID 83340 o: 208.726.7803 | f: 208.726.7812 lenourato@ketchumidaho.org | www.ketchumidaho.org

----Original Message-----

From: Linda Jensen [mailto:lindajensen03@gmail.com]

Sent: Saturday, October 01, 2016 8:37 AM To: Participate participate@ketchumidaho.org>

Subject: Application for Gas station

While I am neither strongly for or against this application, I feel there has been little discussion about the fact that there is already a gas station on the north end of town, located at the corner of Warm Springs Road and 10th Street, only one long block from the proposed station! While the existing station is not on the main road into Ketchum, that station is EASILY FOUND with the advent of phone apps that most people use today to find services! Do we need to duplicate a service that is currently available and easily accessed?

Linda H Jensen Sun Valley, ID

Sent from my iPad Linda Jensen

July 28, 2016 A 21/2years over the "old Bridge"-Deoled cross country sky Tento town - there was cebaut 350 people who really lived here! a Bad Corner - Jokay maybe not of 11:30 pm Why are we so concern about a towns drewing thru - we live here! My father owned "Gas Stations in San Niego - I know, about traffic - Health problems, Ele. Please Sty of the Cook - pet something there other than a Gastation If you have this town once a Willage Sorewed up the South End. I dredy Strewed up the South End. Jamely 1806 to still 2016 CH

SURVEY

Proposed

BRACKEN GAS STATION and CONVENIENCE STORE
MAIN STREET KETCHUM between 10th Street and East 9th Street
(opposite Knob Hill Inn on site of Lizzy's Coffee)

	Check all that you feel apply
,	There are enough gas stations in Ketchum. Ketchum does not need another.
`	Bracken Station will be permanent, visual blight at Ketchum's Northern Gateway
\	Bracken Station will create a toxic environment (air and soil contamination) creating a health hazard, especially for nearby Ketchum residents, workers, businesses
	Bracken Station is not in compliance with stated LI-1 Zoning purpose: "generate little traffic from tourists and the general public"
	Bracken Station's convenience store will lure children from Hemingway and YMCA onto 10 th St. (incredibly dangerous now even without the addition of more vehicles generated by Bracken Gas Station). Bus stop riders will be lured across highway from the bus stop located directly opposite of proposed Bracken Station.
\	Bracken Station will dramatically increase traffic on North Main, 10 th Street and other adjacent streets endangering pedestrians and bicyclists. (Note: Bracken Station is designed for use by extra heavy trucks (e.g. dump trucks, tractor-trailers with long hitches, semis, 5 th wheels, large RVs, snowmobile /horse trailers, etc.)
\	If Bracken Station is approved, I understand that City of Ketchum could not prevent sale to a large gas station e.g. Stinker Store, Exxon, Chevron, etc. or the opening of a Fast Food franchise, such as Subway, McDonald's, Sonic, Arby's, etc.
	Bracken Station does not comply with Ketchum's Comp Plan: "to promote the health, safety and general welfare of present and future inhabitants of Ketchum"
	All of the above None of the above
	ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org)
\	
	I disapprove of Bracken Station on Main Street, Ketchum. You may list my name among others who dopose this development. City of Residence Sun Valley Signature Print
,	Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

SURVEY

Proposed

BRACKEN GAS STATION and CONVENIENCE STORE MAIN STREET KETCHUM between 10th Street and East 9th Street (opposite Knob Hill Inn on site of Lizzy's Coffee)

Check all that you feel apply

Bracken Station does not comply with Ketchum's Comp Plan: "to promote the health, safety and general welfare of present and future inhabitants of Ketchum" All of the above None of the above ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org) I disapprove of Bracken Station on Main Street, Ketchum. You may list my name among others who oppose this development. City of Residence Paine County Print Olic VIII Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM	
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	Print_ Jolie Duni

SURVEY

Proposed

BRACKEN GAS STATION and CONVENIENCE STORE
MAIN STREET KETCHUM between 10th Street and East 9th Street
(opposite Knob Hill Inn on site of Lizzy's Coffee)

Check all that you feel apply

Check all that you leel apply
There are enough gas stations in Ketchum. Ketchum does not need another.
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Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

From: <u>Leo Brieske</u>
To: <u>Brittany Skelton</u>

 Subject:
 July 11 2016 Bracken Station P & Z

 Date:
 Monday, July 11, 2016 10:00:38 AM

Reference proposal drawings A2 & A6: Rail Road Tie Retaining Walls

When I reviewed the drawings, it is my understanding that the paved alley extension will end at the lower north R.R. tie retaining wall.

In the A6 drawing there is a R.R. tie wall to the south which will be in the right of way behind LOT2 that is the rear of my property at 920 Leadville. Questions:

1) Is it necessary to have the southern R.R. Tie Wall ?? This will entail the removal of current landscaping that I have maintained in the right of way for the past 14 years.

It will also possibly weaken the retaining wall on my property. 2) Will the developer be responsible for repairs of my wall?

Your consideration is appreciated.

Leo Brieske 920 Leadville Ave. Lot 2 Ketchum ID ----Original Message-----

From: Peter Breck [mailto:peter@glenbrookconsumer.com]

Sent: Monday, July 11, 2016 4:58 PM

To: Participate < participate@ketchumidaho.org>

Cc: Toni Breck < tonibreck@gmail.com >; barbi@annereedgallery.com

Subject: Bracken Gas Station

I am writing to say that I respectfully, but very strongly, disapprove of the Bracken Gas Station and Convenience Store for ALL of the reasons Outlined in the on-Line survey (which would not accept my responses in the email I received asking for comment). Thank You!

Peter Breck

Full-time Resident at 13350 Hwy 75, Ketchum

Sent from my iPhone

From: Michael John [mailto:MJohn@MJJCapitalMgt.com]

Sent: Thursday, July 07, 2016 1:28 PM

To: Micah Austin < <u>maustin@ketchumidaho.org</u>>

Subject: Proposed gas station north of town on hwy 75

Dear Micah

Our family lives at 780 Alpine Way in Ketchum and we have resided here for 16 years. Over this time we have seen the increased traffic patterns in our area and as a result we strongly believe that the proposed gas station that is being considered north of town is a very bad idea. It would increase traffic in our area and increase the likelihood for both vehicle and pedestrian accidents.

Please note that we are opposed to this concept.

Thank you for your time.

Michael & Barbie John

PS Thank you for the time you commit to improve our community!

From: Debbie Flood [mailto:dflood1@cox.net]

Sent: Sunday, July 10, 2016 4:26 PM

To: Participate < participate@ketchumidaho.org >

Cc: Deborah Flood < dflood1@cox.net >

Subject: Gas Station and Convenience Store south of 10th street

Dear Planning and Zoning Commissioners,

I have watched the traffic patters on the highway at 10th street for the past 49 years.

I was on the Ketchum Planning and Zoning Board for some years and know traffic patterns. I have lived in the same house at 302 Broadway Blvd. over the one-way bridge thus knowing the traffic patterns from Warm Spring Road east on 10th to the highway and into Ketchum from Warm Springs Road. Each year we have more traffic.

That particular corner where someone wants to put a gas station on the West side of the highway just south of 10th street is already a hazardous place for traffic and would impact the safety and welfare of our community. We have high volume traffic problems there already. Putting a Gas Station and Convenience Store with cars, trucks, long trucks, motor homes pulling cars behind them, going in and out of that location to the highway is just plain dangerous. It would create a higher density of traffic coming in and trying to get back on the highway. Getting back on the highway would be a big problem especially pulling a load behind you.

From Warm Spring Road going east on 10th street to the highway I have found it difficult to cross the highway to go north or south because the huge amount of traffic coming from the north and south. The traffic consists of residents, landscape trucks, flat bed truck hauling big machinery and the motor homes and trailers in the summer months.

Just north of 10th street is the 10th Street Center with 11 businesses: Stoecklein Photography, SV Kitchen, Bull Frogs Spas, Fire Place Outfitter, Brennan's Carpets Fish Appliances, St. Francis Pet Clinic, Vacation International, LeeAnn's Pet Salon, First Light and Boulder Mt. Clay Art Gallery. People are coming and going with the variety of businesses there and trying to get on or off the highway. Across the street is a Hotel with traffic coming and going of guests and service people day and night. 10th street and the highway are congested now. The Gas Station and Convenience store would add more traffic congestion and would create a safety problem and the general welfare problem to our community.

Coming from Ketchum turning left to go down Warms Springs Road many times there is a huge back up of cars and trucks and I am unable to turn left. If I would keep going on the highway to turn left on 10th street, I would have the same problem with the amount of traffic.

Do not grant a Conditional Use Permit for this Gas Station and Convenience store.

- 1. Traffic is already a problem getting on and off the highway and the Gas Station and Convenience store would contribute to even more congestion to that area and adding a risk to the safety and welfare to our community
- 2. The LI-1 is a transition area providing limited commercial service industries, limited retail, small light manufacturing, research and development, and office related to building, maintenance and construction and which generate little traffic from tourist and the general public". The GAS STATION AND CONVENIENCE STORE would create more traffic problems getting in and back on the highway and making an already traffic congestion problem into a much bigger safety problem.
- 3. There are many bikers who use this section and it would be hazardous for them with cars, trucks and trailers trying to get back on the highway from the gas station.

Thank you for your time

Debbie Flood – 208-720-5709

LISA ENOURATO | CITY OF KETCHUM

Assistant City Administrator P.O. Box 2315 | 480 East Ave. N. | Ketchum, ID 83340

o: 208.726.7803 | f: 208.726.7812

lenourato@ketchumidaho.org | www.ketchumidaho.org

From: Patricia Davies [mailto:edppd@cox.net]

Sent: Friday, July 08, 2016 6:10 PM

Subject: gas station at North Entrance to Ketchum-WHY???

Dear P& Z:

Having lived in Blaine County for 48 years, (thus not able to vote in Ketchum) I've witnessed the TRAVESTY going up at the South entrance of Ketchum- the Limelight Hotel! In my opinion, the P&Z made a HUGE mistake in allowing ANY 5-story structure scar our landscape and limit our views. And NOW, the P&Z is considering approval of ANOTHER gasstation/minimart at the NORTH entrance to Ketchum- WHY???When there is a busy gas station/minimart(one of 5) a mere 500 yards away in Warm Springs? PLEEEZ –don't let this happen!!!!

Pat Davies

LISA ENOURATO | CITY OF KETCHUM

Assistant City Administrator

P.O. Box 2315 | 480 East Ave. N. | Ketchum, ID 83340

o: 208.726.7803 | f: 208.726.7812

lenourato@ketchumidaho.org | www.ketchumidaho.org

From: Patricia Davies [mailto:edppd@cox.net]

Sent: Monday, July 11, 2016 12:21 PM

To: Participate < participate@ketchumidaho.org >

Subject: RE: gas station at North Entrance to Ketchum-WHY???

PS to my previous letter

I now understand just OPPOSING ANOTHER gas station is not enough!

The main reason needed for the P&Z to approve it, is that it would not interfere with existing traffic flow(Cars were backed up all the way to the Knob Hill on Fri. at 4pm) and it could cause an accident because of the cross traffic there.

Pat Davies

From: Lisa Enourato [mailto:LEnourato@ketchumidaho.org] On Behalf Of Participate

Sent: Monday, July 11, 2016 10:24 AM **To:** Patricia Davies <<u>edppd@cox.net</u>>

Subject: RE: gas station at North Entrance to Ketchum-WHY???

Hi Pat. Your email has been received and will be presented to the P&Z at tonight's meeting. Thank you for your comments.

Lisa

LISA ENOURATO | CITY OF KETCHUM

Assistant City Administrator

P.O. Box 2315 | 480 East Ave. N. | Ketchum, ID 83340

o: 208.726.7803 | f: 208.726.7812

lenourato@ketchumidaho.org | www.ketchumidaho.org

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Sent: Friday, July 08, 2016 6:10 PM

To: Participate < <u>participate@ketchumidaho.org</u>>

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PLEEEEZ —don't let this happen!!!!
Pat Davies

From: Terri Moore [mailto:tmoore4935@aol.com]

Sent: Monday, July 11, 2016 11:08 AM

To: Mayor's Office < <u>mayorsoffice@ketchumidaho.org</u>> **Subject:** Comment on Ketchum Commercial Property

Dear Mayor,

As a business person and property owner in Ketchum and Hailey, I am commenting on the proposed property that is being considered for development at the north end of Ketchum. I currently own property in the light industrial area in Ketchum, I am a native of Ketchum, and have been a Valley resident for over 40 years.

First and foremost, this property is zoned for a "convenience store, gas station". The proposed property development is in compliance with the current, particularly-stated zoning. My understanding is that Mr. Bracken intends to comply with all city, state and federal laws (which are many for a gas dispensing station) as well as installing the correct traffic flow lanes and corridors as required by ITD. This is a positive development for this parcel.

Secondly, Ms. Reed, who is the party who initially raised objections to this proposed development, purchased her property across the street with full knowledge of the fact that the property to her west was zoned light industrial, and she surely should or could have known what is included in LI zoning.

Thirdly, there are many people who own motor homes, (and there are going to be many more in the future with the huge amount of baby boomers retiring and choosing motor homes and rv's as a way of travel--read the studies and data), who are unable to access the current stations that are in the light industrial area due to the size and maneuverability of their vehicles. For me to attempt to drive my motor home off of the main Highway 75, over Saddle Road, and into the Sun Valley resort area where there are young children and tourists on bicylces as well as a considerable amount of pedestrian traffic, is unreasonable at best and at worst potentially hazardous. I would simply not do it. Also, vehicle traffic in general, from out of the area and just driving through, would, most likely, not be aware of the stations that are located out of this core Highway 75 area. The current property under consideration is a perfect place for fueling and stopping out of the Ketchum core. I also understand that Mr. Bracken intends to provide electrical hook-ups for those cars needing that service. All of this will provide a much-needed service to the area.

Lastly, the City would be able to capture the considerable amount of tax revenue from the establishment.

Thank you,

Terri Moore

When 9 first heard about the groposed Bracken Station project of thought it was just a chazy currier. It made absolutely no sense under any terms. O know people have conflicting ideas about how a property should be developed, whether businesses are located there or not, but this is a very BAD IDEA. I watch the traffic go by on the highway & 10 th of five Olays a veet from my place of bismos. I can always cautious extring onto the Hwy. from 10th St Ond especially walking a cross 1 on the winter a cup of coffee? On the winter I is extremely noncloss. Thus is not a vote against Change & development Dt is a Vote for Sofety. The Bracken Station project would be an accident waiting to happen in many ways.

Lee Ann's Ret Salon (191 10th St Unit A8 Leeden KeeleNot sure if this is scramyall's email or what. Do what you will.

LISA ENOURATO | CITY OF KETCHUM

Assistant City Administrator

P.O. Box 2315 | 480 East Ave. N. | Ketchum, ID 83340

o: 208.726.7803 | f: 208.726.7812

lenourato@ketchumidaho.org | www.ketchumidaho.org

From: SCRAM Y'all [mailto:scramyall@gmail.com]

Sent: Saturday, July 09, 2016 9:12 AM

To: Participate participate@ketchumidaho.org>;; Mayor's Office <mayorsoffice@ketchumidaho.org>;

Anne Corrock acorrock@ketchumidaho.org

Subject: Gas Station - Highway 75

• Freakin' Brilliant posted at 5:58 am on Fri, Jul 8, 2016.



Posts: 344

What the author of this letter doesn't tell you is that she is the owner of the newly built house directly across the street from this proposal.

Did you not notice the commercial zoning when you bought this lot or submitted your plans to P&Z?

Are you claiming ignorance?

Maybe you should have been "proactive" before you bought that property instead of being "reactive" now.

Maybe resubmit a revised letter to this paper and come clean on your real interest in this matter.

But I guess transparency is not your aim.

Unfreakingbelievable.

[Report]

Link

Reply

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O BCYoung posted at 12:51 pm on Fri, Jul 8, 2016.



Posts: 161

I just looked at Ketchum's Zoning Map and it appears to me the dirt is zoned L-I 1, not Commercial, which would permit retail sales. Long ago here, when zoning maps became the rule, L-I was not intended to be retail, was it....? Is that why the developer is seeking TWO Conditional Use permits, Freaking....?

If those permits are granted,by P&Z and the city council, about 24,000 gallons of high octane fuel would necessarily be stored on (or under) the property there. I think that should be a concern not only for neighbors, but for the entire community, as well, notwithstanding Ms. Reed's concern for her new home.

And, from a purely common sense perspective, ain't four gas stations here enough for a town of 2700 residents? Competition isn't an economic factor, as all four of them fix the price of fuel, anyway.

Freakin' Brilliant posted at 3:30 pm on Fri, Jul 8, 2016.



Posts: 344

Are not the other 3 gas stations all in the light industrial zone?

We have base Camp, Shell, and Texaco all within a couple of blocks of each other. This new station would be 2 blocks from the others in the L1.

If this current proposal was zoned something like residential, then I would fully support the home owners in their quest to deny the application. I personally cannot stand when zonings are completely changed in or close by a residential area.

The code has provisions for Conditional Use Permits by ordinance. All someone had to do is look at them before building. This homeowner should have done her homework.

My main objection here is someone writing about all her objections without disclosing her skin in the game. Nowhere in this editorial does it mention that the writer has a new home across the street. Why?

Maybe we should shut down the Texaco station because the Community School now has a residential facility steps away. Is that not equally as dangerous (high octane fuel storage) as this proposal?

Changing zoning creates problems in all communities. This, however, is not a zoning change, it involves conditional use, like every commercial or industrial project built in this valley in the last 50 years, and is completely within the code if passed by the P&Z.

If homework was done, this would not be an issue to begin with.

[Report]
 Link
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BCYoung



Once I had an ethics prof who taught that Logic was the most wonderful thing about reasoning - until it became illogical - as your remarks above prove. I wish you had taken her course, Freaking.

Nothing about Conditional Use Permits that result in zoning aberrations are based on Logic, nor is the existence of three fueling depots you provided in your example, where you cite that precedence should prevail over the present Zoning Code. Sec. 17.18.940, et. seq., of Ketchum's Zoning Code provides:

17.18.140: LIGHT INDUSTRIAL DISTRICT NUMBER 1 (LI-1):

A. Purpose. The LI-1 light industrial district number 1 is established as a transition area providing limited commercial service industries, limited retail, small light manufacturing, research and development, and offices related to building,

maintenance and construction and which generate little traffic from tourists and the general public.

17.18.150: LIGHT INDUSTRIAL DISTRICT NUMBER 2 (LI-2):

A. Purpose. The LI-2 light industrial district number 2 is established to provide for a permanent year round employment base and the location of light manufacturing, wholesale trade and distribution,

research and development, service industries, limited related, bulk retail and offices related to building, maintenance and construction and which generate little traffic from tourists and the general public.

17.18.160: LIGHT INDUSTRIAL DISTRICT NUMBER 3 (LI-3):

A. Purpose. The LI-3 light industrial district number 3 is established as a transition area providing for a permanent year round employment base and the location of research and development, wholesale trade and distribution and high technology industries along with offices related to building, maintenance and construction and which generate little traffic from tourists and the general public and providing a mix of deed restricted and market rate housing.

For Ketchum's approval of a major fueling station/lottery ticket purchasing site on Highway 75 near the northern entrance of the city is a violation of its own Zoning Code. It's also illogical, as it will "attract tourists and the general public." That "attraction" is the entire thrust of the permit sought.

'Nuff said.... Deny the permit.

message preview

Print comments



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Proposed

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☐ All of the above ☐ None of the above
ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org)
I disapprove of Bracken Station on Main Street. You may include my name among others who oppose this development. City of Residence <u>Fetchum</u>
Print Karuleen Williaks
Signature Lab Willes
☐ Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

Proposed

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Signature Judi Bartaerin R. Bant
Print Juloi BARTOCCINI Richard Bartoccini
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Print Duch J. Horan
Signature WWW Signature
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM
Contact info

Proposed

BRACKEN GAS STATION and CONVENIENCE STORE
MAIN STREET KETCHUM between 10th Street and East 9th Street
(opposite Knob Hill Inn on site of Lizzy's Coffee)

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Print Mattie GRISWOLD
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

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X	ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org) If approved this project would dininish the Lindict and value of my popety at Frenchmans and hurt the Idisapprove of Bracken Station on Main Street, Ketchum. You may list my name among others who oppose this development. City of Residence Ketchum Signature Print Mank Fosborg 360 E 9th St 76
	Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

Proposed

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Print Musical RETELSEA
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

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ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org) ferrible location for heavy traffic turning on off highway
I disapprove of Bracken Station on Main Street, Ketchum. You may list my name among others who oppose this development. City of Residence Ketchum Signature
Print Collin Collins
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM

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Print KEVIN WANE
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

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Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info		

Proposed

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Remind me about the next P&Z meeting. Monday. 11 July, 5:30 PM

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BRACKEN GAS STATION and CONVENIENCE STORE MAIN STREET KETCHUM between 10th Street and East 9th Street (opposite Knob Hill Inn on site of Lizzy's Coffee)

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Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM

Signature

Contact info

Print

Debra Leeds

Proposed

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W	ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org) Construction of the gas station for all residents north of town to ould give the appearance of a commercial tunde stop which is not in keeping all disapprove of Bracken Station on Main Street, Ketchum. You may list my name among others who oppose this development. City of Residence Signature GRAETE and Norah BRETALL Print GRAETE and Norah BRETALL
	Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

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Proposed

BRACKEN GAS STATION and CONVENIENCE STORE MAIN STREET KETCHUM between 10th and East 9th

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Print / i M/ 1 38 55/0
Signature
Remind me about next P&Z meeting, Monday, 11 July, 5:30 PM

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Print Landa Doon Signature
Remind me about next P&Z meeting, Monday, 11 July, 5:30 PM

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Proposed

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Print Paine Kidder
Signature
☐ Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM
Contact info

Proposed

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Signature_Barbara Gerrish'
Remind me about next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

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Signature // / / / / / / / / / / / / / / / / /
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Signature Multipliff
Remind me about next P&Z meeting, Monday, 11 July, 5:30 PM
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Print Robin R EIDSMO Signature PRI R Endones
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Signature (C)
Remind me about next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

Proposed

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All of the above None of the above
ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org) Allowing a station at this location will be hazarding to pedestrians,
the environment and the nearby homeowners tousiness inners
I disapprove of Bracken Station on Main Street, Ketchum. You may list my name
among others who spoose this development. City of Residence
Print Salvingo & Weindlaire
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info <u>Ł05al on go fa yalloo. Com</u>

Proposed

BRACKEN GAS STATION and CONVENIENCE STORE MAIN STREET KETCHUM between 10th Street and East 9th Street (opposite Knob Hill Inn on site of Lizzy's Coffee)

There are enough gas stations in Ketchum. Ketchum does not need another.
Bracken Station will be permanent, visual blight at Ketchum's Northern Gateway
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All of the above
ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org)
I disapprove of Bracken Station on Main Street, Ketchum. You may list my name among others who oppose this development. City of Residence
Print Claudia Valencia
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info \\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\

Proposed

BRACKEN GAS STATION and CONVENIENCE STORE

MAIN STREET KETCHUM between 10th Street and East 9th Street

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Print Jeanne A. Koleno
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

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All of the above None of the above
ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org) (a) this is one Block from an existing station + 3 Blks From another. (a) thus another station isn't needed
Adisapprove of Bracken Station on Main Street, Ketchum. You may list my name among others who oppose this development. City of Residence 333 Hillside DR Signature Advance
Print Rodwey Kolens
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM
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Print THIA KONIU
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

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<i>f</i>
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Print_ Um Jacols
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District the second sec
Print LUSTY WENDLAND
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info Over Alard Q Uando COM

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Print Marina Schwarzbach
Signature Marino & Mior Parch
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM

Proposed

BRACKEN GAS STATION and CONVENIENCE STORE

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All of the above None of the above			
ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org)			
disapprove of Bracken Station on Main Street, Ketchum. You may list my name among others who oppose this development. City of ResidenceSignature			
Print Pete Patter son			
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info			

Proposed

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ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org)
I disapprove of Bracken Station on Main Street. You may include my name among others who oppose this development. City of Residence
Print Elizabeth Gilman
Signature
☐ Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM
Contact info

Proposed

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I disapprove of Bracken Station on Main Street. You may include my name among others who oppose this development. City of Residence
Print ROVENT GNSWOLD
Signature 2
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

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part of the second seco
I disapprove of Bracken Station on Main Street. You may include my name among
others who oppose this development. City of Residence Brobline, MA
Print LAURAT /Vigher
Signature 3 My Muc
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

Proposed

BRACKEN GAS STATION and CONVENIENCE STORE

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ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org) 10th St 15 tro CMGESTER AS IT 18 THAT STRETCH () 12th St 15 CRAZY Swer PARKMO AT BACKWOTDS and to 12th Lavanies See a are so the Street (8 50 tog H. CRAZ)
I disapprove of Bracken Station on Main Street, Ketchum. You may list my name
among others who oppose this development. City of Residence KETCHUM Signature
The in a C Nil to ten!
Print Francise S. CHESTON
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM
Contact info 208 720 2752

Proposed

BRACKEN GAS STATION and CONVENIENCE STORE MAIN STREET KETCHUM between 10th and East 9th

☐ Bracken Station, if approved, could be sold to Exxon, Chevron, etc. and be a permanent, negative landmark at Ketchum's Northern Gateway
☐ There are enough gas stations in Ketchum. Ketchum does not need another.
Bracken Station will create a toxic environment* and health hazard for nearby Ketchum residents and workers. *(Dripped benzene, fumes, contaminated run off)
☐ Bracken Station is not in compliance with LI-1 Zoning purpose: "generate little traffic from tourists and the general public" and is in direct contradiction to the vision and policies outlined in the Ketchum's 2014 Comprehensive Plan
☐ Bracken Station's convenience store will lure children from Hemingway and YMCA, onto 10 th St. (even without additional gas station generated traffic, dangerous and congested). Bus riders using bus stop directly opposite will be lured across highway.
Bracken Station will dramatically increase traffic on North Main, 10 th Street and other adjacent streets. Station will service extra heavy trucks (dump trucks, cement trucks, tractor-trailer combinations with long hitches, 5 th wheels, snowmobile, horse and long hitched trailers, etc.) endangering pedestrians and bicyclists.
☐ Bracken Station does not meet criterion of zoning ordinance "to promote the health safety and general welfare of present and future inhabitants of Ketchum".
☐ All of the above ☐ None of the above
ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org)
I disapprove of Bracken Station on Main Street. You may include my name among others who oppose this development. City of Residence
Print Cara Stone
Signature Our Com
Remind me about next P&Z meeting, Monday, 11 July, 5:30 PM
Contact info

Proposed

BRACKEN GAS STATION and CONVENIENCE STORE
MAIN STREET KETCHUM between 10th Street and East 9th Street
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Print Amer R Christense
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

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Print Barbi Reed
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Print Destau Superior
Signature Le
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

Proposed

BRACKEN GAS STATION and CONVENIENCE STORE

MAIN STREET KETCHUM between 10th Street and East 9th Street

(opposite Knob Hill Inn on site of Lizzy's Coffee)

•
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Signature CLC C
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ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org) Drive North or South on Hwy 75 approaching a leaving Ketchum and imagine the difference an ugly gas station would make. Next? McDonalds, Walmart will I disapprove of Bracken Station on Main Street. You may include my name among others who oppose this development. City of Residence Ketchum
Print Gary Hornbuckle
Signature Affilia
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Print Kris Olenick
Signature
☐ Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

From: J. Kevin Lawler cowboyjkl@aol.com &

Subject: Returning Completed Survey Date: July 5, 2016 at 9:03 AM To: barbi@annereedgallery.com

Barbi,

See attached completed survey.

cheers, K

J. Kevin Lawler cowboyjkl@aol.com

SURVEY

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You have one already on northwood - called Jackson

SURVEY

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Contact info

Proposed

BRACKEN GAS STATION and CONVENIENCE STORE MAIN STREET KETCHUM between 10th and East 9th

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Print Dr. Roger DeBard
Signature De Band
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Contact info iglobal capital agmail, com

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Contact info

July 7, 2015 Survey Questions

- 1. Do you believe this location is appropriate for a gas station, convenience store and take-out restaurant? Please describe why in the comment section.
- 2. Do you believe there will be any long-term impacts on our community from the project at this location? Please describe in the comment section.
- 3. Should a take-out restaurant be allowed outside of the downtown core? Please describe why in the comment section.
- 4. Do you have concerns over traffic impact and safety at this location? Please describe in the comment section.
- 5. What information do you think is important to evaluate and consider in the review of this project.
- 6. Do you support or oppose this project? Please describe why in the comment section.

Submission Time	Comment	Comment	Comment	Comment	Comment	Comment
7/7/2016 8:30						
7/7/2016 10:35	Keep traffic on the highway where it belongs. Ketchum need a convenience store that is "modern", clean and has proper circulation.	All good!!! There will be very beneficial long terms gains. Enhancing the North Gate way visually is only one. There are numerous others. How about a safer entrance into town!	Bigwood Bread was allowed. Why is this different? Call it a restaurant if you want but it is a sandwich shop for motorist.	No. ITD will surely verify the safety issues.	Overall benefits to the community.Traffic impacts. Assurance that the Owner's right to develop their property as an allowed use with a CUP is protected and preserved.	This is an allowed use. If the applicant can demonstrate compliance with the City Ordinance then they should be allowed to proceed.
7/7/2016 8:47	There are gas station close by in the neighborhood. There should be better signage to let people know where the current gas stations are located if they are coming from or going to Stanley. Having a station at this location would cause traffic and safety issues.	I think it would cause safety issues with car and pedestrian traffic. Long term it will always be a concern for future businesses and surrounding businesses from the possibility of underground fuel tank leakage and possible fire safety concersn.	The restaurants who are playing by the rules will be undercut. There are plenty of food options 100 yards away and in the core to choose from.	I have a deep concern that pedestrian and car safety will be impacted. Possible back ups from cars waiting to turn into the station. Accidents involving cars and pedestrians will be a strong possibility.	Can a study be quickly done to see if better signage directing traffic to the existing gas stations in the area would have an impact on this project?	I oppose this project.
7/7/2016 8:32		Generate additional tax revenues from at least people passing through Ketchum.	Convenient for those living north of town			
7/7/2016 8:37	Not sure on what basis you wouldn't allow it.	could change traffic flow in that area, but that's for the experts to study.	but i'm really afraid of sprawl. so would i be in favor of numbers 2-5 - probably not. so, that's the hard part. and of course, i would care what the take-out restaurant was. a big chain would tarnish the image of our city in a pretty visible location, so i'd certainly be against that.		what a precedent a take-out restaurant like this might set for others who want to start building outside the core. really want to protect against sprawl.	but i'd say it's qualified. don't know enough to say i don't support it.
7/7/2016 8:45	Gas stations are difficult to find in city of Ketchum if not a local citizen.	What are the long-term ramifications of the highway and any potential for future widening.	This kind of restaurant fits better outside of downtown core, which is populated with sit-down restaurants and bars.	This will become an issue as camp trailers will be common and extend onto the highway. Parking and turning on/off the highway will only get worse.	Traffic generation and storage is biggest issue as to how it impacts the rest of the community and roadway users. The business shouldn't become a safety issue for highway users or restrict the roadway.	The site is very restricted for room. Good business idea but might not be the best location or ideal property/site.
7/7/2016 8:36			Relives congestion		It is the northern entrance to town, so it should be appropriately designed	Not right for our northern entrance to town
7/7/2016 8:46	I do not- there is another gas station right around the corner that I really like and is convenient. I like the coffee shop that is in that spot and don't think a gas station would look nice across from Knob Hill and the cemetery. There are much better places to put a gas station- like on HW75 and Broadway Run/Hospital Drive on the east side.	I dont think it will look nice on the strip of 75 that leads out of town.	I dont really know if it would be strong outside of the downtown core. There are enough places that it could go that are empty around town.	I would be concerned about traffic and how it would impact Knob Hill in a negative way	How everything will look Where the current business will go Why it has to be in that location (I dont agree with the location) What other locations would be better	I oppose the project for above reasons- there is a much better location that it could go, I don't think it would be a profitable spot. There is also a gas station walking distance from this one that is a great place.
7/7/2016 8:40	Terrible location for traffic, for the existing businesses & residences nearby, and for the beauty of the entrance to our community. And just a basic question, do we really need yet another gas station? There are at least 4 gas stations within one mile of this location.	See comment above.	Why not? We should expand outside the crowded core!	10th avenue is dangerous and already crowded. And trucks & large vehicles entering & exiting the highway will also bring more safety concerns.	Traffic studies, safety studies, visual impact at north entrance of town.	Oppose for reasons mentioned above
7/7/2016 8:39	People expect these kind of things to be at obvious places, the entrance to town qualifies as an obvious place.	I believe the long term impacts will be positive. More choices for food, gas, etc	This property appears to be directly adjacent the core.	I have faith that the city staff will vet these issues and require the necessary work be done to limit impacts.	Will this benefit the town. Is it allowed under current city laws.	Support. I think it will be an assets to the community and those visiting.
7/7/2016 8:35	It will ease congestion getting in and out of town and it is an appropriate location				future development needs to be concentrated along major traffic corridors and in the downtown core.	

7/7/2016 10:09	The location is on a state highway and in a light industrial area. This location is beneficial to recreational/tourist/travelers traffic passing through Ketchum.	I don't believe there is a long term negative impact, provided the design/architecture/layout/signage is low profile and fits into the feel of Ketchum. Landscaping is important as well.	I don't see why not, though I don't know what "take-out" restaurant is under consideration. A franchised chain? I think it would be beneficial to travelers and locals alike who are headed out of town to recreate/ or just gassing up and on the run.	Speed limits are low on this portion of Main St. / Hwy 75 and traffic is pretty light in this area I think.	Layout of the gas pumps and parking to allow for easy access in & out from Main St. With all the recreational traffic in summer and many towing trailers the space needs to be large enough to minimize parking problems and bottle necks. Building architecture Low height signage Landscaping Proper maintenance of the grounds to make the property appear "welcoming" to travelers coming into town. Appearance and design of the building is key of course.	I believe more travelers who may ordinarily just pass through, will stop in Ketchum with a "nice" facility that is located on the main drag without having to hunt for gas, to stretch their legs, pick up a drink/snack. The Sinclair located south of Ketchum is often a PIA to access. If done right the facility could promote tourism in the area with brochures and photos. By the way, I am a property owner in Ketchum.
7/7/2016 8:51	I can't imagine there will be enough room at that intersection to accommodate a gas station.	Traffic will be horrendous during construction, and I think it will still be bad afterwards.	Unless there is zoning against it.	I think this will cause flow issues and bog down the bottleneck of 75 at that location. If people try to avoid it, it just means they will blow through Warm Springs to take Saddle and avoid it.	?	poorly worded question with answer choices available
7/7/2016 8:40	This location is perfect for a gas station and	No I do not see e this robe an issue	I miss Burger Haus		I don't think this should be an issue let it	I totally support this ,we don't have this
7/7/2016 8:38	convievience store Not appropriate, too much congestion. Not in keeping with the "small mountain town " feel. Most of all a glare against our "night sky ordinance".	Terrible congestion.	No, then businesses, signs and confusion start to seep into the out laying areas. Yuk!	As above.	pass Traffic study, congestion in the Summer months especially.	kind of business in Ketchum Oppose for above reasons.
7/7/2016 8:37	Poor location	Poor location	Poor location	Poor location	Poor location	Poor location
7/7/2016 8:55	The present location is quite blah in appearance having had no upgrades in at least 40 (50?) years. The proposed is a commercial venture completely in keeping with a business use on Main St. As it is on the North side of Ketchum, most people won't see it at all; that includes tourists. If it is attractive in appearance and landscaped, it might well add to the appearance of that corner.	If a business person believes that it makes sense from the standpoint of viability and is willing to build it with available financing (lender doing its own viability study), then it is a reasonable use. I cannot see a real downside from it being there; the argument that children from the local school will frequent it is ridiculous from the start.	McDonalds in Hailey has a dine-in capability.	With appropriate traffic controls and signage I see no significant traffic impact and safety issues at this location.	Knowing how tough the Ketchum P&Z is about reviewing building permits, I feel confident that the proper review steps will answer the concerns that some citizens have raised.	It's time that we in Ketchum had a more business-friendly approach to new commercial enterprises in Ketchum. And it seems particularly important to encourage renovations and upgrades to existing businesses. The private and insular Ketchum of the 1960's and 70's is long gone. People who shun modern development can arrange it to avoid living in or spending much time in a more progressive town environment. There's always Stanley!
7/7/2016 8:41	i say no. if zoning support this usage then the location is 'appropriate' i don't know why we want a gas station when there are 4 gas stations within a mile at this end of town and one just south of town.	it will probably go out of business and there will be underground tanks left at the location	sure, why not. if someone wants to open one in industrial section why not zone for that also.		it's a neck down in that part of town and a bunch of turning in and out of that area will make getting in and out of town from the north more difficult	if zoning is appropriate it is hard to oppose with firm footing i definitely am against the idea of the project and if an exception is required, no way
7/7/2016 8:42	It is on the north end of town on the hiway and easily accessible to Ketchum visitors and residents.	It appears that opposition are Nimbysbut it is a very appropriate use and allowed by zoning	There are plenty eating establishments in the town core much more easily accessible duing busy seasons	It is on the hiwayand no different from access by 10th street light industrial or current uses on a good straight a way visibility good	If allowable by the zoning designationit should be approved. Otherwise change the zoning	support. Appropriate use for the location
7/7/2016 8:41			This location will help the workers in the industrial core. What is the difference of this location to the new Brewery (formally Globus), Base Camp or Grumpy's.		While this would the 4th gas station inside the ciy limits, 2 of them are located in the industrial center and not easy for the tourist to find. the 3rd is at the south end of town and they need competition.	

7/7/2016 8:43	This already is an area of traffic congestion and some danger for vehicles. Having more cars enter and exit Highway 75 at this point would simply increase traffic problems.	More traffic congestion coming into Ketchum from the North.	Letting such a facility be located outside the commercial core of Ketchum will inevitably lead to more urban sprawl. One of Ketchum's qualities is the non-commercial land around the core that preserves the beauty of the natural environment. Letting commercial establishments spread into this area will degrade these scenic values.		Study traffic flows around the area.	Think this is a bad location for drive in facilities.
7/7/2016 8:40	It is along the major highway out of town	Nothing negative	It would be very convenient for people living north of town	It would not make much difference.	All of the issues mentioned above are worth consideration.	We support this. We like to see growth in the community.
7/7/2016 8:53	I think for the tourist the convenience of having a gas station that is easy to find would be helpful. Especially for the people that are just passing through town on their way north or south.	Perhaps. Gas stations are not the prettiest thing to add to main street but feel because its on the out skirts of town the impact would be minimal. That being said, I really haven't thought through the negative impacts. I am sure there are many that others have thought of.	in i	The 10th street access onto and off of Hwy 75 has always been a little awkward. Adding people merging in and out of the gas station location may add more confusion or perhaps it may help.	Traffic, appearance,	If I had to choose I would say I support. But honestly, I am feeling indifferent. I live nearby and use 10th street quite regularly. Personally, I feel I have the resources already with Basecamp and the car wash, but from experience of trying to explain to an out of towner where a gas station is can be a little challenging. Although, I guess the smart phone has helped with that.
7/7/2016 8:41		it would be an easy place to stop coming or leaving town.	there is more to ketchum then just down town.		?	
7/8/2016 11:30	Visual blight: North entry to Ketchum Traffic concerns: backing up, dangerous crossing over lanes Pedestrians endangered accessing convenience store and take-out restaurant Bicyclists endandagered Do NOT need another gas station Ketchum is known to be charminga gas station at this location will serve to eliminate any charm at a very important junction		Fast food restaurant will take business away from established restaurants and	times and 10th Street is notoriously dangerous and uncomfortably packed with large landscape trucks, supply trucks and over-scale vehicles. With development of the former Anderson Lumber, 10th Street and access to and from the highway will become even more problematic. ANY business that is traffic dependent should	and Environmental hazards (both to land, ground and surface water, air pollution). Serious, documented by banks/lenders/realtors on decreased property values of residential and commercial/tourist properties adjacent. Loans questionable. Gas stations present inherent environmental and health concerns: opening another gas station when the community has enough (there are never lines at any of them!) is unconscionable. No proven study of what would happen with an earthquake (cleverly the proposed re-purposing of the existing building would not require for earthquake mitigation) No soil study to date: what soil and whether location will support hidden tanks No thorough study of the ramifications of a gas station (fumes, spillage, dripping of gas, other toxic liquids) Impingement and encroachment on existing business.	
7/7/2016 8:41	There is an existing gas station a block away, and this location serves as an entry to the City, we can do better than a gas station.	This type of business will drive down the value of surrounding businesses and make development of the adjacent business park more utilitarian	I have no concerns with a take-out restaurant in a convenient location where you don't have to fight the downtown core traffic	Existing congestion will only get worse	Is this type of development consistent with the values of the City Master Plan?	Oppose - we don't need another gas station so close to downtown core
7/7/2016 8:45	Ruins land. Likely to go out of business. Damages the scenic entryway to north end of town. Any spills will run downhill and destroy those properties.	Do we really want a gas station being the 1st thing you see when you arrive in Ketchum? Once tanks are put in it's very difficult to re develop the land in the future.	Capitalism. If the business is good people will find it. Protecting the core restaurants no matter how poorly they operate is stupid.	What location are you referring to? If gas station, yes, there is a high end hotel across the street and we encourage walking and bike traffic.	All of the above.	This isn't a yes no question. I oppose. It's pointless, a poor location, and likely to go out of business. We don't need more c stores.
7/7/2016 8:38		It will definitely affect traffic and will remove some inexpensive low impact retail and small business		I don't think safety will be affected but traffic will be slowed and congested at both the highway and Warm Springs Road.		
7/7/2016 8:41	Convenient		During peak seasons we need more food sources and getting into town and parking are huge traffic problems.		Traffic and parking downtown	We need more restaurants and take out options

7/7/2016 8:38						
7/7/2016 8:51			There isn't enough room left in the downtown core for anything, even parking.	This is close to a blind corner for northbound traffic. As well, people who lives in the multifamily building next door, on the hillside above, and stay in the one nice Kaetchum hotel across the street, don't need to be subjected to the fumes, noise, and light pollution from a large gas station. The parcel isn't large enough for parking! Think about the neighbors, for a change, please. Those gas station jobs aren't living wage positions and aren't worth ruining the property values for those	right next door. The fact that the street coming up from warm springs can become impassable in the winter due to ice on a very steep grade. The only access will be on and off the highway, which will congest that area of 75 greatly as northbound drivers wait to make a left turn. This is not the right location for a gas station/convenience	
7/7/2016 8:44	There is no need for another gas station and convenience store with the amount already in town.		As long as it isn't a major chain conglomerate.	The turning visibility and parking is not good there. Look at the parking for Grumpys and leftys already.	for future projects.	This is not a yes or no question I oppose this project as we have a lot of good restaurants and local stores already in town.
7/7/2016 9:13	It is not the P+Z's jurisdiction to make a determination on business plans, if the zoning code allows the use, it would be a lawsuit waiting to happen to turn it down.	YES!!!!!There are traffic and pedestrian access concerns, but I'm sure the business would be welcome to those traveling through.	There are lots of places where restaurants could thrive, many cities have restaurants in special little pockets outside the core, and they add to the charm.		only question, the zoning code is the final	if it is allowed per municipal code, it is not up to the citizens to decide the fate of every business/project.
7/7/2016 8:40			Why not!		If it complys with the zoning ord and the building code it should be allowed	
7/7/2016 8:55	I would need to see more details to answer.	There are always long term impacts. I believe the proposal is a result of a lack of foresight by the leadership to rewrite the zoning code for the LI districts.	I think that part of it is fine. There are some businesses that make sense to remove from the core. Pick-up businesses like a take out restaurant and businesses that eat the parking up are the prime ones. A grocery store would be another.	intersection there. With Ketchum Auto and	Traffic flow, speed limits, pedestrian traffic, etc	I don't have enough info right now, but it would take some changes to that road to make this work.
7/7/2016 8:48			Would reduce traffic in the downtown core, which is becoming overwhelming during the peak summer months.	, ,	gas station at the bottom of 10th street	My yes vote of support should be interpreted more as a statement that I would not oppose it.
7/7/2016 8:46		Traffic hazard and limited visibility	But at a gas station.????	See above	Traffic pattern on 75 and turn traffic going north onto 75 from 10th street	Do not support

	The station is located on Main Street, north of the primary "downtown core" area, which means that traffic coming in to town will not need to navigate side streets to find fuel. It's located on the west side of the street, which is most appropriate for vehicles coming south from the Sawtooth.	Increased LOT revenue.	Spreading some of the commercial uses (including small restaurants and stores) into other areas of town will promote more walkability, ease of access, and alleviate perceived impacts on parking in the core. I'd like to see MORE restaurants and stores in other neighborhoods in town.	Left turns from north bound traffic must be mitigated with turn lanes and/or signaling.	Planning and zoning decisions are quasi- judicial and/or ministerial. They are based on the existing code and the applicability of the proposed use to the code. I think that polling the community to determine whether or not a service station is an appropriate use is a terrible idea and sets a bad precedent for land use decisions. Polling the at-large citizenry for a decision might also actually abridge the applicant of their due process rights and constitute a violation under the 5th and 14th Amendments. You have a noticing procedure in place, and unless the applicant specifically proposed this method (or agreed to it) then you're in dangerous legal waters. Also, you're just asking if people like these ideas. I don't think you're asking the right questions or providing the right information. Is this permitted at the current location? What specific zoning approvals are being requested?	I support the right of property owners to maximize the use of their property (within the legal confines of the code) as they envision.
7/7/2016 8:46					Public benefits by another gas station & affordable dining option for working people	
7/7/2016 8:51	Its on the highway and is very convenient for tourists.	More taxes from tourism passing through town.	Yes. Collect money from traffic passing through town.	It might lessen impacts of tourists trying to get off the highway at Main and SV Road going to Atkinsons. So it would be a good thing.	What is better for the greater good as opposed to NIMBY's?	More competition is always better for any town.
7/7/2016 8:54	Not sure about the restaurant. It is convenient for gas station		Not sure should be a choice. I have no educated opinion on yes or no	If 10th becomes a problem then put in a light	if the development is well done I see no issues or conflicts of this site	
7/7/2016 8:50	There is one already around the corner and a convenience store down main street		There is already in west Ketchum.		Redundant type of businesses to close together.	Oppose- not necessary to put so many in such close proximity to each other.
	There already are 2 gas station convenience stores within blocks of this location. Do we really need another?	Can't the property be put to much better use with community housing for example?	If maybe were a possible answer I would have chosen that. I don't know what the zoning is on take out restaurants. I myself would be open to take out restaurants outside the downtown core.	I do have concerns regarding traffic and safety at this location.	I think we should consider what does the community as a whole need most to help us be sustainable. How will this business contribute to community needs. Will quality of life improve for our community with this specific business.	Any chance of the space being retrofitted
7/7/2016 8:47		Negative			Character of the city	Oppose project
	It is only if we want to turn Ketchum into a clone of every other "strip town along the Highway". We have a gas station within about a block, and a convenience store a block south. I have no problem with a takeout restaurant. Tourists can be notified of both by a discreet blue highway sign.	We will lose our current character as a quaint western town and become like every other town as you drive down a highway with gas stations the mandatory introduction with their ubiquitous signs hawking prices.	Restaurants are relatively discreet store fronts. That area already has storefronts and this would have little if any impact-unless it is a "fast food joint" with a huge parking lot.	Shouldn't. Though some might make a left turn across the highway, the turn to Warm Springs is little different and the speed limit is already at the city standard.	Why make waivers for competition for stores already here, and particularly where to do so changes the character of our town.	
	The city does not need this business. We have plenty of gas stations and convenience stores.	It adds no growth or cultural value	The downtown core is already over crowded.		what does it add to the growth or cultural value of the community ?	No growth or cultural value
	the proposed project is between 19th street which is a mixed use of light industrial and commercial, and community housing to the south. across from the project is residential housing and a hotel. the project does not fit in this neighborhood.	improper use of the property	parking is easier. I have seen this use of property outside of other cities core. it is very successful and it works.	this is a very difficult intersection. the heavy traffic generated by the proposed use would exasperate and already dangerous situation	look at the surrounding uses. and the intersection.	no I do not support this project for all of the reasons I have already stated

7/7/2016 8:49					Does it serve the community? What is the benefit to residents and visits? Is it consistent with the Comp Plan?	Support - Economic development is always a good thing in a tourism based economy.
7/7/2016 9:08	I believe there could be too gas stations/convenience stores in this area of Ketchum however it is for investors to determine this. If someone wants to put up the money and invest here then I'm for this. I have no view on the environmental permitting side of this as many have more knowledge than I do on this aspect.	As touched on above, I will be curious as to how the other convenience stores/ gas stations will survive competition creates better businesses.	yes, as a West Ketchum resident - our family is always looking for a good, inexpensive takeout place. Current places La Cabanita, KBs, Lefties, etc get old. Places like the Pio, Ketchum Grill, etc are more dine in restaurants than takeout for the price.	Safety would be environmental - I would leave this up to a professional to determine. Traffic impact - I don't foresee this as a problem if possibly the business puts in the correct infrastructure - such as sidewalks and other small items deemed to be part of current new remodel/building codes a business would pay for. As a Ketchum resident - I would be for Ketchum taxpayers paying a small amount to increase the desirability of town keeping in mind the tax rev. coming from this new business	As I didn't have enough characters above - I would like to add the only other issue I could foresee would be visibility coming up 10th street looking south I can't picture exactly how far off of 10th street this location is and if this would be a possible traffic issue.	I'm always for new business with little red tape.
7/7/2016 8:56	The edge of town is a more appropriate place for gas stations then the middle. If that's an allowed use you can let the market decide if it succeeds or fails.	They may be good or bad.	By definition a take out restaurant requires driving and parking. These are appropriate for out of the core to minimize traffic.	No more so that anywhere else	Does the new business comply with current zoning laws and regulations. This should not be an arbitrary decision. Seems to me it does.	If someone wants to open a business in this town the government's job should be to help them do it. Not impede their efforts.
7/7/2016 8:47	Not enough information to form an informed point of view.				What gas station chain is this, what convenience store and what kind of take- out restaurant? Who are the brands and people behind these, how capable are they of running and funding successful businesses, what precedent do any of them establish, if any, and what is the design and look of their structures? Your questionnaire is so lacking in specifics I hope you don't draw any conclusions from it.	
7/7/2016 8:58			Having worked here for 30 years , I would appreciate the ability to get something to go on my way home.	A stop light might be appropriate. There are two side streets at that location. Views might be blocked.	I would ask Barbie Reed what she feels about it. She is building the house on the hill across the street. I am pretty sure it will affect her. Hours of operation will be an issue There are no gas stations on Main Street. When I moved here there were 3. It would be easier for tourists to find. It might affect the gas station at the corner in Warm Springs.	
7/7/2016 9:04	It's on the highway.		A few conditions should exist: allowed commercial kitchen, allowed retail, available parking.		The use is allowed on the private property. Traffic, etc is allowed on public streets.	Opposing neighbor is building her house on a highway, not in a quiet residential neighborhood. She should expect highway traffic. The current existing businesses also generate short traffic visits. There may be less traffic with the new business than now exists, the same, or more. Traffic should not be an issue in a business zoned lot. If the project fits design review requirements, it should pass.

7/7/2016 9:00	From a general perspective, yes, I think this seems to fit in with the surrounding businesses, and from a cursory review of the zoning map (http://www.ketchumidaho.org/documentcenter/view/4386) it seems appropriate, although further review of each zone's code should be reviewed.	There are always long term impacts; that's what the planning process is about, weighing the benefits and the drawbacks to reach a conclusion.	Yes, I understand that we want to keep businesses in our downtown core, but I wonder if the comprehensive plan has given sufficient space in that downtown core, or mixed use adjacent to downtown to allow businesses to come in and prosper.	This is already a difficult intersection - make sure not to make it any worse.	* Is there sufficient space for the businesses that are currently in this area to re-locate? How many of these current businesses will it displace? * Traffic patterns - this is already a difficult intersection - make sure not to make it any worse * Regular planning discussion such as zoning, design, etc.	It's a hard to tell with out seeing plans & only brief descriptions, but overall I think it is worth considering. As a resident with a background in landscape architecture & planning, I think this format for discussion may not be great. I know many people do not attend city meetings, but what's missing from this format is greater context of the project and discussion around it. Hopefully these survey results are weighted accordingly. Links to the plans & zoning would be helpful.
7/8/2016 18:00	Traffic will be backed up Pedestrian safety in any of these streets bordering but th sides of Main Street at that intersection. Difficulty in driving visibility for turns left or right off or onto 10th. Take out restaurant deli could revert to fast food chain Alcohol- beer/ liquor & cigarettes is the BIG cash cow. Inadequate parking for those needing walk in to the store Encouragement of a "well designed business mountain complex" would help young entry enterpreneurs into this valley.	Stated above. Pedestrian safety Air quality control Traffic deterrent	Stated We have plenty in downtown and in other parts.	will undoubtedly back up traffic	will jeapordize	Too many reasons stated above, but mostly because I think the studies have been inadequate. Much more research should be presented and openly reported by the city and the local papers. In addition, why did it take the City until yesterday morning to openly distribute this survey? Especially nearing within two business days of reaching any Ketchum resident who might be here on Friday or Monday? Why not have announced after the last meeting, that there will be a survey created, and emailed out?
7/7/2016 9:10	It makes sense, especially for tourists/travelers who don't know where to find a gas station when traveling through town. Although, I am very very concerned about the traffic flow in this area, and cars coming and going. There needs to be a very clear and marked in and out. (not like at mountain view, where you have multiple cars trying to get on the hwy in multiple spots at the same time.)	yes, it might make it a little more difficult for people who work in the industrial area to get to and from work - if there is an increase in traffic from the gas station.	absolutely!!! this notion that the downtown will suffer if restaurants - or grocery stores!!!! - are allowed outside of the downtown core is just ridiculous. the town has grown, and there is plenty of business to go around. Furthermore, I already choose not to go into downtown and eat during peak times b/c it's too busy or too difficult to find parking - so those downtown places already don't have my business! seriously, it's time to expand out of downtown!	at this location. I am worried it will be a bit chaotic. Too many people turning off and on the hwy - either into the gas station or onto 10th, or into Knob Hill, or into Fisher/St. Francis, etc. It really does worry me!	The most important is definitely the traffic flow, and any safety issues involving the extra traffic. I noticed the last question is about where I live. I think you were remiss and should have also asked where I work too. I work in the industrial area and drive pass this location 4 times a day!!!! I feel like my answers will be given less weight, as I am not a Ketchum voter. That kind of upsets me! As this will impact me on a daily basis, much more than a lot of residents.	Overall, Yes, but am concerned about the extra traffic.
7/7/2016 8:55					type of food service on offer.	I support.
7/7/2016 8:57	impact on traffic	poor visual to entry to town from the north	too much traffic		need for additional gas station	poorly worded. Oppose
	The intersection is already somewhat hazardous because of the 10th Street grade in winter. During the winter/summer peak afternoon traffic hours, traffic is backed up on HWY 75 going south all the way back to 10th Street. There are already gas stations available, most of which are underutilized for much of the day; and there are no shortages of convenience stores and takeout food locations. It is hard to see how Ketchum's economy will be bolstered by shifting sales from existing businesses.		As a matter of economic principle, the location of take-out places should be left to the marketplace and its customers, All other things being equal, let customers decide if they want to support take-out places outside of the core.		Will there be any negative traffic impacts. Will the project enhance the immediate area and make it more of an attractive entrance to the City. Will the project negatively affect other businesses and residents in the area. Does the project comply with the goals of the Comp Plan.	For the reasons provided in the previous questions, the project seems to create more disadvantages than improvements for the City.
7/7/2016 8:58	Conveniently located on hwy @ edge of townfrankly you have allowed so much development why would this be an issue		Again this should not be an issueother restaurants in the area	It's a traffic nightmare in Ketchum during high seasonthis location takes some of the pressure off core	I think theses questions I am answering are good ones.	We could definitely use another gas station located on edge of town for coming and going

	This is the main road that people experience		I don't necessarily have a problem with a	More people entering and exiting Main St	Traffic impact, safety concerns, making sure	I think Ketchum should be very careful
	Ketchum on. I would prefer that less aesthetic uses be off of main st.		restaurant outside the core, but I think it is better to have them in the commercial areas that have already been established.	seems like it will have a negative impact on traffic and cause more accident risk.		about the perception created as people drive t.hrough on Main St. It is our front door and we want it to show as well as possible. Also traffic on Main St can already get backed up at certain times of day so I hesitate to do anything that will make it worse.
	Yes. Nearby residents will always resist any change and often resort to the "good idea, wrong place" reasoning. Think about your travels. Gas stations and convenience stores are often the first thing you see. It is quite common.	Actually, my answer is probably yes. But I think you are really asking about negative impacts. I don't think there will be long-term negative impacts. I do, however, think there could be long-term positive impacts in terms of choice and variety for our residents and visitors alike.		"Concerns" can always be mitigated.	other things matter to the P&Z or council,	Support. I think it meets the standards in the code and that any negative impacts can be properly mitigated. These mitigation efforts should be part of the conditions of approval.
	There are no gas stations on main street. We need one.	Much better than what is there now.	there are workers and businesses outside of the downtown core.		Is it valuable to the public. Will it create jobs. Is it allowed by zoning.	Fully support.
7/7/2016 9:01	The corner of 10th st and Main st is very strange and busy. There is chronic speeding and too many traffic directions as is. Adding		Just more traffic.	Yes, as noted in answer one, this corner has heavy car, bike and pedestrian volume and is extremely dangerous as is. There is no safe travel down 10th st to hemingway.	Impact on adjacent properties. What will happen to all of the properties on Leadville? Values could be affected even more. Businesses may not be able to acquire lending without EPA studies on these LI zoned lots.	I do not support this project.
	Housing is too close by and it already is too congested of an area. I would like to see that parcel be redeveloped for small businesses that need road frontage for consumers similar to the Wood River Lock Shop. There are no other areas left for thiese type of businesses. If the new owner of what was the Clarion ever redevelops that land, all those businesses will probably go away that are there now. We need that type of small commercial space.	Light pollution for one. Noise. It would also be a nice parcel for a middle income condo project if it had underground parking. The views there are spectacular. A waste for a place to pump gas. I buy gas at the Sun Valley Car Wash and have never had to wait in line. Maybe a gas next right sign could be placed on the hwy leading people to the gas station across from Bigwood Bread.	My guess is that it would be a fast food chain. The joy if coming to Ketchum is that we don't have those. I am opposed to it.	It is a congested area already with people slowing down from being on Hwy 75. It is already difficult at times to enter the Hwy from 10th St. More noise for either Knob Hill or The irher hotel won't be good either.	Esthetics, noise, best use of property, traffic, it really is the north gateway to our community. Let's keep it special.	I oppse for all reasons stated in previos question.
	it is important to consider first impressions. we need to provide services for visitors and locals but having their first impression of town be a convenience store does not tell our story as a small mountain town that wants to encourage adventure and discovery.				how to balance convienience, service needs while protecting the qualities and flavor that makes Ketchum a unique mountain experience.	
7/7/2016 8:58			Parking	Not really	If it can stay in business, we don't need another vacant building	Nice to have options
	LEt the free economy work. If someone is willing to make a business and put effort into the business there is no reason why ketchum should stop it.	Let the economy figure it out. Either it is profitable and it stays or not.	yes. With the precident being set with Big Wood Bread, you can not stop it now. Not everyone wants to go downtown to eat	put a round a bout in to slow traffic down but to keep the flow of traffic moving.	not sure	support. more options, more jobs, more better.
7/7/2016 8:57 7/7/2016 8:57						
7/7/2016 9:08		Will cause more traffic slowdowns along main street.	Contributes to the on-going sprawl of commercialism in the valley.	Will contribute to more traffic slowdowns.	Traffic density on main street during peak seasons and times of day.	Opposed based on my comments for questions 1-5.
7/7/2016 9:00	eyesore along main street.	main street. Too close to trail creek. What about tank leakage?	commercialism in the valley.	The road will need to be widened. Who pays for this?	Do we need another gas station? What about he proposed station at the north end of town? If both went through we would have 5 gas stations between Ketchum and sun valley. Seems like overkill.	questiviis 1-5.
7/7/2016 9:07			don,t start the strip mall type development. Ketchum is too classy for that type of business.		does the current zoning laws allow for a yes or no answer. if notwhy not.	poorly worded question

7/7/2016 9:08	stores down in the industrial district and also the one south of town near the Elkhorn light?	Increase congestion at that difficult place on Main St. near the Warm Springs turnoff. Will probably necessitate another traffic light. Reduce aesthetic appeal of Main St.	and so another restaurant just adds yet	Parking needs, pedestrians crossing the road, more congestion, turning vehiclesall these will increase at that spot, with potential for danger as well as inconvenience for those transiting through Ketchum.	Do we really need more tax benefit from this project, at the price of the quality of life and charm of our little town? That is the fundamental question.	I vote NO on this project for all the reasons I have stated above. Does not add any new service that is not already covered in town. Detracts aesthetically from the town main street. Detracts from the quality of life for those who live here and will have to deal with it. Main add more cost to town if a new traffic light is required. Tax benefit does not outweigh the negatives.
7/7/2016 9:03	It is the wrong business at the gateway to town	It will adversely affect traffic circulation and in particular bicycle and pedestrian traffic.		see above	aesthetics of the building and traffic circulation.	This question is flawed. I oppose the project for the reasons described above.
7/7/2016 10:48	Unsightly. Unnecessary.	Types of establishments could continue and grow the community in a way that is not in alignment with our positive development.	It is only the beginning.		Ketchum's future development.	
7/7/2016 9:03		Traffic	I support a take-out option, but am concerned about this location.		Traffic and parking are a major concern.	Oppose, concerns above.
7/7/2016 9:05	The north end of town is quieter therefore is a good location for such services for locals and guests to gas up, get food etc before embarking on touring up north	Obviously any business creates activity in and near it's location, however we are a seasonal tourist town and frankly are lacking in some of these thingsand we have been busy trying to grow this town economicallywhy wouldn't we allow this.	Againconvenience at edge of town since parking in town continues to be a nightmare	There are always concerns over traffic and safety where cars are involved. This location however has little pedestrian activity as it's on the highway.	Time to move on	Building our services and economy in our community
7/7/2016 9:01					Aesthetics	
7/7/2016 9:00	it's the gateway to Ketchum NOT BLUE LAKES BLVD	The industrial park and Warm Springs already have 3 fuel stations. This area should be for "softer" street design and appeal. Even residential.	Anything but a gas station.	10th St. is already an unsafe intersection due to line of sight issues		It is the wrong business or development for this location
7/7/2016 9:08	This is a narrow corridor of space and seems that there would be too many possibilities for accidents, drivers pulling in and out. Also there isn't a proper sidewalk between Backwoods / Clarion and 10th street, don't think this is safe.	Same as above, it will turn into too much clutter in a small area. Not opposed to the idea, just the location seems problematic.	A take out restaurant is good outside the downtown core because in the busy seasons driving into town is a crowded pain in the ass. But the proposed location is still not outside of the core enough, it would cause a lot of traffic in a space that cannot handle so many ins and outs.	Yes, too small a space for a bunch of frequent stops and maneuvering for gas. That problem already exists on that blind corner of Basecamp gas station down on Warm Springs, which is very unsafe.	The way people drive in ketchum, especially tourists who think they can drive fast and zip in and out. This location would bottleneck the most direct route in and out of town and is unsafe.	Oppose for the reasons listed above
7/7/2016 9:08	It's,to narrow and congested	See above	Provided its in the right location	Already unsafe	Detailed map and rendering	Oppose for reasons above
7/7/2016 9:17			Take-out restaurant yes, convenience store and gas station no.		Have you ever seen a gas station/convenience store/fast food restaurant that was aesthetically pleasing, or didn't attract a seedy element? Would you want this project near your house? Answer to both questions is NO. We don't need this project and it's not an improvement to our town. No current shortage of available gas. Atkinsons IS "convenient". Take out food yes but far more demand for this product is in the core than at the entrance to the town.	See #5
7/7/2016 9:06					This survey is the right action	
7/7/2016 9:29	State Highway 75 is the perfect place for a gas station.	Easier for visitors traveling to and from the north to fuel up.	Its not a take out restaurant. Misleading description meant to encourage opposition.	State highways are meant for heavy public use.	5th Amendment of the U.S. Constitution. The owner has a right to develop his property appropriately.	Location appropriate. Use appropriate. Will improve the appearance of the site.
7/7/2016 9:10			Not in residential, but You've already allowed Bigwood Bread in the LI, so this is appropriate.		Does it meet zoning requirements? It's a simple yes or no	I think a gas station on main is needed. The LI can be difficult to navigate for tourists.

7/7/2016 9:27 7/7/2016 9:11	It doesn't matter what I think. It is either a permitted use in the zone or not. I am being forced to answer each question with a yes or no. There should be a tab that says I need more information. This is like chasing rumors with no facts. I am going to answer yes on all but would have preferred another choice. I know what's there now isn't great - but a	What is a long term impact. Positive? Negative? Everything built has long term impact. That will move us closer to a Park City vibe -	If it is specifically allowable in the zone or if there is a planning mechanism that allows it. Public opinion is not how you execute planning. I still can't believe the city allowed the Big	Concerns? Do a traffic study if the planning commission has concerns. How would I know the traffic engineering that gives rise to legitimate concern.	The same information you would consistently request from any applicant on any application. No more and no less. This survey should not be considered when making the decision. Who is benefiting?	Is that yes I support or yes I oppose. You did not ask a yes or no question. To ask that question without providing detailed information to uninformed people gets you a guaranteed flawed answer. Have the staff, the P&Z and the Council perform their functions. Good and proper planning is not a popularity contest and this Survey is terribly inappropriate. You also have no idea of the personal or self serving agendas behind the answers.
	gas station at the northern entrance to Ketchum is definitely not an improvement. Also - the traffic is something you really have to pay attention to there - cars going in and out of a gas station will make the situation worse.	not a small town one.	Wood Bread restaurant in the industrial section.			
7/7/2016 9:11	On main street and basically out of town	Commercial growth	Would relieve congestion in the core.	I believe that the drivers will be able to see and be able to navigate the traffic.	Whether it is allowed in that zoning	Increase in commercial activity
7/7/2016 9:10		There will be severe traffic issues, there is already a gas station 1 block away Basecamp			There is already enough info, it does not work there	Makes no sense, see above
7/7/2016 9:11	Visitors to town may not know where off- highway gas statins are located. I do think it is important that it be designed attractively, as it is the entrance to Ketchum.	Anything at the entrance to Ketchum will have an impact. Appropriate design is critical here.	Seems fine as long as it's not in a residential area.		As noted earlier, design is key.	Do you support or oppose this project cannot be answered with yes or not. I have no strong opinion one way or the other.
7/7/2016 9:26	Great location for business such as that without clogging up the downtown area. A gas station is needed on Hwy 75 corridor on this side of town and this would be out of the downtown area.	Long term positive impact of providing community with needed services for community as well as people passing through the area that may not know about the gas station on Warm Springs Road.	People passing through the area that might not otherwise stop, would be captured which would bring further positive economic flow into the community. It would probably be more affordable than the downtown "take out", which would provide needed service for construction workers as well as working locals who do not have access to affordable take out food.	That should be part of the planning and presentation to City of the potential business.	Parking and traffic flow. The City should promote business, especially with the new hotel and another proposed hotel, this would be support service for those who drive into Ketchum and would need gas, etc. Also the employees of these hotels and other businesses need more affordable eating establishments. Why would the City be against this?	It brings positive economic impact to the community and provides a needed service for people who want a "quick stop". If the take out is quality and affordable, which I understand it is, why would you be against it. It provides a service. Everything in town should not be high end. It is good to have a mix. You want "affordable housing". You should also be accepting of a service such as this that would provide affordable take out food.
7/7/2016 9:11		Wrong look at north entrance to city core	Does not have to be at this location.		location, location	
7/7/2016 9:14	Gateway entrance to Ketchum and would be unsightly, and present traffic and noise to the area.	It will take away from the aesthetic north entrance to town and is not needed	Keep business in the core	10th street will impact the additional traffic on the hwy and add a dangerous intersection where it is not needed	Traffic study and overall impact to the city	For reasons stated above
7/7/2016 9:10 7/7/2016 9:13	We definitely need more gas stations, especially north of town.		Nice to not always have to go into town for take out.		We need another gas station, especially north. Nice to get food without going into town.	
7/7/2016 9:16	It would be an inappropriate way to welcome visitors. There are also traffic concerns, particularly with trucks using 10th street.		Some already exist, and they seem to be good for all concerned		All relevant information.	I oppose it for aesthetic and safety reasons.
7/7/2016 9:26	The core of the city needs to expand and allowing places like that into light industrial is exactly how that will happen.	It will hopefully get more retail type establishments in that area. Much like Big Wood Bread and Grumpys.	Why not? There are not enough locations in the core for new restaurants or new retail locations in the core, we must expand.		Our town is growing and new business want to start, currently in the core there are no locations for these new businesses. We must expand the retail area to fill these needs.	Support this. We need growth!

	Easy access in and out from Hwy 75 especially for camp trailors and RV's. Will give competition to other local gas stations and convenience stores towards competitive pricing. Look at the impact the new country store south valley has done. Everybody goes there including myself fuel is cheaper. Every Resort town that I have been to has gas/convenience store on both ends of town with easy access that get visitors to stop and potentially explore town, instead they pass through to Bellevue.	It will Get people to stop and spend money in Ketchum.	Why not there are plenty of take-out or sit in restaurants outside the core. and they probably to better than the ones in the the core because of access and parking. Why does everything have to be within the core. Some of the best restaurants and eateries are off the beaten path. We need to start thinking out of the core and look at Ketchum as a whole. I live here and would rather stay out of the core as much as possible due to congestion.	No, The north end of Town is not as hectic as the south. The hotel traffic will probably have a greater impact than the station north of town.	That it is has the appeal factor of being the gateway to the Sawtooths and represents what Ketchum is about. I travel a lot and I always stop at the same Stations/convenience stores for that reason.	We need to open are wings more and start trying to get people to stop and enjoy what we have to offer. We can't keep putting up a brick wall to growth especially outside the core. Our goal is to get tourists to stop here not Bellevue due to accessibility of a station for campers and trailers. Also it is in a location that people can hit up a handful of outdoor shops within walking distance.
	BigNo-A convenience store gas station the first "welcome" that people see entering town and among the last leaving? The location is at the top of a hill that carries quite a bit of traffic but poor visibility. Adding a gas station with steady in/out traffic would not be appropriate for traffic flow. The scope and type of lighting necessary at gas pumps & convenience stores is not fitting for that location when alternatives can be small shops and services or other more agreeable options.	The appearance and atmosphere of a quick stop is not appropriate for entering and leaving a community that has worked hard to have a sense of small town charm. It would be possible to have a sign entering town that depicts the current 4 options for gasoline both in the light industrial, the bottom of the hill at 10th street and 1 mile south of ketchum. Ketchum needs to address traffic flow at the bottom of 10 before a new gas station "quick stop" is built at the top of the hill. See other notes	It depends what you mean by a "take out" restaurantthe devil is in the details, what type, franchised? or more like Bigwood Grill, Kb's etc. The community has worked hard to not have big box, macDonalds or Starbucks chains in facilities that do not match the feel of the community. See comments below about visitors remarks how Ketchum has a great feel and why	Mentioned above, the hill and turning in/out as well as other businesses around and other streets.		of the Anderson Lumber parcel. We are trying to be a forward thinking community in terms of all aspects of resiliency. A "get it and go" is not desirable
	This would be the first gas for drivers from the north since leaving Smiley Creek	Traffic I do not foresee long term negative impacts.	This is a good location because it would not require parking on a city street.	Visibility in both directions is good	Traffic safety. Parking for persons patronizing the convenience store.	Who authored this question? Of course the answer is yes. What you want to know is which of those two alternatives do I believe. I support the project.
	too much conjestion; an eyesore coming into the city; other gas stations are available (but for tourists, would be good to ensure they know where they are)	I think it will spawn sprawl to the north	sprawn is a detriment to the cohesiveness of Ketchum			
	The safety issue of increased complexity of traffic flow.	a better use of the property would be affordable housing	perhaps, on a case by case basis but never in a location such as this with serious traffic safety concerns This is a required question. Please select an item below to continue. (SRES_001)	this intersection is quite busy and to attract more traffic next to it would be less safe than the existing intersection	safety other options for development such as employee housing	Yes, I oppose this project this question was poorly written and I believe that surveys such as this should offer the choice "undecided" to insure an accurate answer and to have required an answer is bad This is a required question. Please select an item below to continue. (SRES_001)
7/7/2016 9:21 7/7/2016 9:19				The 10th Street corridor and intersection with Warm Springs is dangerous and the City has failed to address the need for a pedestrian corridor. This would be a great time to provide a great solution.	See above	

7/7/2016 9:24	We have no visible gas stations on Main	No more than any other business.	Depends on what type of restaurant.	traffic study has been completed.	Local residence point of view.	We need visible gas stations on Main Street.
7/7/2016 9:21	Street.		Access and parking		Traffic flow	
7/7/2016 9:19			Access and parking		Traffic flow	
7/7/2016 10:11	makes it easier for people traveling to find gas since you have to search for gas stations in ketchum if you don't know where they are.	more commuters due to gas station attendants being a low income job.	it kind of kills the kethchum vibe also if it hs late hours people would go there after bars etc and possibly be a safety concern.	the hours of operation seem like an issue. if its open late and is a take out restaurant it would encourage late night foot traffic to a not well lit area. It also	hours of operation. impact from another job for commuters or more affordable housing for employees.	just be cautious
7/7/2016 9:20	There is a gas station 2 blocks away, at 10th and Warm Springs Road, so does it make sense to put in another station so close? I'm concerned about the lighting after dark. I'm also not wild about another corporate fast food chain coming into town, as one of the unique things about Ketchum is it's local establishments.	There will be long term impacts, but what those are will not be fully appreciated until it's built. It is the entry to our community from the north, so is that what we want people to see? How big will it be?		Could be a problem depending on the size. There is no middle lane for left hand turns and that could be an issue.	Size of the project, aesthetics, traffic concerns, lighting	
7/7/2016 9:33	easy access. gas is always an issue in ketchum. NO NEON SIGNS!	Planning and design should be highly regulated by the City development like this can either improve flow and experience, or turn the entrance to town into a carlot with neon signs and energy drink marketing banners make it classy	why the hell not. Almost everybody that works in that town has to drive the highway to get there.		Make it classy. Make it something that fits	AS LONG AS THERE ARE NO NEON SIGNS AND NO ENERGY DRINK POSTERS. PICKLED EGGS AT THE REGISTER ARE A MUST. GROWLER FILLING STATIONS WOULD BE NICE. HOPEFULLY THEY MAKE A MEAN POUTINE.
7/7/2016 9:29	This location is zoned for this use and is on the main highway out of town. It is not in the core area and would be easy for travelers as well as residents to access without having to clog up side streets.	This will give the residents another option for fueling without causing unnecessary side street traffic congestion.	The core area has limited parking opportunities and is already too congested.	This area has less traffic than most other locations in the city.	needs of travelers as well as residents.	I am pro business and think that the City should support new opportunities instead of being non-growth obstructionists.
7/7/2016 9:23	I think it's a really convenient location, but not in the walking core.		There are a number of restaurants outside of the downtown core and they are often more convenient—especially for takeout.			This is a project that would actually be beneficial to residents and tourists alike.
7/7/2016 9:28			Would be nice to have affordable food, maybe it will even be open on Sundays!		Folks coming through town would love it. Ketchum is not very inviting to travelers.	
7/7/2016 9:31		Just more convenient for travelers to get gas and provisions coming from the North.	Bigwood bread has the same thing going in the industrial park.	Should probably be a center turning lane, with some kind of traffic calming, but this is long overdue anyway.		They probably have the right to do this. Ketchum just needs to control how it is developed.
7/7/2016 9:33					Traffic safety, aesthetics of the proposal to fit within the overall community vision. Will this draw more travelers into the City or will this create an avenue that draws support/customer dollars away from local, downtown businesses? Is the proposed location going to create a burden, or require substantial changes in water/sewer and other necessary infrastructure?	
7/7/2016 9:36	If you think about locations it is on the highway and the last stop to pick up supplies and fuel before heading north.	If you look at convenience for our tourist population there is only one gas and convenience store in the city. It is also difficult to get in and out of that one store.	We are a growing community as are our needs. With parking in our city not easy to come by it would be nice to have other options.	As long as proper studies and speed limits are enforced I think it should be fine.		I think it's a benefit for both residents and tourist to have a place to grab last minute supplies, fuel, and s'mores:) before going camping!

7/7/2016 10:51	This location is appropriately zoned and is easily accessible without adding significant traffic volumes to local streets.	I believe this project will improve the now uncontrolled vehicular access at this site and will improve pedestrian safety in the area. Traffic movements for entering and exiting vehicles in the existing site configuration are not standard. Nonstandard intersections where drivers cannot be anticipated are dangerous.	The proposed deli should be allowed in the Light Industrial zone. The City of Ketchum has already allowed the development of a take-out restaurant in the Light Industrial zone. Bigwood Bread is an existing take-out restaurant located in the heart of the light industrial zone. The proposed deli location would be adjacent to the Community Core. The deli would also replace and existing take-out food service establishment, Lizzy's Coffee.	The Idaho Transportation Department and the City Of Ketchum engineering departments have approved the preliminary design. Professionals have provided a safe and functional design. With the addition of the left turn lane and new-found direction for vehicles entering and exiting the site, traffic safety will be improved.	This project satisfies the existing zoning. All requests by the Planning & Zoning Commission have been addressed. The Idaho Transportation Department and the City of Ketchum comments have been addressed and all governing codes are being met.	I support this project because it meets the current zoning code, improves vehicular and pedestrian safety at the site, and will enhance the property. The current building configuration encroaches upon state right-of-way and and is poorly maintained. The addition of Bracken Station would greatly improve the state of the property.
7/7/2016 9:34	These facilities are usually rather "tacky" in appearance. So unless their plans are visually pleasant, and have "arrival appeal" to Ketchum, it would be better not to approve this application.	Initially, yes. Long-range, no.	Traffic problems.	Would tend to be heavy for both parking and traffic at certain times of the day/evening.	That is for P&Z to judge.	See above. This question is poorly posed.
7/7/2016 9:32 7/7/2016 9:37	I think appropriate but will cause a lot of	Traffic issues	Competition is good for everyone	See above	Traffic specifically and relocation of current	If the right research is done. Open to new
7,7,2010 3.37	traffic issues on hwy	The includes	compension is good for everyone		business tenants	business in our community thriving
7/7/2016 9:39	No. It brings garish light and un-needed traffic to a location that is essentially a scenic gateway.	Convenience stores are unsightly. Their lighting is garish and stays on all night, especially at gas pumps. Convenience stores generate trash and recyclables that do not make it to receptacles.	Take-out restaurants are convenient for workers and others who need a quick meal. As long as the location is managed and appropriate. But fast food generates trash and recyclables that need to be managed.		What light will be cast from the inside of the store? what light will be cast from the outside? In the winter time light reflects much more from snow. what will the signage look like? internally lit signs are a BLIGHT for anyone that can see them, for many hundreds of yards in every direction.	will look tawdry and tarnish the scenic entry to town.
7/7/2016 10:08	That intersection is VERY congested and increasing traffic pulling in and out of that location will exacerbate the problem. The gas station at the west end of 10th poses a similar problem. In fact that section of 10th St needs a full review because of the parked cars and congestion. The proposed location is the north 'gateway' to downtown Ketchum and I do not feel it is an appropriate use or location.	Entry to the downtown area would be permanently and negatively affected. A CU for a gas station sets a precedence that could have LT impacts.	Lots of folks work outside the core and there is no reason why the traffic associated with a take-out restaurant needs to be in the core. The 'industrial' area has very little 'industry'; it is mostly service businesses/offices and take-out fits this use.		Traffic/congestion Entry to the downtown area there is already a gas station in the same block If there is a need for gas on Main St./HWY 75, perhaps signage on the Hwy indicating where the other nearby gas stations are would be a better fix.	oppose. see previous comments.
7/7/2016 11:25	Seems like a perfect location. I wouldn't want it in the center of town, the entrance to town is the appropriate place.	A gas station and a convenience store would be there, each of which have associated impacts. It will add to the services provided to the community which is a good thing.	Helps keep traffic out of core where it is difficult to park	Traffic safety can be addressed with appropriate signage. Traffic in general at the entrance of town should be expected and welcomed.	Should use the same criteria as every other project. Soliciting input such as this survey is inappropriate as there is no way to control an appropriate cross section of people who respond to the questionaire.	I support smart development in our community so that we may have a thriving community. Processes that discourage growth and development hurt our economy and our commnity.
7/7/2016 9:54	This is a congested corner already, with Issuesthe steep grade above, 10th street industrial traffic below, all the cemetery and hotel traffic added in. Plus there is traffic approaching at speed from the north, not completely decelerated. Also, not enough parking/turnaround space by the pumps. Ugly, congested and ripe for road rage incidents. Lewis Street is a better location.	This reeks of strip development, is an ugly intro to the town from the north. Plus, we don't need more junk food emporiums.	I have less of a problem with a quality takeout restaurant (after all, Grumpy's provides the same service for North Ketchum.) What does need to be addressedparking and safe access to the highway. In fact, more restaurants outside the downtown core are a good thing, because parking in town is crap. I often decide not to visit a restaurant because there is no parking. No parking, no shopping, no business.	It is already congested, has a lot of irregular foot traffic as well as bad visibility on the 10th street intersection. Adding a ton of entries and exits mid block will cause accidents. Adding gasoline to the mix-what could go wrong? Glad I am not a property owner nearby.	Traffic flow, pedestrian flow, bike flow (this is a lousy stretch of highway for bike riders as well, very narrow.) Also, it is a gas station and fast food joint. There is no way you can clean up that esthetic. Just look at Mountain View and the Camo gas stop. Gas stations are ugly. Why trash up the city gate? There are plenty of open business lots on Lewis Street, and better traffic flow. Safety—this is a bottleneck for emergency vehicles going north and south, fire, police, rescue. Don't add more traffic on the curve.	I travel this stretch of road every day. I see no upside to locating a gas station in the bottleneck after the blind curve from town, and a lot of traffic flow and safety issues that seem to be unaddressed.

7/7/2016 9:46	There are three gas stations within a few block of this location already and three convenience stores. It will make the area more industrial and commercial in nature, alterning the character of the area which more upscale due to the Knob Hill Inn and nice homes across the street is improving in appearance.	Once it is built it is there and gas stations have storage tanks which can create a longer-term environmental issue if the business closes and use needs to be changed. It would possibly be more difficult to sell the property and leave a vacant eyesore at the north entrance to town.		The corner coming up the hill - 10th street to the highway is can be a more dangerous corner. I use that turn a lot as I live in Warm Springs as an alternative and also pick up coffee from Lizzy's Fresh Coffee. It is an awkward intersection.	More aesthetic and appropriate use of the location that would involve less traffic in and out of property and address a need in the community versus a service that is already readily available at several locations nearby. It seems a mixed use building that works well in the community with retail/restaurant on the main floor, office space and living units above would be much better.	The question can't be answered with a yes/no as it asks both sides of the question. Oppose - for all the reasons given above.
7/7/2016 9:46		More gas stations may produce competition and thus lower gas prices	Parking in the core is a nightmare	Right now there is much less traffic in this location	architecture conception	I clearly do oppose or support this project ???????????????
7/7/2016 9:39	I think this compromises the personality of our town and opens the door for further degradation of our town's unique look and feel.	I think this compromises the personality of our town and opens the door for further degradation of our town's unique look and feel.	I think this compromises the personality of our town and opens the door for further degradation of our town's unique look and feel.			
7/7/2016 9:47	look at traffic flow now. it would cause such a jam and it is terrible at beginning and end of work day as it is in our peak	traffic issues and don't like it with residential above. You have a perfectly good station down on WS road and out south of town.	why not?	see below	see above	oppose. see above
7/7/2016 9:51	I feel that have enough gas stations already in Ketchum. I also believe that we would lose an opportunity to have something special located at our city's northern gateway.	It has the potential of a visual blight that will be with us for a very long time.	I'm not opposed to a restaurant.	This is the usual NAMBI excuse. I think we can be more truthful in our reasoning to not have a gas station at this location.	What type of businesses and structures would we ultimately like to have in this high profile location?	Oppose. We need another gas station like we need another bank.
7/7/2016 9:45						
7/7/2016 9:58	Creeping commercial development on the outer fringe of the town core can grow and render the town center less vital.	same comment as question 1	same comment as question 1	traffic congestion on the Highway leading into Ketchum	This project will lead to more of the same and render the town core less important and commercially vital.	same comment as question 1
7/7/2016 9:55					We have enough gas stations.	
7/7/2016 10:00	Not sure why necessary with two or three other gas stations nearby			Needs study	Impact on traffic safety	Like warm either way
7/7/2016 10:07	Unfortunate first impression on entering business core	change in character of town	With guidelines as to appearance, traffic, necessity	This is a location where traffic begins to become congested.	Appearance Traffic concerns Effect on other businesses Necessity for the community	
7/7/2016 10:05	Since it's not in the pedestrian-friendly downtown core, I think this is an ideal spot. It's no different from the spot on Warm Springs, honestly, and is probably a better location. It's ideally located for those heading north to gas up.	Well, it would be a gas station where there are now some little office/shop things. That's a permanent change with long-term impact.	Why not? I'd prefer to not see a chain, but whatever. Bigwood Bread is already outside the downtown core, as is the convenience store on Lewis Street. Right now, there's a coffee roaster in this exact location. I don't see the issue.	It's no less safe than the four-way stop by Atkinsons'.	Consider a locally-owned takeout restaurant. Ketchum has done such a great job at avoiding chains (with the exception of Starbucks, which is another issue) that it would be a shame to throw an Arby's or something in there.	People need gas stations. It's ridiculous not to approve this project, and it's ridiculous that there is only one gas station in Ketchum, period.

7/7/2016 10:29	Not an appropriate location for all this increased activity, which will complicate traffic in both directions on the highway and up that hill from Warm Springs. There's no shortage of gas stations anyway!	pricing out local businesses like that vacuum shop, etc.	intersection.	That intersection is a major connector with cars going to/coming from Warm Springs and headed north/south from Hulen Meadows/Lake Creek and Galena, many pulling gear and trailers. Please don't even consider putting a high-traffic generating business in that intersection.	Please listen to the community! Years ago, clueless, non-responsive P&Z "planners" packed our Main Street with a line-up of huge, unwelcoming bank buildings, bad news for a tourist destination. Our Main Street, also a highway, is a major thoroughfare, stuffed with trucks, motor homes and local traffic too. And now you're on the verge of creating yet another a big traffic jam at the north end of that street. As you must know, there's already a jam at the south end of the Big Woods Sports building where people are trying to get to Warm Springs and/or head north, blocking traffic in both directions constantly as it is. Let's not make another bad decision with yet another blockage just a short distance up the highway. We count on P&Z to think these things through professionally, not just to cave in to any bank or gas station that shows up with a check book. Please stop making our main street into an endless dangerous traffic jam!!!	See my comments above. And THANK YOU for asking for community input. This gas station is a bad idea!
7/7/2016 10:07	If you mean fast food restaurant I say no.	No clue	If you mean fast food restaurant I say no.		No fast food restaurants in Ketchum.	
7/7/2016 10:05	Doesn't fit with the neighborhood. Would cause a lot of congestion there. Better in the industrial park	There would be a lot of congestion from traffic going in and out. Already congested there because of merger with Warm Springs road.	If in the right location. Would help with people wanting to take food on hikes and picnics. Also visitors who don't want to stock a whole kitchen for a short stay.	see above comments		see above comments
7/7/2016 10:23	There are two other gas stations within blocks of this location, but any development in Ketchum should be welcomed by the city. Finally, there is someone who wants to pump some money into this area.		There are already restaurants outside the core. Why restrict it. If the business owner thinks he can make it a successful location, why should the city stand in the way? There are lots of people who don't want to go downtown to get a quick bite.		There needs to be parameters that every new building or redesign meets, but the city takes it so far as to discourage businesses from locating here. You cannot have a vibrant city if you are always standing in the way of progress and business development. Consider the look of the building. Consider the traffic impact if any, but also consider the traffic impact if any, but also consider the tax revenue to the city of Ketchum who needs things like new city halls and such. The city should be encouraging development within parameters that work, but don't make those parameters so obstructive that nothing is done (like the last 10 years). This survey is nothing more than an excuse for the city to tell this particular project no. It is clear by the way the questions are written that you are soliciting the NIMBYS and the nay sayers so that you can back up your reasons for saying no of which there are no really good ones.	Businesses should be welcomed with open arms by the city of Ketchum. It is an investment into the community which should be the goal of the city. Ketchum officials are always preaching vitality, but then stand in the way of businesses that would like to create it. Be more accomodating.
7/7/2016 10:17	Certainly is on the main thourgh fare going to Redfish etc. Needs to attractive from the streetthat is up to the Design and Review board. We do not need a typical looking gas station that is not attractive on that corner.		I am sure they are not designing a restaurant but more of a convieance store like we now have in our town where you can pick up things when you arrive from the airport etcwe call that essentails.	If design properly, this should not be a problem.	How does the entire community feel about having a gas station on Main Street. Living here over 50 years, we always had gas stations on main street.	

7/9/2016 11:34	A gas station and convenience store is just not appropriate for downtown Ketchum's Main Street, and this location is an extension of Main Street.	An all-night lit up garish and messy gas station is just not an appropriate structure for Ketchum's downtown Main Street. Bad enough we have an old railroad dining car with its full side on Main Street facing Cimino Park. The gas station will end up being pretty schlocky.	It just causes more in and out slowing traffic on the main north-south street and road through downtown Ketchum.	The gas station, convenience store, and take out eatery will require lots of curb cuts and will have lots of traffic slowing and turning off and cutting across lanes of traffic.	empty-tank traffic into Ketchum from the north have immediate access to a schlocky	It is just the wrong kind of business and business activity to be on the main street leading into downtown Ketchum. It will be another traffic impediment.
7/7/2016 11:36	I think it is an ideal location for the uses that are proposed. Ketchum has nothing on 'main'street (Hwy 75) that caters to people that are passing through in either direction. I am totally supportive of the proposal. Doug Clemens.	Of course. There will always be impacts both positive and negative for anything new. But, I still think it is not only an excellent location for automobile oriented business, it is probably the very last chance to provide such a location for such a service in the City of Ketchum.	That said, it depends a lot on what sort of "take-out" restaurant it is. A small version of a Subway or McDonalds such as they have in the new farm supply in Bellevue would probably be appropriate and well-used. It would save a trip to Hailey for locals and be a boon for people passing through with hungry kids.	Always. It would take attractive and readable signs with subdued lighting just as was required of Mac's in Hailey. Traffic is another matter. You may end-up with a light that is activated by traffic coming up the hill. I is an interesting corner but a valuable and needed business that does not yet exist.	Lighting. Good but not overwhelming, and certainly not directed up or intended to light the sky or the neighbors. It can be done. Use an expert to give you some direction and not a committee.	I've already exposed my willingness to support the proposed project. But lastly, you should squeeze the hell out of their architect or designer to make this entire project one that will be a winner and a very nice complex since it will be the first commercial venture you will see coming in from the north and the last thing you will see leaving town and it better be damn nice and make people want to stop there whenever they come through town and easy to get the lunch crowd in and out.
7/7/2016 10:36	More changes to change the small town feel of Ketchum which is important to tourists and residents.	Companies going out of business in the industrial area.		Company planning to build new venue should be required to pay to change impact of traffic	Impact on other gas stations and companies in the area (cafes etc) Traffic impact. Not another traffic light!	See comments above
7/7/2016 10:32		Traffic impacting a city that is already under stress with lacking infrastructure that cannot handle our peak season traffic as it is. When it goes out of business there will be clean up and remolding costs to attract another business to that location. Therefore it could sit empty for years as other businesses in town have.	Any business should be allowed where it is zone appropriately. Again there is a series traffic problem in the city that needs to be addressed and that should be considered for the success of any new business and the city as well.	SEE ABOVE	Traffic and is this business a viable addition to our town.	For all the above reasons
	It's the northern gateway to town. This project wouldn't be indicative of the kind of town Ketchum is. It's also the last thing you see before you leave town, leaving an ugly impression. It destroys the view, the feeling of the town.	front of any convenience store and gas station - dirty, unsightly, no matter how hard you try to clean it. Underground fuel	Again - the trash left behind. Plus, it's ugly, it's not in the core feeling of Ketchum small town-ness.	Increased traffic flows of cars and trucks trying to get gas. Summer would be a nightmare as trucks with travel trailers trying to refuel.	study of area especially this time of year. Elevation plan. Noise study. Review from	Overall, it just doesn't fit with the small town feel of Ketchum. It's not welcoming. It makes the statement that we don't care about our town, our quality of life.

7/7/2016 10:50	There are already 4 locations for the purchase of gas in Ketchum and Sun Valley. In addition, this would be an eyesore across from the Knob Hill Inn and adjacent properties, even thought they are commercial.	One of the assets or salient aspects of Ketchum is that Main Street and the areas North and South of town are not riddled with gas stations and convenience stores. This development, on a large site, would be the first of its kind and create a new poor visual standard for Ketchum.	This part of the proposal is simply another vehicle for hurting the other small restaurants that already provide take-out service along with sit down dining. The type of "take-out" food in the proposed establishment would cater to highway 75 users and cause them not to buy food at other local businesses. That's why we have a downtown core. Let's not allow a "strip" to develop along Highway 75, like so many other towns.	This is for the transportation experts to measure. Certainly, there will be many more turning movements at this intersection.	Visualschange in the entry to Ketchum from the North. Creating a different image of the town. NEEDwhy does such a small community need 5 gas stations. Tourists can fill up in Bellevue and Hailey also. Take-out served beyond the downtown core. TRAFFIC Dislocation of all the small businesses currently on the site. JOB CREATIONmay actually lose jobs as existing business relocate to the south. TOWN CHARACTER	While I may vote elsewhere Ketchum is what I call "home" due to its outstanding character and vibrant community that is full of small entrepreneurs. I oppose this project as it just doesn't make sense in the first place (we have enough gas stations)and it will have lasting impact on the community as it doesn't "dovetail" with what our community stands for.
7/7/2016 10:34	A gas station on main entrance to Ketchum, how ugly. In addition it is a terrible intersection, I think it would cause alot on congestation	I really don't think you want visitors to see is a gas station just as they are entering from the north, or leaving the city, does not leave a pretty picture of the town.	No, personally I find those take out junk food places extremely unhealthy. Not the type of impression you want for Ketchum, I believe the image you are promoting is healthy, athlete environment.	It is pretty congestion in that area all ready.	What I would like to know is where are all the business that are already there going to go and find reasonable rent.	Look above
7/7/2016 10:28					traffic concerns	
7/7/2016 10:35			Not in my neighborhood, but there are already restaurants and coffee shops in the light industrial park.	There are several businesses now on this property. Traffic may reduce.	I think you decide if it meets your code. If it does you approve, if not, deny. The city needs to treat all applications the same. With this survey you are singleing out this application. This is not only unfair, it feels sneaky and illegal. Like much of the city's business in the last year.	
7/7/2016 10:34	There is already a gas station within a few blocks of this location and the topography of this location makes it unsafe and unsuitable.		Town suffers from a dearth of eating opportunity prior to 5:00, especially between 10-5:00, and numerous locals have commented upon such a need and it would be very beneficial for the people that work here that do not go to lunch in the middle of the day and still need to eat.		There is already a gas station within a few blocks of this location and the topography of this location makes it unsafe and unsuitable.	There is already a gas station within a few blocks of this location and the topography of this location makes it unsafe and unsuitable.
7/7/2016 10:45	The application requires a CUP. CUP's should only be granted when the applicant can show the use is something that is currently needed and/or in short supply. That is not the case.	Do not believe that a business of this the reflects well on the appearance of rthe commercial core.	we have a serious shortage of take out restaurants in Ketchum. No reason not to allow this as a permitted use outside the core. Conversely we do not have a shortage of gas station convenience stores.	See previous comments	Not a permitted use.	
7/7/2016 10:41	Already that exact business down the hill. The traffic regulation is not present there to make it easily accessible for northbound traffic which I imagine is the target	As above I think an additional traffic light or stop sign would need to be in place to keep the traffic flowing into town. Support current businesses that are already in close proximity. There are also supposed drainage issues in that area and a gas station at the top of that drainage is terrible environmentally	Need more of a definition of take-out restaurant.		See above comments	
7/7/2016 10:39	Just make sure traffic controls provide safety. Ketchum needs some affordable places to eat.	Better service to our community.		But I am sure it will be addressed to the satisfaction of the town.	Needed services for those who live in and travel through Ketchum.	We need more choices for gas and less expensive food.

	It is a dangerous intersection. The traffic there is haphazard and there are many pedestrians crossing from the Knob Hill Inn. The bus stops there and in the winter, there are many skiers as well. It's a terrible place f=to locate a gas station.	There are so many long term impacts. Naming just a few:The walkability impact to locals and visitors - basically this is an unsafe area to place a gas station, to the surrounding homes and businesses - homes will decline in value and businesses will be negatively impacted by increased traffic, to the businesses of other gas stations in the area (there are three within 3 minutes of this location), to the entrance and exit to our beautiful town which would be forever changed in a negative way. There will be traffic implications (negative)	We don't need a Bellevue-like gas station/subway KFC. It's a HORRIBLE idea. If the location is in the so called LI I see no	Described above Terrible idea! See above #2	Safety Character of the city Visual impact Effects on other businesses Effects on surrounding homes and hotels Impacts on walkability Impacts on view corridor Impacts on first impressions of a beautiful community forever sundered by a poorly conceived idea. There must be a geology study of the	It's an ill-conceived project. The community does not need the services the project purports to be providing. The economic impact on other gas stations in the area, already established, would be negative. The visual impact would be negative. The pedestrian impact would be negative. The traffic impact would be negative. The traffic impact would be negative. There are simply no porsitives to this. None.
7/7/2010 10:44		probably resulting in a traffic control light or lights at the intersection of 10th and Hwy 75. Probably in perpetuity.	problemevery thing else there is "take out". Like "take out" lumber, rental equipment, party goods etc.	See above #2	property as they will be burying fuel tanks there on ground above the flood plane and the other properties in the immediate area.	
7/7/2016 10:40	Good corner at the north end of town.		Typical part of a convenience store operation, ala Base Camp at 10th & Wm		Aesthetics/access.	
	It is in LI so good place for a gas station. Very convenient for travelers that don't know about the hidden gas stations in other parts of town. Most of the others can be very difficult to get in and out of if you have a trailer.	Yes, but good impacts. Travelers will find it convenient and maybe decide to stay in Ketchum for a while. A modern gas station will be much better for the environment than the old ones built in the 70's, 80's and 90's.	Sprgs. Yes. To help alleviate downtown traffic and allow workers on the north end of town an easy place for lunch. The downtown core is getting impossible to get around. I usually try to do it on a bicycle or motorcycle and that is even scary.	Perfect spot for a gas station. Safer to get in and out of compared to driving a truck and travel trailer down 10th St. to find out you can't get in and out of Base Camp or the Shell station.	The differences between 30 year old gas stations and one built following EPA Regulations in 2016. I think a new one will have less environmental impacts than the others. I also think it is important to note that most of the active, current opposition have chosen to be in, or live adjacent to LI. The convenience for travelers and residents with large RVs or towing big trailers. The City thinks that gas stations are safe enough to allow 40 Community School kids to live within 340 feet of an active 24 hour large vehicle fueling station.	We need an easy to get in and out of gas station so travelers don't go all the way to Valley Co-Op in Bellevue. It will be safer and less environmental problems than the other old gas stations. It's in LI. It's a free country and the City should support new business.
7/7/2016 10:42			more tax revenue to build new city hall		cc&r's	
	I don't think the it is a good representation of what the rest of Ketchum is about. I am not a fan of junk food restaurants in general and not to the northern entrance to our town.	Creepage. The beginning of letting junk food in our town. Can you say Twin Falls.	We have a lot of great restaurants that have take-out already. I'm not sure but is this a McDonalds kind of place?	If it is planned properly, I don't think it would but do we really need another gas station or convenience store when there is one just below in Warm Springs and one on Main Street less than a mile away.	The big picture of how we are viewed and the impact of the loss of business to the other existing business of the same nature	For the above reasons. I am not an anti growth person. I think density in the core is great. I would like to see the industrial are have more affordable housing. How about some apartment buildings!!
7/7/2016 10:44 7/7/2016 10:55	terrible/unsightly idea for our northern	traffic issues described above.	as long as there is appropriate parking and	absolutely. stand on main street in the	traffic	unsightly
	gateway. traffic danger w all the summer campers turning left twice to get gas before they go north. pedestrian and bike danger.	pedestrian and bike safety issues. gas station wont get prettier over time	as foils as there is appropriate parking and ingress/egress, I don't think all take our restaurants MUST be in the core. industrial area is fine. not residential neighborhood and not someplace that creates safety issues	absolutely. Stand of inflant street in the summer and count all the RVs/campers/trailer vehicles heading north. think about them trying to stop at the last gas station for 35 miles. two left turns across hwy 75 required. ugh! also, all the bikes that come along 75 instead of following the bike path down to the YMCA will have to navigate the traffic this facility creates.	consistency with numerous aspects of our comp plan (safety, gateway beauty, eco-friendly, etc) possibility of a major gas station chain coming to our town.	traffic back up and danger pedestrian safety bike safety

7/7/2016 10:57	A gas station, convenience store exists at the bottom of tenth street just down the road. Veltex is also very close proximity. I also believe we should have a project with greater curb appeal for the entrance to town. Traffic is also a huge consideration as town is already congested at that point with out trying to have turning vehicles at this	I believe the efforts of the CDC and the Downtown Master Plan were to draw people into the heart of downtown for store and restaurants - we need people to stop and enjoy, not hit the outskirts on their way to or from the Sawtooths, etc	see number 2	see above	Downtown Master Plan Business owners in vicinity Nearby residents, hotel guests	see all above answers
7/7/2016 10:54	It is clearly visible and would be good from a business perspective, but we have sufficient gas station coverage in Ketchum. We should perhaps put up signs indicating where gas stations are located in the Industrial area, but adding one on 75 is not necessary.	Each step like this makes Ketchum more commercial and less "small town" with the charm we take for granted.	This aspect of the project is perhaps more defensible because the market is not well served with a convenient take-out. But it wouldn't justify the whole project.	Every change that brings more turning and lane-crossing traffic will be more congested and less safe.	Not much. A traffic study would perhaps illuminate the safety and congestion issues. In the main, the matter is simply, "do we allow business interests to rule the development process, or not?"	Have already stated that it does not appear to be necessary to serve the market.
7/7/2016 11:02					a map to show 10th and Highway 74 and size etc. of the building. Traffic and whether in out levels to the hihway or 10th street	See 5 -not enough data to make a realistic understanding
7/7/2016 11:00					Traffic congestion may put Pedestrians in danger zone	
7/7/2016 10:53					dunger zone	He owns the property
7/7/2016 11:08	Plenty already	eye sore - possible environmental impact	OK with me	Off and on the highway is tough in gateway	We already have a gas station / convenience store a block away	
7/7/2016 11:21	There are really no place to get gas and get items before going out of town. I feel that this is a great benefit for the City.	No There will be not impacts tot he community. We need to get off our high horse and realize that these amenities need to be in our City.	Yes, having a restaurant outside of the downtown core will not hurt anything and be a benefit to the overall City	I do not see any traffic problems in this area. The proposed gas station is well north of the Main Street, Warm Spring Avenue intersection and should not be a problem.	I would like to see curb and gutter and sidewalks along Main Street with curb cuts. adequate parking will also need to be addressed	I think this will be a great amenity of the City and cause more people to stop in our town instead of passing through
7/7/2016 11:10			Yes, the one coming into town does not bother me.		Put it in the industrial area if you must.	For the reasons stated above.
7/7/2016 11:25	No unless the current convenience store 2" drive is the one moving there! We don't need 2 stations so close Maybe by the hospital would be more useful!	Dirty refuse on Hgway 75 too close to Knob Hill Inn & not nice looking for North direction.	Maybe South of Ketchum where people are leaving town but architecture must NOT BE STRIP MALLSOMETHING CHARMING LOG HOUSE, in line with "old Ketchum Western town"		What happens to the existing station & convenience place in town? Maybe it should be bought by the developer and made to look better!	Too close to existing similar business and detracts from Ketchum charm.
7/7/2016 11:54	gas station: there is already one at the bottom of 10th street. maybe putting up a small sign on hwy 75 that better shows that location would help those travelers looking for petrol. if a new station is built up there, it will put the other station out of business without a doubt. also i don't want to see our city's north entrance defined by everything we are against; fast food and fossil fuels. these stations never look good no matter how they are designed. never!	increased traffic congestion. unattractive. gas stations are dirty no matter how they are constructed. all those petro chemicals from the vehicles themselves will create additional toxic chemicals. the rain and snow concentrates all those chemicals to wash downhill on 10th street. design-wise, i don't see how this project coordinates with the current businesses in that location; the cemetery, knob hill inn, etc.	isn't our goal to get people out of their cars to walk around the downtown core? we aren't twin falls. our community is a destination. it is not a drive-thru.	just look at 10th street. do you really want vehicles lining up on this steep street during the winter? it seems impossible. currently, it is unusual if even just three vehicles are lined up at that 10th st and hwy 75 intersection. there will certainly be vehicles that will have problems in the winter climbing 10th street from a complete stop. definitely NOT a good idea.	area (this project is a major diversion); environmental impact (petro chemicals concentrated in an area that has a concentrated water flow via 10th st); traffic problems with the terrain (the steepness of 10th st, especially in winter); the impact on	city planning has the responsibility to define the nature of our community. the northern entrance to ketchum should not be defined by this project. my biggest objections are the negative impact of traffic patterns and the environmental impacts of pollution from increased concentration of vehicles in that area.
7/7/2016 11:29	Not an appropriate gateway business for Ketchum.	Yes, this affects community character.	Proposals could be evaluated by a CUP process to see if location is appropriate.		Traffic study Landscaping Plan	
7/7/2016 11:39	north end could benefit from this. Most of these services are south.	I think it's an appropriate location. The hodgepodge of businesses there now itinerant it seems.	why not?		reduce town traffic so warm springs and north ketchum can not go into town	

2/7/2016 12:27 Besides Mountain View and Base Camp to get gas, it would be very convenient for those pasking through 1 have easy access to gas/flood. Going down Marm Springs and to Base Camp to all to get gas, it would be very convenient for those pasking through 1 have easy access to gas/flood. Going down Marm Springs and to Base Camp to get gas, it would be very convenient for those pasking through 1 have easy access to gas/flood. Going down Marm Springs and to Base Camp to get gas the would allow customers to avoid having to park downtown to pick up food. 7/7/2016 12:13 The pasking through 1 have easy access to gas/flood. Going down Marm Springs and to Base Camp to get gas the would allow customers to avoid having to park downtown to pick up food. I believe that as ketchum grows more of those businesses go and the employees? The through the sa a lack of san space night now in the city of ketchum. but on the light industrial district. The pasking through 1 have easy access to gas/flood. Going down Marm Springs and the gas and the employees? The same of the pask where on the same of the pask where the gas to gas that the gas that for any through 1 have the gas tablosis in the ketchum and the control of those businesses go and the employees? The through 1 have any through 1 have a same and more of them are even they, as how the training the gas tablosis in the ketchum and the pask of the gas tablosis in the ketchum and the gas tablosis in the gas tablosis in the ketchum and the	7/7/2016 13:42	With only two lanes and 10th street being such a steep turn, this is a very dangerous place for such a high traffic business, such as a gas station. Also, there are 3 gas stations within a 2 minute drive, one being LESS THAN a block away.	There is an unfortunate amount of potential for car-accidents in this type of business being placed at this location. It would also be very unfortunate for a gas station to be the North entrance to our beautiful resort town.	The downtown core is for restaurants. Our town is proud of it's local businesses and a take-out restaurant away from the downtown core would absolutely detract from the other local businesses. Not to mention, it would encourage people to travel straight through as they head north or south, once again detracting from the local businesses.	There is definitely not enough time to signal when turning into this location heading south, considering 10th street is basically connected to the property. It is extremely dangerous when turning in from either direction if traffic is high! This could also cause a lot of unnecessary traffic build up in the light industrial area.	allowing this area to become highly traffic. There is too much potential for accidents. It should also be considered how unnecessary it is to have a gas station as the north entrance to our beautiful town, especially	It is too dangerous.
in a larrody invance for the buses to memory extrought the promote of the road and into town at peak traffic hours. Ask nor not south register, the promote of the road and into town at peak traffic hours. Ask nor not south register, the promote of the road and into town at peak traffic hours. Ask nor not south register, the promote of the road and into the road and into the promote of the promote of the road and into the road and into the promote of the road and into the promote of the promote of the road and into the promote of the promote	7/7/2016 12:07	traffic stopped while cars turning in and out while backing up traffic through town to the busiest intersection in town at Warm	to the city are envisioned. Hopefully we will not end up with the typical conenience store gas station as the defining elements for the entries to the city - like so many	town. And it would be too disconnected - it	traffic stopped while cars turning in and out while backing up traffic through town to the busiest intersection in town at Warm	entry/exit character for the town. This is by far the most important planning and zoning issue that should be addressed - it is how we all experience and define our town,	See above.
convenient for prospective customers. tasker for floring would compete with higher quality offerings in the core. It as convenience for customers not a foodie destination. 7/7/2016 12.27 Besides Mountain Even and Base Camp to get East of those passing through to have easy access to get the core of those passing through to have easy access to group the street of those passing through to have easy access to the room for veilide with traillers, or the core of the convenience for those passing through to have easy access to the core of the passing through to have easy access to group the street of the passing through to have easy access to group the street of the passing through to have easy access to group the street of the passing through to have easy access to group the street of the passing through to have easy access to group the street of the passing through to have easy access to get the passing through the p	7/7/2016 11:56	it is already insane for the buses to maneuver through that portion of the road and into town at peak traffic hours. Also not sure visibility with north bound traffic - thus	see above.	avoided. not having fast food in ketchum makes us unique. i also think it's important to support the local restaurants as much as possible. we've survived this long w/o it think we can continue to survive without	see question 1.	-value that this business will bring being at this location vs. another location -job creation - although important will this business truly create enough jobs that it	
pet gas, it would be very convenient for those passing through to have easy access to gas/food. Going down Warm Springs road to Base Camp Is cumbersome and not all of rome for vehicles with trailers, etc. 7/7/2016 12:18 at is a great location for this type of a conveniency/gas station, extiferation to all of rome for vehicles with trailers, etc. 1 believe that as ketchum grows more of these types of projects will occur. this is part of growth and it think the only long term of these ks a lack of such space right now in the city of ketchum. But on the fligsiste where of those businesses go and the employee? There is a lack of such space right now in the city of ketchum are and none of them are ever busy. Also, when i moved here 20 years ago we had where are trailer to the gas stations on main street, it was always a pain to get into and our of with the thing to the very come, just trying to get in and out of the space of the project is a strained by the complex of the project is a strained by the complex of the project is a strained by the complex of the project is a strained by the complex of the project is a strained by the complex of the project is a strained by the complex of the project is the project in the light industrial district. 7/7/2016 12:15 We have four gas stations in the ketchum are and none of them are ever busy. Also, when in moved here 20 years ago we had where are trained by the project is busy in the get into and out of with the triangle project is busy in the get into and out of with the triangle project is busy in the get into and out of with the project is busy in the get into and out of with the triangle project is busy in the get into and out of with the triangle project in the light industrial, so give a conditional use permit the get in the project is a station one block one with the first thing you see coming into town. 7/7/2016 12:19 Yes, Perfect location for list-minute items before heading north to adventure. 7/7/2016 12:20 The project is the project is project in the lin	7/7/2016 12:12	convenient for prospective customers.	the City will consider all viable business	offering would compete with higher quality offerings in the core - its a convenience for	rather it eill seek to make customers of	Ensure quality design of all facilities	economies. If you wont allow a gas station on a state highway and main arterial, where
convenince/gas station. exit/entrance to ketchum. but on the flip side where do those businesses go and the employees? there is a lack of such space right now in the city of ketchum. 7/7/2016 12:15 We have four gas stations in the ketchum are and none of them are ever busy. Also, when I moved here 20 years ago we had three gas stations on main street, it was always a pain to get into and out of with the traffic. I think the city needs to possible with the city needs to provide the infertion of town. 7/7/2016 12:13 Yes. Perfect location for last-minute items before heading north to adventure. 7/7/2016 12:23 Yes. Perfect location for last-minute items before heading north to adventure. 7/7/2016 12:23 Open invite for more potential fast food outlets!	7/7/2016 12:27	get gas, it would be very convenient for those passing through to have easy access to gas/food. Going down Warm Springs road to Base Camp is cumbersome and not		out food can be a bit challenging to find a place to park. This location seems like it would allow customers to avoid having to	build up on 10th St and also have people	Traffic flow.	
area and none of them are ever busy. Also, when I moved here 20 years ago we had three gas stations on main street, it was always a pain to get into and out of with the traffic. I think the city needs to put signs up telling people where to go when you come in our out of town. 7/7/2016 12:13 7/7/2016 12:09 7/7/2016 12:23	7/7/2016 12:18	convenince/gas station. exit/entrance to ketchum. but on the flip side where do those businesses go and the employees? there is a lack of such space right now in the	these types of projects will occur. this is part of growth and I think the only long	valley and precedent has been set with big	going to be an impact on safety. some kind of traffic alteration will need to be made. i.e. widening of the road, a speed hump to	those displaced businesses need some place to go. Where are they going to go? Also the traffic survey needs to be updated to reflect the current state of that corner. It is very busy and some form of traffic control will	and where are they going to go? I support it because I believe in order for ketchum to grow and become an even better city infrastructure like this needs to be put into
before heading north to adventure. this sort of thing. when allowed, most restaurants in the valley are not set up well for take-out business. 7/7/2016 12:29 7/7/2016 12:23 i do not see the need for another gascombo fast food station, kinda inviting Open invite for more potential fast food outlets! when allowed, most restaurants in the valley are not set up well for take-out business. when allowed, most restaurants in the valley are not set up well for take-out business. providing unmet needs. We need to keep Our Small Feeling, no comment needed PERIOD!!	7/7/2016 12:15	area and none of them are ever busy. Also, when I moved here 20 years ago we had three gas stations on main street, it was always a pain to get into and out of with the traffic. I think the city needs to put signs up telling people where to go when you come	just trying to get in and out of the space now is a pain. 10th street is busy now as well. Who wants to look at a gas station as the first thing you see coming into town. Plus there is a station one block away that is	industrial, so give a conditional use permit	people come in and out of town, try and turn on 10th street, the bus route is right there, and then mix in bikers and you will see that it is a very busy corner. Add people coming in and out of the gas station and it	run off with snow.	to live. Ketchum has a big problem with affordable space for small start-ups, anchor businesses, and growth. Taking out the 7 businesses and over 30 people working in this location for a gas station seems short
7/7/2016 12:23 i do not see the need for another gas- combo fast food station, kinda inviting Open invite for more potential fast food outlets! Open invite for more potential fast food outlets on comment needed Over Small Feeling, Open invite for more potential fast food outlets outlets on comment needed Open Small Feeling, Open invite for more potential fast food outlets				when allowed, most restaurants in the valley are not set up well for take-out		established businesses in the area. How? By	
more.		combo fast food station, kinda inviting	The state of the s	no comment needed	no comment needed		no comment needed

7/7/2016 13:32	Yes and no actually, but more no. Directly across the street is the Knob Hill Inn and a new, high-end residence on the corner. I can't imagine owning a multi-million dollar home and having a gas station directly across the street. Just because the property is zoned as commercial, there needs to be a buffer between the commercial properties and the residential properties.	gas station. There certainly aren't many options on lewis street. I believe some would simply go out of business. Not good.	care. But if it was like a fast-foot restaurant I would say definately no because it would be unfair to the other restaurants in the area.	to wonder how a fuel tanker truck or a truck pulling a large trailer are going to make the turn into or out of the gas station without interrupting traffic on the highway or on 10th street. We certainly don't want another traffic light.	(not prioritized) 1. Long-term benefit to the City (taxes, jobs, etc.) 2. Loss of small business locations. 2. Traffic in and out of the gas station. 3. Signage. 4. Landscaping 5. Gas fumes. 6. Lighting- dark sky ordinance. 7. The existing alley. 8. Pedestrian path and access. 9. Take-out food regulations. 10. Delivery truck access (fuel & other) and hours. 11. Employee parking. 12. Snow removal/storage. 13. Future use and impact to Stock Building Supply property. 14. Existing telephone pole removal. 15. KURA support. 16. Loss of business to existing 10th street gas station. 17. Hours of operation. 18. Fuel spillage protection.	Initially I thought if was acceptable. After further discussions with friends I've changed my mind. There's already a gas station on 10th street at Warm Springs Road. We don't need another gas station. More importantly, the proposed site should be used to attract more small businesses outside the City core.
7/7/2016 12:28	thinits a stupid idea	nothing good or positive will begin the end of small town feel of ketchum that still exists. this is not twin falls	dont belive all restaurants need to be only in downtown core but that location on the highway is not appropriate	there is already a problem with traffic flow coming into ketchum from the north this will only aggravate it and cause accidents	the spot where anderson used to be is a much more viable space large enough to accomodate paring and entrance and exit issues	dont believe we need another gas station in the city
7/7/2016 12:42			competition is good!		speed limits should be enforced in the area	
7/7/2016 12:38	There is already a gas station one block from this location.	Currently there are multiple business with 30 employees at this location. Those business and employees would need to find new locations and/or jobswhich are extremely limited in our valley.	More options for food is greatbut this location is not appropriate because of the reasons I listed in question #2.	More people crossing the street in this area does not seem safe.	Potential relocation for businesses already existing in that location.	Currently there are multiple business with 30 employees at this location. Those business and employees would need to find new locations and/or jobswhich are extremely limited in our valley.
7/7/2016 12:43	Why pull 5 businesses and 30 happily employed people out of a location to stock it with 1 business and potentially 5-10. This does nothing to progress the employment ratio in the valley nor does it fulfill any unidentified needs that aren't already met many times over within the town.	See above, jobs lost and along with that an interest in the valley is lost. We do not need another unsightly and unnecessary gas station in a town that already has plenty of fueling options. The plan for mitigating traffic patterns both vehicular and foot traffic needs to be addressed much better than the forethought that's currently in place for this plan.	Why? This shouldn't even be a question. If you asked, should a nature preserve or an arboretum be allowed outside of the downtown core then the answer is without a doubt, yes. We have plenty of options currently in town and this will not benefit us in any way.	would have long term impacts.	The people of the town. We don't need gas stations on every corner, we either moved here or stayed here for the beauty and charm of life in a small town. The excessive growth (read: multiple hotel projects on Main St.) are already doing enough damage to our humble mountain town. If I wanted to live in Vail or Aspen, then I'd move to Vail or Aspen.	I have stated all of my oppositions in the comment sections above on each question.
7/7/2016 13:21	Because it is the north entrance to Ketchum and the first impression many visitors will have of our mountain town. My opinion of Jackson, Wy. is diminished very time I approach it from the south because you pass a mile of strip malls before you ultimately reach a lovely town. Especially on my first visit it was responsible for my short term impression. Nothing like that exists here in Ketchum.	Only to the extent that the before mentioned impression has a impact on visitors and ultimately on the taxpayers.	Take out does not reduce the city core evening energy and helps lessen parking challenges.	It seems like there may be some challenges to making a turn from the warm springs connection road to the highway when people are braking from the north and turning left in the north bound lane.	Just what I've already mentioned.	Beauty is a relative quality, but I've not seen a handsome gas station. And I think this is one of the two most important impressions we need to protect.
7/7/2016 12:47	i believe it is a perfect location for a gas station, on the North edge of town.	Competition breeds excellence, and reduced costs to consumers	Yes, Why shouldn't it be allowed. The downtown core concept is flawed at best. "Downtown" is occupied by a bunch of businesses that the common person has no use for. Where does a guy go to buy a pair of everyday socks in this town? A pair of Levis?	A gas station/convenience store at this location would take some of the pressure off of "Base Camp Warm Springs" where there is significant impact to traffic	Architectural design and adequate parking	

7/7/2016 12:43	It is a gateway from the north and sends the wrong message to visitors. It would be an eyesore. We already have ample choices for gas stations!	Potential environmental hazards.	It's a chain that detracts from the "Big Life, Small Town" mission of our town.	the corner of 10th is a blind corner, especially with bus traffic	all of the above	
7/7/2016 12:52					Best location for the gas station is at the edge of the town. Good location for both locals and tourists.	
7/7/2016 12:53	I think the location is appropriate.			Turning lane for people travelling north? Blind corner coming up 10th?	Homeowners directly below Parking Traffic Lighting Height Design and the fact that there is already a gas station just down the block	
7/7/2016 12:55	I do not think the traffic resulting from the in and out of a gas station can be handled safely.	I think the impact is negative.	I am fine with a take out restaurant depending upon design, etc.	See response to question 1	Design, safety, projected number of cars.	I am mainly opposed to the gas station element of the project.
7/7/2016 13:07	we have gas station 5 with sun valley and restaurantssss	location parkingssss constraction no thank you			we have gas stations we have restaurantss and we have convenience stores	
7/7/2016 13:07	It is out of the downtown core so it will not detract from the uniqueness of Ketchum,,,,,just as the existing gas station on 75 closer to Weyyakin does not detract from the Ketum downtown.	Yes, revenue spent in the community.	I don't have much of an opinion on this. I suppose some may say visitors driving from the north would spend money there instead of visiting downtown restaurants. But I could go either way on this issue.	This may require another stoplight.	Traffic and potential revenue	I think a fueling station on the north end of town is good for passing travelers & provides another opportunity for money in the community. I am registered to vote in WA but have been living here for a few years (visited for 20 yrs) and am now relocating here full time as an employee in the community.
7/7/2016 12:58	easy to get to as you head north out of town				Growth is good.	
7/7/2016 13:11	There are 3 gas stations right in the immediate area.	Commercial lots for development are hard to come by. This lot could be used for something more useful.		I believe traffic congestion will increase and back up into town more. Traffic is already an issue.	Toxicology reports of hazardous waste for site. Traffic study.	Different locations might work.
7/7/2016 13:28	Hard to get in + out to the highway from the existing building. Hard to turn left on to highway from the smaller street. There is already a gas station + convenience store 1000 feet away. Not attractive for the gateway to Ketchum on the North end of town. It is so NOT the right thing. A takeout restaurant? That is really funny. Give me a break. There is not enough parking, too much in + out traffic. It isn't necessary, it's tacky and should be somewhere else like ARCO.	Yes	But a "take-out" at a gas station with a convience store? why don't you tell the community what this "take-out" restaurant is? Mc Donalds? Carl's? What? It can't be great if it's attached to a convenience store and gas station. I mean who are you trying to appeal to with a "take-out" restaurant at a gas station?	too busy as it is. difficult stopping at the stop sign and turning left on to the highway going North. blind spots.		I think you got my message. I don't support this AT ALL!
7/7/2016 13:04	It's too congested in that area already.	More accidents with people and cars.	We don't want to indulge in urban sprawl!			See comments above.
7/7/2016 13:15	No for the gas station at this location, OK for the store and restaurant. If a gas station could be located 1/2 mi to 1 mi north of this location, it would probably serve the community well.		You already have one in the industrial zone: Big Wood Bread restaurant. I don't particularly see a problem with that.	Noted above.	Whether or not other properties further north could be acquired for the gas station. There are no stations in this part of Ketchum which could easily service pass-through traffic. On the other hand, traffic into the industrial zone has access to stations, so the local market is probably already reasonably well-served.	Opposition based entirely on the inclusion of the gas station in the project, otherwise neutral.
7/7/2016 13:13	We have enough gas stations.	this kind of establishment attracts a mess and people who do not mesh well with the general population	There are plenty of take out restaurants and mixing it with a convenience store and gas station attracts a bad class of customers.	The location is right where two heavily used roads coincide and traffic pulling in and out will make a mess	What is we are trying to encourage in this small and beautiful city? This is the kind of enterprise you would find in a poor area of a much larger city.	
7/8/2016 14:28 7/7/2016 13:23		Increased	as long as it is local.		kjl;ghklj Do we want to expand our commercial	We have 4 gas stations and 3 mini marts in
.,.,2010 13.23					outside of the core	town That is enough

	There is a gas station and convenience store one block away. It is also not a great intersection for heavy traffic due to restricted view from curves.	downtown core. Continually allowing sprawl	The restaurant options should stay in the downtown core. Continually allowing sprawl will take away from the small town feel and character of Ketchum. we also do currently allow "fast food" optionsthis seems like a slippery slope. Not to mention most exhausting restaurants would likely want more customers not competition.	This area of hwy 75 can already get congested and be dangerous! The curve at Backwoods when they have sales/heavy foot traffic is already an issue. The road there is too narrow as it currently is leading into that curve, which has always been a problem.	All of my above comments.	All of the reasons I've listed. In addition, do we really need two gas stations right by each other? The only good I see out of this would be competitive pricing on gas, but how likely is that?
	Seems like a good location for above with plenty of parking		Take - out only - then it does not affect the core vibrancy		Quality of the take out is key - don't want McDonalds etc	But need to know more details
7/7/2016 13:53			Allowing it out of the downtown core would mitigate associated traffic congestion with the restaurant.		Does the project meet established building codes.	I think the project will benefit the citizens of Ketchum and the surrounding communities.
	We have this convenience at the south entrance, having this convenience at the north entrance seems appropriate, especially for seasonal travelers that are only passing through town.	positive - jobs, add to local economy negative - environmental	more the merrier, this town needs more options for the different kinds of visitors / travelers parking in the core is not always easy		hours of operation signage ingress and egress	
	This is a more logical locale then the Base Camp which is a traffic hazard! Also easier for tourist traffic to find gas.	Depends on design and traffic flows! Please design appropriately for mountain town and nice Knob Hill to the north and Frenchmen's to the south!		The intersection of Warms Springs and 10th street is so difficult visually when vehicles are parked at the pumps that nothing could be worse! I feel this location is a snap for south bound, with northbound having no more difficulty than accessing 10th currently.	There will be motorhomes pulling in from both directions at the same time. There must be clear drivability and maneuverability, plenty of parking and ease of highway visibility at all times to be successful. And please don't look so ugly as all other mini mart/gas stops!	
	It would affect the area, other businesses and traffic.	Consider that we have another gas station a block away Basecamp!	We don't have to protect the core with excessive restrictions on development.	This is a difficult corner and would be greatly impacted by additional traffic.	Do we actually need another gas station? Could the owner of the property do as well or better by considering another use? Could the town benefit by posting signs to tell travelers passing through Ketchum that gas is available off of the highway a block or two?	The above considerations need answers.
7/7/2016 13:52	Not neighborhood friendly					
	If I were the Knob Hill Inn, I wouldn't want that out of my front entrance.	Unless I'm mistaken, this would replace the existing office/small business locations there. These businesses will have to relocate.	Less of an issue at other locations. Less traffic and demand for parking downtown is probably better		See above	
7/7/2016 14:03			I own Two restaurants in the downtown core sushi on second and the Cellar Pub. I believe in free market but I don't agree with cart vending because they don't abide by the same rules as brick and mortar.		I think we need more visible gas stations. It would be great to have one on the main thorough fare. The two we have now are kind of hidden.	
	It fails to allow a smooth transition from development to natural landscapes.	=	Such is not needed. Ample service exists in town.	Traffic congestion will appear with predictable unsafe conditions.	What specific contributions does such a project provide to the beauty and attraction of the area? How does customer convenience outweigh the natural conditions of the local environment? Why drag business away from the city by degrading the environment?	Each act of development reduces the very conditions that attract people to the area.
7/7/2016 14:11			t		All relevant info.	No. and Control of the Control
7/7/2016 14:35	yes	no	why not	поре	Ketchum is only catering to the 1%	Its good for growth and the middle class who can't always shop at the high end stores

7/7/2016 14:35	Too dangerous pulling On and off the highway there	Traffic accidents, already a busy corner there	No problem with that	Yes the safety there is my biggest concern	Yes the safety there is my biggest concernthere are already three gas stations very close by. I don't think we need another one there	Strongly oppose. Thanks for doing the survey
7/7/2016 14:43	Location on the highway.		This may help alleviate traffic in the core. Food cold be picked up as tourists, hikers, etc. leave town for activities in the SNRA.		What will the take out restaurant be? It should not be a McDonalds or any other large store. It should retain the character of the town.	
7/7/2016 14:42	Not an appropriate and attractive gateway to town. Creates competition for existing nearby stations on Lewis Stbetter to have 2 successful stations/businesses than 3 marginal one. Marginal businesses do not maintain or beautify.		If we're going to have zoning, stick with it.	Already a dense traffic One	See above	
7/7/2016 14:53	The congestion at that intersection is already a problem. Plus there is another business just like this a block away, and that business doesn't seem to be that busy whenever I go by or stop in.	Once a gas station is installed it would be difficult to turn it into anything else if that business fails.	A restaurant would be appropriate, but not one attached to a gas station. It would be a great spot to grab some good food on the way out of town, but if it's just the same junk food as basecamp and veltex have, it would be a waste of space.	Yes, that hill on 10th is a nightmare in the winter, if there is stop and go traffic on it, people will be sliding into each other. I worked at the building on that corner and watched this first hand.	I don't believe that this would be a profitable business for someone given the other gas stations and convenience stores within a block or two. If the business closes, what would happen to the space? Abandoned gas stations are an eyesore. The traffic is a concern.	100 % Oppose. It is not needed. There are plenty of options to get gas and junk food in town.
7/7/2016 14:52	The community should support the private industry and capitalism	Provides jobs	Why not?	Traffic in Ketchum has always been a problem.	The cities responsibility is to provide opportunities for growth and jobs.	Its the right thing to do.
7/7/2016 15:07	Perhaps a well-designed sign welcoming guests to Ketchum would be more attractive and inviting at the northern entrance to our City.	It might cause a traffic bottleneck.	This might set an undesirable precedent.	It could impede the smooth flow of traffic on highway 75 and also on 10th St.		All of the reasons given above. Also, it is very near our home and we do not see a benefit to having it in this location.
7/7/2016 15:15	In my opinion would think a convenient store, gas station and take-out restaurant would be highly inappropriate as an entry way to the city of Ketchum so not in keeping with the character of the city.	Would only be negative in that this kind of use would only lead to other use that I believe would be inappropriate to the entry way to the city i.e McDonald's or fast food and frankly take-out restaurants at gas stations is basically a fast food restaurant	See comments on section 2 not in keeping with character of the city	Given that this is the last gas station, take- out restaurant for a very long distance would provide a dangerous precedent for the many vehicles taking a left turn into the project	the use of a gas station with a convenience store for sure not a proper use of land a gas	It's not even close. This is just a totally poor use of land property in the beautiful environment of Ketchum and its surrounding environment. To repeat convenience stores and gas station should not be gateway to the city.
7/7/2016 15:16	Is it zoned for such usage? If so, then "yes". If it is not, then "no". I live north of town in a neighborhood full of NIMBYS. Nothing will ever satisfy them, so of course they are against it.	Yes, people passing through heading north will have a convenient place to fuel up on their way out of town.	Because then it might have a chance to be economically feasible. Being in the "core" is economically challenging	I don't foresee any traffic impacts or safety concerns that don't already exist everywhere else that brain dead people drive	lighting, parking, hazmat containment, zoning and ordinance compliance	Mostly because people traveling north do not have a place to grab their last minute items (including fuel) without leaving Main St. once they enter town.
7/7/2016 15:10	Folks who live north, as well as the huge number of residents in warm springs, need not have to go to town square to shop. This is positive, especially for parking and traffic.		Any, and I mean ant expansion of services for residents and visitors is positive. Why should the city govt dictate which businesses are appropriate? Let the market decide. Ketchum is still part of America, right?		Common sense. Let go of the past, and get on with it!	
7/7/2016 15:40	It is the first impression of our city from the North, not a good description of what we are. The traffic on 75 is pretty busy and this would be a congested area with traffic going in and out.	many places to get a bite to eat. It is out of character for Kethum. It sounds like a 7/11	think the people who have helped the town grow in a nice way by not letting box stores and fast food restaurants cover Ketchum has helped make it the place people want to come and recreate.	impact the safety of the bicycle and pedestrian traffic at that location as well as the vehicle traffic coming and going on 75.	two most important to me is changing the image of Ketcum and the traffic issues.	I definitely oppose this project, the type of project and where it is to be located.
7/7/2016 15:23	More competition is a good thing.	L	Why not	Easy in. Easy out.	Parking	

7/7/2016 15:36	This is across from the Ketchum Cemetery & in appropriate for entrance to Ketchum.	Traffic impacts, visual impact from a gas station that could service trucks, etc. from a gateway to Ketchum from the North.	Probably all right, if not connected to a gas station, & small in size.	There is a lot of traffic along the highway & only small shops should be allowed.	I think the local input is very important to consider!	I feel it is too large a project for this location - there are other gas stations nearby, & the visual impact would be unfavorable!
7/7/2016 15:30	Base Camp is one like a block away.		Define take-out restaurant. I'm more okay with take out as opposed to a gas station.	I feel like the traffic impact of projects like these always are underestimated. Just an observation.	Choose wisely.	I don't think it's needed.
7/7/2016 15:44	This is the Northern entrance to our town. A gas station and convenience store is NOT the kind of image I would appreciate seeing as I come into town. It sends the wrong message and would make our mountain town look more like Boise or Twin Falls than any of us would want. While the Ketchum Arts Commission is working on designs for friendly and beautiful signs for our entrances to town, this gas station would be in absolute opposition to that effort. Three other gas stations are close by	Some stated above. A bad precedent that could make the entrance to town a "Strip" like Twin and Boise and any other urban setting in this country	Easier access for workers at lunch time not to have to go into the center of town. however, that tied with a gas station is unacceptable to me.	Pedestrians and bicyclist could be dangerously impacted	Traffic counts in high summer season Bike traffic counts Need - three other gas stations within very close proximity Aesthetics	All the above
7/7/2016 16:01	This would be terrible and a blight. Hwy 75 at 10th is already very congested. There is a major landscaping company and an auto shop on 10th that make riding a bike/walking and driving there a nightmare. I know because I do it every day. Adding all the trucks and cars that this new gas station would attract would be horrible. There is already a gas station ONE BLOCK AWAY!	As I said, the congestion in this area, 9th, 10th, Hwy 75 is already very very bad. Cars are parked along the hwy I. Front of Knob Hill and on the other side, there are cars parked both sides of 10th day and night which almost makes it a one way road. I ride my bike there and it's dangerous. The noise, congestion and pollution added by a gas station would be terrible.	Take out restaurants are frequented by cars. As stated above, we are very congested and there is no surrounding parking AT ALL!	I have detailed my concerns about traffic and safety above. In addition, I cross HWY a 75 from my condo every day. It is dangerous, and I use a cross walk. You will essentially be inviting people to park at the cemetery and run across the highway as there is NO available parking in 10th or 9th or on HWY a 75 nearby.	Please consider those of us who ride our bikes in the area. Mit is already dangerous. With the addition of delivery trucks, fuel trucks and garbage trucks, it will be worse. You are encouraging students from Hemingway to walk up a very busy street to go to a "take out" restaurant. The added pollution and environmental impact of idling trucks and cars, as well as underground gas tanks is something Ketchum should not even be thinking about. We live here! The quality of our lives will be negatively impacted by this. Only two people benefit: the buyer and the seller.	Please do not spend any more time considering this ill-advised blight on our city.
7/7/2016 15:58	Traffic problems Don't need another with a gas station only a few hundred yards away. Moves people away from downtown	It will take away business from other same- type businesses. Gas stations are ugly!	Why not? Its their nickel.	Too many turning vehicles for this location.	Outside design. Environmental factors. Traffic study. Tourist reaction to be introduced to the town by a gas station.	
7/7/2016 16:03			Let's keep businesses like that in the core.		Traffic safety and sight lines at that rather odd-shaped intersection.	Great place for a gas station and, if well designed, it would be a good gateway for tourists coming in to town from north of Ketchum.
7/7/2016 16:11	I think a health take-out restaurant would be a great fit for the location. I don't think it's an appropriate fit for a gas station.				I feel there are already enough gas station in that area.	
7/7/2016 20:59	It will be dangerous and an eyesore. It is not what we need in such a visible part of our community.	The impacts will be negative: it will hurt the feel of this community and it may also lead to physical harm.	I worry about the noise and traffic from it.		Fundamentally, it is unnecessary and its harm far outweighs any benefits (what benefits?). The harm from the poisons of a gas station, the pollution, the traffic, the idling. We have enough gas stations, they are where they belong away from downtown and homes and non-industrial businesses. Convenience stores undermine the character of Ketchum. They are unhealthy and unnecessary.	It is unnecessary and harmful!

7/7/2016 16:29	This location is in a scenic corridor, it would harm views north as one drives out of town. There are enough gas stations very close to this location anyway.	Scenic views would be degraded - scenery is one of the city's most valuable assets.			Please don't impact the scenery in this majestic gateway to town.	
7/7/2016 16:34	Particularly for a gas station as it would be placed at a convenient location for tourists entering or exiting town.				Traffic patterns	
7/7/2016 16:39	It is very visible on the way out of town.		Better parking		Traffic study	I am actually fairly neutral.
7/7/2016 16:48	I think we have enough gas stations. I think it like other towns when you enter make it look junky, trashy and I think it will cause even more congestion, then already exists	Terrible entrance for a small town. Congestion will be even worse than it is. People are already honking, yelling and flipping people off, with the weird entrance with Warm spring road and everyone trying to switch lanes	I thought it was decided long ago no chains in Ketchum. If that changes, I will be pissed. It is enough we have Starbucks.		Why are you so set on making this town like Vail or some other big resort. This town has been fine until all this big hotels etc.	
7/7/2016 16:45	I feel this location is a narrow point in the road heading south into town and would create more congestion leaving and entering town. I also feel that it would make the 10th street intersection more challenging and unsafe. I am also concerned about the gas storage tanks. How deep will they have to be buried? If there is a leak will it affect the properties downhill of this location?	As mentioned above, this is a natural narrowing of the road and a gas station/restaurant will create more traffic and problems entering and exiting the highway. The intersection to 10th street will also be affected as well. Instead of an easy flow coming into town from the north there will be more congestion, confusion and problems.			I believe the traffic implications and problems are the most concerning for this project. I believe that a gas station at this location impedes the traffic flow to and from town. I am also concerned about the gas storage tanks and the possibility of leaks. The lighting issues should also be taken into consideration. Also, as an entrance into Ketchum this would not be a very welcoming sight. I feel that having a take-out (does this mean McDonald's) restaurant may prevent people from enjoying other dining options that make our town special.	I oppose this project because I am concerned about the traffic congestion and safety at this location. Heading into town from the north, this location is where the road becomes narrower. I also do not feel this is the best entrance into Ketchum.
7/7/2016 16:58		Congestion in and out of town. Access to Industrial area and YMCA, 10th St. and Knb Hill Inn.		See above	Trips per day for each of the establishments.	Traffic access and congestion
7/7/2016 17:06	I like the idea of a store and gas north of Ketchum. I miss the North Fork Store up by the SNRA.	All good. Existing Gas is hard for tourists to find. I think it's good to have gas and stuff on Main St.	the more cheap eats the better. I think we need a taco truck / food truck scene gas up and go.	Nope. It's be easy on and easy off.	Hey, it's a free country. Let the guy do whatever he wants as long as it's not obscene. This is Idaho.	I think it's a great idea. Perhaps we can keep Lizzys on the property or find her a cool place elsewhere that's cheap rent.
7/7/2016 17:11	It's outside the main core of the city and it provides a service to our visitors who can't find the light industrial area	There will be fewer people driving around lost looking for a gas station	As long as it is already zoned for it and impacts are mitigated. Why not?	To ensure impartiality, the city should choose a traffic engineer for a study paid for by the applicant.	The existing zoning code and all the other adopted building and fire codes.	Mild support. Not enough information to make a truly informed decision.
7/7/2016 17:18	people live right there. don't need another gas station. the new hotel is ugly enough. What were you thinking. Can't even see baldy anymore. I have heard so many negative comments!		Sure but maybe not there		Talk to the neighbors first. Aren't there businesses already there?	
7/7/2016 17:17 7/7/2016 17:15	10th Street can be dangerous in the winter, but actually year round. Additional traffic at that intersection will be a problem - especially crossing the highway, leaving and turning down 10th St., not to mention the 10th St strip commercial across from the gas station.	not need another gas station. There's one a block down the hill. We already have 5 is a 1 mile radius. Furthermore, the environmental impact is pollution. As to the	,		I've answered this above.	Absolutely oppose. If the current owners want to sell, find a buyer with an appropriate use. I could see more affordable housing there, or another condo development, or mixed use similar to the nearby Frenchman's project.

7/7/2016 17:28	There are 4 gas stations in a 1 mile radius from this location. We used to have several gas stations on Main Str. They are all gone and replaced by better looking businesses.	Yes an environmental impact for sure. Constant checking on underground leakage etc. and then of course a construction site on the main drag!	Why not. People don't just live in the core, plus the current parking situation, where you cannot park twice in the same block in a day, does not make a take out attractive in the core.	A gas station is always connected to traffic. There is only 1 main road going north and lines at gas stations would not be good. Plus most of the times there is a convenience store with a gas station, so people leave their car either at the pump or in the area next to it. There would not be enough room for that.	Necessity of another gas station Parking Access to main street	For the reasons given above
7/7/2016 18:20	This is not the first impression of Ketchum that should be seen when entering from the north.	Giving a strip mall feeling or cheap feeling upon entering from the north. It would be better to see a nice building or entry to town.	Too many ch traffic congestion. See answer to question 2.	Traffic could back up onto 75/Main St. And drivers might divert onto Dollar road toward Zenergy or the Y leading to congestion there and unsafe conditions with the bike paths.	hours of operation, what kind of fast food	All the reasons and concerns I commented on above.
7/7/2016 18:19		Detrimental in all areas. Environmental and otherwise. Add charging stations not gas stations. We have to change.	Yes, but not here.		Ketchum. Energy source of the past not the	Environmentally. Scenic. Not to mention I have never had to wait in a line to get gas - there are plenty of stations.
7/7/2016 18:17	there is a gas station one block down the street. we have Veltex nearby and what is next? Mac Donalds?	It seems to cheapen the experience especially at the entrance of Ketchum. Put in a nice restaurant like the old Ciro with a take out deli. That would liven this part of town. Then sidewalks would be put in to accommodate people walking from town. The Knob Hill would benefit from the competition. It would be more aesthetically pleasing that a gas station and a fast food joint.	why? I think I described my opinion in the above. #2	Safety always. Maybe there will have to be a stop sign, but traffic to a pleasing destination (such as Lissy's Coffee!) may be inevitable. It depends on the numbers of part time and full time residents.		as described, it sounds like it would not be a quality hub that many people would want to visit. Maybe need, as in gas, but there is one down the street. All you need is a sign telling people it is there. I think that the aesthetics of this project should be carefully considered as they reflect the identity of our wonderful town that we love.
7/7/2016 18:29	i do not see the need for another gas station. there are potential traffic issues. trash issues with take out. not a good fit for Ketchum	as above traffic issues. also may create an entry to fast food chains and other chains not appropriate for present and future Ketchum	trash, as above	any addition anywhere in Ketchum has negative traffic impact. creating entrances/exits to the road with passing traffic, bikes, peds is asking for trouble	??	above
7/7/2016 18:39	This is a great spot for a C Store/gas station, as long as traffic considerations (turn lane into station, good turning radius onto 10th St.) are taken seriously.	Only positive impacts.	Yes, if it is contained inside the C Store and not 100% stand alone.	Access to 10th St. needs to be maintained and proper traffic flow including North and South turning lanes into the retail property will need to be considered	Ordinances currently in place and the overall theme of providing good, convenient services to our residents and guests.	See above responses.
7/7/2016 19:04	only if they are willing to fund two electric charging stations free to public and patrons in perpetuity.	possibly to the gas station already in operation at the bottom of 10th and Warm Springs. Competition is not entirely bad.	not a good idea to decentralize the core more than is under way. Walking to this location is out of the question unless you're crossing the street from Knob Hill Inn or 10th street. Maybe requiring some loner bikes, in Summer months, would facilitate exploring town while their car was charging.	I	as lighting and height limits. Access should	As a commercial core owner of real estate, I could support this project if the comments I've made were seriously considered and implemented.
7/7/2016 18:55		Traffic!! It's crazy!!			Traffic study	
7/7/2016 19:18	Absolutely NOT! First, we do not need another gas station, mini market. There is another one, just one block away. It is a terrible traffic risk to the public. Also, there is risk in building on that property, safety as well as health. It is currently home to many local businesses, as it should be, we need to protect our local businesses.	We do not need to have the presence of an Exxon Gas station when we already have plenty of options and locations for people		As noted above. Completely dangerous to all!	Safety, health, environment.	

7/7/2016 20:45	Anyone coming into or out of town at the north end can not find a gas station without winding through side streets. The gas stations in the LI are not to modern standards and do not meeting the definition of "fueling stations" because they only dispense gas and have no attendants on duty to sell other automotive products. Mostly, you can't turn around in them, and the Base Camp one is so inadequate that traffic is stalled while people try to use it.	People will now be able to find a convenient and modern version of a gas station in Ketchum - something they have come to expect everywhere else - and most likely it will help to lower the very high prices being charged by the current gas dispensaries.	If you mean a convenience-store type of food outlet, by all means. It is something Ketchum does not have and many people like to avail themselves of such outlets when they don't have time for a sit-down restaurant experience that Ketchum has in abundance.	It is a low traffic area, with hardly any pedestrians, located at the edge of town where gas stations typically are put. That's why the ordinances allowed for this use in this place. They planned for the traffic impacts of a simple gas station to be concentrated beyond the core where there is less confusion. It's a good place for this.	How the project conforms to the zoning rules. If it does, then those people should be allowed to develop their property. "Public opinion" as represented by self-interested factions is not public opinion, it's special interests. There are always plenty of those and they will always distort what is thought to be "information." This matter needs to be considered in terms of whether it conforms to the zoning codes. Nothing is ever going to change if we keep allowing special interests to scare away people who would make improvements. The proposal for a modern supermarket where the lumber yard used to be is another case. One very powerful special interest called out all their employees to pack the public comment session. This made it seem like "the public" was against it. I very much doubt that the public was, but you cannot get "the public" to express themselves when they don't have passionate self-interest to match the naysayers. They leave their public officials, and the rules, to do that.	Let's move our town into the 21st century by allowing some things to change - even if they get in someone's view! Our town is stalled because of all the people who don't want things to change getting loud and assertive. It is truly disgusting.
7/7/2016 20:10						
7/7/2016 20:35	Worst location for any of above mentioned				Traffic, aesthetics, and impact when it goes	
7/7/2016 20:58	items				out of business Who owns the contemplated project and do	I neither "Support" or "Oppose" this project
					they have the financial capability to sustain a long term use; such that the project does not fall to wasted site that later becomes a ghetto-like eyesore in later years. There must be a substantial financial commitment upfront and sufficient guarantees that the project will sustain for many years to come. Fast-Food Takeout Restaurants often have very short lives; so the sustainability of an ongoing business is important—for multiple reasons. Notwithstanding the fact that even the "service station" is going head-on in competition with "Basecamp", which has had a very good history to date Can someone fully define the label/term "Service Station" (mechanical)("Full Service?)(?)	However, it is not yet fully defined Need to see some definitive facts and renderings and financial capability and commitment to sustain in to the future
7/7/2016 21:29	does not fit character of town. Is not	it just sets our 'charm' back a notch	we should be concentrating on the core and	1	does it fit the zoning? If not we should not	
	needed with gas station and store at		not encourage sprawl at the edges	already	give an exception.	
7/7/2016 22:25	we need a 4th gas station in close proximity to 3 others and a 3rd convenience store?	what are stats for police calls/crime at other Ketchum convenience stores?	I don't have issue with that as long as it's not fast food chain	See above comment for safety question	Crime stats/police call stats at other convenience stores. Traffic studies Pedestrian studies Relocation options for many displaced businesses given lack of affordable retail venues	Unsure
7/7/2016 22:30	, , , ,	, , , ,	, , , , ,	, , , ,	Surveys are NOT a good way to make zoning	, , , ,
	decisions.	decisions.	decisions.	decisions.	decisions.	decisions.

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7/7/2016 23:34	A gas station will impinge on image of the City for visitors and travelers coming from the north.	Possibly. It will help determine the appearance of the north end of Ketchum for many years.		store aspect of the project will increase traffic beyond those just coming to use the gas station. During busy seasons, the increase in traffic may lead to some accidents. Too much potential for cross traffic.	If the project goes forward, its appearance should be as ungas station-like as possible.	
7/8/2016 0:17	Ithere are several gas stations in that area & it will dilute the business. Do we really have a need for another one? I wouldn't think that is a great location because of odd corner shape and no easy way to come or go on to highway.	Diluted business to the stores that are here	Core is where it should be	Yes. Odd "corner". Not easy to see getting on to highway. Potential accident waiting to happen if increased traffic in location	Putting lots of businesses in position to have to find zoning to be able to work. Traffic safety. Lights at night.	
7/8/2016 3:27	too close to the downtown core.	Gas stations do not visually and environmentally hold up for very long. In 10 years it will be an eye sore and hard to remedy. show me a 10 yr old gas station that is not an eye sore and an environmental mess!		turning in and out of the location	long term impact gas stations do not visually and environmentally hold up for very long. In 10 years it will be an eye sore and hard to remedy. show me a 10 yr old gas station that is not an eye sore and an environmental mess!	I lived in the valley year round for 5 years and have been visiting for 35 years hold on to the flavor and the environment of Ketchum don't lose it!
7/8/2016 3:44					Inappropriate, tacky, dangerous, non- conforming. Perhaps some directing signs to fuel and food in the LI2. There's a rockpile next to Smith which is below eye-level, conforming, safe, and does not violate the entry to Ketchum.	
7/8/2016 6:35	It is a location that can actually serve the visiting tourist and traveler because they will be able to find an actual service/gas station.	Any impacts will be positive.	What does it matterthis location is still at the very edge of downtown and is on Main Street.	cause any traffic or safety issues.	Most important is the need for a service station that will serve the needs of our tourist industry.	I believe that this project will be a benefit to our growing tourist industry.
7/8/2016 6:57	It is on the main highway. We only have one gas station presently and it is on the southern entrance. It would keep traffic out of warm springs and the LI zone. Campers and large vehicles will have safer and much more convenience.		Why would this hurt anything. We have take take out at mtn view and bigwood bakery.	I believe there is a 35 mile an hour north of this and 25 in this area.	Safety and traffic flow.	A needed service.
7/8/2016 7:40	The traffic pattern at this location is hazardous as it is. Putting a high volume attraction there without closing access to 75 from 9th St. would be folly.	Without strong architectural controls, which I don't think Ketchum has, it would be an ugly addition to the start of the downtown area.		A thorough traffic study should be undertaken.	Traffic, lighting, design,	
7/8/2016 8:07						
7/8/2016 8:25					Environmental and safety	
7/8/2016 8:42		nothing adverse	If this use complies with the Comp Plan.		Comp plan impact on properties in close proximity. Is a zoninc change needed?	It is only one block from a similar use at the Base Camp C-store
7/8/2016 8:49	It will create unneeded traffic on the hiway and clog up entering town from the north where we live. also there is a gas station one block away. don't need two gas stations in two blocks	I can't help but thin a gas station/convenience store will be a junky addition to the north end of town. There are no gas stations on the main hiway anywhere else in town and I don't think we need one where this one is planned.				
7/8/2016 9:05					Community Input	
7/8/2016 16:04		Traffic congestion and safety will be a problem at the entrance to and from the highway access to service station pedestrian safety is a problem, Lighting pollution is 24 hours per day 7 days per week			Traffic study Site circulation diagrams for cars or trucks with boat trailers on site. Site circulation diagrams for fuel truck and trailer on site	

7/8/2016 9:48		Detracts from the uniqueness of our town in that we don't currently have "strip" development.	I don't have any problem with a take out restaurant being located around town so everyone doesn't have to go into the tourist zone just to get a quick lunch.	Absolutely unsafe especially due to the proximity to 10th street.	Vehicular Circulation: The vehicular circulation to the gas pumps, especially for a rig with a trailer, does not work. Need for right of way to make this project work. They need to ba balbe to screen on their property. Need for taking of alley with retaining walls. Alleys should remain passable to wheeled vehicles. It is public property.	It just does not work. The internal circulation does not work. They should not be allowed to use the right of way for their landscape buffering, to in effect make their proposal work. They should not be able to effectively close off the alley.
7/8/2016 10:16			not sure- if not zoned for take out then NO		Lighting Speed control in the area (already exists) adequate access to get off road.	
	Bad location. Take out restaurant should be in downtown core.	Additional traffic congestion on Highway as traffic enters and exists the property at a location is not conducive to it.	Keep such uses in the downtown core.	Intersection is already busy and having this business will increase problem.	Traffic and negative impact on allowing this type of businesses outside the downtown core.	
7/8/2016 10:43			You guys have got broaden your definition of Down Town Core — its limiting and small minded. Why should Lizzy's take out coffee be allowed and not a take out restaurant? COME ON!		The number of jobs it will create for members of the Wood River Valley Community	
7/8/2016 11:43					is important to understand this will change the flow of traffic, and a plan must be in place to cater to the growth of Ketchum.	If we want to allow Ketchum to continue to grow as a city, we must realize the current boundaries must be pushed. By this I mean that Ketchum will need to expand outward from the city core in order to keep growth alive and prices for rentals, property etc. low.
	The entry into Ketchum from the north should be attractive and inviting. I don't think a gas station and convenience store in this location is appropriate for a gateway into town. There is already one, Base camp in Warm springs in the industrial area where it should be	I think it will detract from the beauty and community feel of the north entrance to Ketchum	I think restaurants with take out that invite more traffic in and out should be within town not on the outskirts	This is a busy area for those coming into town and it may get congested	Aesthetics, traffic and need	
	Ketchum needs an accessible gas station. That is a great location for one.	no negative ones.	Great place to grab lunch on the way north.	No more than any other location.	Pay attention to the zoning regulations and not the political impacts on our leaders from the NIMBYS.	100%
	I think it is fabulous. Since we have no more gas stations on main street. People have a difficult time finding a gas station.		it is exactly what we are missing. where are all our workers suppose to eat?		I have seen the design and love it.	
	This is absolutely the best location in town. I am happy that we will again have a nice gas station.	There will be positive impacts for years to come. We lost all our fueling stationsd years ago to the banks which have had a horrible impact on MAIN street. Finally we will have a convenient fueling station that people can depend on when they come to town and through town to explore our incredible national forests for many years to come. As years progress this fueling station will be used more and more and counted on as final stop through Ketchum.	Absolutley. There already is a take-out restuarnt out of th core called "Big Wood Bread". Take-out should be out of the core just as the City Hall should be out of the core.	The gas station will increase safety due to proper planning and slow vehicle traffic which will add to saefty. Traffic will then flow steady and fsaster AFTER the saddle road light.	, -	We need a modern and efficient fueling station for the many people that pass through town going North. Many people don't stop in our town because there is nothing to stop for.

7/8/2016 12:28	It is zoned for it.		Grumpy's, Bigwood Bread.	It will alleviate the large RV's bogging up Warm Springs trying to squeeze into Base Camp.	Is it a project that conforms to current city zoning laws?	Ketchum needs better access to gas stations, and it conforms to current city zoning laws, and would look better than the decrepit buildings currently on site.
7/8/2016 12:29			Why not		The environmental and geological safety of the installation of gas tanks would be my concern. Professional survey and geological land study could easily answer these concerns.	
7/8/2016 12:37				but no more than a large hotel on Main St.	Zoning regulations, future traffic flows if we ever get highway south of Ketchum completed,	Private property land issues if the regulations allow construction
7/8/2016 13:35	Great route for going north on the highway. Ketchum needs a nice gas station.	I know that one person chose to build near the station. Decisions are for the good of the whole, not the one.	Why not?	As long as the space is handled correctly and the buildings are not right next to the road.	AppearanceAccessibilitynot heavily lit at night.	Needed
7/8/2016 12:51	If you allow a giant hotel that impacts the views on main street I believe that a gas station is gas station on Main St is more needed for tourists	In a positive way	Most restaurants allow take out food already, should you stop them from doing so?		NEED	
7/8/2016 12:58 7/8/2016 13:20	development rights	development rights	development rights Big Wood really works	development rights	Development rights Design consistent with current standards.	
7/10/2016 17:01			It'll hurt the downtown core. Plus, regardless of what it starts as, it could be a major chain down the road, which will certainly harm the local restaurants in the corewho are where they are because that's where they're supposed to be.	When turning off 10th street onto 75 in the winter months is already so difficult and dangerous, how can the city even consider this?	The safety of all the workers who already navigate that and struggle with that intersection. This is crazy to even consider! There are also environmental concerns and impacts to surrounding properties that were mentioned in public commentary. This proposal is not in line with the general wellbeing of the community.	For reasons stated above.
7/8/2016 13:29						
7/8/2016 13:45	There are several reasons why this location is not appropriate: First, it would be an ugly eyesore at the entrance to our town. Second, the location already has serious traffic and pedestrian problems, the new gas station would make them worse. Third, the proximity of residential areas poses potential health dangers and risks fumes, explosions, etc. Finally, the proximity to the school and park would attract children to the area.	The last thing we need is another gas station, another convenience store and another take-out restaurant in our town. They contribute nothing to the welfare of the town nor the economy (these goods and services are already being provided!). The station could be bought by a multinational company and we would be powerless to stop it. Finally, the potential for a fire and the health risks are serious long-term negative impacts that far outweigh the potential benefit.	The restaurant would only increase the traffic in this problematic area while the city should instead be encouraging pedestrian traffic inside the core.	Absolutely! This is hands-down terrible location for a gas station and it's hard to imagine the proposal has gotten as far as it has. This area already has serious traffic problems, adding a gas station could drastically increase traffic, especially traffic from trucks and large vehicles, creating safety risks for pedestrians, and especially for children. As a child, I used to walk all the time to Circle K (now Veltex) to buy candy, soda, etc. The same will surely happen here.	The P and Z and the City of Ketchum need to think hard about the future of our town when evaluating this project. We are growing and will continue to grow, but we cannot accept growth for the sake of growth. This proposed station offers nothing to our town and the risks are tremendous. It seems like a no-brainer case for the P and Z and I hope the right decision is made.	Please see the comments above.
7/8/2016 14:28	Perfect Location. Would serve community and Valley wonderfully.	Would only get better and better		I am sure that can be handled.	That it looks architecturally nice and in keeping with the surrounding area.	I wholeheartedly support this project. Why would the Major of the City of Ketchum be
						getting involved with a survey for a piece of property that is to be developedto me that is B.S. This is not the role of the Major. Is the major going to do a survey now for every piece of property that is going to be developed in the City of Ketchum? Is the major not pro-businessonly the businesses that she subjectively thinks are "right" for the City of Ketchum. Who does she think she is????
7/8/2016 14:13	There is already a convenience store a half block away and already three gas stations in the area.	Excess traffic at that first light entering town to the north. Today I was driving at 1 PM and already saw a back up of a quarter of a mile at that light.		As stated before there could be an excess of traffic at that first light entering town to the north. Today I was driving at 1 PM and already saw a back up of a quarter of a mile at that light.	The state of the s	property that is to be developedto me that is B.S. This is not the role of the Major. Is the major going to do a survey now for every piece of property that is going to be developed in the City of Ketchum? Is the major not pro-businessonly the businesses that she subjectively thinks are "right" for the City of Ketchum. Who does she think

7/8/2016 14:21	Lowering prices	More competition ðŸ~Ž lower prices	Construction workers		City rules/ the architectural look	City core traffic could be eased at prime
,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,						times.
7/8/2016 14:50	If that is what is zoned for.		If that is withing the zoneing		?	?
7/8/2016 14:56	There is already a local gas station and store in the area	Traffic increase. Competition to local business already there	Convenience for those of us that live in warm springs	Increased traffic would be a bummer. A lot of children, cyclists and families use that area on foot and on bike and the configuration of the streets converging make it easier for pedestrian incidents or accidents to occur.	Is it needed? Does what currently exists fit the need? Gas station wise I think we already have a fix in place. I would like to see a take out place but think more gas stations is not the answer.	
7/8/2016 15:03	Good accessible location.	No different than any other LI use.	take out restaurants should be kept near the core to keep the core vibrant.	northbound traffic will slow down and southbound traffic will have easy access.	Emergency back up generator to run gas pumps and accessories during extended outages.	Gas station north of city core with easy access from the highway is needed.
7/8/2016 15:23					traffic study	
7/8/2016 15:32	If I owned the property I would have serious issues with business being conducted this way.	Have no idea what this means. It appears to be implying that there would be long term negative impacts and that doesn't seem fair to have people guessing on.	Of course. It is around the corner from Grumpy's and they do take-out along with a movable food truck. Not sure what the issue is here.		it would be a taking, regardless of public	You should have an option of "neutral" since some issues are not that clear cut and you can't leave the survey without checking one of the other. I get that is the point but do a realistic "survey". I was leaning toward "oppose" but clicked "support" since this survey seems to be skewed against the property owner. If I owned the property I would have serious issues with business being conducted this way.
7/8/2016 16:29	Current zoning allows for development of the property for these businesses. If the development is within code restrictions and owner/developer proves to the best of their ability there will be no significant negative impacts to community and neighbors they should be allowed to develop the property as they choose.	As a blue route Mountain Rides user and pedestrian that frequently utilizes 10th St. I would hope that development of this property would improve both the corner AND the entirety of 10th St. hill for ALL vehicles and pedestrians using 10th to connect with Hwy. 75, and perhaps slow the plethora of speeders on that portion of the highway.	As with any new business in any area the reality is they will have to establish a following. I see this as simply a convenience effort for travelers and locals and would be shocked if it significantly impacts other area restaurants.	especially when it is bordered on 2 sides by	-Vehicular and pedestrian safety -Impact on neighboring businesses and residences -Environmental impact/Environmental safety -Character of development of adjacent business and residences, notably Stock Lumber	Truly neither support or oppose. As mentioned previously the owner should have the right to develop property as they choose if zoning, code, and public safety compliance is followed.
7/8/2016 15:44						
7/8/2016 16:27	The North end of town is a great location for these type of services and would enhance the town's ability to service travelers and visitors.		Yes, if people had a take-out option outside of the core than it would relieve parking congestion when you just need a quick bite to eat during the noon hour.			
7/9/2016 21:46	Visitors have often asked me where the heck is a gas station in this town. Great Location. Something else will end up there that will have just as many issues to solve. Just do it.	Yes. Positive impacts. Tax revenues up for the city. Some infrastructure issues to be dealt with but doable. We're just giving the business to Hailey and south. Don't we want people to stop in Ketchum and drop some cash?	Yes. Lets diversify a bit. We're big enough to spread it out. Junk.	No. Some adjustments may have to be made but it It will never be more open than it is now. There is enough room to engineer a solution by some creative people. Why should the Knob Hill automatically get 10 parking spaces on the highway. Move those overflow spaces to the north or elsewhere to gain some room.	businesses. This isn't a 1950's grease pit gas station. It can be done nicely and be a real addition to the town. We need to have a visable gas stop for tourists. Isn't that the basis of our towns economy?	people going north will probably already have gotten gas and a greater % will need it coming south. An easy right turn.
7/8/2016 18:21	IONAL	Clutter, congestion, Road is already BUSY.	Julik.		Congestion @ the "North End" of Ketchum.	100 Much Stuff டூ this area aiready

7/8/2016 18:35	Dangerous corner from a traffic standpoint. Its bad enough as it is turning right or left to or from 10th and the highway.		No we already have eough local restaurants owned by locals that offer take out. If you are referring to a take out style restaurant in this location along with a gas station it will jsut make it one more part of the congestion.	yes see above,,,, it will completely create more congestion. Its already bad ad dangerous enough crossing the darn street in ketchum.	Traffic and congestion and dangerous to the public. Both from a pedestian standpoint and Sooner or later there will be an accident there either from a car and or pedestrian. There is no good way for travellers or bikes or walkers to travel from warm srpings to town. No safe way. Even if they take the bike path they need to get from Hemingway school up to town via streets. This is the only corner that allows cars to get up to town and access the West side of kethcum and if you add a congested gas station with cars coming in and out getting gas etc and supplies it will be even more lame. Not to mention, there is no need for it. We have plenty of gas stations in town.	It will suck and create congestion. Its already bad enough to the southnow to the north? No thanks
7/8/2016 18:32	Changes the ambiance/character of the					
7/8/2016 21:18	town. Heavy traffic area. No turning lanes and too narrow an area for widening the highway.				Traffic	
7/8/2016 22:04	As I understand the present zoning of this property, it is zoned for a gas station, convenience store, or take-out restaurant. What I don't understand is when and why the city of Ketchum decided to do away with the P&Z commission and its duties to the residents of Ketchum in ulholding current zoning ordinances. What exactly IS the purpose of the P&Z commission if decisions like this are now going to be decided by popular vote? And why just this potential business?	Yes, but unlike what I believe you are meaning by "impact," I believe it will have a long-term beneficial impact. It will be a visible and convenient stop for those entering or leaving town, especially those towing trailers, snow machines, off-road vehicles, boats, etc. I feel it can fill the gap made with the loss of the North Fork store and gas station.	As I mentioned in the previous answer, those passing through town for recreation, work, or whatever, will have easy access for quick in-and-out stops. I can't help but feel the owner of the new hous't help but feel intersection must have a friend in city hall to have caused this survey. If so, the zoning has not changed since that lot was purchased so why should that homeowner have a say in a potential new allowed business?	I'm sure the safety of pedestrians crossing the highway who park on the stretch in front of Knob Hill Inn will be much better once at least a crosswalk is put in, if not a light.	What is the allowed use of this property under the current zoning?	Please refer to all above comments. And I'd like to know, what is so different about this property than, say, the two new hotels at the south end of town to cause a survey? Or all the big box banks on Main Street?
7/8/2016 21:29	Would impede traffic in a dangerous area	An eyesore		Not a good place for cars exiting or entering the hwy	Estimated traffic effect	Enough gas stations already
7/8/2016 21:39				,	The usual due diligence that P and Z would	
7/10/2016 20:40		Potentially would set a precedent for more like type commercial uses (fast food,etc). I believe this would destroy the character and uniqueness of our town	Precedent to create strip malls and would homogenize Ketchum to look and feel like so many other towns and cities - I am prodevelopment but absolutely believe we need to protect Ketchum's uniqueness (the very reason we all moved here!) The responsibility falls squarely in our laps NOW as the current residents and council members to preserve Ketchum's beauty, uniqueness and charm for future residents and visitors alike!	could create bottleneck in north/south traffic and would be dangerous without creating a middle turn lane. Putting in a stop light would be even worse!	give it, including traffic studies. Components of the overall project: Gas station brand name, convenience store brand and fast food operator. Will there be a carwash? Detailed information about operators before giving carte blance approval for any and all operators	Enough said in above comments
7/8/2016 23:49	Already a busy major road	Too much merging traffic	No need	75 is already too busy	A really bad idea	Traffic flow
7/9/2016 9:35	Because there is one 1 block away on Warm Springs road. Main Street should be reserved for buildings/businesses that enhance our town and community and make it more bike/ped friendly.	Les bike/ped friendly, don't need another gas station, eliminates commercial space for small businesses.	You ever seen a good take-out restaurant at a gas station?	More cars = less bikes/ped. Let's make Ketchum a bike/ped friendly town.	The interests/opinions of a large cross section of local full time residents.	See above.

7/9/2016 10:38	There are already three gas stations within a short distance and another is absolutely not needed or appropriate.	Gas stations are inherently ugly. The North entry and exit to Kethum is currently tree lined and attractive. We do not want Ketchum to look like every other blighted town wth streets lined with fast food restaurants, gas stations and cheap motels.	Would cause increased traffic in an already busy and hazardous intersection. 10th street is already a disaster intersection and traffic would greatly increase on 9th street.	I travel 10th street every day. There are many large trucks serving Evergreen landscaping and tha 10th Street Light Industrial complex, dozens of cars involved with Ketcum Auto and many vehicles going in and out of the existing gas station on Warm Springs Road. The area is already congested and dangerous.	Gas stations are inherently bad for the environment, ugly, and cause property values near gas stations do be dramatically reduced. Can a city with any regard for its citizens add a gas station so close to a high end residential area? If this happens Ketcum will be liable for the damage caused.	Opposed for reasons of traffic congestion, environmental health of nearby residents, and turning Ketchum into an ugly blighted town like most other small towns in America. Now Ketcum is unique. Let's keep it that way. The developers behind this proposal are only interested in flipping it and making a profit. They will sell it to a large chain very soon after its built, creating an even worse situation at this location.
7/9/2016 10:20			only if its healthy good food		see above	need healthy food options in Ketchum
7/9/2016 10:49	We have three gas stations in town. I feel this is more than enough. There is no way to make a gas station look appealing. I would much prefer to see a restaurant or a retail store go into the location.	It will back up traffic getting in and out of the location on a stretch of road that is already very busy and only has one lane running in each direction. I feel it will create a bottleneck with cars traveling north and trying to make a left turn into the gas station. It will also be an eye sore. Face itgas stations are ugly!	I'm fine with a take-out restaurant so long as the architecture is tastefully done and there is plenty of parking. I would be VERY opposed to a chain restaurant though. I implore you to keep Ketchum unique and charming by NOT allowing a national chain to open up a location here in our beautiful city!!!!!	Absolutely! As I mentioned above this stretch of road is only one lane in each direction. You already have high traffic coming & going across the street at Knob Hill and this gas station would jam things up even more creating a safety issue for drivers, cyclists, and pedestrians.	Why do we need another gas station? There is one less than two blocks from this proposed location.	For all the reasons I mentioned above; ugly, not necessary, creates unsafe traffic congestion.
7/9/2016 12:43	outside core	wil help reduce price of gas in our	convience for travelrs	back ups to south on busy summer	traffic flows	competition
7/9/2016 13:00	We have enough outlets for food already and Hailey has plenty of gas stations.	commuinthy It will increase the congestion on 75	Done right there should be no problem.	weekends Again congestion on 75 through the town is already a problem this would add to it.	The traffic flow that would result from the project at this location.	It's the wrong location.
7/9/2016 13:55			only if it is done with care			
7/9/2016 14:33	It's too congested already.	More congestion	What's a take out restaurant? Fast food? Please don't allow a Burger King or similar to be built in Ketchum. Is t the burger grill enough?	It's already congested there, especially in the am and pm.	Where is the need showing up for these three businesses? From citizens? Or is it just a developer wanting to cash in? We don't need another gas station when there's one right nearby. We don't need a convenience store when velux is right nearby.	
7/9/2016 15:15	Locating this type of facility north on Highway 75 will allow people heading north for recreation opportunities to get fuel for the car and passengers away from the congestion of the core area. I doubt it will take away business from "core" businesses because my experience has been when opportunities are widely distributed they're widely supported.	I believe there should be long-term positive impacts because of the distribution of access.	Diversity is the key. I don't know what this proposed take-out restaurant will supply but I believe if the product is good people will want to patronize the business.	Over time it will be apparent what adjustments might need to be made for public safety and access.	It would be important to me to know what will be available. I don't want to see some junky facility because that is not what Ketchum is about. If the owners want to be in the community I would expect them to contribute to the overall character of the area and culture. If they don't, I expect they won't be in business for long. I visit the Wood River Valley several times each year to spend time with family and friends. It's always a joy to see new things and meet new people there. I appreciate the opportunity to comment on this proposal.	I always support considering new projects. I also wish the planning process is long and complete enough to discover potential problems so that solutions can be developed. It's all about growth and progress and meeting the needs of visitors and residents of the area.
7/9/2016 15:12	The entry from the north should not be compromised. Furthermore there seem to be enough gas stations in townand there is a convenience store a few blocks down the highway, or on Warm Springs road, or past town opposite Land Ranch.	It will change the character of north of town, making seem more commercial	Whereit depends. Lewis Street, why not? this location, no	People making left turns off of 75 going north, could back up traffic at rush hour. Likewise, extra traffic coming out of the gas station will add to the problems of merging into 75	What it will look like, what the lighting will be, how many bays, what hours it will be open (late hours would not be good given the general quiet of the neighborhoods north of town) how big will the signage be; will they be displaying flags or banner; how disruptive to generally bucolic nature of the north of town will this be. I understand that there us commerce there now, but it is very quiet, both visually and traffic wise. And there is no night time lighting there now to speak of.	For all the reasons and implications in the above comments

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7/9/2016 17:37	Perfect location.	I think it will be viewed as a community asset.	OK on a small scale.	I think the new plan will be an improvement.	If the project is allowed inside the current zoning criteria it should be apprved as it will	
					be an asset to the community.	
7/9/2016 18:21	entrance to town from north, zoned that	Better than what is there at the moment.	core too crowded, workers need to get		legal building for that site,	the opposition is purely political
, , , , , , , ,	way, I think the opposition is from	Unless the city buys it for police/fire station	lunch etc			
	interested parties who are manipulating the process					
7/10/2016 8:05	Traffic sight lines	see above	spoils small town feel. Will make it harder to	see above		
			maintain the diversity of restaurants downtown.			
7/10/2016 9:22		too much congestion.	depending on location	totally inappropriate location especially on	congestion, possible necessary addition of	not appropriate or necessary. we have three
				the State highway	traffic light, etc.	in Ketchum and one in SV. JUST BECAUSE SOMEONE WANTS TO BUILD SOMETHING DOESN'T MEAN IT SHOULD BE DONE.
7/10/2016 9:59	This is not needed and will detract from the	We will start to look like Park City or Jackson			Consider the impact of this project on the	
,,	charm of our town.	Hole as these projects are approved. Those towns are forever ruined.			character of our resort town. Chain stores do not belong here and are not needed.	
7/10/2016 10:53					traffic, congestion, noise	
7/10/2016 12:54	Ketchum is probably the last and only town	Those who are ignorant of history are	If you want to destroy Ketchum's	Of course. I'm particularly concerned about	Please consider the overall beauty and	Do I really need to mention anything in
	left in the USA that doesn't look like a strip mall from Southern California. I realize that	bound to repeat the same mistakes from the past. Just look at ANY other town in the	downtown core, go right ahead.	the tectonic movements caused by all the residents of the Ketchum Cemetery as they	unique charm of Ketchum. Nobody comes to Ketchum to enjoy the beauty and unique	addition to what I already wrote above?
	it's all a matter of taste, and some people	USA that has allowed this kind of		turn in their graves in utter disgust. How	charm of a strip mall, because everybody	
	may find strip malls to be the most beautiful			can they Rest In Peace?	who comes here is trying to take a break	
	things in the entire world.	long-term impact of such a short-sighted decision.			from such eye sores.	
7/10/2016 20:45	We don't need another gas station. We	Profoundly negative from both a cultural	I am assuming you mean a chain? Most		See above	Though I am a registered voter in the State
	certainly don't need any fast food operations. A huge part of the charm of	and safety standpoint.	locally owned and operated restaurants already provide take out and need as much			of WA, we have been property owners here in Ketchum for over 30 years. This project
	Ketchum is that we are free from Chains. Do		support as possible especially during slack.			would be a travesty to our beautiful
	we really want to look like Twin Falls?		No community needs more needs more			community that has already seen enough
			fried, processed fast food.			questionable development.
7/10/2016 11:25		We have enough gas stations/convenience	We have zoning rules for a reason. Follow	Cars are still going fast on Main Street at	A thorough study regarding traffic needs to	We have enough gas stations/convenience
		stores in town.	them.	that location.	be evaluated.	stores. It simply doesn't fit in Ketchum.
7/10/2016 11:59	dangerous intersection for such commercial		I would be for it if it were not a chain such	dangerous intersection for such commercial	Traffic patterns, neighborhood impact,	dangerous intersection for such commercial
	activity and only a couple of blocks from another such establishment with better		as taco bell, Kentucky fried chicken etc.	activity and only a couple of blocks from another such establishment with better	setting a wrong prescident, HWY 75 is our only road in and out of town	activity and only a couple of blocks from another such establishment with better
	ingress and egress.			ingress and egress.	to the north - more turns	ingress and egress.
					on and off it seem dangerous.	
7/10/2016 13:03	We already have a gas station just around the corner and down the hill from this	It will be a complete eye-sore.	Not necessary.	Traffic can be difficult in this area - due to the fork in the road heading down to Warm	We do not need a gas station in this location. We have one already just down	Really not in favor of a fast food resaurant in our community - and I think it is a really
	location.			Springs and to have another congestion just		bad location for traffic.
				past in not a good idea.		
7/10/2016 13:10						
7/10/2016 13:31						
7/10/2016 13:43	fear it will all go to a national franchise and	A thin wedge into losing what we are.	already have takeout with Big Bread. This	This is a quiet end of town and should be	What permanent restrictions can be	
	compete with locals. We don't need that here, just like we don't need a Starbucks.		will go to a chain and don't want that.	kept that way.	permanently placed to make sure a chain store, chain restaurant, chain gas station	
	·				won't move in?	
7/10/2016 15:21	I'm not sure whether the location is	Loss of retail space at this site, unsightly	Let's give people passing through a reason	A gas station would make the already	We have a lot of gas stations. Maybe there	Loss of retail space at this site, unsightly
	appropriate or not, but I don't see the need for another gas station and convenience	business at the northern gateway to the city, and oversaturation of the gas station	to visit the downtown core for food.	awkward intersection of Hwy 75 and 10th St even worse.	able to find them so they can gas up in	business at the northern gateway to the city, and oversaturation of the gas station
	store in Ketchum, and I know that gas	and convenience store markets in Ketchum.			Ketchum.	and convenience store markets in Ketchum.
	stations leave a legacy of contamination, so this kind of decision can't be undone.					
	this kind of decision can t be undone.					
7/10/2016 14:04	This will only make a dangerous section of	Ketchum/Sun Valley already has two gas	I've no idea what is meant by take-out	This area is already plagued by traffic	How it will effect traffic and pedestrian	See above
	hwy. 75 more so.	stations in the area. There is no justification for the potential traffic hasards such an	restaurant. If we are talking about a KFC or Mac Donalds, I'm adamantly opposed	problems. It's extremely difficult for pedestrians to cross Hwy 75; for cars to turn	safety and the aesthetics of Ketchum.	
		addition will bring. Furthermore, a gas	because this would have change the	onto the hwy from the road that that		
		station is an extremely unattractive gateway	character of our beautiful, unique	connects hwy 75 to Warm Springs; and this		
		to Ketchum.	community.	section of the hwy is plagued by speeders.		
7/10/2016 13:51						

7/10/2016 14:03			No drive throughs! Take out counter is fine.		Would be on in industrial core. NOT MAIN	The view scape should be protected.
//10/2016 14.03			ino drive tilloughs: Take out counter is line.		ST.	The view scape should be protected.
7/10/2016 14:13		Yes, I believe it would become an even more dangerous portion of our highway going north. I believe it is not in keeping with the aesthetic that we have tried be removing the existing gas stations that were previously on Main Street.		Yes, is already very dangerous for travelers in going north and south on 75. It is already dangerous if you are trying to turn off of 75 to businesses on both sides of the highway.	Traffic safety Pedestrian and bike safety Aesthetics of the north entrance to Ketchum Displacing existing businesses and forcing them out of Industrial into more expensive locations.	I absolutely oppose this project.
7/10/2016 14:22	There are more than enough gas stations in Ketchum. 4 to be exact, and never lines at any of them. Take out restaurants are fine, if it is similar to KB's or Wrap City. But we don't want or need franchises. A convenience store is probably fine,though I doubt it will be successful on its own.	If it's gas or a franchises you bet I do. For one, the town needs to support places like the Knob Hill who have been a vital staying place in town. Putting any of the proposed stores in cheapens the area. The town should work at enhancing the properties opposite the Knob Hill. Not make them worse.	One of the nice parts of Ketchum is the fact it doesn't allow them. Once you do it's a slippery slope		Nothing more than it shouldn't be approved because the rules in place keeps the charm of Ketchum. Not saying the properties in the area shouldn't be use or advanced, but yeah options you are talking about is a not an advancement	The town should work on advancing the area. No doubt. But this is not an advance. Further, it will affect a beautiful property that is the Knob Hill as well as the cemetery.
7/10/2016 14:17	We have enough gas stations. WE do not want nor need another convenience store or take-out restaurant.	This is not what Ketchum is about. This appallingand would NOT enhance our beautiful valley.	No chain/tacky take-out restaurants. We already have home-owned restaurants that allow take-out.,,,do not need any more.		We do not need another gas station or fast food take out spot. Would much rather see more locally owned and operated food carts	
7/10/2016 14:21					- impact on the immediate neighborhood - impact on the flow of traffic through this area - cost/benefit of additional businesses in the city	
7/10/2016 14:47					Noise at all hours for nearby condos (Frenchman's) also lights after dark for nearby residential condos not a safe in and out for traffic will cause a very congested area-not a safe location	
7/10/2016 14:27	Please preserve the old west feeling of Ketchum! We already have an ugly main street made uglier by banks and giant hotelsNO!!!!				If people want to live in a place like Twin Falls, they should move there. Our tourism depends on the beauty of our town!!	for above reasons
7/10/2016 14:34				This area is far to dangerous for added traffic that would come from this.	This is not the sort of business that is prevalent in Ketchum	Not the right business for Ketchum especially at this location.
7/10/2016 16:48	It doesn't meet the zoning district and conditional permit requirements. Thus development proposal is intended for tourists and the general public, which is expressly prohibited in the zoning code.	It will detract from our mountain town beauty and poses a threat to the community because that intersection cannot handle a high traffic destination like this.	Maintaining the strength of the city core as well as the strength of the industrial district in our city depends upon keeping their uses separate, just as the are on the law books.	Traffic there is already horrendous at that intersection and this will create numerous daily "trips" from customers resulting in additional slow downs and probably accidents.	Probably the laws, and not opinions like this survey. The results of this survey shouldn't even be presented at the hearing, as this is not a popularity contest.	Expensity at this location.
7/10/2016 14:58	There are multiple gas stations within a mile of that location.	The main impacts I foresee are traffic, congestion, and possible gasoline leaks.	I prefer keeping businesses in the downtown core.	It's a hazardous intersection. Someone ran into the side of our car as she was trying to turn north onto Hwy 75.	Is this project really necessary? Would signage on Hwy 75, indicating the presence of other gas stations (On Warm Springs, in the Industrial Center, and in Sun Valley) solve the problem?	I don't think it's a suitable business for that location and I worry about the environmental impact.
7/10/2016 15:19	I wouldn't want to add this type of congestion to this intersection. Also I think we have enough gas stations in Ketchum and this one proposed is too close to the existing one at the bottom of 10th.	This is a poor location for a gas station at the northern entrance to the city. I think the visual aspects of Main Street are better without a any gas stations.	As I see it this would be a primarily drive in restaurant and that creates a "strip development" appearance, which is not attractive in my opinion.	With the eventual development of the Anderson Hardware site at the bottom of the hill, 10th will become much busier and won't work well with a gas station up on Main.	This is a conditional use permit request and there is no need to force this project into that location. I would take a closer look at the purpose of CUP's. I don't think they should be granted to allow just about any project to go forward. They should be the uncommon exception that is a real benefit to the community and not just a profit opportunity for the land owner.	See all my comments above.

	This is the north entrance to our town. Are you kidding? Let them build this in the industrial area with the car wash etc.	This is a defining moment for the PZC, what are we going to be? The driveby impact will be honky tonk not the first class resort or quality image we want.It will hurt the direction we are going in, send the wrong signal for future zonings, discourage first class development in the future.	The downtown core is pretty well defined. So is the industrial area. Many resturants have take out food, we don't want a gas station or "fast food" place in this location. Think you may be confusing "fast food" with "take out".		The uses that define our Main Street abiance and aesthetics.This is a no brainer.	for reasons sited above 1)Wrong zoning precedence 2)wrong place for this use 3)Main street should be saved for aesthetic uses that promote quality and what the town wants to be.
7/10/2016 15:15					Effect on neighborhood is downgrading	
	I think those sort so amenities should not be on Main Street. It detracts from the charm of our city. We are already dealing with a HUGE hotel and it will take awhile to get used to that.	Yes. I think it will create a bottleneck right on our main drag. Not good for the energy flow let alone the human flow.	Again if it were in the industrial area, a few streets down from Main Street, I don't think it would matter.	Above	P&L laws, effect on environmen I think that it is important to look at this structurally. Main Street is only a two-lane highway. And we have a gas station just a blocks down and over. What is the benefit when our needs are already being met?	
7/10/2016 15:27	Please no gas station on top of that.				This is already a fast food/mini convenience store. A KY Chicken is more attractive than a mini-mart so long as signage is controlled.	
	This is a "gateway" location to Ketchum, across the street from a residential area, so ideally would be improved with something having less negative visual, traffic, and extended hour noise impact.	As above, a less than desireable first (or lasting) impression for Ketchum to make on those approaching from the North. Service station and drive in restaurants usually generate considerable noise /traffic(left - turns across 75 ?), and could have other negative impacts as well.		See above comments on traffic	Given the strategic location of the proposed site, need to fully understand and evaluate proposed uses and better determine impacts on Ketchum (gateway impact) as well as neighboring properties.	REasons outlined above
7/10/2016 15:44	Do not need another gas station Especially in that location	Inappropriate location Will be traffic issues	Same traffic issues Don't want a fast food place anywhere	Already did	I have all the information I need now	Already explained
7/10/2016 16:06	more traffic, less safety, and an ugly entrance to ketchum.	see above, as well as encouragement for other developments that will compromise the quality of life for the town.	see response above. it is important to preserve the character of the town.	it is already a dangerous intersection; this would make it far worse.	all of what i've already noted.	
7/10/2016 16:12				If designed correctly	Zoning-does the zoning allow it?	
7/10/2016 16:57 7/10/2016 17:29					Not this survey. Taking a survey should not be the way to	
					decide such matters. The procedure is already laid out in planning and zoning.	
	This is the final Ketchum outpost on the corridor to the Boulder-White Cloud Wilderness. It would be a terrible eye-sore!	It would negatively impact the current 4 gas stations in this same general location, as well as the convenience store already on Main St. Moreover, it would change the face of our no-fast-food-strip-mall community and look really ugly.		The southern traffic through town at rush hour is already backed up almost to 10th street, not to mention conflict with snow removal vehicles in winter.	We do not need another gas station and convenience store; we need better signage and apps to direct tourists to existing establishments. The visual impact and environmental pollution (gas/oil, lights, noise, plastic, trash, traffic, etc.)	
	There are already 2 gas stations near that location and another would further lessen the flavor of the town that we strive to preserve.	across from Knob Hill will be detrimental in the long run. Think of what Veltex has created.	I live in the WRV precisely because we don't have a culture of strip malls and fast food places.	Don't know enough about traffic impact there to comment but could not submit survey without a response.	A project of this kind opens the door to a reevaluation of the building codes in a detrimental way for the town.	See all previous comments.
7/10/2016 16:45	Ketchum doesn't need another gassyation, particularly one on 75 at the north end of town. There is no question that it will aggravate congestion. Please deny this application.	Congestion bottleneck and safety issues	We don't need this type of sprawl. Those types of resturants belong in the City core	See above	An unbiased traffic study and a study to determine if in fact Ketchum needs another gas station. The existing gas stations appear to be under utilized which would suggest we don't need another one.	It is the wrong project in the wrong area. It makes no sense to put an unneeded gas station and take out restaurant in this location causing back ups at times in town.
7/10/2016 16:53	It's hard enough to turn on and off 10st	When it goes out of biz it will be an eye sore	That part I'm ok with	North end of Walnut? Is dangerous in the winter. People slide across 75 all the time.	Is it in the best interest of the people/city?	As a long time local I don't think it's needed. There used to be 3 gas stations on Main st. They all when the under at the town matured.
7/10/2016 16:50						

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12-10 Page 12-	7/10/2016 17:10		It'll be a horrible eye-sore and stands for	We should encourage the foot traffic in the	It'll be a disaster trying to get off of 10th,	The threats and damage it poses to our	
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	7/10/2016 17:54					The safety of foot/bike traffic with	
						increased "get it and go" car traffic	

7/10/2016 17:56					environment	
7/10/2016 17:58					We don't need anymore damn gas-air	
, ,					polluting stations around here!	
7/10/2016 17:58					?	
7/10/2016 18:03	Parking is terrible. Traffic will be held up by	Loitering and traffic.	This type of restaurant woo wreck havoc		Traffic as well as parking.	
	people turning left.		with traffic and people loitering.			
7/10/2016 17:59					Environment	
7/10/2016 17:55	There is already a gas station in that area.	More holding tanks for gas can mean more	I feel a locally owned business rather than a	Traffic could become more hazardous and	H	
	They aren't pretty and too many together doesn't make sense.	potential ground contamination.	chain would be appropriate.	add longer delays to an already strenuous ordeal.		
7/10/2016 17:59					Gas leakage	
7/10/2016 18:01		Development is good for the community.	We have minimal take-out restaurants in Ketchum.		The proposed project will look much better than the old building that is there now.	
7/10/2016 17:59	Northern entrance to town					
7/10/2016 18:35	There are a number of reasons this location is not appropriate for a gas station. No one has done a lighting study to determine how far light will reflex from installed fixtures. Car headlights will be pointing directly at residential properties at all times. The intersection at 10th and Highway 75 is already one of the top three most congested intersections in Ketchum and will would be considerably worse. Environmental issues could lead to costly soil sample analysis when purchasing in the area	I'm concerned that issuing a conditional use permit approving a gas station is such an inappropriate location shows that the city is not interested in it's residents and current zoning laws. it will open the gates to additional requests for inappropriate applications. Certainly from an entrance to Ketchum from the north standpoint it is difficult to image how something less attractive could be the gateway to our community. I certainly think Ketchum is a beautiful city but this is a step backward	I think that it could be appropriate in the industrial park area. Bigwood cafe already operates there and has take out food. I'm not in favor of it everywhere but it needs to be considered at this point since it essentially has been granted once.		Consider that this project has overwhelming opposition and nearly no support. Ketchum Residents don't feel it's necessary with so many other gas stations available in such a small area and they feel that it is unsightly.	All of the reasons above. It is a terrible location for such a commercial project from both a residential standpoint in the very near area and also as a very unattractive option for the north gateway to Ketchum.
7/10/2016 18:05 7/10/2016 18:07					Environmental problems	
7/10/2016 18:07					Usual	
7/10/2016 18:08		We currently have three stations in the		too tight of an intersection at 10th	We Why does the city continue to allow	see above
		north valley.] This new location could of course cause problems for the existing businesses.			uses in areas that are not zoned for them.	
7/10/2016 18:14		We don't need another gas station!			The ground will become toxic.	No it will be a toxic waste station, there are better uses.
7/10/2016 18:20	its ok if traffic study works.		Why not?	A study needs to be doneI am not an expert. I don't see it as any different than stopping anywhere on Main St.	Traffic/safety	If works with traffic and safety compared with other locations on Main
7/10/2016 18:17 7/10/2016 18:19					Traffic Traffic	
7/10/2016 18:22					The zoning code and general purpose section of the zoning code. The traffic problems it poses. Not fixable. Applicant must be denied.	
7/10/2016 18:29	This location at the north end of town should bookend our commercial area with an attractive building with a less intrusive use.	Yes, I think it will set a precedent for allowing uses where they do not make sense.		That is a goofy area where the traffic slows because of speed limit changes and I cannot imagine cars pulling in and out of the station at that location.	The aesthetic impact of the proposed project - this is just too junky to be located on Main Street at the north entrance to town. The impact on the Knob Hill Inn - traffic, lighting, people hanging out. Traffic concerns with cars pulling in and out of the gas station and take out restaurant.	Please do not approve this project.
7/10/2016 18:34		Fast food can be on our corner. Horrible. Look around at our town. Is this all progress. I think not!!		It's a terrible location for this	Quality of life We are losing that daily here!!	lt's a horrible idea
7/10/2016 18:32		-				
7/10/2016 18:39	traffic concerns, impact of noise & traffic on residential neighbors & Knob Hill Inn	see above	Concern with traffic/noise impacting residential neighbors/Knob Hill Inn	definite concerns + concern re noise from traffic, customers, & fuel trucks backing up	impact on local traffic/safety, noise/visual impact on nearby residences, concern with visuals of entering Ketchum	
7/10/2016 18:40	Optimal location as you leave or enter town	No	The more the merrier	No	Type of restaurant	I support economic growth

	Inconsistent and contrary to the LI-1 zone. Cannot meet the criteria for a Conditional Use Permit. Will adversely impact nearby residences (regardless of design or mitigation measures)	Toxic location: Traffic congestion and north bound turning conflicts.		Indeed, increased trip generation, stacking south and north bound, conflicts with pedestrian and bicyclists.	Compliance (lack thereof) with the requirements for a Conditional Use Permit under the Code.	
	No, although I think it's a very convenient location for a gas station and would be well situated and easily locatable by visitors unfamiliar with the other venues in town where they can purchase gas. I don't love the idea that this would be what greets our visitors when entering from the North and that this is what our visitors traveling from South to North would see as their last impression of town. Can't we put it somewhere a bit less visible?	I think it will contribute to a 'strip mall' type of atmosphere in the vicinity of this location and that is something to be avoided. Our town is too precious to trash it up in this manner.		I actually do have serious traffic concerns. It's a pretty confusing location for many drivers even as simply as it's currently laid out. I'm certain a traffic light would be needed if something that would draw vehicular activity to the magnitude that this would, is approved.	town to look like?' Years from now, will we look back and be proud of what we've allowed if we roll over and let a GAS STATION go in at one of our most impressionable locations.	I'm opposing it for the reasons listed above. My office is in Ketchum. I've lived here for 37 years. When considering the future of our town, I don't think this would be the most beneficial use of this property for the City of Ketchum and residents of the Wood River Valley. It will make us look cheap and that we've sold out.
7/10/2016 19:01					Impact to the community will be negative	
7/10/2016 19:17					inappropreiate use for the neighborhood, look of the project and traffic conjestion for the area.	
7/10/2016 19:12					The inability of Bracken's team to provide any evidence that they are actually in conformity with the code or that they are not posing a material public welfare risk. Thank you.	
	Main Street in such an historic district shouldn't have a gas station and mini-mart. Reserve that for another less important locale. Our heritage is precious and once marred can't be restored.	Car congestion and visual pollution	Like the Big Wood Bakeryit's a big asset where it is.		Historic District Preservation. No, don't allow a gas station and minimartNo, No , NO!	See above
7/10/2016 19:21			My concern is that it could then be any kind of cheap take-out and that will not enhance that location as one heads north.		The area is not conducive to having traffic pulling in and out onto the roadway. That should be discouraged.	
	Ketchum does not need another gas station, now, or in 10 years.	Visitors entering from the North & South will envision a gas station as their lasting shot of Ketchum/SV. Nor good - something other can be done, but not a 'gas station'.	Why not?	Yes, traffic issues and social issues will be active in a business like this - compare other local entry /exit business's in our town, but also in other mountain resort towns.	Compare other resort entry/exit resort towns - the social issues could impact the town?	
	We must provide services for the people that pay our bills. We cannot be a 'head in the sand' community	It is a benefit	Let's face reality.	Plan access properly & it will work	We need to provide services for our guests	
	This is completely opposite of what our mountain community should be. Leave fast food to Hailey - better yet - Twin Falls.	This will open the door to other businesses that will spread urban sprawl. None of us moved here for chain stores and strip malls. Don't let this happen.	Same as above. This project will allow a sprall of cheap fast food that should not be in Ketchum.	Obviously there will be more traffic.	Somebody wants to make a buck and in so doing will denigrate our town. Somebody should have stopped all the banks on Main Street. As a result we have a very boring downtown. Don't add to that with cheap fast food.	I already commented above.

7/10/2016 19:47				See below	I live off of 10th street and regularly see the	
					traffic density and the struggle to get up	
					and down and off of 10th. I also think and	
					fear a fire in my area. Permitting this	
					development to occur would add more	
					pressure to the traffic and reduce even	
					17	
					further the fire department's ability to	
					respond to a fire on our block. This is the	
					last thing our neighborhood needs piled on.	
					I think this risk to us residents and even to	
					the workers should be seriously considered	
					when reviewing the project. For all our	
					safety and for the sake of public services	
					being able to save us, please deny this	
					application.	
7/10/2016 20:28					Seems like it's pretty close to a bunch of	
7/10/2016 20:40	Brohlom with traffic and nodestrian Cineville	Not a your attractive view as your entering	What post a 7/11 at caddle road	Mainly podostrian	residential area	Wrong location for this kind of husings-
	Problem with traffic and pedestrian Cingular		What next a 7/11 at saddle road	Mainly pedestrian.	Is there a real need for another gas station	Wrong location for this kind of business
	and going from this location.	town from the north.	To recovery		seems there are plenty of locations to get	
					fuel allready.	
7/10/2016 20:26						
7/10/2016 21:00	Gas stations belong on major thoroughfares	Modernization/ face-lift of an older group of	Takeout food implies a quick, in and out by	10th Street traffic turning onto the highway	-a traffic study	I support. I disagree with those who oppose
	where they are easily accessible. This new	buildings; easier access to fuel especially for	the customer. Why cause more congestion	would have to compete with gas station	-an estimate of.on site parking capacity,	(knob hill Inn, new homeowner who built
	location could relieve congestion resulting	tourists who may have to drive all around	and parking issues/ delays in downtown for	The state of the s	realistic estimate of required turning radius	next to the highway) when the root of their
	from the gas station at the corner of warm	ketchum in search of gas if they skipped	something that is meant as a quick trip	to a backup of cars on the 10th St. Hill	for large vehicles (rv's and vehicles with	complaints are unsightliness, and
	Springs Road and 10th Street.	mountain view grocery on their way into	something that is meant as a quick trip	which could be bad in winter if the road is	trailers)	incompatibility with their uses. The zoning
	Springs Road and Toth Street.				1	
		town		slick	- design review (that isn't overly	of proposed gas station parcel allows for
					burdensome to the developer) to ensure a	this use and a homeowner and hotelshould
					look that is comp	have done their due diligence before
						locating where they did.
i .						
7/10/2016 21:57					Normal studies	
7/10/2016 21:57	Lighting for a gas station is incompatible		Exception should be made in the II districts		Normal studies	Potential nuicance impacts on single family
7/10/2016 20:59	Lighting for a gas station is incompatible		Exception should be made in the LI districts		Normal studies Impact on Dark Sky	Potential nuisance impacts on single family
7/10/2016 20:59	with the LR and Tourist zoned uses to the		Exception should be made in the LI districts			residential neighborhood and hotel to the
7/10/2016 20:59			Exception should be made in the LI districts			
7/10/2016 20:59	with the LR and Tourist zoned uses to the		Exception should be made in the LI districts		Impact on Dark Sky	residential neighborhood and hotel to the
7/10/2016 20:59	with the LR and Tourist zoned uses to the		Exception should be made in the LI districts		Impact on Dark Sky We don't need any fast food restaurants in	residential neighborhood and hotel to the
7/10/2016 20:59	with the LR and Tourist zoned uses to the		Exception should be made in the LI districts		Impact on Dark Sky	residential neighborhood and hotel to the
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7/10/2016 20:59 7/10/2016 21:16	with the LR and Tourist zoned uses to the	not the highest and best use of the property			Impact on Dark Sky We don't need any fast food restaurants in the valley, and this has the potential to	residential neighborhood and hotel to the
7/10/2016 20:59 7/10/2016 21:16 7/10/2016 21:56	with the LR and Tourist zoned uses to the East of the proposed project. gas station right down the hill		in LI		Impact on Dark Sky We don't need any fast food restaurants in the valley, and this has the potential to become a KFC! location	residential neighborhood and hotel to the East
7/10/2016 20:59 7/10/2016 21:16 7/10/2016 21:56	with the LR and Tourist zoned uses to the East of the proposed project. gas station right down the hill There is already a gas station two blocks to	A gas station is not aesthetically pleasing to	in LI We have never allowed chain food or fast	Traffic impact on the highway for those	Impact on Dark Sky We don't need any fast food restaurants in the valley, and this has the potential to become a KFC! location Does Ketchum really need another gas	residential neighborhood and hotel to the East The only one who benefits from this project
7/10/2016 20:59 7/10/2016 21:16 7/10/2016 21:56	with the LR and Tourist zoned uses to the East of the proposed project. gas station right down the hill There is already a gas station two blocks to the west. It will diminish property values of	A gas station is not aesthetically pleasing to the north end of our town. What about 24	in LI We have never allowed chain food or fast food in Ketchum before so why now ? It will	turning left from the south will definitely	Impact on Dark Sky We don't need any fast food restaurants in the valley, and this has the potential to become a KFC! location Does Ketchum really need another gas	residential neighborhood and hotel to the East The only one who benefits from this project is the owner of the property and the
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7/10/2016 22:35	There is already a gas station/convenience		I'm not opposed to a take out restaurant	See answer in # 1	A traffic study would be really important as	
	store one block away. The impact on traffic		outside of the downtown core, there are		well as market research in to the need for	
	with cars and trucks pulling in and out of a		already a few. I don't think this is the right		another gas station/convenience store.	
	gas station at this location would be		location for this.			
	terrible. It's already tricky with cars coming					
	up from 10th street on to Hwy 75.					
7/40/2046 22 42		No. 10 Control of the			In continuous de l'estate de la continuous	
7/10/2016 23:43	Access concerns. Busy corner made worse	Negative image impact. Just another strip			Is another gas station necessary and how	
	with gas station ingress and egress.	town fast food joint creating visual blight to			does it affect the character of our	
		character of our unique western town.			community. Especially on the main road	
7/11/2016 0:13		Typical block in east LA! The ghetto begins	The worst type of take outfried chicken!		entering/exiting Ketchum. The revenue value verses the smell, filt, and	Will power spend a dime and watch it like a
//11/2016 0.13		with the smell of fried dirty oil food!				predictor
		with the shell of fried dirty on food:	Put it closer to St Lukes for the heart patent.		garbarage around a fried chilcken dump!	predictor
					On the row going to multimillion dollar row!	
					What a idea! Put it where the	
					demographics warrant it, BOISE,	
7/11/2016 2:34					n	
7/11/2016 5:48	More business is a good thing.	I assume you mean negative. All good	Yesdowntown core should be walking mall	Better than the station closer to bike path	Tax revenue and jobs as well as good for	Good for growth and allow development
		except for the NIMBY class	and free market should decide if take out	and school	travelers	rather than just obnoxious monster NIMBY
			works else where			homes
7/11/2016 6:20	That location is already difficult to navigate	If allowed and a chain fast food restaurant is	Absolutly against any chance that a fast	If the city of Ketchum is really concerned	The overall impact on the committee. Does	I don't want or think it's good for the
	especially when there are lots of visitors. It	allowed it will tarnish the image, quaintness	food restaurant could move to Ketchum.	about pedestrian safety then this proposal	the benefit directly correlate to or exceed	communities image to have a fast food
	would be a nightmare.	and uniqueness of Ketchum. The economic		is a non starter. Having a gas station there	the consequence. What is the likely hood	chain even close to Ketchum. Beyond that
		benefits would not cover the negative		would create opportunities for major	that it would cause major congestion. And	there are plenty of gas stations in the valley.
		impact or effect such an establishment		accidents.	the increased risk of pedestrians getting hit	This would be a disaster.
		might have on our communities image.			or other accidents. If Ketchum is serious	
					about pedestrian safety then this would not	
					have made it this far.	
7/11/2016 7:02	This is already a challenging intersection for	. ,	see above and sub in dozens of restaurants.			See preceding comments
	crossing traffic and heading north especially			at this location and have seen accidents and	be a far better location for affordable	
	with 9th street here	people waiting to pump gas. We don't need	about. Lets not welcome visitors with a fast-	near accidents	housing.	
		to take money out of the pockets of those	food chain			
		who have struggled to build businesses in				
		our town.				
7/11/2016 7:03						
7/11/2016 7:43		It could lessen the gridlock at Atkinson's.	So handy for the campers that come			The core of Ketchum is a mess and so hard
, , , , , , , , , , , , , , , , , , , ,		That would be a very positive change.	through and their way north. They can't get			to park and get around in that any option
		,	to the core anyway.			that offers quick gas and supplies would be
			, ,			a nice option!
7/11/2016 8:01	Terrible entranceway to town and traffic				Impact on traffic	·
	could become problematic					
7/11/2016 8:34	Now that there are residences so close and	If it proves a traffic problem then something	I don't have a problem w/ a small restaurant	see above	traffic flow.	I need more info/studies first.
	Knob Hill, the flow of vehicles gets a bit	would have to be done and I would hate to	there (again NOT fast food or a chain) -		impact on existing business one block	Want to see a mock up.
	confusing there anyway -I think it would	see a light in that area.	there is a restaurant @ Knob and a coffee		down.	
	end up just like Mt. View to the south -not		shop there anyway - I just think if it's		community need?	
	good during rush hour. I'd be worried about		attached to a gas station/store THEN there		AESTHETICS!!!	
	the trailer/camper traffic in the summer		would be too much traffic flow disruption.			
	months and SO many landscaping trucks.				I understand that growth keeps a town vital	
	Also - no need since there's a gas station				but we make a living partly on this towns	
	one block away on WS.				beauty. first impressions mean a lot. The	
	What KIND of "take out" restaurant? We				South end has now been choked off by two	
	have a certain esthetic for our town that				hotels. The loss of the view of Baldy to the	
	does not include "fast food" PLEASE keep it				left as you come up the hill and into town is	
	that way!				just a crime. I cannot believe still the radical	
					change that has forever altered the feel of	
					our quaint ski town. Please be careful not to close off the North end too - the expansive	
					view of the mountains that is afforded	
					partly by the golf course should be	
					preserved.	
					•	

	North Main Street has a clean no-gas- station-no-convenience-store look. That look is part of Ketchum's identity and charm. Let's not spoil it! Gas is readily available. Perhaps small generic signs indicating to outsiders where they should turn to find gas would be helpful like the signs at freeway exits. No more than that is needed.	This will open the way to more projects of the same nature, and could completely change the character of Main Street. One bank >>> a street full of banks. One gas station >>>> who knows what? Chevron signs up and down Main Street?	A restaurant helps bring life to a neighborhood. Bigwood Bread has only improved the LI zone. The City is obsessed with telling small businesses what they can do where. The treatment of the Spot is a scandal. You can't have a vibrant business and cultural community in a straight-jacket. Cities evolve, and zoning should support orderly evolution. Ketchum is moving much too slowly in reviewing and revising its zoning. Workers in the LI area should not be obliged to go downtown for lunch.	Traffic in and out of the 10th Street Center and the Lock Shop - Lizzy's center is already tricky to navigate because of the 10th St intersection. A gas station would only make it worse.	don't need more of that look which is inevitable with the type of project proposed. We rejoice at the prospect of two new upscale hotels and the sort of tourists they will bring. Would you consider allowing a gas station-convenience store at the	would seriously lower the quality of this neighborhood. PLUS, we need to protect reasonable rent locations for the sorts of essential service businesses that currently occupy the two centers lock shop, Mike's,
7/11/2016 8:15			Ketchum needs to focus on keeping our community what it is—uncluttered non urbanized and understanding what charm and beautiful really mean	This is a no brainier that doesn't even deserve a comment	NEGATIVE IMPACT IN EVERY POSSIBLE WAY ON OUR COMMUNITY	WHY? Why would anyone opposethis question has to be a joke
7/11/2016 8:25	good for community	nothing negative	competition spurs growth		nothing of importance other than that	
7/11/2016 8:34	It is the gateway to the north	It is already congested	No we do not need one	It is already too congested.	commented on above We have enough gas stations and	
7/11/2016 9:17	Because it is zoned that way.	It is zoned for such a use. If the city or neighbors don't like this proposal, then work on changing the zoning. Don't use an unscientific survey to make a decision.	If zoning allows it, then yes.	This project will probably improve such issues. It's up to the city to ensure it's done responsibly within zoning code.	restaurants in this small town. Whether it meets the zoning code. That's it. Just because you may not like it, it doesn't mean it shouldn't be approved if it's an allowed use in the zone. Using a survey to make a zoning decision is a great way to chill business development in Ketchum.	It doesn't matter whether I support or oppose it. It matters whether it meets zoning code. That's it. This survey is ridiculous. It is no way to make zoning decisions. This could chill business development in Ketchum if you subject zoning decisions to the whims of the public and ignore zoning codes and procedures. I urge you to cease using such surveys for "controversial" proposals, and do the jobs you were elected or hired to do.
7/11/2016 9:12		Increased traffic congestion, increased wear and tear on surroundings, increased pollution of the city, and fall in property values and tax income in the area.	There is no need for another takeout restaurant outside of the core. Exceptions to zoning should not be made.	Traffic congestion will increase and this is already a dangerous intersection for pedestrians. We don't need more accidents downtown.	The appropriate zoning law.	I am opposed to any project that is not in the interest of the city of Ketchum and its citizens.
	I don't believe gas stations and "C" stores serve as the appropriate gateway to our community. We're better than that.	If successful, traffic congestion without question. I would also add the inability or difficulty to change this business out to something else over time should it fail.		Absolutely. This has the potential to attract multi vehicle trailers and long axle recreational vehicles. It won't take many of these along with normal traffic to bottleneck the intersection at 10th and main. I'm also concerned you'll have people parking on both sides of main street trying to get to the C store creating additional risk for pedestrians.	This is clearly feels like an LI use. I don't know why after spending so much time and money trying to define what Ketchum is only to stuff a gas station at it's front door. Seems incredibly short sided to me. I'm certainly not trying to discourage business from being in Ketchum, God knows our communities need it, but our communities have to grow intelligently. We can't do what Hailey did and put an auto parts store as it's southern gateway. Is this how we're going to physically brand our cities in the WRV? Additionally, we have spent decades removing gas stations from our main street in hopes of better uses more fitting of who we are as a downtown. Seems as though we're simply rolling back the clock just to get something on the books.	
7/11/2016 9:14	outside of core	all negative though	no exceptions!	obviously traffic would be worse and safety impinged	what kind of town do we want? is this what the zoning code permits?	it's an ugly addition that we don't need, especially on such a high-visibility lot.

					T	
7/11/2016 9:40	This will cause traffic issues at the		l '			Another gas station is not needed, and the
	intersection, and it is not one of the first		restaurant businesses in the core, and to let	encounter due to people speeding up on	convenience store is needed, what the	intersection at which this project is
	things visitors should see when they're		them start expanding would be harmful to	the highway. There aren't many pedestrians		proposed could be better put to use as an
	entering Ketchum.		the city core.	at this intersection, but I do believe it would	available away from the city core.	office space or community housing.
				cause safety impacts to cars and bicycles.		
7/11/2016 9:47			There are already restaurants out of the		Don't put a take out restaurant in the	I think it's a great idea for Ketchum. It's
			downtown core. Whats the big deal?		station if you are opposed to it. I think this	outside of the downtown core so it won't
					town needs a full service station that a large	be a problem for downtown Main St. We
					motorhome or truck and trailer can pull	need one gas station/ convenience store
					into. There is not one station in Ketchum	that can be of service to the growing
					where a 30 ft. motorhome or truck and	population. Hailey has 4 stations. Ketchum
					trailer can pull into. Tourists do not know	has 2 without stores and 2 with tiny stores.
					about the stations in the industrial, nor can	None of those 4 stations can accommodate
					they drive a big rig into those stations. No,	a large rig.I wish I had thought of this first.
					they're not going to drive to Sun Valley.	
					When driving a big rig, you want to stay as	
					close to the highway as possible. Easy off,	
					Easy on.	
					,	
7/11/2016 9:49	There's already a gas station (base camp)	Franchise fast food restaurants go against	Keep the McDonalds in Hailey, not		Do we really want to open the door to	
	right down the street, and we don't want	the mountain-vibe of the town, and it takes	Ketchum.		franchise fast food restaurants? If one is	
	any chain fast food restaurants here!	away from local businesses.			allowed here, then what's stopping all of	
					main street and the downtown core from	
					turning into chain/franchise stores and	
					restaurants? Keep Ketchum authentic and	
					local.	
7/11/2016 9:47					what the people of ketchum want	
7/11/2016 9:50					Environment	
7/11/2016 10:32	Because it is light industrial area	no	there are a lot of people who work north of	There was talk about a roundabout at one	not sure	more the merrier
			town which will be easier for them to have	point. that might be a good solution.		
			lunch. Lots of people frequent The Haven			
			food truck so there must be a need			

Dusty Wendland, for the record; Re: Bracken Station; July 11, 2016

1. The Hales traffic study is materially deficient in so far as it uses speculative data that is unfounded and inaccurate and, therefore, results in inaccurate projections of the Levels of Service and the length of delays and queuing for both Highway 75 and 10th Street, if this development is permitted by the Commission.

In Table 3 of Hales study, the Trip Generation analysis, Hales used a "Project Total Daily Trips" which it calculated from the Institute of Transportation Engineers *Trip Generation (9th Edition, 2012)*. This method of calculating total daily trips to the project resulted in a projection of 1304 daily trips, 652 of which were ingress and 652 of which were egress. In effect, Hales projected that this project would generate 652 daily customers, each of which pulls in and out, resulting in a total of 1304 daily trips. This projection is significantly misrepresentative of the reality.

Please see Exhibit A, which I offer to the Commission for the record. It shows the customer counts for the four most recent weeks at my store, Base Camp Warm Springs, which is located just one block from the perspective site and is essentially identical, though about 50% smaller in scale, to the Applicant's proposal. At this location, where I do not have the benefit of Highway 75 traffic levels, my store served 28,096 customers in the most recent four weeks. Dividing this number by 28 days results in 1003 daily customers served by my business on average. Per the traffic study's method, this number would be multiplied by 2, for ingress and egress, resulting in an average of 2006 total daily trips--a quantity 54% greater than the Total Daily Trips used in Hales projections.

Furthermore, as Hales traffic study indicates, on page 4, that they seek to provide "worse-case scenario" analysis, I'd prefer to have supplied the Commission with four weeks of customer counts from my store for July or August, during our peak traffic season, rather than using numbers reaching back into June, which still includes some portion of our slack season. Unfortunately that is not yet available to me. However, the average daily traffic counts during those peak months are likely at least 10% stronger than the reported numbers in Exhibit A. This would suggest that my store has an average of 2206 daily trips, which is 69% greater than the Trip Generation projections used by Hales.

Given that the Applicant's proposal comprises a store that is 50% larger than my store, and which intends to be located on property that is 50% larger than the parcel on which my store is located, and which intends to include a restaurant, and which intends to be located directly on Highway 75 with highway levels of traffic, even the average daily trips of my store during peak season grossly underestimates the reality of likely traffic levels to this project. Hales should have projected a Trip Generation number that is much nearer to 3000 or more Total Daily Trips to this project, which is a number 130% greater than the projection made by Hales.

Because Hales grossly misrepresented this Trip Generation number, their subsequent projections for Levels of Service on Highway 75 and 10th Street, as well as their queuing analysis, for both 2016 and 2020 are utterly misinformed and, consequently, of no value whatsoever to the Commission. If this project is permitted, the impact to the Levels of Service on Highway 75 and 10th Street, as well as the

queuing in the proposed turn lane, will be much greater than Hales and the Applicant has misled the Commission into believing.

In the last two hearings, the public has vociferously cried out against the traffic problems that this project would produce. Mr. Williamson and Mr. Cook have both instructed the Commission that these tirades from the public are merely the misinformed opinions of the proletariat and that the professional and informed regulators are much more capable of analyzing the traffic-related risks and of offering a qualified opinion on the concern. The Applicant and his legal counsel would have the Commission dismiss the public outcry on the grounds that is all sound and fury, signify nothing. However, as the facts demonstrate, the public outcry is much more in line with reality than the Applicant's uninformed professionals.

- 2. The Commission and the City Staff requested that the Applicant provide, for today's meeting, traffic study analysis of:
- a) the vehicle makeup: what percentage of the vehicles, likely to use the services offered by the proposed project, will be RVs, construction trailers, etc.; and
- b) potential back-up of northbound traffic lining up to make a left turn into the gas station and the implications of exceeding the length of the proposed turn lane.

In response to this request, Hales Engineering replies, in their most recent memo, that data was collected at a local gas station over two days and that only 7% of the vehicles were "larger vehicles." Consequently, Hales determined it was unnecessary to adjust their average vehicle length of 20 feet used in the previous traffic study. Furthermore, as they did not find it necessary to adjust the average vehicle length, they also found it unnecessary to change their recommendations or concerns regarding queuing of northbound traffic beyond the length of the turn lane.

In the City Staff report, however, we learn that the "data" for this "independent study" was collected by the Applicant, Roy Bracken, by supposedly counting vehicles and determining their natures and their lengths by sitting across the street from a gas station on Lewis Street for two days.

As though Hales' traffic study didn't already have enough issues, now the Applicant's direct involvement in Hales' "independent traffic study" completely undermines the study's integrity. The Applicant, quite obviously, has excessive motivation to report false data. Subsequently, this unreliable data resulted in an inaccurate projection of vehicle makeup and queuing implications in the turn lane by Hales Engineering. In effect, Hales Engineering states that the average vehicle length is still 20 feet and that queuing in the turn lane still won't be a problem, *because* that is what the Applicant indicated to them that they should conclude. While this may be harmless error on behalf of the Applicant and Hales, it could also be a patently unethical action committed by the Applicant and Hales alike. Either way, the integrity of the study falls yet further into ill-repute and irrelevancy.

It further bears noting that the *location* of data collection and the *weekdays* used for data collection both prevent the admissibility of the data as well. As the Bracken Station purports especially to service

"larger vehicles," it hardly seems reasonable to seek out and count vehicles at the smallest fueling location with the worst possible accessibility for larger vehicles. No doubt larger vehicles specifically do not traffic the Shell station on Lewis Street for precisely this reason. An accurate count of the percentage of oversized vehicles likely to frequent the proposed location should be conducted at a location such as the Valley Co-op in Bellevue, which, like the Applicant's intentions, especially services "larger vehicles." I am confident that the percentage of "larger vehicles" at such a location vastly exceeds the percentage of vehicles supposedly counted by the Applicant at the gas station on Lewis Street.

Moreover, the Applicant collected the data on Saturday and Sunday, which are the two days of the week that work trailers and larger vehicles are least likely to be buying fuel, as many such vehicles are, in their natures, work-related vehicles.

For worst-case scenario analysis, the data should have been collected by an independent source, by a source qualified to judge vehicle lengths and types, at a location specifically similar to the proposed station, and on a day of the week that such vehicles would most likely frequent that establishment. As the Applicant has failed to do any of the above, he provides evidence that he has either not taken the Commissioners' requests for additional traffic studies seriously or that he is willing to unethically implicate himself in a study specifically intended to provide objective unbiased analysis. Either way, he has wasted the Commission's valuable time and attention by supplying a traffic study wholly devoid of objectivity and accuracy.

Given that this proposal's correct Trip Generation count will exceed 3000 trips per day, as has already been evidenced, and given that the average vehicle length will indeed significantly exceed 20 feet, and given that Hales Engineering reports, on Page 7 of their study, that 85% of these trips will come from and exit back to the *south*, how can Hales possibly suggest that there will not be queuing problems in the turn lane and a significant reduction in the Level of Service at the intersection of Highway 75 and 10th Street?

In Mr. Cook's follow up commentary in the June 13th meeting, he incorrectly informed the Commission that (and I quote), "We know that we can accommodate up to five cars, waiting, and that's a lot, five cars to turn in. We can accommodate that based on a 20 foot length for each car."

Based on a 20 foot average vehicle length, the turn lane would have to be well in excess of 100 feet, after spacing between vehicles. The proposed turn lane on the Site Plan is noted as "71.2' (50' MIN.)". This turn lane is *not* in excess of 100 feet, and it will not accommodate 5 cars, despite Mr. Cook's beliefs to the contrary. Even with a 20 foot average vehicle length, it will accommodate only three cars. If the vehicles are significantly longer on average, as surely they are, the turn lane will fit only two or maybe even only one vehicle. Incidentally, while the turn lane is proposed to be only 71.2 feet, the fuel transporter that will deliver fuel to this location is 75 feet long and won't even fit into the turn lane by itself.

Despite Mr. Cook's assurance and Hales traffic study, it is clear that the proposed turn lane will not adequately service the project's trip generation levels. Northbound traffic will backup towards

Backwoods Outfitters and delay the flow of traffic on Highway 75. An accurate and objective traffic study would have easily and quickly identified these facts.

3. Commissioner Smith requested that the applicant provide a "turning radius study." When Mr. Cook indicated that he was unsure to what she referred, Ms. Smith went to great lengths to explain that (and I quote), "it's something like an industry study that you can get done for gas stations.... It's just an analysis of, if you got a tanker there and a fire truck that's coming in at the same time, can they negotiate around the pumps and around those sites adequately."

In response to this request, the Applicant provided a "Site Vehicle Turn Exhibit," produced by Benchmark Associates, which shows the circulation of a 30 foot truck and a 48.7 foot car with camper. The circulation drawings indicate, as the City Staff noted, that such vehicles can get into and out of the parking lot as long as there aren't any other vehicles using the store's gas pumps at the time of circulation. This is quite an incontestable point when looking at Benchmark's rendering of the site's circulation. Moreover, this is only using, at most, a 48.7 foot vehicle. This doesn't address the circulation of the fuel transporter that will deliver fuel to station, which has a total length of 75 feet. Benchmark's turning radius study indicates that such a transporter likely could not negotiate the aisles and the pumps even if it were the only vehicle in the parking lot. In point of fact, the transporter may have to reverse back out onto Highway 75 based on the current Site Plan.

Nevertheless, such preliminary criticism of the Site Plan still does not get at Commissioner Smith's inquiry and request: namely, can a fire truck negotiate the pumps and the customers onsite and still get into the parking lot to provide emergency services if there is a fuel transporter already parked on the site. The obvious answer to this question, which the Applicant obviously does not wish to provide, is no: a fire truck could not negotiate the pumps and provide emergency services unless the site is already completely vacated by customers and fuel tankers alike. In the event of an emergency, first responders cannot respond, based on the proposed Site Plan.

In the Municipal Code of Ketchum, the fourth criterion of the Conditional Use Permit instructs the Commission:

D. The conditional use will be supported by adequate public facilities or services and will not adversely affect public services to the surrounding area, or conditions can be established to mitigate adverse impacts;

Although the City Staff Report for today's meeting has preliminarily marked the Applicant in compliance with this particular criterion of the C.U.P., upon closer inspection it is clear that the Applicant has not satisfied this criterion either. In fact, impeding the flow of northbound traffic on Highway 75, as has been evidenced, coupled with the complete lack of adequate circulation on the Site Plan, poses a grave fire danger not only to the employees and clients of the proposed development, but also to the surrounding businesses and residences as well. As such, it is incumbent upon the Commission, per this requirement of the C.U.P., to require mitigation by the Applicant or to deny the application. Any future proposed mitigation to these issues by the Applicant should be provided with an *accurate and objective* analysis of turning radiuses and specifically address Commissioner Smith's concerns regarding a fire

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truck negotiating the fuel pumps while customers and a fuel tanker occupy the parking lot, since such a scenario is not at all farfetched, but it is, indeed, very dangerous.

It should be noted that the Applicant is either remiss in not having provided this study upon the Commissioner's first request, such that it be available for today's hearing; or, alternatively, the Applicant has withheld the study because it will provide damaging information. Either way, the Applicant has not provided adequate evidence that they can mitigate traffic related risks that impede the delivery of public emergency services; and, as such, this application should be denied the Conditional Use Permit on grounds of failure to meet the requirements of Condition D of the permit criteria.

Date-7/4/2046 to 7/40/2016

Section: 6400 to 6400

Target: 003 Base Camp 3 Warm Springs

SXHIBIT A 1/5



Description	Qty	Amount
Statistics	·	
Items with ID		
Customers	6770	
Items		
Items scanned		
Sales Keyed		
Time complete trans		
Time on sub-total		
Time on sale		
No sales		
Percent scanned		
Items / minute		
Items / cust		
Sales / cust		
Sales / item		

Wendlands -

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Date: 6/2//2616 to 7/3/2016 Section: 5400 to 5400

Target: 003 Base Camp 3 Warm Springs

EXHIBIT
A
2-15



Description	Qty	Amount		
Statistics		*		
Customers with ID				
Items with ID				
Customers	7929			
Items				
Items scanned	78.00			
Sales Keyed				
Time complete trans				
Time on sub-total		•		
Time on sale				
No sales				
Percent scanned				
Items / minute				
Items / cust				
Sales / cust				
Sales / item				

Date: 67 // (CAS) 5 / (CAS) 2016 Section : (CAS) 5 / (CAS)

Target: 003 Base Camp 3 Warm Springs

EXHIBIT A 3/5



Description Qty **Amount Statistics** Customers with ID Items with ID Customers 7268 Items Items scanned Sales Keyed Time complete trans Time on sub-total Time on sale No sales Percent scanned Items / minute Items / cust Sales / cust Sales / item

1,1%

SEPEMBLE

Date: 6/13/2016 to 6/19/2016

Section: 6400 to 6400

Target: 003 Base Camp 3 Warm Springs

EXHIBIT
4/5



Description

Qty

Amount

Statistics

Customers with ID

Items with ID

Customers

Items

Items scanned

Sales Keyed

Time complete trans

Time on sub-total

Time on sale

No sales

Percent scanned

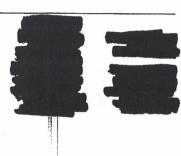
Items / minute

Items / cust

Sales / cust

Sales / item







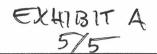




		Table 3 ID Ketchum Gas St Trip Generat						
Weekday Daily	Number of	Unit	Trip		%	Trips	Trips	Total Daily
Land Use ¹	Units	Type	Generation	Entering	Exiting	Entering	Exiting	Trips
Gasoline/Service Station with Convenience Market Project Total Daily Trips	8	Vehicle Fueling Positions	1,304	50%	50%	652 652	652 652	1,304 1,304
A.M. Peak Hour	Number of	Unit	Trip			Trips	Trips	Total a.m.
Land Use ¹	Units	Type	Generation	Entering	Exiting	Entering	Exiting	Trips
Gasoline/Service Station with Convenience Market Project Total a.m. Peak Hour Trips	8	Vehicle Fueling Positions	82	50%	50%	41 41	41 41	82 82
P.M. Peak Hour	Number of	Unit	Trip			Trips		Total p.m.
Land Use	Units	Туре	Generation	Entering	Exiting	Entering	Exiting	Trips
Gasoline/Service Station with Convenience Market	8	Vehicle Fueling Positions	110	50%	50%	55	55	110
Project Total p.m. Peak Hour Trips						55	55	110

E. Access

The proposed access for the site will be gained at the following locations (see also site plan in Appendix C):

Main Street (SH-75):

One full-movement "boulevard approach" accesses is proposed on Main Street (SH-75), one approximately 60 feet south of 10th Street. A "boulevard approach" consists of two forty foot wide openings in the curb separated by a small island. One opening is for ingress movements, and the other for egress movements.

From: Steven Shafran [mailto:Steven@theshafrans.com]

Sent: Wednesday, June 29, 2016 9:15 AM

To: Micah Austin < maustin@ketchumidaho.org>

Subject: Bracken Station

Micah:

I hope this note finds you well. I am writing this note to you to express my negative feelings about the proposed development of a Gas Station/Convenience Store on the corner of 10th Street and Highway 75.

At this point, based on the information that I have seen prepared by the applicant, it appears that the development is a really bad idea. I have the following concerns:

- 1.""The idea that the northern gateway to the town should be a gas station is completely at odds with the letter and spirit of our Comprehensive Plan. We spend so much time developing and maintaining these plans for precisely this type of situation. The citizens have spoken through the Plan about what is important to them. Words like "Visual quality of the entryway corridor" are in the plan, and should be respected. I have spoken to numerous residents who live in the city and North of town. No one thinks a gas station should be our "Welcome to Ketchum" visual.
- 2. Think that this development is going to create substantial traffic problems at the north end of town. How are all the RVs and Trailer pulling vehicles heading north for the Stanley Basin going to turn left across the highway to get to the last gas station for 35 miles? And what will it do to traffic when all those vehicles need to turn left again to get back on the road heading north? I read the traffic study prepared for the P&Z in connection with this proposal. While I respect the professionalism of Hales, why does anyone think a study performed in the winter of 2008 during the recession will be helpful in determining traffic in the summer of 2017? There are no RVs or trailers heading north in the winter. How can we possibly use this data to understand the impact of the proposed development?
- 3. The Comprehensive Plan also talks at length about pedestrian and bike safety. We all know we have a serious safety issue from the north edge of town to Saddle road, where no sidewalks or bike paths exist. When I bike to warm springs from my home, I go down 10th street to get to the bike path. A gas station on this site will make the current dangerous situation more treacherous for pedestrians and bikers.

I look forward to attending the July 11th meeting. I expect the P&Z to be considering this application in accordance with our Comprehensive Plan and thoughtful public policy.

Best Steve Shafran From: Jeff Lamoureux

Sent: Wednesday, June 29, 2016 11:02 AM **To:** Casey Finegan <<u>casey@alarycs.com</u>>

Cc: Micah Austin < <u>maustin@ketchumidaho.org</u>>

Subject: Re: Gas Station

Casey,

Thanks for your comments. I am forwarding to Micah Austin, Planning Director and the will be included in the public record. We have received several similar comments and have requested additional info from the applicant to evaluate. This will be discussed at our next meeting on 7/27.

I shouldn't discuss details with you prior to the meeting but you can call Micah to discuss further. He can be reached at (208) 727-5084

Jeff Lamoureux

On Jun 29, 2016, at 9:07 AM, Casey Finegan <casey@alarycs.com> wrote:

Hi Jeff,

I'm not sure of how to figure out something in relation to the new Gas Station project and thought you would have a pretty good idea. My concerns are numerous but one of them is the Gas Station lighting. My unit in Frenchman's looks directly at the proposed site and I'm thinking that a gas station will be flooding nearly all windows of my unit and others with light at night which would be pretty unfortunate. As the President of the Frenchman's board, I have been asked many similar questions. I'm not sure if this is a conflict for you to respond, I just don't know how to find out.

Thanks,

Casey Finegan Alary Computer Services 208-721-3044 Casey@alarycs.com From: ÓæàǽŒ; }^ÁÜ^^åÁ\àæààæ æ; }^\^\^å*æ∮^\^\æ\{_N'}
Subject: others to talk to re. water contamination"

Date: R' } ^ ÁHEÉÁGEFÎ ÁBEÁ KGÏ KK FÁÚT ÁT ÖV"

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From: leslie andrews

Sent: Tuesday, July 05, 2016 7:53 AM

To: Participate < participate@ketchumidaho.org >

Subject: Please. No Bracken Station!

Oh, please, no! Ketchum has already lost too much of it's small town charm and allure. Please don't add another gaping wound to our sweet town.

Leslie Andrews Hailey

SURVEY

Proposed

BRACKEN GAS STATION and CONVENIENCE STORE

MAIN STREET KETCHUM between 10th Street and East 9th Street

(opposite Knob Hill Inn on site of Lizzy's Coffee)

Check all that you feel apply
There are enough gas stations in Ketchum. Ketchum does not need another.
Bracken Station will be permanent, visual blight at Ketchum's Northern Gateway
Bracken Station will create a toxic environment (air and soil contamination) creating a health hazard, especially for nearby Ketchum residents, workers, businesses
Bracken Station is not in compliance with stated LI-1 Zoning purpose: "generate little traffic from tourists and the general public"
Bracken Station's convenience store will lure children from Hemingway and YMCA onto 10 th St. (incredibly dangerous now even without the addition of more vehicles generated by Bracken Gas Station). Bus stop riders will be lured across highway from the bus stop located directly opposite of proposed Bracken Station.
Bracken Station will dramatically increase traffic on North Main,10 th Street and other adjacent streets endangering pedestrians and bicyclists. (Note: Bracken Station designed for use by extra heavy trucks (e.g.dump trucks, tractor-trailers with long hitches, semis, 5 th wheels, large RVs, snowmobile /horse trailers, etc.)
If Bracken Station is approved, I understand that City of Ketchum could not preversale to a large gas station e.g. Stinker Store, Exxon, Chevron, etc. or the opening of a Fast Food franchise, such as Subway, McDonald's, Sonic, Arby's, etc.
Bracken Station does not comply with Ketchum's Comp Plan: "to promote the health, safety and general welfare of present and future inhabitants of Ketchum"
All of the above None of the above
ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org)
Signature Jaluny Lotor
Print JOHNAY BOLTON
I disapprove of Bracken Station on Main Street, Ketchum. You may list my name among others who oppose this development. City of Residence KFTCHVM
☐ Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

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☐ All of the above ☐ None of the above
ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org)
Signature Valerie Ashbaryh
Print Valerie J. Ashbaugh
I disapprove of Bracken Station on Main Street, Ketchum. You may list my name among others who oppose this development. City of Residence Ketchum
☐ Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

TO: Planning and Zoning Commission, City of Ketchum

FROM: Brian Emerick

RE: Bracken Station Conditional Use Permit

Dear Planning and Zoning Commission,

I am writing in strong support of the Bracken Station Conditional Use Permit as proposed for the 911 N. Main Street location. From an objective review of criteria for CUP's within the LI-1 District, the application goes above and beyond what is required for this zoning district and has to date answered and met every condition that has been placed before it - and there have been many such conditional "hoops" that the applicant has professionally and politely jumped through. From a more subjective measure of the value of the application to the LI-1 District specifically and the City of Ketchum in general, the proposal for a first rate motor vehicle fueling station and food service establishment can only be seen as a significant and undeniable improvement over the non-conforming, dangerous, and unsightly use that is currently occupying the oddly-shaped and challenging parcel. Although there have been numerous emotional plea's made to the Commission to deny the Bracken Station Conditional Use Permit application, those shrill voices that are primarily objecting from plain positions of immediate self-interest are not compelling when answered by the fact-based information as compiled and presented by the applicant and the professionals reporting on the application and duly recognized by the Planning and Zoning Staff in its Staff Report dated June 13, 2016. In short, the Planning and Zoning Commission must consider the Bracken Station CUP application using the criteria for evaluating conditional use permits - not emotional-based cries -- and when it does so, the Commission must find that motor vehicle fueling stations and food service establishments are allowed within the LI-1 District and that the application, having again and again answered and agreed to the conditions placed before it, should be unanimously approved by the Commission with the result being a clear improvement over existing conditions at the site and a useful and attractive feather in the cap of a growing and prosperous City of Ketchum!

Briefly, here are some facts in regard to the Bracken Station Conditional Use Permit application:

- Motor vehicle fueling stations and food service establishments are allowed within the LI-1 District (if a CUP is approved) . . . and the property in question is within the LI-1 District, not a residential nor commercial district, for example, so the CUP does not need to comply with criteria for districts other than LI-1, even if those other districts are neighboring to the LI-1;
- According to the Zoning Ordinance, CUP's may only be allowed if the Commission determines that there would be no impact to the public health, safety, and welfare of the community, and per the professional opinion as clearly stated in the Planning and Zoning Staff Report, "As identified in Attachment C, staff believes a conditional use permit can be granted subject to the conditions and improvements identified in the attachments." At Attachment C, it is noteworthy that Staff shows that four of the five criteria listed are plainly met for the CUP, with the lone mention of a question being whether the conditional use is in conflict with the Comprehensive Plan, but notes that the Comprehensive Plan designates the property for mixed-industrial use . . . for which the application falls within the guidelines for mixed-industrial use. As noted, the Commission would need to make a discretionary decision that a new fueling station and restaurant are not needed within the LI-1 District, but such a subjective determination would be a very slippery slope for the Commission to stand on, essentially determining that the Commission knows that a certain number of given business types are "good" or "bad," needed or not needed, and who is to say that in a given part of the City that there should not be a new restaurant or a new book store or a new real estate office or a new coffee shop or a new clothing store or a new fueling station . . . providing the business is approved for the particular zoning district in question? If a business person is willing to embark upon a business venture and that business venture is to be situated in an already-approved part of town, how could the Commission deny that decision by the business person? Such a decision would be a heavy cross for the Commission to bare and would blatantly invalidate the much more professional and objective findings of Staff that the first four criteria for a CUP are met by the applicant. And, such a decision that denies the objective criteria presented leaves the City open to charges of being arbitrary and capricious in its decision making, which would present potential grounds for legal action against the City that would be costly to defend, a situation which no City office would want to be the cause of.

And what are the voices against the applicant saying?

■ That the application will create new and increased traffic and that previously-cited traffic studies are outdated and not applicable. Indeed, the applicant is in many ways proposing to make the current traffic situation much more safe and orderly, a true benefit to this currently dicey traffic area where there is a real need for safety improvements. And he has paid for a new traffic study that shows that there will not be traffic problems associated with the development. And, let us not forget

that one of the loudest voices from the June 13, 2016 meeting crying out about traffic studies would like to see the property remain just as it is because they use the property for after business hours parking, blatantly "borrowing" the private property for their own use! Another voice decrying the unsafe vehicle and pedestrian traffic situation in the area is a current tenant at the Northtown Center – the current name of the property – who from a self-interested perspective would like to see no change at the property as change would mean that she would need to relocate. And, this is not to mention that she would like to see the application denied as she herself is known to desire to purchase the Northtown Center property, albeit at a fraction of its market value. For these self-serving reasons, these "no" vote commenters would have the Commission deny the application and continue with the status quo of unsafe vehicle and pedestrian traffic, denying the much safer traffic solutions as proposed by the applicant!

- That by improving the 10th Street area with sidewalks and lighting and cross walks that this will be a safety hazard, encouraging people to traverse up and down the street! Indeed, the applicant will need to make many costly and necessary improvements to the 10th Street area and this will be a very good thing! But, it is not the applicants obligation to improve neighboring properties along 10th Street that are currently not in compliance with numerous zoning regulations having been "grandfathered" in ahead of the current rules if and when those neighboring properties seek to change or re-develop, they, too, will have to toe the line and improve their sites, but in the meantime, the applicant will be greatly improving this part of the neighborhood from what it currently is and this can only be seen as a positive contribution to 10th Street and the City.
- That there will be light pollution from the new business. The applicant will have to abide by all necessary rules and codes in regard to this subject and has even taken extra and additional steps to be a good neighbor and limit the potential for after dark lighting. And, the applicant has taken these extra efforts to appease the cries from a neighbor across 10th Street who is not even open for set hours of business during the day time, much less during non-business hours at night!
- That gas stations are inherently unsafe to be around. In a perfect world, we would not need nor use cars. But it is not a perfect world and we as Americans use cars every day . . . a lot! And to use our cars, we need fuel. We are not going to change this nation-wide fact by giving a thumbs up or a thumbs down to this local permit application, but to make fueling stations as safe as possible, we entrust numerous governmental agencies to protect the public welfare and those governmental agencies will oversee this fueling station as they do all other such facilities. And we are thankful for this professional oversight. But, for those who are potentially hyper sensitive to living near the LI-1 District where fueling stations are allowed, perhaps those individuals should choose not to buy condominiums that are already near other fueling stations at Warm Springs Road and 10th Street . . . or to not build new homes that are on 9th Street just across the road from the LI-1 District!

But what if the Bracken Station Conditional Use Permit was miraculously denied? What would happen at what is now called the Northtown Center? There have been suggestions that another good buyer would come along and re-develop the property and all would be right and proper. But to date that has not been the case as the property has been for sale for more than 10 years with nary a helpful buyer. And if the current applicant were denied after jumping through so many conditional "hoops", how likely would another potential buyer be willing to be subjected to the same gauntlet, spending time and money with the outcome of their efforts very much in doubt? And how does that bode for a city that is trying to find its economic footing, trying to encourage change and growth and vitality? The answer is, it would be discouraging to a potential future buyer of the property and it would not be productive for the City as it seeks to shed its reputation as being unfriendly to development and progress and finding ways for businesses to succeed and provide opportunities for residents to find gainful employment in Ketchum.

The current owners of Northtown Center are good people who have already added to the prosperity of Ketchum at this and other locations within the City, but they are not young and they are not wealthy and they do not have the time nor the money to re-develop Northtown Center to make it a more vibrant and safe property than it currently is. Indeed, if the Bracken Station Conditional Use Permit is denied and the current owners are forced to retain the property, it will not be improved and it will not be safer and it will not be a positive contribution to Ketchum moving forward – at best, will remain as is, but it could easily become worse than it currently is with fractured pavement and peeling paint and dangling wires, no great example of a welcome to Ketchum.

For all the right reasons, I politely urge the Ketchum Planning and Zoning Commission to approve the Bracken Station Conditional Use Permit and help to move the City forward toward improvement and prosperity.

Sincerely,

Brian Emerick

Roian Gueridz

From: Jack Burgess

Sent: Tuesday, July 05, 2016 1:19 PM

To: Participate < <u>participate@ketchumidaho.org</u>>

Subject: Bracken Station

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Subject: Gas station and convenience store on 10th Ave

I would like to express my position in favor of the gas station on the corner of the highway and 10th Ave. A property owner who purchases land in the "Industrial Zone" should be able to develop what he wishes within reason, and for out of town people passing through Ketchum not knowing where to get gas, would have the opportunity to gas up and purchase items before heading north. In reading the article in the Mountain Express about the decision being on hold because of a homeowner building a residence across the street from the proposed site, should have known the Industrial zone was there when they decided to build.

Thank you for your consideration.

Janet Gentile Homeowner - Ketchum

Wednesday July 7, 2016, 10 am MST

To: Ketchum P&Z

From: Kathy and Nick Gyurkey owners, North Town Center

corner of Tenth Street & Highway 75

Re: In support of gas station proposal

We are the owners of North Town Center, a property under consideration by the P&Z for redevelopment as a gas station and convenience store. Given that these are conditional uses in the LI, our understanding is that the applicant has satisfied every condition that he could identify for conforming to existing codes and ordinances. Our question is this: If people who would redevelop and improve substandard properties in clearly delineated city zones cannot rely upon the written rules in planning a project, how will the city ever move forward to correct deficiencies and provide higher quality services for residents and visitors?

As you know, the property in question sits at the northern perimeter of the city right on Highway 75, a Scenic Byway that our town hopes will bring increasing numbers of visitors into our tourist-based economy. Anyone who owns property adjacent to a scenic byway, and adjacent to a Light Industrial zone as well, should be prepared for and should welcome new development that will improve convenience, traffic flow and safety for the entire community and its guests. The subject application, which was put together over a year of consultation with all manner of experts including city officials, surveyors, engineers, hired consultants and legal minds, is specifically intended to fulfill those conditions as anticipated by the zoning code. For example:

As it sits, a grandfathered building abuts the property line (no setbacks) at the corner of Tenth Street and Highway 75, partially occluding the view of traffic arriving up Tenth to access the highway. With the proposed redevelopment, that building would be gone. In its place would be newly engineered access to the property as well as sidewalks and appropriate lighting. Thus, not only will a new gas station conveniently provide a modern service for people who are looking for that very thing as they arrive in or exit the town, but because of its compliance with modern zoning ordinances, designed and written into the zoning code for just that purpose by elected officials and hired experts, it will be a decisive improvement over existing traffic and safety conditions at its own location or at any of the existing gas stations in the LI (none of which could come close to qualifying under current codes.)

Our understanding is that this is what Ketchum's zoning codes and city ordinances are supposed to be for: moving the town forward using periodically updated guidelines to induce developers to put in higher-quality installations to keep our community current and attractive while answering the obvious needs of our residents and visitors. Gasoline could hardly be a more obvious need, and right now the only gas stations in the north end must be accessed by secondary streets with whom guests are unfamiliar, rolling them through numerous intersections that could and do have frequent pedestrian use. (We see almost no pedestrians at

the corner of Tenth Street and Highway 75). Some larger vehicles, when they do locate existing gas stations, have a hard time navigating the substandard, not-to-code layouts of the pumps, sometimes causing traffic jams as they attempt to maneuver in tight spaces. Even worse, vehicles servicing the convenience store called Base Camp are frequently seen stopped in the middle of the street holding up traffic in more than one direction.

Aren't unsafe situations like these what the zoning ordinances are designed to correct over time? This project would seem to fulfill that purpose because many individuals on the city's staff have gone on record to note that Roy Bracken has complied with every objectively-achievable city ordinance governing conditions for fueling stations - and he proposes it at a location where people new to town won't have to go searching through side streets for something as essential as gasoline.

Conditional uses have conditions that are specifically designed to further the goals of the community such as safety. I've seen much comment in the press about safety safety safety from people who clearly haven't studied the matter in light of the fact that the applicant has satisfied all those conditions. This calls into question what their real motives might be for attempting to obstruct Roy Bracken's CUP application. One late comer to the Scenic Byway "neighborhood," Barbie Reed, purports to be worried about safety while she constructs a multimillion dollar residence that, in light of her NIMBY attitudes, probably should have been located across the street from other residences rather than from the LI zone. I'm told that another "neighbor" alludes to safety concerns - but maybe is more worried about the continued availability of free overflow parking for his employees and the vehicles of his valet-parking patrons.

You have a bunch of objectors who are affiliated with one of our tenants worrying about "safety" too. Putting aside such obfuscations, we sympathize with her desire for her business to enjoy easy access and visibility at a low rent rate. But that advantage is not some kind of tenant right that she is entitled to assert by virtue of having a lot of friends who will show up at P&Z meetings. Put another way, property rights belong to owners and should be something that can be identified within zoning codes and building ordinances, not defined by popularity contests or sentiments about maintaining the status quo so that some small businesses won't have to relocate.

I hear too that residents at Frenchman's Place condos do not want a CUP issued, as if denying this will somehow offset the fact that their own complex is built in the LI and has LI tenants right on the premises. Maybe they think that our part of the LI should now become subject to their more residential sensibilities rather than to the zoning code.

Then there's the objector who operates a gallery across Tenth Street and is outraged because the proposed canopy lighting will filter in to his place of business - even though according to a sign on the door his establishment is not even open when the lights would be on. Some say he's part of the above-referenced pressure group that would like to buy the property if Roy Bracken's good faith application for a CUP for some reason fails. Trust me, no one who is part of that group is going to buy the property for what they want to pay; we prefer to keep it, status

quo.

To that point, let me conclude by saying that we have owned North Town Center (the newspaper for some reason thinks it's called "the building that houses Lizzy's Fresh Coffee") for about fifteen years and have kept it in as good a shape as our means permit. It is not exactly "an eyesore" but parts of it are pretty obsolete and can't undergo meaningful upgrading even if the rents could provide funds for that. There's also the fact that parts of it can't be improved because they are grandfathered struactures. The only way this property can be improved is by redevelopment. If the current attempt to sell our property fails for reasons that can't be found in the ordinances, it will be difficult for us to find another buyer who will risk being turned down by the city after spending huge amounts of time and money to comply with every guideline in the Planning Department's book, as Roy Bracken has done. But should that eventuality come to pass, whenever it is that another project arrives before this body of public servants, I confidently expect that it will NOT be something that Barbie Reed likes any better - and I hope that however far down the road that day will be our public servants will recognize that Ketchum is ready and willing to follow its own written rules rather than unwritten laws of NIMBY-ism, emotion and in-crowd opportunism.

My husband and I, locals of long standing ourselves, sincerely hope that people dedicated to good planning, fair play and the rule of law will swiftly approve this application.

Nick and Kathy Gyurkey PO Box 21 Ketchum ID 83340

208-720-0599

Tuesday, July 5, 2016

To the Ketchum P&Z In favor of the gas station application for conditional use permit

I think you have a deadline, and I would like to put in my two cents about the gas station. This is a much needed improvement for Ketchum. As it is now, nobody can find a gas station in town if they don't know their way around. About as close as we can come to a visible, easily-located gas station would be south of town at Mountain View. It's old and run down and unattractive, hardly the kind of place that well-off visitors are accustomed to going for gas.

If you're headed out north, you've got to detour off of main street and go searching around down in the LI, looking for a place that you can fuel up - and probably paying more than the going rate to boot. The first place you find is more convenience store than gas station with limited pumps and not enough room to get around if trucks are using one of the pumps. So you wait and you're blocking the next guy from trying to get in too. Or you go to the car wash place, if you can find it.... I mean, how many self-respecting towns put their tourists through that kind of effort just to get gas?

Corner of Tenth and Highway 75 is just an ideal place for a gas station. It's not such a great place for a NIMBY palace like the one going up on the other side of the highway. If that woman was looking for views she should have realized that the LI was going to be part of her views.

Let's approve this, for all of us.,

Rick Sharbinin 307 S. Main St. Ketchum ID 83340

208-481-0496

From: Scovell Susan [mailto:sscovell@gmail.com]
Sent: Wednesday, July 06, 2016 11:08 AM
To Advantage of the series of the series

To: Micah Austin < maustin@ketchumidaho.org >

Subject: Bracken Gas Station

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From: Barbi Anne Reed
To: Brittany Skelton

Subject: two further comments on the Bracken Station...a contradiction and dangerous blind spots!

Date: Thursday, July 07, 2016 2:48:17 PM

Brittany, two points:

1. It is my understanding that the City of Ketchum is encouraging bicyclists and pedestrian walking on its city streets. In view of this, I have heard and it was verified by your department that builders of new buildings are exempt paying an in lieu fee of approximately \$40,000 and being forgiven the requirement for one parking space if they install bike racks and a shower in their building to allow employees and employers the opportunity to shower before or after physical activity or after riding to walk.

Allowing a gas station on Main Street attracting large vehicles which will endanger bicyclists is a clear contradiction to what the City is attempting to do by offering "forgiveness" at a substantial figure!

This is ironical at the very least and confusing.

2. If the City's intention is to create more more people on bikes, the burden is on the city to make it a safer place to ride bikes! See the attached re. the dangers for bicyclists (and this does not count bicycles with kid trailers and recumbent bicyclists and inexperienced bicyclists). In addition bicyclists are forced by the proposed gas station site as there is a gap in the bike path. With a bike shop down the street and vehicles on the highway traveling north and south, turning into and out of Cemetery, Knob Hill Inn, 10th Street Light Industrial, 10th Street, 9th Street, Frenchmen's allowing a business whose main purpose is to attract high volume of traffic and necessitates that vehicles cross lanes will invite accidents.

See blind spot info below which P&Z should consider as major safety risk to both pedestrians walking to the convenience store as well as bicyclists using the road.





If you're a cyclist approaching an intersection in a bike lane where a car is already waiting to turn right, there is a pretty good likelihood that they will not be able to see you before they make the turn due to the vehicle's blindspots--even if they are actively looking for a cyclist

With large vehicles anxious to turn left and or right into or out of the gas station on this busy stretch of Main Street (Highway), with vehicles going more than 25 MPH

Top Ways Cyclists Get Hit By a Car

The majority of collisions between cyclists and vehicles happen in the following situations. All of these are driver error largely because they don't know the cyclist is there.

The Pull Out

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Cyclist's Action

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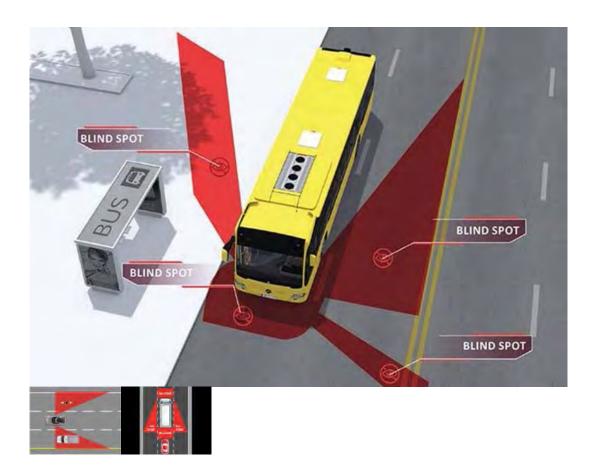
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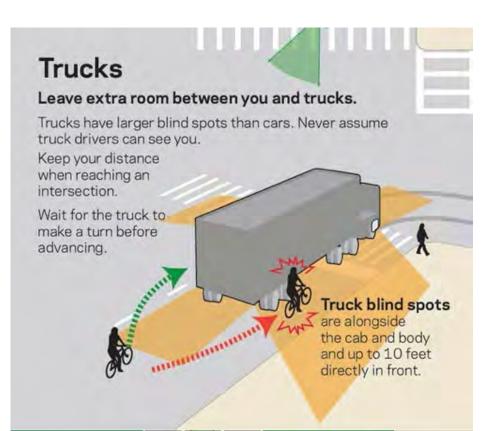
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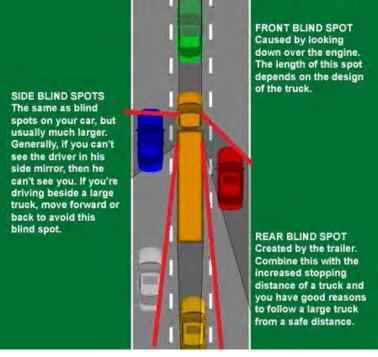
Traffic Lights

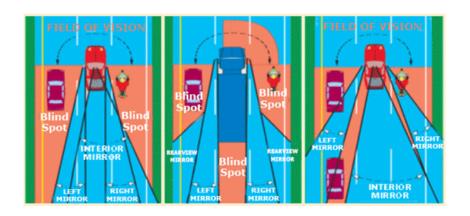
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see: http://www.ilovebicycling.com/how-to-not-get-hit-by-a-car-riding-your-bike/

















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J. KEVIN LAWLER

July7, 2016

Planning and Zoning Commission Department of Planning and Building City of Ketchum PO Box 2315 Ketchum, ID 83340

Via email to:participate@ketchumidaho.org

RE: Bracken Station – CUP Continuation Hearing

To Staff and Planning Commissioners:

I am again writing to strongly object to the proposed Bracken Station project at 911 N. Main. At the time of my previous correspondence I did not have the benefit of examining the applicant's submission for two Conditional Use Permits ("CUP"), or reviewing the June 13th Staff Report or observing P&Z hearing on June 14th on this matter.

My objections to the Bracken Station CUP application are as follows:

- 1. The applicant's submission for a CUP appended to the Staff Report is materially deficient. The plain text of the Code, Section 17.116.030 requires: "A conditional use permit shall be granted by the commission only if the applicant demonstrates". There is nothing in the record indicating the applicant has attempted to address nor can they satisfy all of the 5 criteria for either of proposed Conditional Uses for the proposed Bracken Station project.
- 2. The Staff Report (File # 16-34) is incomplete and thus has a bias in favor of the Bracken Station's CUP application. Appendices B and C of the Staff Report conspicuously fails to consider the defined purpose of the LI-1 zone, Section 17.18.140: "Purpose. The LI-1 light industrial district number 1 is established as a transition area providing limited commercial service industries, limited retail, small light manufacturing, research and development, and offices related to building, maintenance and construction and which generate little traffic from tourists and the general public.
- 3. Competent verbal and written testimony has been provided, and thus far largely ignored that there is not a "necessity" for an additional gas station in the LI-1 District or the City of Ketchum, in general. A representative of Base Station testified at the Commission's June 13th meeting that the

volumes of fuel sales in Ketchum are anemic compared to Hailey. The Staff Report clearly indicates there are 3 existing gas stations serving the LI-1 District. A report prepared by Richard Klein, Community Environmental Defense Services ('CEDS') documents the LI-1 District is currently over-supplied with gas stations.

- 4. Neither Planning Department staff nor the Commission has yet to fully evaluate the proposed Bracken Station project relative to the standard of "health, safety and general welfare". Specially not yet evaluated are:
 - ✓ The potential negative environmental impact on proximate residents and businesses from toxic fueling fumes and elevated exhaust associated with increased traffic generation attributable to the Bracken Station project. Reference: *Journal of Environmental Management*, 2010; 91 (12): 2754 DOI: 10.1016/j.jenvman.2010.08.009 and Health Effects, Community & Environmental Defense Services, CEDS.org.
 - ✓ The potential decrease in residential property values proximate to the Bracken Station project, and the corresponding difficulty and increased costs owners will incur in selling, refinancing and insuring their residences Reference: HUD Handbook 4000.1 II.B.3.c. iii.(C)(7).

As an owner in Frenchman's Place for nearly a decade, I am deeply concerned the proposed Bracken Station project, if approved via Conditional Use Permits, will substantially diminish the value of my residence and irreparably damage the lifestyle I have enjoyed residing in the City of Ketchum.

Respectfully,

J. Kevin Lawler

PETITION IN SUPPORT OF A SERVICE STATION ON

LOT 5A BLOCK 30 KETCHUM IDAHO

Name LUCAS	Address Lews St Ketchum
Rob Breier	PO 6257 Ketehum
Keith Morgan	po 10185 Ketchum
Pable Fougle	POBOX 1197.
Juan Giraldo	PO BOX 1192
BREUNA LBARRA	P.O. ROX 3153
Fyle Moore	P. OBOX 1601 Sunlalley
Josh Morell	601 Cayon Rd
Connor Hall	241 2nd Ave 5. #2
Daid Marks	P.O. Box 1605
Myl Prin	Box 513 Soul Vally fronthman
Hally Hone Prior	360 Fast 945t condo 17 Keldnen
Vicial Notrol	BOX 553 Kor Td.
BEETT UMDELLENGERG	BOX 4227 KETCHIN
THE Buck	Box 1482 Melahern
Rain Wade	BOX 1896 Letchum
Natalie Batti Stone	Box 1896 Ketomm

PETITION IN SUPPORT OF A SERVICE STATION ON

LOT 5A BLOCK 30 KETCHUM IDAHO

Name ENGIOCE JAMOS	Address P.BOX 4 Hailey 83333
Shawn Menahan	Box 5305 Ketcheun
child	P.O. 520 Hailey ID 83333
Calloper	Box 26.7-
Janu Valuica	Box 2786 Hauls
Attyu	Box 4821 Hailes
Marin Bantos	P.O. Box 4 Hailey
Matte Linka	PO BOX 1555 RETCHUM
SHIM	Po. By 4985 B3340
Fernando H.	PO Box 264 Bellevue 11
Marco Alvarez	· Corret Idaho
Harry Schultz	412 N 3rd Street Bellevic ID
Mai -Prior	PO BOX 1171 14
Perry Boyle	P.O BOX 4200 Petder
Belg Sets	1.0 Box 6890 Ketchim
Kich Wielling	Por Sax Zoz S. 3. D. Ave Hailey
Brys (Vrunge	90 feet 640 SV. 8333
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PETITION IN SUPPORT OF A SERVICE STATION ON

LOT 5A BLOCK 30 KETCHUM IDAHO

	<u> </u>
Name Patrick Jam 1900	120 Buddern dr. Hailey ID. 8338
Esne Sloy	2121 W Hailey
Som Hunthausen	2713 Gold Rus Are Helen MI = 918
Christoper Cook	105 Red Will ld Vetchum
JOEL R. EUGHS	P.D. BOX 2002 KETCHUM, ID
JON TRAPP	BOX 381, KETCHUM ID
Alberto Lorez	403 3rd Ave W. Jerowe ID
Paule Lopez	208 Hailey eld
Enic famuall	2630 Woodside blind.
MICHAEL Whyte	2545 Woodside BLUID.
Stopher Golde	2630 wordsi de blud
Bully Whitherserg	491 East Av. N
Nany Warner	155 TOWNSONS ROAD
BRUCE SMITH	ZII4 WARM Spas RD. KETCHUM
Mork Phillips	941 Cherry Creek Dr., Hoiley
, ,	P.O. Box 4676 Hailey
Carla Agreno	P.O. BOX 725 KETCHUM IDAHO

PETITION IN SUPPORT OF A SERVICE STATION ON

LOT 5A BLOCK 30 KETCHUM IDAHO

Name Bill Flanagan	Address PO Box 1059 Kg
Cinky ashburn	BOX1246 R
John Shins	331 Melvose St.
Jan 16	Box 357. Bellevue
The Might	By 3956 Kotchan
	· · · · · · · · · · · · · · · · · · ·

PETITION IN SUPPORT OF A SERVICE STATION ON

LOT 5A BLOCK 30 KETCHUM IDAHO

Name	<u>Address</u>
Keith hom	Bellevue
Alaine Woodall	Mailey
Mike Wingmson	Hailey Hailey
Scott Noel	Hailey
	V

SURVEY

Proposed

BRACKEN GAS STATION and CONVENIENCE STORE

MAIN STREET KETCHUM between 10th Street and East 9th Street (opposite Knob Hill Inn on site of Lizzy's Coffee)

Check all that you feel apply
There are enough gas stations in Ketchum. Ketchum does not need another.
Bracken Station will be permanent, visual blight at Ketchum's Northern Gateway
Bracken Station will create a toxic environment (air and soil contamination) creating a health hazard, especially for nearby Ketchum residents, workers, businesses
Bracken Station is not in compliance with stated LI-1 Zoning purpose: "generate little traffic from tourists and the general public"
Bracken Station's convenience store will lure children from Hemingway and YMCA, onto 10 th St. (incredibly dangerous now even without the addition of more vehicles generated by Bracken Gas Station). Bus stop riders will be lured across highway from the bus stop located directly opposite of proposed Bracken Station.
Bracken Station will dramatically increase traffic on North Main, 10 th Street and other adjacent streets endangering pedestrians and bicyclists. (Note: Bracken Station is designed for use by extra heavy trucks (e.g. dump trucks, tractor-trailers with long hitches, semis, 5 th wheels, large RVs, snowmobile /horse trailers, etc.)
If Bracken Station is approved, I understand that City of Ketchum could not prevent sale to a large gas station e.g. Stinker Store, Exxon, Chevron, etc. or the opening of a Fast Food franchise, such as Subway, McDonald's, Sonic, Arby's, etc.
Bracken Station does not comply with Ketchum's Comp Plan: "to promote the health, safety and general welfare of present and future inhabitants of Ketchum"
All of the above None of the above
ADDITIONAL COMMENT: (or submit to: pzcomments@ketchumidaho.org) Construction of the gas station for all residents north of town + Would give the appearance of a commercial touch stop which is not in keeping I disapprove of Bracken Station on Main Street, Ketchum. You may list my name with the
among others who oppose this development. City of Residence
Print GRAENE and Novah BRETALL
Remind me about the next P&Z meeting, Monday, 11 July, 5:30 PM Contact info

miresto

Dusty Wendland 600 N. Main St. Hailey ID 83333

July 7, 2016

Planning & Zoning Commission City of Ketchum Box 2315 Ketchum ID 83340

Re: Bracken Station CUP Application

To the City of Ketchum and Planning & Zoning Commisioners,

The intention of this letter is to assist the P&Z Commission in deciding the permissibility of the CUP application brought forth by Bracken et al., especially by providing for the public record certain factual and relevant information that may provide a framework for a legal and judicious decision by the Commission in this matter. In so doing, it is also my intention to empower the Commission to decide the matter for itself, rather than to permit the presupposition to stand that these Public Hearings are only *pro forma*, because the matter has already been decided *for* the Commission and *for* the City by the applicant and his legal counsel --which is simply unacceptable and insulting to the Commissioners and the public alike.

There are three subject matters that I would like to attend to in some detail, if you will provide my contribution to the discussion with a patient and attentive eye and ear. Here are the subjects I wish to cover:

- 1. The effects upon adjacent real estate if the proposed use, a gas station, is permitted by the Commission.
- 2. The architecture of underground tanks and their impact on air space.
- 3. The relevant zoning code of the City of Ketchum.

I have attached two addendums at the end of this letter, which will be referenced in the subsequent analysis.

1. The effects upon adjacent real estate if the proposed use, a gas station, is permitted by the Commission.

I indicated in the public commentary at the last Bracken Station Hearing that properties adjacent to a gas station could have their real estate values adversely affected as a consequence of additional difficulties and costs in obtaining a loan because of their proximity to a gas station. I personally had this experience on property I own adjacent to a gas station just last year on the south side of Ketchum. To

substantiate the validity of my experience, I contacted the lending banks with whom I had inquired and requested a written explanation of their underwriting requirements for lending on properties adjacent to a gas station. Please see Addendum A, a letter from the commercial lending officer at D.L. Evans bank.

If the Bracken Station is granted a CUP, there are two substantial consequences that follow from the development of the subject property as a gas station, which are indicated in this letter:

- 1. Consequences to the subject property itself: The subject property itself becomes more difficult and more expensive to redevelop for a future use in the event that the current developer fails. Please recall, as I indicated at the first Bracken Station Hearing, that based on the real fuel volumes and the size of the overall pie in Ketchum, which I know intimately, failure is a definite possibility for this development. Consider, for purposes of understanding the implications here, that this is effectively the "Hitchrack situation" in Hailey, where a fueling station failed and the property is now encumbered by its former usage. Any potential buyers of that former fuel site, who are not all-cash purchasers, would be required by a bank to jump through the expensive, requisite hoops of EPA studies and, potentially, subsequent remediation prior to securing financing. Consequently, the property becomes "damaged goods," difficult and expensive to secure a mortgage upon, and, therefore, has a significant reduction in potential future buyers for redevelopment. This is why the Hitchrack location has been for sale for 8 years and remains perhaps the greatest eyesore in the city of Hailey. It is damaged and encumbered beyond cost-effective redevelopment. Ketchum could be herein permitting the erection of its own Hitchrack at one end of town: book-ending the valley, for sure, as Mr. Cook stated, but perhaps not in the manner in which he was suggesting.
- 2. <u>Consequences to adjacent landowners:</u> Whether surrounding properties be commercial or residential in usage, those properties are also impacted by the development of an adjacent property as a gas station. A lending bank could require Phase 1 and Phase 2 EPA studies and subsequent remediation *on the adjacent properties* prior to lending on said properties. Because these studies are prohibitively expensive and time-sensitive, sellers of adjacent properties may need to pay for and provide such "clean soil" studies for their potential buyers at the time of each potential sale. Should the sellers choose not to incur this cost when listing their properties for sale, the buyers could be required by their lenders to pay such costs prior to securing a mortgage or closing a sale on an adjacent property. This functions as an encumbrance to the adjacent properties, making the sale and transfer of such properties more costly and more difficult. Again, such costs and difficulties reduce the number of potential buyers, decreasing demand and driving down the values of all adjacent properties.

Finally, this bank letter indicates that the necessity of such EPA studies would be dependent upon an appraiser's environmental impact analysis of the adjacent property's proximity to a fuel-dispensing site. Considering that fuel, like water, flows down hill, I would suspect that adjacent property owners, most of whom are downhill from the proposed site, could very likely find themselves subject to expensive EPA studies and remediation at the behest of the appraiser and, hence, of the lending bank.

I also spoke with Melissa Humphreys at US Bank's commercial lending department. While she would not provide a letter for me, she did provide a great deal of relevant information regarding her bank's lending processes in these circumstances. She informed me:

1. Lending on commercial property is more stringent and is, thus, far more likely to affect adjacent commercial properties than to affect the residential properties nearby. Phase 1 and Phase 2 EPA studies would likely become necessary for bank underwriting on commercial loans for "adjacent properties." I

have placed that "adjacent property" in quotations because she subsequently provided an example of a real "adjacent property" scenario in which the subject property was *across the street* (across the four lanes of Main St. in Hailey, no less) and *two lots south* of a dry cleaner. She did the commercial loan on this "adjacent property" and required Phase 1 & 2 studies to mitigate risk for loan underwriting because the property was deemed potentially impacted by the dry cleaner--even at that distance (four lanes and two lots away) from the impacting property. She said the concern in that case was both the nature of the chemicals used at the dry cleaners coupled with the subject "adjacent property" being located *downhill* for purposes of both rain water and ground water.

- 2. Residential lending has more lenient standards at her bank and likely would not need such studies unless otherwise noted by the appraiser. She encouraged calling appraisers for both commercial and residential property, however, as they likely would indicate that adjacent properties will be significantly negatively affected for a multitude of reasons--all of which they take into account when they determine the appraised value of the property--which of course is used for the loan-to-value ratio for a potential buyer of the property.
- 3. She pointed out that even though the Frenchman's condos are currently used as residential and thus subject to the residential lending standards—this might not always be the case. Whether it's currently zoned for mixed use or for light industrial, she said, its future usage could certainly change years down the road. If, at any future point, it changes to mixed use with commercial or to a light industrial usage, it will become subject to the commercial lending standards and absolutely require such EPA studies for underwriting then.

2. The architecture of underground tanks and their impact on air space.

Please see Addendum B, which comprises three images referenced herein.

The first image below reveals the underlying architecture of an underground storage tank. The link directly below the photo will direct you to a website that explains each item in the configuration. Let me point out that letter "M", the pipe sticking up above the rooftop of the store, is the underground tank vapor vent pipe. The proposed Bracken Station likely has *two* 12,000 gallon underground tanks slated for installation. A fuel transporter (which is the standard full-size fuel tanker you see on the road and which usually pulls a pup trailer in addition to the main truck, see second picture in Addendum B) carries 12,000 gallons of fuel *for a single delivery*. Having two underground fuel storage tanks is like having two Greyhound buses buried in the parking lot; the proportion is a bit hard to grasp when referencing the gallon capacity. These are very large tanks. A single fuel transport delivery will fill *one* of the two underground tanks, not both.

For each compartment in an underground tank--which is determined by how many different fuel products you carry--so 85, 91, and diesel would be three compartments in the underground tank--for each one of these, the tank needs a vent pipe to release its vapors from that compartment as the transporter drops the fuel down into the ground. The third photo in Addendum B shows a real gas station with multiple vent pipes for multiple compartments. When a transporter drops 12,000 gallons of fuel into an underground storage tank, it results in the evacuation of 12,000 gallons of vapor via the fuel vent pipes. These vent pipes are typically located alongside the store and reach above the roofline (as drawn in the first photo) or along the canopy (as pictured in the third photo, thus protruding from the top of the canopy and releasing the vapors above it). They extend quite high, usually about 17 feet, as the fuel vapors they emit are toxic and highly flammable. Venting the pipe above the eave of the store or atop the canopy places the vapor-release process a fair distance from customers and allows the

vapors to dissipate at a distance deemed safe for the general public. However, as anyone who works at a service station knows, when there is no wind, the vapors simply cascade down and settle into the parking lot, accumulating all 12,000 gallons of gas vapor in the parking lot. It is not uncommon to fear, on those days, that a customer might try to light a cigarette in the parking lot. I'm not sure what the consequence would be during such windless fuel deliveries. On the other hand, if the prevailing winds are in the direction of nearby residences with open windows, I'm confident that they would be the ones unpleasantly impacted by these vapors.

As for what is meant here by vapors, just consider that these underground tanks are no different than giant gas cans. When the quantity of liquid in the tank diminishes, a greater quantity of gas evaporates to fill the empty cavity. These are *not* pressurized tanks (which would maintain the fuel's liquid status). They are just holding tanks with open breathing "vent pipes." When the underground tank inventory is quite low, the full volume of the tank is filled with flammable gas fumes. When the transporter suddenly drops 12,000 gallons of fuel, 12,000 gallons of this vapor-vested air needs to get out of the way of the incoming fuel quickly. This air--all of it--evacuates via the vent pipes and wends whither the wind bears it.

Gas cans now frustrate us every time we fill our lawn mowers precisely because they now have vaporloss prevention systems--which also make it tremendously difficult to get the gas to come out of the gas can even when you want it to--perhaps some of you are familiar with this new frustration. In the case of a 12,000 gallon gas tank, however, no such requirement is in place, and the architectural design of the underground tank precisely intends for a free-flow evacuation of the 12,000 gallons of vapor with every 12,000 gallon fuel drop.

3. The relevant zoning code of the City of Ketchum.

While I'm extremely reluctant to be pedantic, let alone presume to educate the Commission regarding their job or concerning the City of Ketchum Municipal Code, I fear the applicant has provided substantially misleading information to the Commission that requires redress. Please forgive the extent to which the following analysis may rehearse ad nauseam certain portions of the code with which you are no doubt already guite familiar.

Please *carefully* review the following stated purpose of the zoning ordinance of the City of Ketchum:

17.04.020: PURPOSE: 🍨 🖃

These regulations are designed and enacted in accordance with Idaho Code, chapter 65, title 67 for the purpose of promoting the health, safety and general welfare of the present and future inhabitants of Ketchum, Idaho, by accomplishing, among others, the following specific purposes:

A. Residential areas should be protected against fire, explosion, noxious fumes, floods, avalanches, and other hazards; offensive noise, vibration, smoke, dust, odors, heat, glare and other objectionable influences; the invasion of abnormal vehicular traffic; and excessive congestion of buildings.

- B. Residential and tourist areas should have space off public streets for parking; access for light and air to windows; privacy by means of controls over the location of buildings; usable open space on the same lot; land to meet the needs of probable expansion, appropriate sites for those public services which are needed; and tracts for quasi-public uses which provide essential health and welfare services.
- C. Business and industrial developments should be protected against the establishment of uses which would create serious hazards or exceptional noise, vibration, smoke, dust, odors, heat or glare.
- D. Business and industrial developments should have area in appropriate locations for the transaction of all types of activities; space off public streets for parking and unloading; and opportunities to concentrate for the mutual advantage of merchants, customers and employees. (Ord. 1135, 2015)

A few important and relevant items from the above stated Purpose merit our careful attendance:

- 1. A relevant definition of what is meant by the "general welfare" is stated in the Purpose of the code. As we already know, the Conditional Use Permit, per section 17.116.030 of the city code, requires that the applicant meet five requirements. The second of these requirements (B) reads as follows: "The conditional use will not materially endanger the health, safety and welfare of the community." What is the P&Z Commission to take into consideration under the purview of the "health, safety and welfare of the community"? Lacking a specific definition provided in the "Definitions" section of the City Code, they will be legally required to use the definition supplied elsewhere in the same codified document. The above, then, is the legal definition of the "general welfare," and it constitutes the criteria that the P&Z Commission is obliged to consider in respect to this requirement for a Conditional Use Permit. The terms "health, safety, and welfare of the community" are not ambiguous; they are specific, and they specifically include the following two relevant items:
- 2. Criterion A: Residential areas should be protected against noxious fumes...odors...glare...the invasion of abnormal vehicular traffic. As already detailed in the previous section, nearby residences will be impacted with noxious fumes and odors. A further argument can be made that nearby residences will be adversely affected by abnormal vehicular traffic and glare, not just from vehicular headlamps, but also from the canopy lights. The city is legally required to protect the residents from this impact.
- 3. Criterion C: Business and industrial developments should be protected against the establishment of uses which would create serious hazards...odors...glare. As with the residences, nearby businesses will be exposed to the fuel vapor odors and excessive vehicular headlamp and canopy glare. Additionally, the heightened risk to pedestrian and vehicular traffic, if this usage is permitted in this location, could be tantamount to a serious hazard for employees and customers of nearby businesses—a hazard from which the city is legally required to protect them.

Next, let us read the rules of interpretation of the code as stated by the City of Ketchum code itself:

17.04.040: INTERPRETATION: 🗣 🖃



In the interpretation and application of the provisions of this title, the following regulations shall govern:

- A. Provisions Are Minimum Requirements: In their interpretation and application, the provisions of this title shall be regarded as the minimum requirements for the protection of the public health, safety, comfort, morals, convenience, prosperity and welfare. All provisions shall be liberally construed to further its underlying purposes.
- B. Application Of Overlapping Regulations: Whenever the provisions of this title, or a provision in this title and any provision in any other ordinance, resolution, rule or regulation of any kind, contain any restrictions covering the same subject matter, the more restrictive or higher standards or requirements shall govern. All uses and all locations and bulk permitted under the terms of this title shall be in conformity with all other provisions of law.
- C. Existing Permits And Private Agreements: This title is not intended to abrogate or annul:
- 1. Any permits issued before the effective date hereof; or
- 2. Any easement, covenant or any other private agreement. (Ord. 1135, 2015)

Of significant note here are the following points:

- 1. In their interpretation and application, the provisions of the code are minimums for the protection of the public health, safety, welfare, etc., and all provisions shall be liberally construed to further its underlying purpose. The meaning of this is that the P&Z Commission and the City of Ketchum are obligated to abide by the written statutes of the code as a minimum level of protection of the general welfare and that all written provisions shall be interpreted liberally, i.e., with latitude, by the Commission and the Council to effect the stated Purpose of the municipal code: to protect and promote the safety and general welfare of the community, including specific protections against noxious fumes, odors, glare, traffic congestion, and serious hazards. The code herein requires that the City bias its interpretation of the code in favor of protecting and promoting the basic purpose and intention of the whole document: i.e., the general welfare.
- 2. If there are any two or more ordinances, resolutions, rules, regulations or restrictions of any kind affecting a subject matter or proposal, the P&Z Commission and the City is herein instructed by the code to follow the highest available standard, not the lowest permissible standard that an applicant can root out in favor of a proposal. In effect, an applicant for a CUP must meet the highest standard available in the city code when this standard is liberally interpreted by the Commission in favor of protecting the general welfare, as defined above. This is the City's obligation and its duty to its citizens.

Lest there is any question regarding this, the document itself does define its usage of the word "shall" as follows (17.08.010 C): "The word 'shall' is always mandatory and not directory."

Next, let us read the zoning district's purpose specific to the LI-1 district:

17.18.140: LIGHT INDUSTRIAL DISTRICT NUMBER 1 (LI-1): 🔮 🖃

A. Purpose: The LI-1 light industrial district number 1 is established as a transition area providing limited commercial service industries, limited retail, small light manufacturing, research and development, and offices related to building, maintenance and construction and which generate little traffic from tourists and the general public. (Ord. 1135, 2015)

Of significant note here, we must review the following points:

- 1. The district is intended to be transitional and provide *limited* commercial services, *limited* retail, etc., *related to building, maintenance and construction*. This is a technical, legal sentence that must be read, understood, and interpreted correctly by the P&Z Commission and the City. While the LI-1 district does permit *limited* commercial services and *limited* retail--it does so *only if those services and retail are related to the uses subsequently detailed*: building, maintenance and construction. In effect, commercial services and retail are permissible in the district *only* if they are related to building, maintenance, and construction in their nature--e.g., a paint store, a tile store, or a lumber yard. A toy store and a grocery store, on the other hand, are not permissible retail uses in the LI-1 district, as they are not, in their nature, related in any way to building, construction, or maintenance. Any proposed retail or commercial use not specifically related to these industrial uses *is not permitted in the LI-1 district per the city code*. The applicant in no way has suggested that the proposed use would service the building, construction, or maintenance industries; in fact, he has stated the opposite.
- 2. The permitted LI-1 uses *must generate little traffic from tourists and the general public*. As the applicant has clearly and repeatedly stated on the record, the proposed gas station is specifically intended to service tourist traffic and the general public. Furthermore, the proposed station will undoubtedly generate substantial traffic from these tourists and the general public alike: hence the outpouring of public concern for the safety of pedestrian traffic walking to the proposed station and the public outrage that the proposed use would produce significant traffic congestion at surrounding intersections. The proposed use is a high-traffic, public, and tourist destination and is specifically intended to be so, per the applicant's own statements and per the proposed business's nature and necessity. Consequently, the proposed usage is not permitted in the LI-1 district, per the municipal code.

In connection with these limited uses in the LI-1 district, it bears mentioning that the uses are limited *with cause*: the intention of limiting uses in the industrial districts is specifically to keep the number of businesses competing for space down, which effectively lowers demand for leasable spaces in the industrial zone. As a consequence of the diminished demand resulting from the limited uses, the rent values in the industrial area remain reasonable for local business operators, allowing them to continue to conduct business in the City of Ketchum. If the allowed uses in the industrial districts are permitted to balloon, so, too, will the number of tenants bidding on available space. As a consequence, rent values go up, and businesses (and their jobs) move to a more affordable city with a more affordable industrial zone. The zoning code limits the uses of the LI-1 and LI-2 districts with intentionality toward this end. It is not the prerogative of the Commission, under the purview of a CUP application, to modify the permitted uses of the industrial zone. In fact, this particular application is bound by the uses permitted in the zoning code at the time of the filing of this CUP application, as has already been determined in the Idaho Supreme Court Case, Urrutia v. Blaine County, 134 Idaho 353 (2000).

And, finally, let us read the codified definition of the Conditional Use Permit itself:

17.116.010: CONDITIONAL USE PERMIT: 4 ==

Conditional uses by definition possess characteristics such as to require review and appraisal by the commission to determine whether or not the use would cause any public health, safety or welfare concerns. Accordingly, conditional uses, as have been designated throughout this title, shall be allowed only upon the approval of the commission, subject to such conditions as the commission may attach. Such approval shall be in the form of a written permit. (Ord. 1135, 2015)

17.116.030: CONDITIONAL USE PERMIT CRITERIA: 🗣 🖃

A conditional use permit shall be granted by the commission only if the applicant demonstrates that:

- A. The characteristics of the conditional use will not be unreasonably incompatible with the types of uses permitted in the applicable zoning district;
- B. The conditional use will not materially endanger the health, safety and welfare of the community;
- C. The conditional use is such that pedestrian and vehicular traffic associated with the use will not be hazardous or conflict with existing and anticipated traffic in the neighborhood;
- D. The conditional use will be supported by adequate public facilities or services and will not adversely affect public services to the surrounding area, or conditions can be established to mitigate adverse impacts; and
- E. The conditional use is not in conflict with the policies of the comprehensive plan or the basic purposes of this chapter. (Ord. 1135, 2015)

The Conditional Use Permit is conditional and allowed only by the approval of the Commission. Such approval is contingent upon one overarching principal: safeguarding the general welfare of the citizens. The applicant's legal counsel, Mr. Williamson, somewhat threateningly informed the Commission that there is nothing to consider and that the Commission "must approve" the CUP application because the applicant "clearly" meets the first four criteria (A-D) and the fifth criterion (E) lacks legal standing. Mr. Williamson therein implied that he and the applicant will bring legal action against the City should the Commission deny the CUP on the grounds of the 5th criterion. While the intention of this "schooling" of the Commission was no doubt to put the City in a corner and to strong-arm the Commission into an immediate, if not reluctant, approval of the CUP, the Commission, not the applicant or his attorney, is in the seat of authority, and the Commission has the legal right and the duty to do its job with due diligence. The Commission has both the right to exercise its discretion and the latitude to interpret and to construe the written code specifically to the ends of promoting the municipal code's stated underlying purpose: "protection of public health, safety, and welfare." This clause recurs in the city code over and over as the highest aim of the document--and it clearly defines what is intended by that clause. Mr. Cook's and Mr. Williamson's claims notwithstanding, the Commission has a great deal to consider relevant to the CUP criteria, and the Commission has ample legal ground and, indeed, even a legal obligation to the citizens of its city, to deny the application with cause--not because the application fails the fifth criterion of the CUP, but *because it fails all of the other four criteria as well*. This is a country of laws and not of men, and for this reason, the Commission and City have an obligation to follow their own laws. Mr. Williamson has indicated that the City and the Commission *must* approve the application because the City *must* follow its own laws. I couldn't agree more: the City must follow its own laws, even if that means denying this application.

Regards,

Dusty Wendland

ADDENDUM A:



7/7/16

Re: Lending and environmental risk

To Whom it May Concern:

Thank you for your inquiry regarding how banks consider environmental risk when assessing lending on real property. All lending institutions are faced with financial risks created by adverse environmental factors such as hazardous waste contamination, asbestos insulation, underground storage tanks for fuel, surface impoundment's, septic tank systems or oil and gas wells.

A Gas Station would be considered a high risk property and would require a Phase I Environmental Report be completed. This report is typically paid for by the borrower. Depending on the findings of that report a Phase II might be required. Depending on the findings of the Phase II the lender may require remediation work be complete at the expense of the borrower prior to any loan approval.

I would advise you contact ACS-Assessment / Jane Rosen at 208-788-5649 who performs these type of reports to inquire about cost of these reports as they can vary widely.

Our residential mortgage department handles requests on a case by case basis and their level of due diligence into a any particular mortgage loan request on a property located near a gas station would be highly dependent on the appraisal report. You may want to contact a local appraiser to inquire about their environmental risk reporting requirements.

Please feel free to contact me should you have any questions.

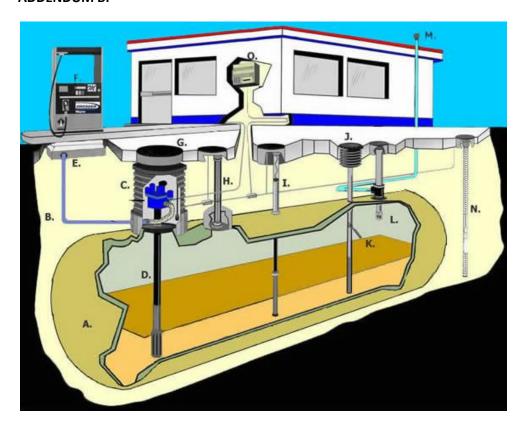
Sincerely,

Mike Schlatter Vice President

D.L. Evans Bank

208-788-2130

ADDENDUM B:



http://www.mascottec.com/UST%20layout.html





From: Barbi Anne Reed [mailto:barbi@annereedgallery.com]

Sent: Friday, July 08, 2016 10:56 AM

To: Brittany Skelton

Subject: short and to the point! Hope you can include...not a single criterion of CUP requirements is met

and in direct conflict with zoning Purpose

It seems a bit crazy that the development of this project is still being considered when not one of the CUP criteria is being met and the project is in direct conflict with one of the stated purposes established for zoning of LI-1. See below:

17.18.140: LIGHT INDUSTRIAL DISTRICT NUMBER 1 (LI-1): A. Purpose. The LI-1 light industrial district number 1 is established as a transition area providing limited commercial service industries, limited retail, small light manufacturing, research and development, and offices related to building, maintenance and construction and which generate little traffic from tourists and the general public.

IMPOSSIBLE AS BOTH A CONVENIENCE STORE AND GAS STATION ARE HIGH TRAFFIC DEPENDENT

17.116.030: CONDITIONAL USE PERMIT CRITERIA: A conditional use permit shall be granted by the Commission only if the applicant demonstrates that:

A. The characteristics of the conditional use will not be unreasonably incompatible with the types of uses permitted in the applicable zoning district;

THE FACT THAT THIS PROPERTY AS DIRECTLY ADJACENT TO 5 OTHER ZONES (LI-2, TOURIST, LIMITED RESIDENTIAL, COMMUNITY CORE, RECREATION USE)

SHOULD BE CONSIDERED AS A GAS STATION IS INCOMPATIBLE WITH THE TYPES OF USES FOR FOUR OF THESE ZONES

B. The conditional use will not materially endanger the health, safety and welfare of the community;

STRONG, DOCUMENTED, UNBIASED AND LENGTHY INFORMATION HAS BEEN SUBMITED TO P&Z TO SUBSTANTIATE THAT THIS PROJECT WILL MATERIALLY ENDANGER THE HEALTH, SAFETY AND WELFARE OF THE COMMUNITY

C. The conditional use is such that pedestrian and vehicular traffic associated with the use will not be hazardous or conflict with existing and anticipated traffic in the neighborhood;

PEDESTRIAN, VEHICULAR TRAFFIC HAS BEEN DOCUMENTED TO BE HAZARDOUS AND WILL CONFLICT WITH EXISTING TRAFFIC AND ANTICIPATED

TRAFFIC AS A RESULT OF THE DEVELOPMENT OF BRACKEN STATION AND DEVELOMENT OF THE PROPERTY ON WARMSPRINGS AND 10TH STREET WILL ONLY FURTHER ADD TO THIS CONDITION NOT BEING MET

D. The conditional use will be supported by adequate public facilities or services and will not adversely affect public services to the surrounding area, or conditions can be established to mitigate adverse impacts

BACKED UP TRAFFIC GOING SOUTH AT NIGHT/NORTH DURING THE DAY, TRAFFIC ON 10TH STREET ARTERY (A 77' TRUCK (DOCUMENTED AS USING THIS STREET ON JUNE 28); LARGE TRUCKS FROM 5 LANDSCAPE COMPANIES IN THE IMMEDIATE NEIGHBORHOOD, SUPPLY TRUCKS TO KNOB HILL INN (DOCUMENTED PARKING FACING THE WRONG WAY IN FRONT OF KNOB HILL INN AND UNLOADING ALONG SIDE CURRENT SITE), ETC. COULD CAUSE POTENTIAL BLOCKAGE SLOWING DOWN EMERGENCY VEHICLES

E. The conditional use is not in conflict with the policies of the comprehensive plan or the basic purposes of this chapter.

IN DIRECT CONFLICT IN MULTIPLE PAGES AS SUBMITTED IN DETAIL BY BARBI REED PRIOR TO P&Z'S MEETING JUNE, 13.

B. (Barbi) Anne Reed ANNE REED GALLERY

barbi@annereedgallery.com 208-841-9200 208-774-0400 (cabin)

PO Box 597 100 Sage Road - A Ketchum, ID 83340 From: Richard D. Klein [mailto:Rklein@ceds.org]

Sent: Tuesday, June 21, 2016 9:38 AM **To:** Andrew Wall < awall@Knobhillinn.com > **Subject:** Ketchum - Other Issues Researched

WELL CONTAMINATION

Attached is a modified Map 2 from the <u>City of Ketchum Comprehensive Plan</u>. The modifications are the addition of labels showing the location of the Bracken Station site and three existing gas stations which appear to be up-groundwater-gradient of three of the Cities six wells. In other words, groundwater may flow from the four locations towards the three wells.

Depending upon the type of proposed stormwater Best Management Practices (BMPs) and activities at the station, a significant potential may exist of increasing the probability of causing contamination of these water supply wells.

The applicant's plans contained in the <u>Conditional Use Permit Staff Report</u> did not show any proposed BMPs. According to Ketchum planner Brittany Skelton (208.726.7801), the City has asked the applicant to submit a drainage report showing what BMPs are needed.

As I'm sure you'll recall, I was concerned that the area shown on the lower level of the proposed site plan would be for vehicle maintenance and repair. Past studies have shown that the stormwater runoff from vehicle maintenance-repair facilities contains unusually high levels of pollutants. Ms. Skelton told me yesterday that no maintenance-repair services are proposed.

However, I believe a well contamination concern still exists.

Research conducted by <u>Johns Hopkins University faculty</u> has shown that significant quantities of gasoline spilled onto the concrete pads at gas stations leaks through the concrete to potentially contaminate groundwater.

Again, the attached modified Map 2 shows the location of the Bracken Station site and three existing gas stations. All three existing stations are arguably up-groundwater-gradient of three of the City's six wells. Approving the Bracken Station project would add a fourth station, which elevates the probability of causing contamination of half the wells providing water to the people of Ketchum. It is unlikely the applicant could propose conditions that would resolve this threat.

This threat conflicts with the following Section 17.116.030: Conditional Use Permit Criteria, of the City of Ketchum Zoning Regulations which requires that:

"A conditional use permit shall be granted by the commission only if the applicant demonstrates that:

B. The conditional use will not materially endanger the health, safety and welfare of the community;" [Emphasis added]

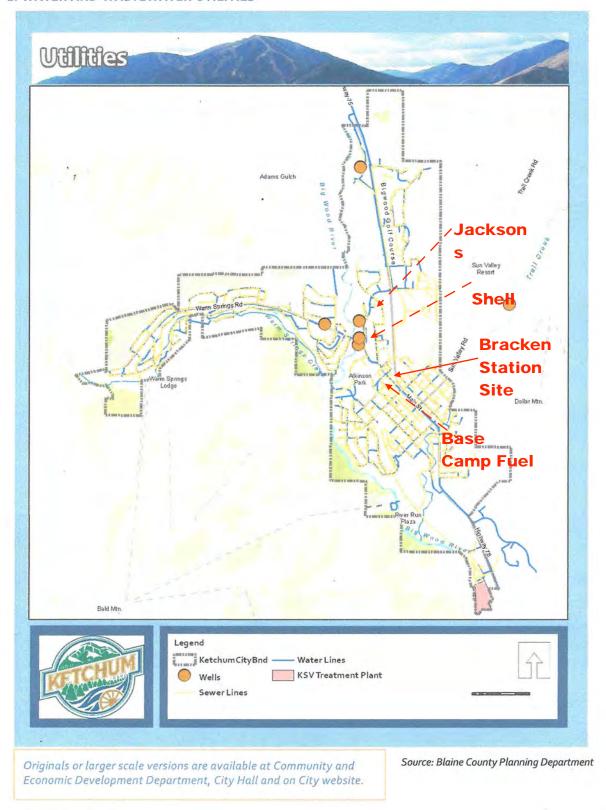
Posing a potential threat of increasing contamination of the City's wells would certainly materially endanger the community's health.

Richard D. Klein Community & Environmental Defense Services 21300 Heathcote Road Freeland, Maryland 21053 410-654-3021

Main Website: ceds.org

CEDS News Service: cedsnews.com

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Science Daily

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From: Richard D. Klein [mailto:Rklein@ceds.org]

Sent: Tuesday, June 21, 2016 8:21 AM **To:** Andrew Wall awall@Knobhillinn.com>

Subject: Ketchum - Health Effects Research Results

Andrew

Following is the second of two messages detailing the results of my research into issues regarding the Bracken Station project.

This research shows that the proposed gas station poses an excessive threat to the health of those attending Ernest Hemingway Elementary School and the Wood Valley Community YMCA. Othersensitive-receptors in the area may be at risk as well.

The health threat is posed by benzene and other pollutants released to the atmosphere while gasoline is being dispensed. As explained below, USEPA guidance calls for assessing the potential effects of this exposure for any *sensitive-receptor* within 1,000 feet of a proposed gas station. The attached aerial photo shows that at least two such receptors are present within 1,000 feet of the Bracken Station site – the school and the YMCA. The aerial photo also shows that two existing gas stations are within 1,000 feet. Adding a third station would greatly increase the potential health impact. The basis for this analysis is presented in the remainder of this message.

In the Executive Summary of the <u>Air Quality and Land Use Handbook: A Community Health Perspective</u>, the California Air Resources Board (ARB) states:

"Also, ARB community health risk assessments and regulatory programs have produced important air quality information about certain types of facilities that should be considered when siting new residences, schools, day care centers, playgrounds, and medical facilities (i.e., sensitive land uses). Sensitive land uses deserve special attention because children, pregnant women, the elderly, and those with existing health problems are especially vulnerable to the non-cancer effects of air pollution. There is also substantial evidence that children are more sensitive to cancer-causing chemicals."

The following text appears on pages 30-31, of the ARB Handbook:

"Refueling at gasoline dispensing facilities releases benzene into the air. Benzene is a potent carcinogen and is one of the highest risk air pollutants regulated by ARB. Motor vehicles and motor vehicle-related activity account for over 90 percent of benzene emissions in California. While gasoline-dispensing facilities account for a small part of total benzene emissions, near source exposures for large facilities can be significant.

Since 1990, benzene in the air has been reduced by over 75 percent statewide, primarily due to the implementation of emissions controls on motor vehicle vapor recovery equipment at gas stations, and a reduction in benzene levels in gasoline. However, benzene levels are still significant. In urban areas, average benzene exposure is equivalent to about 50 in one million.

Gasoline dispensing facilities tend to be located in areas close to residential and shopping areas.

Benzene emissions from the largest gas stations may result in near source health risk beyond the regional background and district health risk thresholds. The emergence of very high gasoline throughput

at large retail or wholesale outlets makes this a concern as these types of outlets are projected to account for an increasing market share in the next few years."

By high gasoline throughput, ARB is referring to stations that dispense 3 million gallons per year or more. The attached need analysis shows that on average Idaho gas stations dispense about 1.5 million gallons of gasoline per year. The attached aerial photo shows that there are two existing gas stations in the immediate vicinity of the Bracken Station site. This could raise the gasoline throughput to 4.5 million gallons per year within this rather small area.

The ARB Handbook (page 2) recommended against siting new gas stations in the vicinity of locations where sensitive individuals are present for extended periods:

"Sensitive individuals refer to those segments of the population most susceptible to poor air quality (i.e., children, the elderly, and those with pre-existing serious health problems affected by air quality). Land uses where sensitive individuals are most likely to spend time include schools and schoolyards, parks and playgrounds, daycare centers, nursing homes, hospitals, and residential communities (sensitive sites or sensitive land uses)."

In <u>School Siting Guidelines</u> (page 59) the U.S. Environmental Protection Agency recommends screening gas stations proposed for sites within 1,000 feet of a school for potential health effects due to pollutants released to the atmosphere.

As shown in the attached aerial photo, there are at least two sensitive land uses within 1,000 feet of the Bracken Station site:

- ******Ernest Hemingway Elementary School; and
- ··········Wood River Community YMCA.

Both are also within 1,000 feet of two existing stations – the Shell and Base Camp Fuel. Adding a third – Bracken Station – would significantly elevate the health threat to the children attending the elementary school and the young-elderly who frequent the YMCA as well as those living in the area.

Section 17.116.030: Conditional Use Permit Criteria, of the City of Ketchum Zoning Regulations requires that:

"A conditional use permit shall be granted by the commission only if the applicant demonstrates that:

B. The conditional use will not materially endanger the health, safety and welfare of the community;" [Emphasis added]

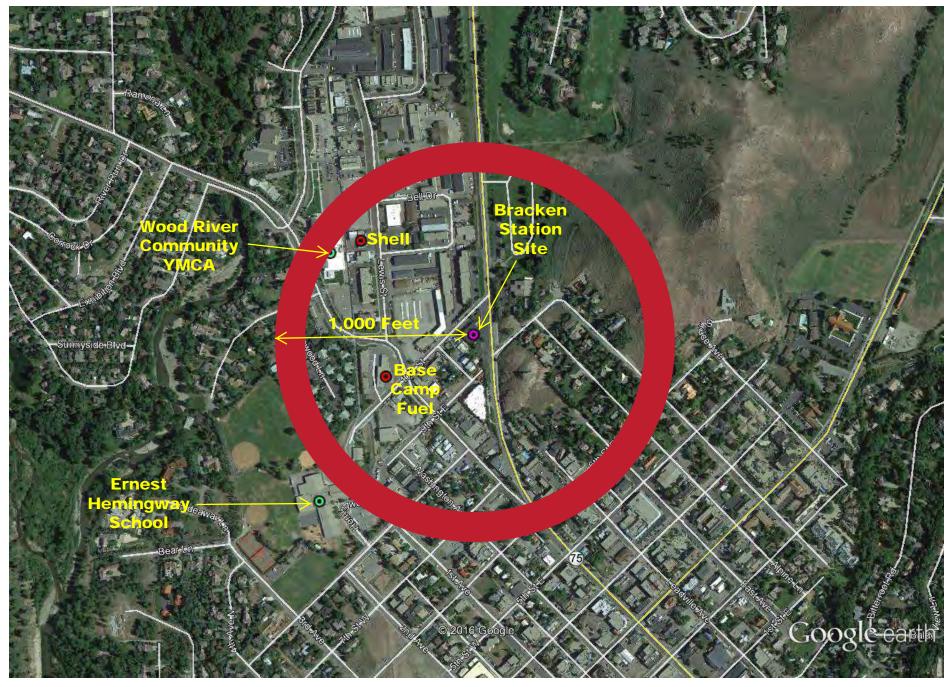
This text indicates the Commission can and should consider the health effects of a gas station, then deny the Conditional Use Permit since even if Bracken Station complies with all current air pollution control requirements it will still elevate the health risk to an unacceptable level. There are no conditions that could accompany approval which would resolve the risk.

Richard D. Klein Community & Environmental Defense Services 21300 Heathcote Road Freeland, Maryland 21053 410-654-3021

Main Website: ceds.org

CEDS News Service: <u>cedsnews.com</u>

"



Google earth feet km

RECEIVED

APR 29 2016

CITY OF KETCHUM

PP 1 . . .

CITY OF KETCHUM CONDITIONAL USE PERMIT APPLICATION

	No.: 16-634
File	No.: 10 009
ION	16-035 Review

Name of Applicant: NOULC
Name of Owner of Record: NORTH TOWN PARTNERS LLC
Mailing Address: P.O.BOY 5277 KETCHUM, ID. 83340
Contact Phone Number: 208.721.0080
Street Address of Property Requiring a CUP: 911 N MAIN STREET
Legal Description of Property Requiring a CUP: FETCHUM AMENDED LOT GA BLK-30
RPK0000030005A
Description of Proposed Conditional Use: MOTOR VEHICLE FUELING STATION
PROPOSED: FECESSED SOFFIT DN Description of Proposed and Existing Exterior Lighting: <u>UGHTS IN EMSTING ROOF</u> OVERHANG/NEW LED, FLUSH MOUNT 16" X 16" SQ FIXTURES IN CANOP T- 6 TO 8 IN NUMBER-SOFT WARM COLOR AS PER (KELVIN TEMP.)
Zoning District: LT. Overlay District: Flood Avalanche Pedestrian Mountain
The Applicant agrees in the event of a dispute concerning the interpretation or enforcement of the Conditional Use Permit Application in which the City of Ketchum is the prevailing party to pay reasonable attorney fees, including attorney fees on appeal, and expenses of the City of Ketchum. I hereby acknowledge I have filled in this application accurately and provided the required information to the best of by knowledge. Date

Pursuant to Resolution No. 08-123, any direct costs incurred by the City of Ketchum to review this application will be the responsibility of the applicant. Costs include but are not limited to: engineer review, attorney review, legal noticing, and copying costs associated with the application. The City will require a retainer to be paid by the applicant at the time of application submittal to cover said costs. Following a decision or other closure of an application, the applicant will either be reimbursed for unexpended funds or billed for additional costs incurred by the City.

From: Richard D. Klein [mailto:Rklein@ceds.org]

Sent: Tuesday, June 21, 2016 7:15 AM

To: Andrew Wall <a wall@Knobhillinn.com

Subject: Ketchum - Need Research Results

Andrew

Following is the first of several messages detailing the results of my research into issues regarding the Bracken Station project. The attached analysis shows that the Ketchum area is already very oversupplied with existing gas stations. The basis for this analysis is presented in the remainder of this message.

Due to increasing miles per gallon and the trend towards more fuel pumps per new station, the need for gas stations has been declining in the U.S. as well as Idaho.

The attached Census Bureau data shows that in 2008 there were 114,144 gas stations in the U.S. which declined by 2.2% to 111,583 by 2014. In Idaho the same trend has occurred. In 2008 there were 674 gas stations in Idaho then 666 (1.2% less) by 2014.

Census Bureau data also shows that in 2014, Idaho had a population of 1,634,806 (see attached pdf). With 666 gas stations statewide, this data shows that it takes about 2500 residents to support one gas station. These residents must be present within a realistic market area for a proposed station.

The outer limits of a gas station market area is usually about 1.5 miles. The attached analysis shows that there are 5,826 residents within 1.5 miles of the Bracken Station site. This would be sufficient to support two gas stations. However, the attached map shows there are presently five stations within 1.5 miles of the Bracken Station site. Therefore the Ketchum area is already very over-supplied with existing gas stations.

The following text from page 2 of the <u>Conditional Use Permit Staff Report</u> provides a basis for the Planning & Zoning Commission to deny approval based on the need analysis:

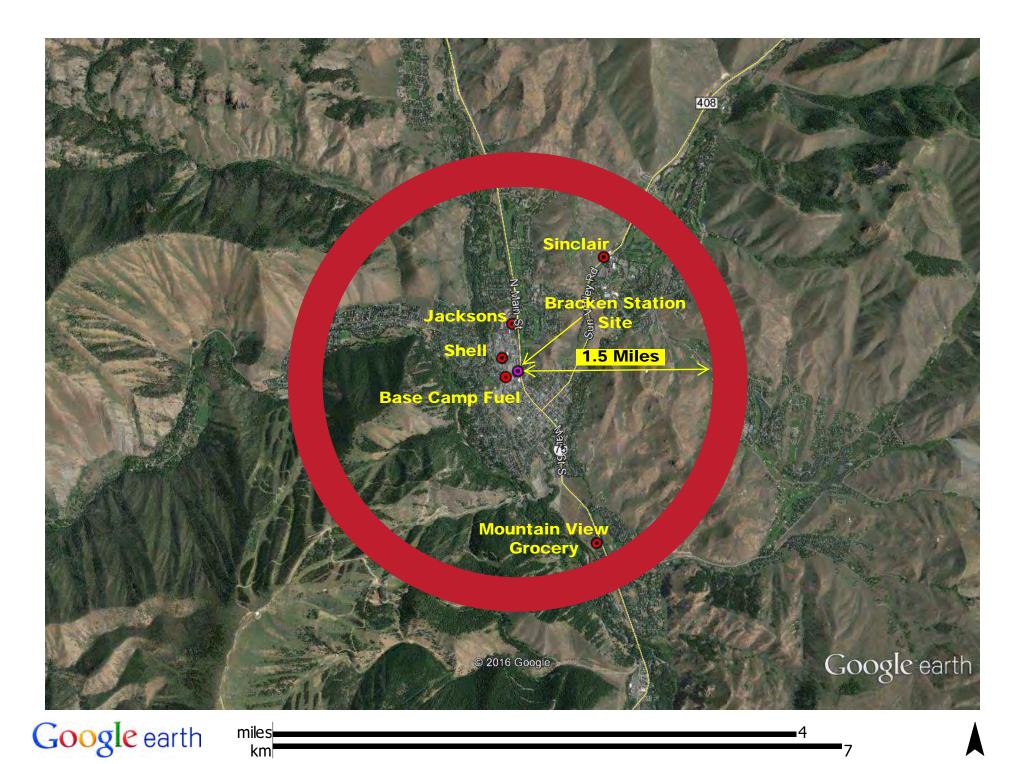
Currently there are three <u>fueling</u> stations in the LI District, two restaurants, and one food mart to service the area. The Commission must decide if the proposed uses are appropriate for the site and location and if the uses are <u>necessary</u> to serve the LI district. [Emphasis added]

This text indicates the Commission can and should consider the need for an additional gas station and/or convenience store.

Richard D. Klein Community & Environmental Defense Services 21300 Heathcote Road Freeland, Maryland 21053 410-654-3021

Main Website: ceds.org

CEDS News Service: <u>cedsnews.com</u>



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NEW MEXICO

KLAHOMA ARKANSAS

ENNESSEE CAROLINA CAROLINA

PEPANNRES

Annual Estimates of the Resident Population: April 1, 2010 to July 1, 2015

2015 Population Estimates

Geography	April 1, 2010		Population Estimate (as of July 1)			
	Census	Estimates Base	2010	2011	2012	2013
ldaho	1,567,582	1,567,652	1,570,986	1,584,134	1,596,097	1,612,785

1 of 2 06/13/2016

Geography	Population Estimate (as of July 1)		
	2014	2015	
Idaho	1,634,806	1,654,930	

Notes:

The estimates are based on the 2010 Census and reflect changes to the April 1, 2010 population due to the Count Question Resolution program and geographic program revisions. See Geographic Terms and Definitions at http://www.census.gov/popest/about/geo/terms.html for a list of the states that are included in each region and division. All geographic boundaries for the 2015 population estimates series except statistical area delineations are as of January 1, 2015. The Office of Management and Budget's statistical area delineations for metropolitan, micropolitan, and combined statistical areas, as well as metropolitan divisions, are those issued by that agency in February 2013 http://www.whitehouse.gov/sites/default/files/omb/bulletins/2013/b13-01.pdf . An "(X)" in the 2010 Census field indicates a locality that was formed or incorporated after the 2010 Census. Additional information on these localities can be found in the Geographic Boundary Change Notes (see http://www.census.gov/geo/reference/boundary-changes.html). For population estimates methodology statements, see http://www.census.gov/popest/methodology/index.html .

The 6,222 people in Bedford city, Virginia, which was an independent city as of the 2010 Census, are not included in the April 1, 2010 Census enumerated population presented in the county estimates. In July 2013, the legal status of Bedford changed from a city to a town and it became dependent within (or part of) Bedford County, Virginia. This population of Bedford town is now included in the April 1, 2010 estimates base and all July 1 estimates for Bedford County. Because it is no longer an independent city, Bedford town is not listed in this table. As a result, the sum of the April 1, 2010 census values for Virginia counties and independent cities does not equal the 2010 Census count for Virginia, and the sum of April 1, 2010 census values for all counties and independent cities in the United States does not equal the 2010 Census count for the United States. Substantial geographic changes to counties can be found on the Census Bureau website at http://www.census.gov/geo/reference/county-changes.html.

Suggested Citation:

Annual Estimates of the Resident Population: April 1, 2010 to July 1, 2015

Source: U.S. Census Bureau, Population Division

Release Dates: For the United States, regions, divisions, states, and Puerto Rico Commonwealth, December 2015. For counties, municipios, metropolitan statistical areas, micropolitan statistical areas, metropolitan divisions, and combined statistical areas, March 2016. For Cities and Towns (Incorporated Places and Minor Civil Divisions), May 2016.

2 of 2 06/13/2016



CB0800A1

2008 County Business Patterns: Geography Area Series: County Business Patterns

2008 Business Patterns

Table Name

Geography Area Series: County Business Patterns: 2008

Release Date/Status

6/30/11 - Complete

Key Table Information

Beginning with reference year 2007, CBP data are released using the Noise disclosure methodology to protect confidentiality. See Survey Methodology (http://www.census.gov/econ/cbp/methodology.htm) for complete information on the coverage and methodology of the County Business Patterns data series.

Universe

The universe of this file is all operating establishments with one or more paid employees. This universe includes most establishments classified in the North American Industry Classification System (NAICS) Codes 11 through 813990. For specific exclusions and inclusions, see Industry Classification of Establishments.

Geography Coverage

The data are shown at the U.S. level and by State, County, and Metropolitan and Micropolitan Statistical Areas. Also available are data for the District of Columbia, Puerto Rico, and the Island Areas (American Samoa, Guam, the Commonwealth of the Northern Mariana Islands, and the U.S. Virgin Islands) at the state and county equivalent levels.

Industry Coverage

The data are shown at the 2- through 6-digit NAICS code levels for all sectors with published data.

Data Items and Other Identifying Records

This file contains data on the number of establishments, total employment, first quarter payroll and annual payroll.

Sort Order

Data are presented in ascending geography by NAICS code sequence.

FTP Download

Download the entire table at http://www2.census.gov/econ2008/CB/sector00/CB0800A1.zip (Approx. 500 MB).

Contact Information

U.S. Census Bureau

Economic Planning & Coordination Division

Register Analysis Branch

Tel: (301)763-2580

Email: epcd.county.business.patterns@census.gov

NOTE: Data based on the 2008 County Business Patterns.

CBP html tables and download files can be found at the County Business Patterns Website. For information on confidentiality protection, sampling error, nonsampling error, and definitions, see Survey Methodology. Data in this table represent those available when this report was created; data may not be available for all NAICS industries or geographies. Excludes most government employees, railroad employees, and self-employed persons.

Geographic area name	2007 NAICS code	Meaning of 2007 NAICS code	Year	Number of establishments	Paid employees for pay period including March 12 (number)	Noise range for paid employees for pay period including March 12 (%)	First-quarter payroll (\$1,000)	Noise range for first-quarter payroll (%)
United States		Gasoline stations	2008	114,144	896,590	G	3,678,691	G
United States		Gasoline stations with convenience stores	2008	95,093	725,298	G	2,825,398	G
Idaho	4471	Gasoline stations	2008	674	6,387	G	23,401	G
Idaho	447110	Gasoline stations with convenience stores	2008	574	5,033	G	17,028	G

1 of 2 06/21/2016

Geographic area name	2007 NAICS code	Meaning of 2007 NAICS code	Year	Annual payroll (\$1,000)	Noise range for annual payroll (%)
United States	4471	Gasoline stations	2008	15,313,367	G
United States		Gasoline stations with convenience stores	2008	11,801,425	G
Idaho	4471	Gasoline stations	2008	99,536	G
Idaho		Gasoline stations with convenience stores	2008	72,935	G

G Low noise infusion

2 of 2 06/21/2016



CB1400A11

Geography Area Series: County Business Patterns

2014 Business Patterns

Table Name

Geography Area Series: County Business Patterns: 2014

Release Schedule

The data in this file were released on April 21, 2016.

Key Table Information

Beginning with reference year 2007, CBP data are released using the Noise disclosure methodology to protect confidentiality. See Survey Methodology (http://www.census.gov/econ/cbp/methodology.htm) for complete information on the coverage and methodology of the County Business Patterns data series.

Universe

The universe of this file is all operating establishments with one or more paid employees. This universe includes most establishments classified in the North American Industry Classification System (NAICS) Codes 11 through 813990. For specific exclusions and inclusions, see Industry Classification of Establishments.

Geography Coverage

The data are shown at the U.S. level and by State, County, Metropolitan and Micropolitan Statistical Areas, and Congressional District. Also available are data for the District of Columbia, Puerto Rico, and the Island Areas (American Samoa, Guam, the Commonwealth of the Northern Mariana Islands, and the U.S. Virgin Islands) at the state and county equivalent levels.

Industry Coverage

The data are shown at the 2- through 6-digit NAICS code levels for all sectors with published data.

Data Items and Other Identifying Records

This file contains data on the number of establishments, total employment, first quarter payroll and annual payroll.

Sort Order

Data are presented in ascending geography by NAICS code sequence.

FTP Download

Download the entire table at http://www2.census.gov/econ2014/CB/sector00/CB1400A11.zip.

Contact Information

U.S. Census Bureau

Economy-Wide Statistics Division

Enterprise Statistics Branch

Tel: (301)763-2580

Email: ewd.county.business.patterns@census.gov

Release Date : 04/21/2016

NOTE: Data based on the 2014 County Business Patterns. CBP html tables and download files can be found at the County Business Patterns Website.

For information on confidentiality protection, sampling error, nonsampling error, and definitions, see Survey Methodology.

Data in this table represent those available when this report was created; data may not be available for all NAICS industries or geographies. Excludes most government employees, railroad employees, and self-employed persons.

Geographic area name	2012 NAICS code	Meaning of 2012 NAICS code	Year	Number of establishments	Paid employees for pay period including March 12 (number)	First-quarter payroll (\$1,000)	Annual payroll (\$1,000)
United States	4471	Gasoline stations	2014	111,583	904,084	4,043,091	17,274,524
United States	447110	Gasoline stations with convenience stores	2014	96,473	756,076	3,237,130	13,816,749
Idaho	4471	Gasoline stations	2014	666	6,562	28,611	123,115
Idaho	447110	Gasoline stations with convenience stores	2014	585	5,428	22,317	96,222

Source: U.S. Census Bureau, 2014 County Business Patterns.

1 of 1 06/21/2016

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From: Will Niedrich [mailto:woodriverlock@gmail.com]

Sent: Saturday, June 25, 2016 11:59 AM

To: Participate < participate@ketchumidaho.org >

Subject: 10th&Main Gas Station Proposal

To whom it may concern

My husband and I own our business Wood River Lock which is on the property that is proposed for a gas station.

Our Opinion:

- 1.) Traffic will affect local residences and can become very dangerous for big trucks on 10th street during the winter.
- 2.) There is already 3 gas stations almost visible from each other.
- 3.) Small restaurant will not bring profit after 5pm when most working people leave town going south.
- 4.) Burning lights all night will be disturbing and illegal for night say rule.
- 5.) Image of Ketchum will be not the same anymore, looking to the north.
- 6.) North Fork can use a gas station it is close to SNRA where most tourists stop often.

Sincerely

Yelena Chestnov

&

William Niedrich

From: Gary Lipton External

Sent: Sunday, June 26, 2016 3:26 PM

To: Participate < participate@ketchumidaho.org >

Subject: Proposed Bracken Gas Station. Please Distruibute to P & Z and City Council.

Participate please distribute to all P & Z members and the members of the City Council. This is my 3rd request and appearance.

Lipton LLC # 1. owns the direct adjacent property to the north side of the proposed Bracken Gas Station complex, Approx. 50 +- feet away. This property is full of windows facing the proposed complex from my property at the 10th street complex. Please review in detail the lighting issue this proposed gas station presents. When the proposed existing buildings # A & B displayed on the Steve Cook blueprints for the proposed Bracken Gas station are demolished as proposed is the time when the CUP will be in violation of parts of the Dark Sky Codes. A LIGHTING PROBLEM FOR MY ADJACENT PROPERTY, without question. I need the P & Z to come to a workable, agreeable solution within the codes regarding my situation., before you issue a CUP.

The Bracken Gas station property slopes down from where the proposed new gas pumps are to be located to the edge of my property at the 10th street complex 20 ft +- where my space windows are located. Then when you add the height of the proposed canopy of appox. 19+- you have a 39ft +- height difference. All the light from underneath the canopy and vehicle headlights will in many ways without question

will shine into my windows. Not acceptable. Now allow, review and please show me how the Proposed Bracken Gas station lighting plan can accommodate my windows, so I wont be affected.

You will have cars, trucks, campers and all other associated vehicles shining direct and or indirect head lights at my location at 10th street, while pumping gas, putting oil in there cars, air in tires, and or parking at this Gas station. Additionally you will have unquestionably the under lighting of the canopy and the Gas Station Advertising signage shining into my location, regardless of the shielded lighting fixtures that code allows.

Please look, review,

dissect the Sterling Codifiers regarding City of Ketchum, Idaho Chapter 17.132 "Dark Skies" and other related codes and sub codes regarding same. Look at B.1. States. "to protect against direct glare and excessive lighting." B.2. States; "only if the light does not cause glare or light to shine on adjacent property." Also review sub section K. "Canopy Lights". States. "All lighting shall be recessed sufficiently so as to ensure that NO light source is visible from or causes glare on public right of way or ADJACENT PROPERTY". This is quite clear. TO the P.& Z. please stay true to the code as cited above, unless you fine me incorrect. No compromise, analyze as the code dictates. Abide by the code, you have a large community audience following this process, its not that difficult to do. I suggest you do not issue a CUP on the basis that this issue is not going to disappear, I am not going anywhere, lets work it out. Please dont adjust the code to help out a fellow P. & Z. commissioner either. The end result will be an appeal in front of the KETCHUM CITY COUNCIL, which will allow the public to fully participate and who knows how it will end up then. I look forward to your unbiased public deliberations. Thank You Gary Lipton Member Lipton LLC 1.

From: <u>Leo Brieske</u>
To: <u>Brittany Skelton</u>

Subject: Fw: Brecken Station Questions

Date: Monday, June 27, 2016 4:20:11 PM

---- Forwarded Message -----

From: Leo Brieske <leobrieske@yahoo.com>

To: "bskelto@ketchumidaho.org" <bskelto@ketchumidaho.org>

Sent: Monday, June 27, 2016 4:14 PM **Subject:** Brecken Station Questions

From: Leo Brieske @yahoo.com

920 Leadville Ave.(LOT #2) 208 726 1030

Ketchum, ID 83340

Please enter into the record on July 11 2016 P&Z meeting:

- 1) The PROPOSED WEST ELEVATION-ALLEY VIEW drawings show WALL MOUNTED LIGHTING on the PATIO. Due to the LOT ELEVATION of the PATIO----any architectural lighting proposal will ILLUMINATE both my back yard and house and invading my privacy.
- 2) The drawings show NO LANDSCAPING within/on the west side of the patio. or within the alley view.
- a) Is it possible to INCORPORATE the EXISTING Evergreen & Aspen trees within the Patio construction versus removal?
- b)IF NOT---could trees be placed within the new patio area or in the alley way? This may alleviate the lighting and privacy issues.
- c) Can the patio railing be raised or fenced to reduce the possibe intrusions of noise/light and privacy?
- 3) Will the Restaurant/Patio conform to LI-1 closing hours of 9PM?
- 4) Is there a building setback ordinance for the rear of properties in the LI-1?? IF there are setback rules, the new construction addition/patio seem to be non conforming!!!!
- 5) If I/neighbors decide to sell property after the gas station construction, will it then be required to have an EPA STUDY for bank financing?
- a) Does the develope rassume the cost of the study??

Your attention to these questions/considerations is appreciated.

Leo Brieske

J. KEVIN LAWLER

July7, 2016

Planning and Zoning Commission
Department of Planning and Building
City of Ketchum
PO Box 2315
Ketchum, ID 83340

Via email to:participate@ketchumidaho.org

RE: Bracken Station – CUP Continuation Hearing

To Staff and Planning Commissioners:

I am again writing to strongly object to the proposed Bracken Station project at 911 N. Main. At the time of my previous correspondence I did not have the benefit of examining the applicant's submission for two Conditional Use Permits ("CUP"), or reviewing the June 13th Staff Report or observing P&Z hearing on June 14th on this matter.

My objections to the Bracken Station CUP application are as follows:

- 1. The applicant's submission for a CUP appended to the Staff Report is materially deficient. The plain text of the Code, Section 17.116.030 requires: "A conditional use permit shall be granted by the commission only if the applicant demonstrates". There is nothing in the record indicating the applicant has attempted to address nor can they satisfy all of the 5 criteria for either of proposed Conditional Uses for the proposed Bracken Station project.
- 2. The Staff Report (File # 16-34) is incomplete and thus has a bias in favor of the Bracken Station's CUP application. Appendices B and C of the Staff Report conspicuously fails to consider the defined purpose of the LI-1 zone, Section 17.18.140: "Purpose. The LI-1 light industrial district number 1 is established as a transition area providing limited commercial service industries, limited retail, small light manufacturing, research and development, and offices related to building, maintenance and construction and which generate little traffic from tourists and the general public.
- 3. Competent verbal and written testimony has been provided, and thus far largely ignored that there is not a "necessity" for an additional gas station in the LI-1 District or the City of Ketchum, in general. A representative of Base Station testified at the Commission's June 13th meeting that the

volumes of fuel sales in Ketchum are anemic compared to Hailey. The Staff Report clearly indicates there are 3 existing gas stations serving the LI-1 District. A report prepared by Richard Klein, Community Environmental Defense Services ('CEDS') documents the LI-1 District is currently over-supplied with gas stations.

- 4. Neither Planning Department staff nor the Commission has yet to fully evaluate the proposed Bracken Station project relative to the standard of "health, safety and general welfare". Specially not yet evaluated are:
 - ✓ The potential negative environmental impact on proximate residents and businesses from toxic fueling fumes and elevated exhaust associated with increased traffic generation attributable to the Bracken Station project. Reference: *Journal of Environmental Management*, 2010; 91 (12): 2754 DOI: 10.1016/j.jenvman.2010.08.009 and Health Effects, Community & Environmental Defense Services, CEDS.org.
 - ✓ The potential decrease in residential property values proximate to the Bracken Station project, and the corresponding difficulty and increased costs owners will incur in selling, refinancing and insuring their residences Reference: HUD Handbook 4000.1 II.B.3.c. iii.(C)(7).

As an owner in Frenchman's Place for nearly a decade, I am deeply concerned the proposed Bracken Station project, if approved via Conditional Use Permits, will substantially diminish the value of my residence and irreparably damage the lifestyle I have enjoyed residing in the City of Ketchum.

Respectfully,

J. Kevin Lawler

May 27, 2016

Kathleen Nichols and Douglas Holen

P.O. Box 14001

Ketchum, ID 83340

City of Ketchum

Planning and Building

P.O. Box 2315

Ketchum, ID 83340

Re: Proposed Bracken Station



We are permanent residents of Ketchum, having purchased a condominium in the building known as Frenchman's located less than a block south of the proposed Bracken Station. When our building was in the planning stages, it was originally conceived as a commercial use. Ketchum asked our developers to include residential units as well. We, along with many others, purchased a unit and are happy to be living here. However, the addition of a new gas station, located within a block of an existing one, would definitely diminish our investment here.

Half a block north of us, on Warm Springs Road, is the Base Camp Gas Station and Convenience store. This new proposed station would be sited less than a block from the existing Base Camp station, which seems absurd. Less than a half mile away, on Lewis, there are two further gas stations. Does this area of a small town really need four gas stations within a half mile?

We strongly urge the City of Ketchum to turn down this application as not necessary, and as having a negative impact on relatively new residential housing that the City of Ketchum wanted to have here.

Thank you for your consideration.

Sincerely,

Kathleen Nichols and Douglas Holen



From: Jacobs, Edward R [mailto:Edward R Jacobs@rl.gov]

Sent: Wednesday, June 01, 2016 3:53 PM

To: Participate < participate@ketchumidaho.org >

Subject: Opposition to Proposed Gas Station at 911 N. Main

Importance: High

I am writing to express my opposition to the gas station, convenience store and food service proposed at 911 North Main Street in Ketchum. I own a residence at Frenchman's Condominiums next to the proposed site and I believe that it is not in best interests of the neighborhood. It will create increased congestion and side traffic off the highway. It will also have a negative impact on residential property values adjacent to the proposed site. Furthermore, there is already an existing gas station and convenience store at the other end of 10th Street and Warm Springs Road.

An additional gas station and convenience store are not needed so close.

Edward Jacobs 360 East 9th Street, #206 (Frenchman's Condominiums) Ketchum, Idaho COMMENT from Barbi Reed re. Bracken Station

From: Barbi Reed

Initial Comments re proposed Bracken Station, 911 North Main Street, Ketchum, Idaho (AM Lot 5A, Block 30, Ketchum Townsite)

Acknowledging that this is a complicated project and as such I'm requesting that P&Z table delay any decision regarding this proposal for a future meeting to allow further examination and study of the UNINTENDED CONSEQUENCES and domino effect of this proposal.

This proposed project is also particularly challenging one for P&Z as it is in actuality 3 projects wrapped into one and should be examined and considered separately as three separate businesses

- a.) Gas station
- b.) Convenience Store
- c.) Food Service Establishment

The project's complexity is further complicated as it involves a gas station which inherently has its own unique set of consequences and impacts.

As the proposed project and its 3 separate entities RELY on high traffic flow, the project on this site needs to be analyzed in light of the consequences of moving vehicles (cars, RVs, horse trailers, snowmobile trailers, big rigs, campers, etc.) and bicyclists and pedestrians in and out as well as "by".

Additionally, the proposed project is possibly at the WORST possible Light Industrial location in the entire city regarding SAFETY and the consequences of increased traffic.

a.) Additional vehicles will be a disaster to the already dangerously congested 10th Street

- b.) East Street and adjoining Walnut and Alpine will see drastically increased traffic (none of these streets is designed to be a thoroughfare)
- c.) crossing AGAINST oncoming traffic will occur at several points and will occur on regular basis:
- I. northbound vehicles crossing south bound traffic to enter proposed project
- II. northbound vehicles crossing south bound traffic to continue north from proposed project
 - III. Southbound vehicles crossing against back-up of northbound traffic to enter East 9th Street

This project is further complicated because the site lies at the confluence and junction points of residential, city core, light industrial and includes housing, condominium, small businesses and a high-end restaurant and hotel close enough to mandate that owners receive a notice of the meeting of Consideration by KetchumP&Z.

Needing further study and analysis:

Gas Station

- 1. Of paramount importance: SAFETY! Is this project suitable at this location?
- a.) how will it affect traffic (vehicular, pedestrian and bicyclists)
- b) what are the health consequences for those in nearby residences
 - 2. Can this project truly:
- a.) does it meet and satisfy the vision and policy of Ketchum's 2104 Comprehensive plan (see Attachment 1)
- b.) will it NOT interfere with the right for quality of life for nearby residents (this includes but not limited to fumes, refilling and restocking in the middle of the night with accompanying loud *BEEP BEEP*s of trucks backing up
 - c.) will it NOT lower property values

- d) will it NOT infringe or encroach on the financial success of existing businesses
 - e.) can it satisfy existing dark sky ordinances
- f.) will it fully operate within the hours now mandated in LI area and to which other businesses in LI are now being held
 - g.) can it meet environmental mandates
 - h.) how will it satisfy noise and light requirements at night when restocking or filling
 - i.) how will it answer to known consequences of gas station/convenience stores: e.g. crime, loitering, litter, fires
 - h.) will it provide answers to community objections

Convenience Store and Fast Food Service: will they be

- 1. Attractive nuisances (meaning will P&Z and others who approve this project have "blood on their hands" if there is an accident as pedestrians cross street, children access store via 10th Street (which is right now and in the foreseeable future a MESS and unsafe for pedestrians, esp. children on foot or bikes. I personally know a number of parents who even now will NOT let their children walk on 10th Street.
- 2. Attractive nuisances (meaning will P&Z and others who approve this project have "blood on their hands" if there is an accident bus riders cross Main Street...Note: light will add other consequences and it's proposed location does not deal with the necessity to cross East
- 3. Will the projected sidewalk attract walkers to enter the Boulevard entry endangering pedestrians as vehicles exit and leave

All of this and more in the pages that follow.

I've been astounded by and overwhelmed by the amount of information available re the impacts of gas station/convenience stores. This business paradigm is not without a great deal of scientific research and study.

Bottom line: this project at this location REQUIRES further considered study. Meeting the guideline of whether it is allowed in LI is understandably the beginning, but I propose that a through and complete understanding of the ramifications of this project should be studied. The immediate neighbors, the immediate businesses, the community as a whole deserve this!

Several of us have commissioned a non-profit land use management organization equipped better than we (or respectfully than P&Z or the City Council) to consider this project in its entirety, based on location only. We expect them to give an honest evaluation and analysis which we hope to have to present to P&Z within the next several weeks.

Personally my concern is always the big and the lasting picture. As such I am asking P&Z to hold off its decision for the forthcoming professional analysis and a more thorough consideration and inquiry into UNINTENDED CONSEQUENCES of this project.

Gas Stations/Convenience Stores are a way of life in the world today, no matter what their impact is. The real question is not whether this project is desirable, needed or even "allowed" in Ketchum's LI, but rather the questions is: "Is this project appropriate for this site and should it be allowed for SAFETY issues alone?"

Therefore, more importantly, a thorough traffic analysis of Main Street, 10th Street, East 9th Street (Walnut and Alpine) is required. Expert analysis of the lasting impact of this project at this site should happen

Further time for consideration of studies and additional research is the only response from P&Z that is justifiable. Other points to consider re. consequences of increased traffic to this destination

- a.) Knob Hill Inn entry and exit will be affected
- b.) Entry and exit from Cemetery (used by tourists to visit Hemingway's grave as well as those visiting loved ones) could be affected
- c.) 10th Street/Main Street intersection will see substantial increased in traffic (in addition, how will this be affected after the former Anderson Lumber is developed!)
- d.) Both 10th Street and East 9th Street are inherently dangerous in winter. What will happen with increased traffic when it's been demonstrated that 4-wheel drive is essential to go West to East on 10th Street. If there were a light at the intersection of Main Street and 10th Street, what happens to traffic impacted by vehicle stuck on the slope.
- e.) The extra wide radius turn at 10th Street/Main Street already presents problems for both vehicles entering 10th and exiting 10th turning left and right...what happens with increased congestion and traffic?
- f.) The pedestrian crosswalks indicated on plan have major faults and need to be given further study for safety reasons
- g.) The proposed turning lane needs to be studied as it does not seem to solve the back-up traffic problem and will cause confusion. It simply looks too tight to be safe.
- h.) The proposed crosswalk to the south of the project does not take into consideration that pedestrians coming from the north have to cross East 9th Street.
- Flashing lights as indicated on plan will not solve the problem or provide safety
- j.) Boulevard Approach needs to be studied...cars egressing and entering will not go as the arrows are marked, width of Boulevard approach is dangerous for pedestrians whether simply walking along sidewalk or wanting to enter Station
- k.) How will increased traffic and turning cross traffic affect bicycle traffic

- 1.) How will increased traffic and backed up north bound traffic affect residents and visitors to Frenchmen's
- m.) Will vehicles turning into the 10th Street Light Industrial to access businesses there be affected?
- n.) What is proposed re. navigating within the site: to the pumps, away from the pumps? The right pumps for gas tanks and those wishing to access free pumps will create variation in vehicular traffic

Barbi Reed, Ketchum resident

Raised in a small town

BA St. Lawrence University (ski team) and University of Grenoble, France Post-graduate study: Columbia University and Yale University

Active Ketchum citizen and Wood River Valley resident since 1981

Worked for Ski Education Foundation, 1968

Fashion Editor of SKI Magazine and Golf Magazine

Publicity Director for Sun Valley under Bill Janss

Offered job for Bertl Neumann, PR Director, Winter Olympics in Innsbruck, 1964

Owner ANNE REED GALLERY Ketchum 1981-2006

Founding member of Sun Valley Gallery Association

Helped create Gallery Walks and instigate art on Fourth Street

Advisory board member for the now "old" Friedman Airport on art installations within the airport

Board Member: Environmental Resource Center.

Created Tag line:

Founding Member Women's Resource Center which was incorporated into St.Luke's Created exhibition Trailing of the Sheep which was the birth of this now celebrated festival

Provided my images of Tibet for the Jumbotron when His Holiness, The Dalai Lama spoke

Raised 2 children in Ketchum (attended Hemingway and Community School)

Active in Hemingway PTA and Parent's Association co-President

Special advisor to and photographer for Sun Valley Writers' Conference 20 years

Artist/contributor to Dos Culturos exhibition Sun Valley Center

Introduced numerous speakers for Community Library

Asked by numerous organizations to provide feedback to consultants for Ketchum City Development as well as Sun Valley Center for the Arts

These notes only to show my commitment to Ketchum, the length of time I've lived here, my involvement in the community and my desire to preserve the quality of life, uniqueness of this community from how it functions to its aesthetic quality....why those of us who live here feel fortunate every day and sometimes are willing to step outside of our comfort zone to express our feelings, to be proactive rather than reactive and to try to remind others of the importance of careful consideration that impart lasting changes for the generations to come.

ATTACHMENT A

Initial research turned up lots of concerns and questions that need to be investigated and analyzed

Health Hazards living near a Gas Station

Health and Safety

Fire: An estimated 5,020 fires and explosions occurred at public service stations per year from 2004-2008. That means that, on average, one in every 13 service stations experienced a fire.

Note: in body of this report increased risk of Leukemia especially among children living near a gas station

October 7, 2014

Small Spills at Gas Stations Could Cause Significant Public Health Risks Over Time...Johns Hopkins Bloomberg School of Public Health

Soil and groundwater may be imperiled more than previously understood

A new study suggests that drops of fuel spilled at gas stations — which occur frequently with fill-ups — could cumulatively be causing long-term environmental damage to soil and groundwater in residential areas in close proximity to the stations.

Few studies have considered the potential environmental impact of routine gasoline spills and instead have focused on problems associated with large-scale leaks. Researchers with the Johns Hopkins Bloomberg School of Public Health, publishing online Sept. 19 in the *Journal of Contaminant Hydrology*, developed a mathematical model and conducted experiments suggesting these small spills may be a larger issue than previously thought.

"Gas station owners have worked very hard to prevent gasoline from leaking out of underground storage tanks," says study leader Markus Hilpert, PhD, a senior scientist in the Department of Environmental Health Sciences in the Johns Hopkins Bloomberg School of Public Health. "But our research shows we should also be paying attention to the small spills that routinely occur when you refill your vehicle's tank."

Over the lifespan of a gas station, Hilpert says, concrete pads underneath the pumps can accumulate significant amounts of gasoline, which can eventually penetrate the concrete and escape into underlying soil and groundwater, potentially impacting the health of those who use wells as a water source. Conservatively, the researchers estimate, roughly 1,500 liters of gasoline are spilled at a typical gas station each decade.

"Even if only a small percentage reaches the ground, this could be problematic because gasoline contains harmful chemicals including benzene, a known human carcinogen," Hilpert says.

Hilpert and Patrick N. Breysse, PhD, a professor in the Department of Environmental Health Sciences, developed a mathematical model to measure the amount of gasoline that permeates through the concrete of the gas-dispensing stations and the amount of gasoline that vaporizes into the air.

The model demonstrates that spilled gasoline droplets remain on concrete surfaces for minutes or longer, and a significant fraction of spilled gasoline droplets infiltrate into the pavement, as concrete is not impervious.

"When gasoline spills onto concrete, the droplet will eventually disappear from the surface. If no stain is left behind, there has been a belief that no gasoline infiltrated the pavement, and all of it evaporated," Hilpert says. "According to our laboratory-based research and supported by our mathematical model, this assumption is incorrect. Our experiments suggest that even the smallest gasoline spills can have a lasting impact."

Since the health effects of living near gasoline stations have not been well studied, Breysse says there is an urgency to look more closely, especially since the new trend is to build larger filling stations with many more pumps. These stations continue to be located near residential areas where soil and groundwater could be affected.

"The environmental and public health impacts of chronic gasoline spills are poorly understood," says Breysse. "Chronic gasoline spills could well become significant public health issues since the gas station industry is currently trending away from small-scale service stations that typically dispense around 100,000 gallons per month to high-volume retailers that dispense more than 10 times this amount."

"In a perfect world, it would be ideal to avoid chronic spills," Hilpert says. "However, if these spills do occur, it is also important to prevent rainwater from flowing over the concrete pads underneath the pumps. Otherwise, storm runoff gets contaminated with benzene and other harmful chemicals and can infiltrate into adjacent soil patches or form storm water that may end up in natural bodies of water."

"Infiltration and Evaporation of Small Hydrocarbon Spills at Gas Stations" was written by Markus Hilpert and Patrick N. Breysse.

###

Media contacts for Johns Hopkins Bloomberg School of Public Health Media: Nicole Hughes at 443-287-2905 or nhughes4@jhu.edu and Stephanie Desmon at 410-955-7619 or sdesmon1@jhu.edu .

Abstract

Small gasoline spills frequently occur at gasoline dispensing stations. We have developed a mathematical model to estimate both the amount of gasoline that infiltrates into the concrete underneath the dispensing stations and the amount of gasoline that evaporates into the typically turbulent atmosphere. Our model shows that the fraction of infiltrated gasoline can exceed the fraction that evaporates from the sessile droplets. Infiltrated gasoline then evaporates and is slowly released to the atmosphere via slow diffusive transport in pores. Tentative experiments show that our theoretical approach captures observed experimental trends. Predictions based on independently estimated model parameters roughly describe the experimental data, except for the very slow vapor release at the end of Stage II evaporation. Our study suggests that, over the lifespan of a gas station, concrete pads underneath gas dispensing stations accumulate significant amounts of gasoline, which could eventually break through into underlying soil and groundwater. Our model also shows that lifetimes of spilled gasoline droplets on concrete surfaces are on the order of minutes or longer. Therefore contamination can be carried away by foot traffic or precipitation runoff. Regulations and guidelines typically do not address subsurface and surface contaminations due to chronic small gasoline spills, even though these spills could result in non-negligible human exposure to toxic and carcinogenic gasoline compounds.

Anyone who has ever pumped their own gas downwind of the tank knows the tell-tale smell of fuel. But even from a distance those fumes linger. Researchers in Spain found that gas fumes contaminate the air up to 100 meters, or 328 feet, away with potential health hazards.

The airborne chemicals came mostly from unburned fuel evaporating during refilling of the stations' storage tanks, during automobile refueling, and from spillage. The researchers from the <u>University of Murcia</u> measured the levels of two common gasoline related pollutants, benzene and hexane, in the area around the stations. They then compared these levels to the contamination caused by normal automobile traffic, and found higher levels in areas around gas stations.



How Crude Oil Can Harm You

"Some airborne organic compounds – such as benzene, which increases the risk of cancer – have been recorded at petrol stations at levels above the average levels for urban areas where traffic is the primary source of emission," said Marta Doval of the University of Murcia, and co-author of the study, in a press release from the Spanish Foundation for Science and Technology Science Information and News Service.

The research was published in the December issue of the Journal of Environmental Management.

The amount of air pollution created depended on a number of factors, including weather, surrounding structures, amount of gasoline pumped, and traffic intensity.

Researchers in Spain found that gas fumes contaminate the air up to 100 meters, or 328 feet, away with potential health hazards.

Traffic could actually serve to mask the effects of the gas stations. Heavy traffic overlapped and overpowered the gas stations as sources of air contamination.

But automobiles and gas stations aren't the only sources of benzene, hexane, and other air contaminants, warned one of the researchers.

"There is not much use in protecting people from petrol stations if the other sources of emission (above all traffic and industries near population hubs) are not controlled or reduced", said Enrique González, leader of the University of Murcia team in a press release.

The proven causal relationship between benzene and cancer is well documented and accepted by the scientific community—and gas stations are classified by the Environmental Protection Agency as a point source for benzene.

In addition, John L. Adgate, PhD, MSPH, Chair of the Department of Environmental and Occupational Health, Colorado School of Public Health, submitted a letter to the DEH board saying, in part, "While there is a limited scientific basis with which to determine an appropriate minimal setback, the potential for human exposure to hazardous air pollutants is real

Examples in other cities where zoning-mandated spacing exists to protect homeowners from the negative health effects of gas stations are Milford, CT where a gas station cannot be within 90 meters (300 ft) of housing

The following quote from a peer-reviewed article in the American Journal of Public Health is unequivocal on the health risks for children: "Increased risk of childhood leukemia was found with residential addresses near gas stations (44, 60, 63), repair garages and nuclear power plants."

How living near environmental hazards contributes to poorer health and disproportionate health outcomes is an ongoing concern. We conducted a substantive review and critique of the literature regarding residential proximity to environmental hazards and adverse pregnancy outcomes, childhood cancer, cardiovascular and respiratory illnesses, end-stage renal disease, and diabetes. Several studies have found that living near hazardous wastes sites, industrial sites, cropland with pesticide applications, highly trafficked roads, nuclear power plants, and gas stations or repair shops is related to an increased risk of adverse health outcomes. Government agencies should consider these findings in establishing rules and permitting and enforcement procedures to reduce pollution from environmentally burdensome facilities and land uses.

Few clear risk factors have been identified for the childhood variant, but exposure to benzene in the workplace has been identified as a possible factor in leukemia in adults, the authors say.

The risk appeared to be even greater for acute non-lymphoblastic leukemia, which was seven times more common among children living close to a fuel station or commercial garage, the research showed.

Fuel Stations May Pose Child Cancer Risk - Study

A study of more than 500 infants found that a child whose home was near a fuel station or vehicle-repair garage was four times as likely to develop leukemia as a child whose home was further away.

And the longer a child had lived nearby, the higher the risk of leukemia seemed to be, showed the research, published in the Occupational and Environmental Medicine journal.

The prevalence of childhood leukemia is four in every 100,000 children, but it is the most common type of childhood cancer in developed countries, say the researchers.

Few clear risk factors have been identified for the childhood variant, but exposure to benzene in the workplace has been identified as a possible factor in leukemia in adults, the authors say.

The risk appeared to be even greater for acute non-lymphoblastic leukemia, which was seven times more common among children living close to a fuel station or commercial garage, the research showed.

http://www.sciencedirect.com/science/article/pii/S2214750015300135

http://patch.com/maryland/wheaton-md/public-health-effects-of-costco-gas-station-questioned

http://www.scientificamerican.com/article/is-it-safe-to-live-near-gas-station/

Living with 100 yards of petrol stations 'damages your health', study claims

Living within 100 yards of petrol stations can damage your health, according to a new study.

16 0 0 16 **Email**

Researchers found that air in the immediate vicinity of garages is often polluted and can harm local residents.

Scientists from the University of Murcia studied the effects of contamination at petrol stations that is potentially harmful to health

Experts say it shows that a "minimum" distance of 50 yards should be maintained between petrol stations and housing.

A 100 yards minimum distance should apply to "especially vulnerable" facilities such as hospitals, health centres, schools and old people's homes.

Marta Doval, co-author of the study and a researcher at the Spanish university, said: "Some airborne organic compounds such as benzene, which increases the risk of cancer have been recorded at petrol stations at levels above the average levels for urban areas where traffic is the primary source of emission."

The study, which has been published in the *Journal of Environmental Management*, shows that the air at petrol stations and in their immediate surroundings is above all affected by emissions stemming from evaporated vehicle fuels which are unburnt fuels from fuel loading and unloading operations, refuelling and liquid spillages.

The research team measured the levels of "typical traffic" pollutants in different parts of the urban area of Murcia, and calculated the quotients for the levels of an aromatic compound (benzene) and a hydrocarbon (n-hexane) at three Murcia petrol stations – near the petrol pumps and surrounding areas – to find the distance at which the service stations stop having an impact.

In the three cases studied a maximum distances of influence of close to 100 metres was found although the average distance of contamination was around 50 metres.

But the distances depend on the number of petrol pumps, the amount of fuel drawn from them, traffic intensity, the structure of the surroundings, and weather conditions.

http://www.telegraph.co.uk/news/health/news/8306786/Living-with-100-yards-of-petrol-stations-damages-your-health-study-claims.html

New Traffic Studies necessary. Traffic study submitted for this proposed project is out of date!

LEFT TURN CRASHES

It is essential to provide adequate lane width and distance adjacent to service stations so that: • vehicles entering and exiting can decelerate and accelerate without adversely affecting the free flow of traffic on the frontage road • through traffic can safely manoeuvre past vehicles entering or exiting the site.

Left turn crashes are the major problem. The main points of interest are: 4.2.1 Midblock sites • small crash data set (four reported injury crashes) • right turn manoeuvres comprise 75 percent of the midblock crash data as follows: (a) left turn-in/rear end (GC, GD, GE)* = 25% (b) left turn-in/left side (LB) = 25% (c) left turn-out/left side (JA) = 25% * See Appendix 5 for crash types. 4.2.2 Intersection sites Right turn manoeuvres comprise at least 75 percent of intersection crash data as follows: • left turn-in/rear end (GC, GD, GE) = 30% • left turn-in/right (LB) = 26% • left turn-out/left side (JA) = 14% • right turn-out/left side (KB) = 6% • left turn maneouvres comprise 6 percent of intersection crash data, i.e. left turn-out/right side (KA) = 6%

Driveways should be designed to reinforce the motorist's obligation to give way to pedestrians.

Generally unsuitable for service stations. High pedestrian flows may cause delays, frustration and on-road queuing problems to motorists wishing to access the site.

Sound engineering judgement and consideration of the following are required: • type of pedestrians, e.g. young children, the older pedestrian • pedestrian environment, e.g. central business district, residential, commercial

The presence of some advertising signs at service stations may compromise road safety in the following ways:

- by directly distracting or confusing motorists
- by presenting a physical obstruction to vehicles moving on or off the carriageway
- by obstructing visibility (advertising or traffic signs).
 To achieve advertising which is safe and effective from a road

What is required for gas station/convenience stores to be successful:

How Gas Stations and Convenience Stores work: http://www.nacsonline.com/YourBusiness/Refresh/Documents/How-Stores-Work.pdf Some gas stations look like obstacle courses, creating situations in which vehicles need to weave around one another and the central pumps to traverse the location. If a gas station fails to have sufficient barriers or warnings about incoming or outgoing traffic, this can lead to devastating accidents. Poorly maintained or operated gas stations and convenience stores also create many situations for slip and fall accidents to occur, including spilled gasoline, icy sidewalks, or spilled food inside the store.

Large driveway widths may allow vehicle entry and exit manoeuvres to be undertaken with more ease but increase a pedestrian's exposure to conflict. The design vehicle, driveway type, e.g. one-way, two-way, and traffic generation are some of the factors affecting driveway width. The width should be restrictive enough to discourage parallel exiting manoeuvres which can result in visibility restrictions and conflicts. The radius (or splay) at the roadway edge will be site specific and determined from the swept paths of the appropriate design vehicle. On any road, all vehicles should be able to undertake their turning manoeuvres without crossing the road centreline, and preferably without encroaching into adjacent lanes on a multi-lane roadway, with the exception of the occasional bulk filling tanker. It may be preferable that bulk filling

An understanding of fundamental traffic engineering principles is necessary to ensure safe roading design. Some of the more important principles are: • reducing the number of conflict points • separating the points of conflict • controlling vehicle speeds • defining vehicle paths. The types of manoeuvres likely to occur at the driveway of a service station need to be known to ensure safe design. The most common manoeuvres are merging, diverging and crossing. Weaving manoeuvres may occur at some sites. Every two-way driveway has nine conflict points (three merge, three diverge, three crossing), therefore midblock sites typically have 18 conflict points (two driveways) and intersection sites typically have 27 conflict points (three driveways). T

in particular the requirement that no driveway should be located within 30 metres of an intersection c Solid medians The installation of a solid median may be the most effective technique for reducing crashes at both intersection and midblock service station sites.

tankers do not use any driveway intended for one-way use. (This is because use by tankers may require widening to a maximum of 9 metres, under which circumstances the driveway is likely to be used as a two-way driveway by other vehicles.) The recommended dimensions shown in Table 5.2 should be measured at the road boundary (the legal boundary between the service station site and the road reserve).

It is desirable that queuing vehicles do not block any driveway because this may cause a conflict between entering vehicles and traffic on the frontage road, or obstruct pedestrian flows on footpaths.

This is a common problem with gas stations (or any kind of driveway) on busy streets. Car drivers come speeding out of the driveway without stopping at the sidewalk so they can quickly merge into the traffic on the busy street. Often, the driver looks only to the left (where cars are approaching) and not to the right (where pedestrians may be approaching). Because the cars are moving so much faster, the pedestrian has no chance to get out of the way.

Pro tip: if you see a sidewalk or crosswalk or intersection, you should expect pedestrians to be coming from one direction or another and you are required by law to check in all directions for pedestrians before proceedings. If you hit the pedestrian anyway, then you are driving distracted and deserve jail time. Hitting a pedestrian on the sidewalk is negligence, not an accident.

FYI How Gas Station/Convenient Stores work: http://www.nacsonline.com/YourBusiness/Refresh/Documents/How-Stores-Work.pdf

ATTACHMENT B

Excerpts from Ketchum 2014 Comprehensive Plan noted in a quick glance that should be considered by P&Z. These excerpts clearly present the Bracken Station Proposal in direct opposition to the desired vision and policy outlined in the Guide

Initial Comments re proposed Bracken Station, 911 North Main Street, Ketchum, Idaho (AM Lot 5A, Block 30, Ketchum Townsite)
911 North Main Street

Motor vehicle fueling station and food service are both conditional uses in that zone, and the definitions are below.

Motor Vehicle Fueling Station - A facility providing the retail sale and direct delivery to motor vehicles of fuel, including electric charging stations, lubricants and minor accessories, and retail sales for the convenience of the motoring public.

Food Service - An establishment where food and drink are prepared, served and consumed on site with associated outdoor dining, or distributed to

customers through take out, delivery or catering. Typical uses include, but are not limited to restaurants, cafes, delis, catering services and brewpubs that do not distribute beer produced for off site consumption. In LI districts catering and food preparation is permitted. Restaurants require a conditional use permit and shall not exceed 1,000 square feet and serve no later than 9:00 P.M. unless expressly permitted through approval of the conditional use permit.

I spent a few minutes looking through the inspiring document Ketchum, 2014 Comprehensive Plan, Expressing a Vision and Policies to Guide Ketchum's Future. In doing so, I was surprised to find so many references that DIRECTLY contradicted the siting of the proposed Bracken Station.

I am hopeful P&Z will read through these excerpts and embrace them in light of their acceptance of the Comprehensive Plan in face of the proposed Gas Station/Convenience Store/Food Service's application

See below excerpts from this document which need to be considered when deciding on the appropriateness of this proposed project on the proposed site:

So aptly included: "Plan for what is difficult while it is easy; do what is great while it is still small." -Sun Tzu Wu, Chinese General and author

The KetchumComprehensive Plan represents a community-based strategy that looks forward 20 years and beyond. It provides the opportunity to set a new direction for a sustainable future.

 Establish the policy framework for land use decisions that preserve and enhance the community

Provide a basis for updating zoning and subdivision regulations and determining whether the regulations support the community's desired future

Ketchum residents clearly value knowing that their community is strong and enduring. This strength is manifested in several ways. Citizens are willing to take local actions to reduce impact on the natural environment, promote long-term economic health

More focus has been put on community SAFE [SIC] walkability

It is envisioned that the
Comprehensive Plan will be
implemented through amendments

to the City Code, special projects and, perhaps more importantly, through collaborative efforts with other public agencies, businesses, and non-profit groups.

"We aspire to be an authentic mountain community with worldclass character, yet small-town feel. We see our community as one with a high quality of life for a local year-round population and a visiting population.

core values broadly address important considerations in making decisions about the community's future. They form the basis for the future land use plan and the underlying goals and polices.

3. Community Character
You know when you have entered
Ketchum; this is a place centered on the
"town" and identifiable from the "country"
by distinct edges. Residents and visitors

desire this clear division that has been lost in so many American cities through strip commercial development

night skies is a priority.

Public transit is convenient and citizens feel safe and comfortable using it.

We want to minimize the negative effects of development such as car pollution, roadway congestion and undesirable environmental impacts

Ketchum understands that there are global challenges too large for any one community to solve, but believes in doing its share to address them. We will strive to integrate best practices in energy conservation, renewable energy use, multimodal transportation, waste reduction and recycling, low-impact development

GOAL

Goals help guide the community's

decisions about public and private investment and development. Goals are not tied to specific dates or targets, but are enduring and provide a general direction for more refined policy and objective statements to assist decision-makers.

Goals and policies are organized within the Plan
Community Character
Preservation

Environmental Quality and Scenic Beauty

Goals help guide the community's decisions about public and private investment and development. Policies are a course of action by which goals are achieved.

maintaining a wellrounded tourism industry, and supporting and strengthening existing businesses.

Additionally, inadequate air access, ground service, and lodging in Ketchum pose challenges to increasing tourism.

encouraging businesses that fit the downtown character [The proposed site, although designated as LI is in fact so close to the down town core and on a street labeled MAIN STREET it has to be considered as part of the down town!]

HOUSING

Ketchum will support and attract businesses and industries that diversify and sustain the local economy and level out seasonal fluctuations.

New employment opportunities will focus primarily on clean industries within the City's industrial areas Recruit small businesses and support local entrepreneurs in bringing in new businesses and industries that fit the small-town atmosphere of Ketchum. Ideally they will be nonseasonal and attractive to younger workers.

Ketchum will continue to support our tourism economic base.

Ketchum depends heavily on tourism to support the local economy and will continue to support this industry. [Knob Hill Inn]

Continue to support tourism-related land uses and businesses including lodging development and venues. [Knob Hill Inn]

The Ketchum community wants the majority of people who work in Ketchum to have an opportunity to reside here

[Were this property used as desireable LI with small businesses on the ground floor and housing on the second floor, Chpater 2 and Chapter 3 can be better realized than via a gas station/convenience store. This is appropriate and fitting use for this property]

Ketchum strives to protect and enhance those elements of the natural and built environment contributing to one of the world's unique mountain resort communities. The following are components of the community's vision

- Protect the visual quality of community and downtown entryways;
- Discourage commercial strip development and keep key commercial needs concentrated in the downtown;
- •Encourage new development to be designed to fit in with Ketchum's character as a small mountain town

Challenges
Protecting Community

Gateways

The northern entrance corridor leading from the Sawtooth National Recreation Area into Ketchum provides the feeling of expansive open space with its low-density residential housing [can't imagine a gas station complex at the closest entry on the north to Ketchum City core was part of the vision of the writers of this plan]

ensure the right blend of building types to maintain Ketchum's small town character.

It is important to maintain and reinforce development quality, particularly in the built-up community core area.

Each new project should be well-designed and attractive, and should complement surrounding land uses and existing neighborhood character.

Policy CD-2.3 Night Sky Conservation Continue to protect the visibility of the stars in the night sky through the lighting code, education, and enforcement. The City should implement policies and programs that enhance opportunities for individuals, businesses, and public organizations to conserve energy and convert to renewable resources. [The city should think 30 years down the road whether a gas station is viable with electric cars and whether this becomes a potentially vacant blight]

To protect ground water quality, the
City will promote implementation of
best management practices for
residential, commercial, industrial
and construction activities. [gas, dump site, waste disposal]

Policy OS-3.6
Roadway Corridors
Establish, preserve, and enhance scenic entryways along major roadways entering the City.

In addition to modifying land use patterns to reduce traffic congestion and vehicle miles travelled, as well as to improve air quality

Policy M-2.5
Pedestrian and Bicycle
Improvements Linked to
Transit
The City will prioritize improvements
of pedestrian and bicycle facilities in
areas served by transit.

Policy M-2.6
Pedestrian Facilities
Supporting Transit
The City will follow best practices for pedestrian safety at intersections and crossings near transit stops.

Policy M-3.2 Roadway Safety The City will strive to maintain an acceptable level of service for roads, which will generally place a priority on pedestrian, bicycle, and vehicle safety.

Policy M-5.2
Pedestrian Level of Service
Standards
Create and use pedestrian level of service (LOS) performance standards for all development.
Policy M-5.3
Safe Routes to School
Continue to support the Safe Routes to School program with the Blaine
County School District, private schools and ITD.

Enhance bicycling
connectivity and comfort.
Policy M-6.1
Complete Bicycle Network
Construct missing links in the bicycle

network especially from outlying areas to the downtown core. Strive for additional bike lanes in streets.

Policy CHW-1.1 Collaborative Efforts to Improve Community Health Partner to develop and achieve the Plan's vision for all residents' health and well-being.

Goal CHW-5
Enhance Access to
Affordable and Local Food
Options for All Residents
The community will attempt to retain
and attract community grocery
stores, so that residents have
convenient access to a variety of
foods. Measures will be supported
that encourage local food access,
including community gardens,
farmers' markets, and small-scale
food production. [This does not mention convenience food or
fast food1]

Reduce generation of air pollutants and noise
The City will promote reductions in air pollution to minimize impact to human health,...improve air quality...

The City will continue to pursue reductions in air emissions/airborne particulates by regulating idling vehicles

Policy CHW-6.2 Noise Pollution Through adopted policy decisions and enforcement, the City will protect residents from adverse noise impacts.

[P&Z needs to always be reminded of the following]:

Ketchum continually strives to be a place where people can be involved in their community on many different levels. In order to achieve the collective vision and goals of the community, it is important that people have the opportunity and

take action to move the community forward.

This plan envisions effective local governance and community-based collaboration, ideas that are based on involving citizens in the planning and decision-making process and actions that move the community forward toward its vision.

The goals and policies in this section reflect the City's commitment to providing exceptional service for an exceptional community. These concepts aim to ensure that community members are effectively represented by their local government and have opportunities to participate in planning and decision-making. They also build on the notion that a range of voices is necessary to identify issues and opportunities, and that the best solutions often result from collaboration and open

communication.

The City will strive for outstanding customer service and work collaboratively with citizens to address issues and resolve problems.

Policy HI 3.3

Collaborative Problem Solving Solicit feedback from the public to identify opportunities to solve problems collectively and creatively.

Vision

The Future Land Use Plan is built on a framework composed of six concepts that are described in the following pages. With this framework, the plan addresses the interrelationship between land-use patterns and mobility, open lands, infrastructure, and other future needs of the

community.

A compact community allows bicycle and pedestrian movement to play an important part in the transportation system.

A Focus on Downtown and Smaller Commercial Centers

There will be only limited expansion of commercial uses outside the downtown in areas

The Plan and policies continue to discourage "strip" commercial development along the community's major roads

The City will also consider ... the development's impact on the streets and transportation system, vehicle accessibility,

Development standards should incorporate operational requirements for non-residential units, i.e., hours of operation, noise attenuation, screening, and other measures, to ensure compatibility with nearby residential units.

(Areas in the ACI with this designation will require evaluation with regard to the provision of access, utility service, safety, and environmental impacts.)

Goal LU-1
Promote a functional,
compact, and mixed-use
pattern that integrates
and balances residential
and non-residential land
uses.

While infill and redevelopment is

desirable infill standards are needed to ensure compatibility with neighborhoods and districts.

Policy LU-2.2

Compatible Residential Infill
Appropriate types of infill include
the new residential units on
vacant lots/areas, additions to
existing units, accessory dwelling
units, and residential units with
businesses. Ensure that residential
infill is compatible in character
and scale within the surrounding
neighborhood.

Background and
Intent - A Living
Document
A key aspect of any plan is how it is carried out after it is adopted. The implementation plan states how the community may best accomplish

the vision, goals and policies outlined in the Comprehensive Plan.

UNINTENDED CONSEQUENCES

Re. The Gas station, Convenience Store, Food Service Establishment, three businesses, known as Bracken Station proposed for 911 Main Street

This project is slated for one of the most unique sites in Ketchum: the junction of Light Industrial, the City Core and residential/tourist (Knob Hill Inn) meet.

Located on the southwest Corner of 10th Street and Main Street (some of us remember the days when Ketchum had 5 gas stations on Main Street!) is sandwiched at the northern entry into or egress out of town.

Being objective, SAFETY is the MOST obvious reason why this site is simply and unequivocally the wrong for a Gas Station, et al.

Vehicular Safety: CONSIDER

(Note: gas stations, convenience stores and fast food establishments depend on high turnover of vehicles and people!).

- increased traffic rvs, campers, trailer towing rigs, crossing back and forth across the highway as they head north
- •East 9th Street, Alpine and Walnut (residential streets without sidewalks) becoming a "sneak through" thoroughfare for those who want to avoid the lineup of cars or quickly access the gas station from East Ketchum.
- •10th Street, already a noted dangerous artery with its congestion of cars, trucks, vehicles moving in and out and its steep slope, becoming even more dangerous with increased traffic

Pedestrian safety: THINK

(It's a fact: Convenience stores and the proposed Fast Food Service are designed to attract pedestrians as well as vehicles.)

- •pedestrians running back and forth, across the highway from the bus
- •MOST frighteningly: children lured from Hemingway to venture up the already treacherous 10th Street and who then must walk amongst the large vehicles at pulling in and out of the pumps and across the sidewalk t
- •pedestrians facing danger from increased traffic through the narrow Alpine, Walnut. East 9th Street
- •Road bicyclists and others on non-motorized vehicles being threatened by turning vehicles

Health risks: IMAGINE

(Think about Shum's Frenchman's Place condos and the young families living there) FACTS:

- children living near a gas station may quadruple the risk of acute leukemia
- •risk is 7 times greater for non-lymphoblastic leukemia.

•Benzene is a known cancer-causing chemical; repeated high exposure to gasoline, even in vapor form, can cause lung, brain and kidney damage according to the NIH.

FIRE: SCARY (Between 2004-2008 there was approximately one fire for every 13 service stations. Some experts say that number is on the rise.)
•igniting Knob Hill grasses could be a real possibility were there a fire putting the entire town of Ketchum in danger

CRIME: (THREATENING American convenience stores are often direct targets of armed robbery. 54% of all shoplifters regularly steal from convenience stores.)
•residences and nearby small businesses risk being targeted

FURTHERMORE: This project does not meet the vision and policies outlined as a guide in Ketchum's 2014 Comprehensive Plan. (Read the plan to find numerous places where this project on this site proves to be in direct contradiction to the plan.)

Based on just the facts above: P&Z needs to analyze and consider the UNINTENDED CONSEQUENCES before giving approval to this project.

Please attend a site visit at 5:00 PM, June 13 followed by a meeting of the P&Z at 5:30 at City Hall. I will be there, encouraging additional study of the ramifications (known and as yet unknown) of the Bracken Station at the proposed site.

Barbi Reed

From: Gary Lipton External

Sent: Monday, June 06, 2016 1:15 PM

To: Participate

Subject: Bracken Station. 911 N. Main St. Ketchum, Idaho

Thank you for having an open meeting regarding the above captioned heading (Bracken Station), 1. Lipton LLC. 1 owns the adjacent property to the north side of the proposed Bracken Station, at the 10th Street complex. 44 ft away.# 2. Reference the Dark Sky Chapter and Code 17.132. First of all its hard for me to fathom how the chairman of the P & Z could even design this gas station complex without comprehensive knowledge regarding chapter 17.132 provisions. There seems to be a complete redirect of architectural priorities. (FEES and MONEY). Please review proposed north 10th street elevations~10th street view. When the existing building is demolished the lighted logo sign on the canopy will shine directly into the windows of my location as will the indirect ambient light from underneath of the canopy. Please review Code general provisions #B1,B3,B4, ie. direct glare, excessive lighting, light trespass etc, also review C3, which states that "any parking yard or building illumination in any zoning district shall be directed as to protect adjacent properties from glare and direct lighting", etc. This logo sign and the excessive glare from the fueling canopy needs to be readdressed. (maybe no canopy) additionally the proposed sign on the north property corner will also come under the codes stated earlier. Please review 17.132 B.3. lets not contribute to additional excessive light pollution. #3. TELEPHONE POLES. If I'm reading the plans correctly, there seems to be 4 telephone poles which are not being addressed at all. The P. & Z. should take a stand NOW and in the future to require any project to replace the telephone poles with underground facilities. This is a win win situation for all. There is a process already established in Ketchum to help fund infrastructure projects such as this called the KURA. I suggest you engage this perspective applicant to engage the P.&Z. and investigate this process with the KURA. The P. & Z. besides considering the Conditional Use Permit etc. also has a responsibility and the power to determine the look of our beautiful entryway to our TOWN. Exercise it, don't be complacent. 4. TRAFFIC STUDY.(Hales Eng). Who paid for this study, and what directions were they instructed to follow? It seems that the data is based on information from Feb 2008 criteria. Something seems out of place here. Could the P. Z. look into this? I think there should be a speed cord stretched across I- 75 so we all can see how the speed limits are being adhered to and then we will have a much better informed perspective. Possibly a proposed traffic light should be placed at the corner of 10th and Main Street. It will slow traffic down, allow for a correct pedestrian crosswalk, set up North and South bound traffic for all kinds of turns. Maybe a relocation of the entrance of the Bracken Stations should be reviewed. #5. FOOD PATIO. I understand that the plans proposed a 490 sq ft. out door eating patio. There is nowhere I have found a review of food regulations about open food hours. The P. & Z. has limited the Big Wood Bread facility L I. Zone a closing time of 9 pm. #6. ALLEY AREA Pursuant to the west view drawings the alley property line is right next to the buildings edge. Applicant is proposing a new door storage area along with over a 24' of a new asphalt apron and upgraded new alley walls etc. These exit and entrance doors present a rescue fire hazard for the Ketchum Fire Department. If by chance there is a fire at lots #1-4 and the alley way is blocked by a utility service vehicle or truck distributing their products to the lower level to the Bracken Station (hence 24' new asphalt), how are the Fire trucks going to perform their civic duty when the alley is blocked. I can recall a similar situation in Ketchum where the Farmers Market on Tuesdays was ordered to redirect their vendor vehicles by not allowing parking in the alley for safety reasons. I think a discussion with the fire chief is appropriate and timely. #7. I need to review the water mitigation plan, Snow removal Plan on 10th street. I have not seen any gas spillage filter separation plan for under the ground around the gas tanks and pumps. Some one needs to explain to the public where does the excess gas mixed water get tested and then dispersed into the Ketchum water system. Most concerning to me and the surrounding businesses is whom will oversees that the Gas Station will carry an appropriate amount of insurance to protect their infrastructure investment and also protect adjacent property so we all will feel secure in any possible mitigation situation. Thank You. Your decisions will affect 8 to 10 existing businesses and their quest to find new spaces in which to operate. Additionally, your legacy as a member of P.& Z. will determine the future look of the Gateway to our City. Analyze and use a discerning approach, That's all we can ask for. Gary Lipton Cell 1 248 561 5120

J. KEVIN LAWLER

June 2, 2016

Department of Planning and Building City of Ketchum PO Box 2315 Ketchum, ID 83340

Via email to:participate@ketchumidaho.org

RE: Bracken Station - 911 North Main

To Staff and Planning Commissioners:

I am writing to strongly object to a proposed motor vehicle fueling station, convenience store and food service establishment to be located at 911 N. Main. The proposed Bracken Station project would have a materially adverse effect on the value of my Ketchum residence.

I have resided (as a seasonal resident) at # 21 360 E. 9th Street (Frenchman's Place) for nearly 10 years. During this time, I have had the 'quiet enjoyment' of living in my seasonal residence. The nature of the proposed Bracken Station project would indisputably change the character and level highway trip generation to the immediate north of Frenchman's Place.

I respectfully request Planning staff and Commissioners to consider the land use patterns and uses of immediately the adjacent property to the South (Frenchman's Place) and to the immediate East (Knob Hill Hotel) to the proposed Bracken Station project. The uses and activity level of the proposed Bracken Station project are incompatible with existing residential and hospitality development. Further, the community is presently well served by gas stations and convenience stores – there are 3 gas stations and 2 convenience stores more appropriately located with a 1/3rd of mile from the proposed Bracken Station project.

Please contact me at 561-762-2602 should you have any questions on the gravity of my concerns and objections to the proposed Bracken Station project.

Respectfully,

J. Kevín Lawler

cc. Sara Gorham
Board Frenchman's Place
Engle & Associates

I am writing to voice my opposition to the proposed project titled "Bracken Station". I reside full time at 360 Frenchman's Place which is located next to the proposed project. If I resided elsewhere within Ketchum I would still oppose the project.

My reasons for opposition follow:

- 1. Ketchum is a small town with a small population. Upon entering Ketchum we find our first gas station/ convenience store at Base Camp I. After entering Ketchum proper we have Base Camp II. We have the Shell station/car wash on Lewis Street and finally we have the Sinclair station in Sun Valley. We have four gas stations for a town with a population of 2,728 residents. I have never waited in line for gas at any of these four locations. The location of Base Camp II should suffice amply for gas needs in this small radius of area.
- 2. The location of the Bracken Station site abuts 10th Street where there is access to Highway 75. That particular stop sign handles traffic coming from the industrial area and Warm Springs, as well as traffic traveling North and South. Ingress and egress of cars at a gas station located so close to 10th street would cause added traffic confusion and danger. I can foresee the addition of another stop light at 10th street in order to alleviate or regulate congestion which would be unwanted and unwarranted if not for the proposed project.
- 3. The Knob Hill Inn is located adjacent to the proposed project. This hotel has been a jewel of an oasis and an example of proper planning. To put a gas station in such close proximity would be a blight.

I can see revamping the existing buildings or even completely redesigning for a new project with retail shops and businesses but I can not see the benefit of a gas station.

At the very least, before a project of this sort is approved I believe a traffic study with an impact report should be required. After fact finding, the project should be reviewed by the city with the intention of including residents of Ketchum in the decision.

Thank you for the consideration of my opposition.

Sincerely,

JODY

VERING

Liz Roquet Owner, Lizzy's Fresh Coffee 971-4 N Main St Ketchum, ID 83340

6/2/16

Ketchum Department of Planning and Building PO Box 2315 Ketchum, ID 83340

Re: Bracken Station Proposal

Dear Ketchum Department of Planning and Building,

My name is Liz Roquet, and I am the owner/operator of Lizzy's Fresh Coffee, located at the Northtown Center, on the property where Bracken Station is proposed to be built. My business has been at this location for eight years, and I am keenly aware of the motorist, pedestrian and bicycle activity on 10th Street and Highway 75.

I have reviewed the proposed plans for Bracken Station, and as a citizen observer, I feel it is important to share my unbiased concerns and observations for a potential high traffic business, such as a gas station, to operate at this location.

A. Traffic turning in/out:

We currently have light traffic in and out of the Northtown Center parking area for the 7 street-level businesses here. I travel in and out of the lot multiple times a day, as do my employees and customers.

My Observations:

- 1. Highway traffic should be traveling at 25 mph, but there is a chronic speeding problem at this location with traffic coming into town from the north, as well as accelerating out of town from the south. Much of the traffic speed exceeds 25 mph, and I often perceive it to be much higher. In my belief, the greater portion of the cars travel past this location between 30-40 mph. There is very seldom any traffic control by the Ketchum Police. Additionally, we have witnessed 3 traffic collisions specifically related to the inbound/outbound parking lot activity from Northtown Center.
- 2. The angle and proximity of 10th Street adds to the confusion in entering and exiting this parking area at Northtown Center. For example, a left turning, northbound car turns onto the highway, and within seconds, the 10th street car also enters to turn right or left because they were only observing existing highway traffic. Every week I come in close contact with another vehicle at this intersection in this scenario. When a southbound driver is not planning to turn on 10th Street, but rather turn into Northtown Center it gets a little unpredictable. I'll use myself as an example driver: As is specified in the Idaho driver's manual, I shouldn't engage my right turn signal until I am past 10th Street. Once past 10th, I engage my signal, and the driver behind me has a mere one second warning of my intention to slow down and turn. In combination with 10th Street, the proposed Bracken parking entrance and exit does not appear to be an improvement over the current configuration, and would possibly pose even more risk with a much higher traffic business like a gas station.

My Concerns:

The traffic study performed on this site bases its information on 2008 data. I believe a new traffic study needs to be performed for all day activity, using current 2016 traffic volumes, to assess speeds, traffic patterns and volumes of 10th Street and the proposed entrance of Bracken Station. Will the increase in traffic turning into and out of this location be safe as it is planned (one center turning lane, no southbound turning lane, and no change to 10th Street configuration)? Idaho driving law states that turn signals must be on 100 feet (in business or residential areas) or five seconds (on freeways or highways). Does the proposed design properly allow for this turn signal requirement, or does the entrance need to be moved farther south in order to allow for safe signal distance?

B. Pedestrian/Bicycle Safety on 10th Street

10th Street is a high traffic road that is traveled by cars, LI related commercial vehicles and semitrailers, and is also accessed by pedestrians and bicyclists every day.

My Observations:

- 1. 10th Street is a walking route to Hemingway school. When my child attended Hemingway, he sometimes walked to or from my business. I used to personally walk him across 10th Street to the sidewalk before allowing him to take the route from there on his own. The current sidewalk, however, only exists down ½ of the street, leaving a pedestrian to carefully navigate the massive congestion of cars and traffic, maxed out with parking and activity by Ketchum Automotive and Clearwater Landscaping.
- 2. The parked and moving vehicle activity on 10th Street can be highly congested and is additionally challenged by winter conditions. The 10th street corner encourages high turning speeds due to its off-angle, and also lends itself to dangerous conditions in the winter due to its steep grade. Delivery trucks, including semis, use 10th Street as entrance to businesses in the LI zone, and any time there is snow, we witness cars and trucks losing traction while attempting to make the uphill ascent. We've seen cars slide backwards, trucks jackknife, and a few minor collisions right out of our window.

My Concerns:

Even with a new south-side sidewalk being added by Bracken Station, it still only travels ½ the distance to Warm Springs Road, leaving pedestrians a very unsafe route. Additionally, Bracken station is proposing five on-street parking spaces on the road right of way. This will narrow 10th street even more with cars parked on both sides, and could cause additional danger for cars, pedestrians and bicyclists travelling on the roadway. What studies have been done on 10th Street to observe traffic from all users? What impact will Bracken Station have on traffic volume on 10th street and how will safety of pedestrians and bicyclists be ensured?

C. Pedestrian Crossing on 10th Street and Highway 75.

Pedestrians cross Highway 75 every day. In my opinion, and due to the traffic pattern, there is currently only one reasonably direct location for pedestrians to cross Highway 75 from 10th Street, which is to cross directly at the intersection of the two roads.

My Observations:

1. The Bracken Station plans show that pedestrian crossings will have to cross 10th Street, then again at the entrance to the station, then again at highway 75 at the south end of the property.

My Concerns:

My opinion is that this is a highly dangerous route that has 2 additional traffic crossings (3 in total) that are unnecessarily dangerous for all, including children travelling to/from school or the Y. Additionally, where are guests of the Knob Hill Inn expected to cross if there is no crosswalk on Hwy 75 directly to 10th Street? What systems for crossing safety are being planned for the road crossings and the Bracken entry/exit? Will there be pedestrian lights, crosswalks, flags or traffic lights to help ensure safe foot travel?

D. Water Drainage

We have witnessed many summer rain storms and winter snow melt on 10th Street.

My Observations:

1. During summer rain storms, water literally flows down 10th Street like a raging river, then forms a massive puddle at the intersection of Warm Springs Road and 10th Street.

My Concerns:

How is Bracken Station planning to manage water drainage from its fueling areas to adjoining streets? Will there be any risk of contaminated water originating from its property's surface? How will the ground water flow be affected by the placement of fuel tanks? Are the home and business owners down the hill and at the bottom of 10th in danger of contamination or flooding due to an existing or new ground-water flow pattern?

Thank you for your time and consideration of these concerns as you review the proposed project plan. I hope that our city's northerly entrance maintains its accessibility and safety for drivers, pedestrians, bicyclists, residents and business owners alike. Thank you for holding the public meeting and allowing residents to comment.

Sincerely,

Liz Roquet

From: Sara Gorham [mailto:saragorham@gmail.com]

Sent: Thursday, June 02, 2016 10:19 PM

To: Participate < participate@ketchumidaho.org >

Subject: Bracken Station proposal

Dear Ladies and Gentlemen

As an owner of a unit in the 10th Street Light Industrial Center on Main Street/ Hwy 75, I received your notice letter regarding the proposed gas station, convenience store and food service establishment at 911 North Main Street in Ketchum.

It's hard to imagine a more inappropriate place for such a business. That is already a constricted stretch of roadway

and a gas station in that location would cause paralyzing traffic congestion as cars, trucks and vehicles hauling trailers wait to turn left across Hwy 75 and into the gas station. In mid-summer that could easily cause gridlock all the way to the south entrance to town.

Additionally, there are residential units in immediate proximity which seems wholly incompatible with the hazards afforded by tanker trucks refilling underground tanks and releasing fumes. There will also be additional noise, lights and late hours of operation, all incompatible with the quiet enjoyment of those properties. The property value of those units will certainly be degraded as will the commercial units to the north, such as my own, as potential customers consider the traffic too much of a hassle to navigate and take their business elsewhere.

I would urge the Planning and Zoning commission in the strongest terms to reject this application as incompatible to the constraints and considerations of this site.

Thank you, Sara Gorham

--

Sara Donart Gorham Associate Broker Windermere Sun Valley www.saragorham.com

Office: 208-622-2700 Mobile: 208-720-3797 Fax: 208-622-9100





IN RE:)	
)	KETCHUM PLANNING AND ZONING COMMISSION -
Lift Tower Lodge)	FINDINGS OF FACT, CONCLUSIONS OF LAW AND
Conditional Use Permit)	DECISION
)	
File Number: 15-006)	

BACKGROUND FACTS

PROJECT: Lift Tower Lodge Conditional Use Permit

FILE NUMBERS: 15-006

OWNER: Lift Tower Lodge, LLC or Blaine County Housing Authority (depending on timing)

REQUEST: Conditional Use Permit (CUP) for a public use

LOCATION: 703 South Main Street (Taxlot 7983)

ZONING: Tourist (T) and Recreational Use (RU) Zoning Districts

NOTICE:

2015 Hearing: Property owners within 300-foot radius of subject property and all properties on the Gem Streets were mailed notice on February 2, 2015. Notice was published in the Legal Notices of the Idaho Mountain Express on February 4, 2015; display ad published in the Idaho Mountain Express on February 11, 2015. Notice was posted on site on February 16, 2015.

August 22, 2016: Notice mailed to property owners within 300' radius of subject property on August 12, 2016. A public hearing notice was posted on site on August 12, 2016.

September 26, 2016: The following notice was mailed to property owners within a 300' radius of the subject property on August 25th, 2016. A public hearing notice was posted on site and in three public locations on August 26, 2016. Notice was published in the Idaho Mountain Express on Wednesday, September 7, 2016.

Meeting Date:	September 26, 2016 (continued from August 22, 2016)
Meeting Time:	5:30 PM, or thereafter as the matter can be heard.
Meeting Location:	City Hall Council Chambers, 480 East Avenue North, Ketchum, Idaho
Project Name:	Lift Tower Lodge
Project Location:	703 South Main Street (FR SWSW TL 7983 SEC 18 4N 18E Survey
	523394, 31-4208 Exempt App Rcvd 2016)
Applicant:	Blaine County Housing Authority (BCHA)
Application Type:	Reevaluation of a Conditional Use Permit (15-006)



Planning and Zoning

Regular Meeting

~ Minutes ~

480 East Avenue North Ketchum, ID 83340 http://ketchumidaho.org/

Keshia Owens (208) 726-7801

Monday, September 26, 2016	5:30 PM	Ketchum City Hall
Commissioners Present:	Steve Cook, Chairperson	
	Jeff Lamoureux, Commissioner	
	Steve Cook, Commissioner	
	Erin Smith, Commissioner	
	Betsy Mizell, Commissioner	
Staff Present:	Micah Austin, Director of Planning & Building	
	Brittany Skelton, Senior Planner	
	Keshia Owens, Planning Technician	
	Members of the Public	

1. 5:30 PM - CALL TO ORDER: City Hall, 480 East Avenue North, Ketchum, Idaho

Commissioner Cook called the meeting to order at 5:33 PM.

2. PUBLIC COMMENT - Communications from the public for items not on the agenda.

Keke Tidwell, Blaine County resident, said that Ketchum has a \$70,000 contract with the BCHA to provide services to them and the BCHA has had twenty-nine new applicants in the first half of 2016. She explained that about 61-71% of those applicants they lose contact with and in their quarterly report it stated that 15% of applicants were placed in affordable housing, which would be about five to six people. She communicated that the BCHA has 14 rental units in addition to the Lift Tower Lodge and the amount the City is spending is not very cost effective for tax payers.

3. COMMUNICATIONS FROM STAFF

a. The current use of the Lift Tower Lodge is long term rental of residential units operated by a public agency (BCHA), with a maximum term of stay not to exceed 1 year, for the purpose of providing transitional and seasonal housing for low-income members of the community. The initial reevaluation took place at the August 22, 2016 meeting. At that meeting the Commission and staff requested additional information from the applicant and the item was continued to the September 26, 2016 meeting.

Current Meeting:

Austin provided an introduction and said that the Lift Tower Lodge was last discussed about a month ago and at that time the Commission had some trouble with the Lodge's Conditional Use Permit.

Skelton reviewed the reevaluation of the Conditional Use Permit for the Lift Tower Lodge and said that based on the Findings of Fact and the recently submitted information Staff cannot affirm that the Lodge has been meeting the following conditions of approval:

- -Low income persons are to be adequately served
- -Person's in need of transitional housing are being adequately served

She then stated that the commission should determine whether the project is in compliance with the permit as approved. She noted that there are three options that can be considered:

- -Rejection of the CUP, at which the use could revert back to a motel
- -Request compliance within a given timeline; or
- -Impose conditions that would further compliance of the project

David Paitre, Executive Director of the BCHA, said that the Commission has another option which would be approving the project without any further conditions. He commented that there is a housing crisis in Ketchum and they are one of the few doing anything about it. He added that this may seem like it's a problem because of misconceptions and added that there have been no outside funds received by the BCHA. He explained that housing for out of towners was not a problem during the recession and said that it is now within their mission to house out of town construction workers. He then commented that they are asking the Commission for a strict review of their approval. He also detailed their previous approval, the categories of workers/people staying at the Lodge, and stated that they are serving the intended demographic.

Public comment:

- Kiki Tidwell said that the commission doesn't have any data to confirm what Paitre is saying. She added that the commission needs to read Blaine County'a reports because they slice and dice the information. She added that the 64% occupancy rate for a hotel is abysmal and you can't operate a hotel at this number. She later said that it is very important for the BCHA to post in the paper in order to meet low income housing needs and it is mindboggling that this has not been done.
- Harry Griffith, Sun Valley Economic Development, said that 64% occupancy is not unusual
 especially for lower income places. He added that we are at the beginning of a crisis in Ketchum
 when it comes to low income housing.
- Jae Hill, City of Sun Valley, said that we are seeing the tip of the iceberg because there are so many people from out of town needing a residence. He added that we look at the Lodge as essential for the community and as a great place for housing low income residents.
- Sean Macinte, Ketchum Resident, said that management is great but his concerns are for the low income, couch surfers. He said that the Lodge is key in creating more housing, but it should be for individuals and not for companies. He pointed out that a company will fill the room with whoever they want and not with someone who may actually need it.

Commissioner Lameroux made a motion and said that this project does meet the standards with the following conditions and to modify the conditions remove condition number six that says the CUP should be. Commissioner Smith seconeded and Commissioner Cook was opposed.

RESULT: ADOPTED [4 TO 1]

MOVER: Jeff Lamoureux, Betsy Mizell SECONDER: Erin Smith, Commissioner

AYES: Jeff Lamoureux, Erin Smith, Betsy Mizell

NAYS: Steve Cook

b. City-initiated Text Amendments to Title 17, Zoning Regulations amending Chapter 17.125, Off Street Parking and Loading to align the parking ordinance with objectives of the Comprehensive Plan, to promote uses that contribute to the vitality of downtown, and to incentivize Community Housing.

Current Meeting:

Austin said that staff heard the message loud and clear at the last meeting and took the Commission's concerns back and looked at five of the most recently approved projects. He said that staff ran various scenarios against these projects and found that Bob Crosby's concerns were very important.

Skelton covered both the residential parking changes and the parking changes that would be reflected in the amendment.

Public comment:

Bob Crosby, SVBR, said that the results as they have seen them in the revised proposal does exactly what staff would want it to do. He added that the results that they found were great because they promote the activities that the City wants. He also said that properties in residential development are carrying a much heavier burden.

Directives:

Regulating vehicle headlights may not be effective

The ordinance may be a little bicycle crazy

The Commission is in favor of parking on the street

There should be an overall master plan

Commissioner Smith motioned to continue the item to October 24 and Commissioner Mizell seconded.

RESULT: ADOPTED [UNANIMOUS]

MOVER: Erin Smith, Commissioner

SECONDER: Betsy Mizell, Commissioner

AYES: Cook, Lamoureux, Smith, Mizell

4. CONSENT CALENDAR

a. Findings:

i. Foxhole Final Plat

Current Meeting:

Commissioner Lamoureux motioned to approve the Foxhole Townhomes Final Plat Findings of Fact and Commissioner Mizell seconded.

b. Minutes:

i. August 22, 2016

Current Meeting:

Page 1 at the bottom, paragraph 3a.

Correct the names of Kiki Tidwell and David Patrie

ii. September 12, 2016

Current Meeting:

Page 1

Commissioners present- Steve is mentioned twice.

3a.

Applicant requested permission for four large barbs.

Page 3

Public comment, 9 lines down, clarify sentence

Page 4

Dennis Hanggi

Kathy Guerkey

Commissioner Cook made a motion to approve the minutes from August 22, 2016 and September 26, 2016 as revised. Commissioner Smith seconded.

5. FUTURE PROJECTS AND NOTICING REQUIREMENTS

a. Waterways Design Review and Floodplain Development Permit – Belling driveway and landscaping, 530 Wood River Drive.

Skelton said that the Belling residence is 100% within the floodplain and it also has a flood way and riparian setback.

6. STAFF REPORTS & CITY COUNCIL MEETING UPDATE

Austin said that we have hired a new planner who will be starting on October 10, 2016.

7. Commission reports and ex parte discussion disclosure

Commissioner Lamoureux asked if there was a time limit to how long someone can speak in our ordinance.

8. ADJOURNMENT

Commissioner Smith Adjourned.

Project Description: The current use of the Lift Tower Lodge is long term rental of

residential units operated by a public agency (BCHA), with a maximum term of stay not to exceed 1 year, for the purpose of transitional and seasonal housing. The initial reevaluation took place at the August 22, 2016 meeting. At that meeting the Commission and staff requested additional information from the applicant and the item was continued to the September 26, 2016 meeting. The property is 0.68 acres in size and zoned Tourist (T) and Recreational Use (RU).

FINDINGS OF FACT

- 1. A Conditional Use Permit for the Lift Tower Lodge to operate as a public use was approved on February 23, 2015 subject to the Findings of Fact adopted March 9, 2015. Condition #6 stated, "This Conditional Use Permit shall be reevaluated in a public hearing by the Planning and Zoning Commission for compliance with all of the KMC Section 17.116.030 CUP evaluation standards and compliance with the above conditions of approval twelve (12) months from the date of the Findings of Fact." The Commission determined that this Condition should be removed.
- 2. Condition #5 states, "This Conditional Use Permit approval is based on representations made and other components of the application presented and approved at the meeting on the date noted herein." As such the Commission reevaluated the Conditional Use Permit for compliance with Ketchum Municipal Code Section 17.116.030, the conditions adopted in the Findings of Fact, and all other components of the application presented and approved at the meeting on February 23, 2015, including the narrative submitted with the original application and the House Rules.
- 3. On August 22, 2016, the Commission reevaluated the Conditional Use Permit. The Commission heard from members of the public who raised concerns about quiet hours and outdoor social activities and the demographic makeup of occupants. David Patrie, Executive Director of Blaine County Housing Authority and representative for the application, addressed concerns about quiet hours and enforcement of the House Rules and stated that rooms have been rented in blocks to the construction company building the Limelight Hotel for use by construction workers and that rooms have also been rented to traveling nurses, seasonal occupants, and people needing transitional housing. Additionally, Mr. Patrie stated that individuals renting a room pay a rate based on their income and rooms rented to a company are charged a flat rate.
- 4. The Commission's deliberation at the August 22, 2016, meeting the Commission discussed whether the practice of renting blocks of rooms to companies, rather than renting exclusively to individuals, met the intent of the Conditional Use Permit as approved and if rooms have been rented to individuals who are not low income, whether such practice met the intent of the permit as approved. The applicant submitted additional information and considered the following for the September 26, 2016, meeting:
 - i. Records for length of stay;

- ii. Records detailing rooms rented to individuals and companies; and
- iii. Reevaluate the quiet hours.

A spreadsheet and graph summarized lengths of stay was submitted by the applicant and was included in Attachment C, records detailing quantities of rooms rented to individuals and rooms rented to companies were not provided. BCHA provided information on revisions to the House Rules, including changing the start of quiet hours from 10:00 p.m. to 9:00 p.m. and prohibiting outdoor gatherings after 9:00 p.m. via e-mail, included as Attachment D.

- 5. Additional conditions of approval of the Conditional Use Permit on February 23, 2015 were as follows:
 - a. Ketchum City Engineer, Streets, Utilities, Fire and Building Department requirements shall be met, including:
 - i. The Fire Department will require strict adherence to the Lift Tower Lodge House Rules and Regulations Tenant Guide;
 - ii. The smoke detectors are the owners' responsibility to maintain and as noted shall not be disabled or tampered with at any time;
 - iii. No cooking devices other than the microwaves will be allowed in the rooms at any time;
 - iv. Barbeques and other open flame cooking devices are required to be at least 10 feet from the building and may not be used on combustible decks or balconies;
 - v. A minimum of one 10-pound fire extinguisher is required in each rental room and in the manager's apartment; and
 - vi. The Lodge shall post a notice informing residents of snow removal operations and the associated noise.
 - b. An on-site manager residing in the two-bedroom apartment on-site or a BCHA representative shall be available 24 hours per day;
 - c. Per Title 17, Section 17.116.080: TERM OF PERMITS: Conditional Use Permit approval shall expire one (1) year from the date of approval if not acted upon within that time frame;
 - d. The maximum term of stay for any occupant other than the on-site manager shall be limited to one year;
- 6. In the August 22, 2016, staff report, and prior to the public hearing on August 22, 2016, staff found that The Lodge was in use as requested and approved in the original application; BCHA addressed compliance with Conditions 1, 2, 3 and 4 as reported in the letter submitted by David Patrie dated August 17, 2016.

7. After the Commission's deliberation at the August 22, 2016, meeting staff further reviewed the Findings of Fact for approval of the Conditional Use Permit and found that the project was required to serve low-income persons. This was stated in the narrative supplied by the BCHA with the original application in addition to references to serving first responders, temporary workers, and seasonal workers.

Blaine County Housing Authority's "Narrative to Support Conditional Use Permit Application":

References to serving a broad demographic in the narrative include:

- Regarding topics discussed at a neighborhood meeting hosted by BCHA on December 9, 2014: "4. We discussed the targeted resident demographic. We are not able to develop an exclusive list because that would unnecessarily limit our ability to react and adapt to the changing needs of the community. The current need, and the demographic we intend to serve, is low-income workers, first responders, temporary workers and seasonal workers." (p. 1)
- Regarding allowing occupancy beyond 30 days: "BCHA does not believe The Lift Tower Lodge is a permanent solution for residents. We actively work with each resident to identify a permanent housing solution. It is very uncommon to find a permanent housing solution in less than 30 days. For example, the current waiting list for a Housing Choice Voucher from IHFA is 2 years. Additionally, some of the workers who come to us are seasonal and are only seeking housing for the season, typically three or four months."

References to serving low-income persons in the narrative include:

- Regarding the status of the Lift Tower Lodge after the December 9, 2014 neighborhood meeting and prior to the February 22, 2015 public hearing: "Since the neighborhood meeting, we have donated rooms to Higher Ground and the Idaho School for the Deaf and Blind to house students and chaperones that came to Sun Valley for a ski program. We are also renting rooms, on a limited basis in accordance with #6 above, to low-income workers. These residents tend to be very low-income (\$8-\$9/hr.) working in the food service and other resort sectors. They have access to cafeterias and other food service options. Previously, these residents were staying on friends or relatives' couches, sleeping in cars or living in studio apartments with several other individuals. The Lift Tower Lodge has provided these workers with a better alternative." (p. 2)
- Regarding the 30-day rental restriction for operating exclusively as a hotel: "The 30-day limit unnecessarily limits BCHA's effectiveness in fulfilling our mission and the benefits that could be realized by the community. It limits the highest and best use of the property with respect to bridging the gap between low incomes and high rents that are typical in resort economies." (p. 2)
- Regarding proposed operation as a public use: "In Section 31-4202(c) the Idaho Legislature declares that "...the providing of safe and sanitary dwelling accommodations for persons of low income are public uses and purposes for which public money may be spent and private property acquired and are governmental functions." (emphasis added [by applicant]) There should be no doubt that BCHA's proposed use of The Lift Tower Lodge is a public use. By granting BCHA a conditional use permit to operate The Lift Tower Lodge as a public use, the City of Ketchum will allow residents to occupy a room for more than 30 days and allow BCHA to fulfill its obligation to the workers of Blaine County." (p. 2)

- 8. Most of the subject property is located in the Tourist Zoning District with the southeastern corner of the lot zoned Recreational Use. That corner of the property contains mostly landscaping, some paved area and an out-building. The Lift Tower Lodge building is located on the portion of the property zoned Tourist. Public Uses are permitted with a Conditional Use Permit in both the Tourist and Recreation Use zones.
- 9. Ketchum Municipal Code defines public use as "a structure or use intended or used for a public purpose by a city, other than the city of Ketchum, a school district, the county, the state, or by any other public agency, or by a public utility."
- 10. Prior to BCHA receiving and operating the property the Lift Tower Lodge operated as a motel, which are limited to short term occupancy, defined in Ketchum Municipal Code as "rental or lease of any unit or structure or portion for a period of not more than thirty (30) days." Because of BCHA's status as a public agency, and because BCHA proposed operation of the Lift Tower Lodge as a public use, the Conditional Use Permit application requesting the ability to rent rooms for up to 365 days was eligible for consideration; an individual or private company would not have been eligible to request to rent motel rooms for longer than 30 days using the Conditional Use Permit procedure because individuals and private companies are not public agencies.
- 11. As a result of the August 22, 2016, meeting, based on the information submitted by BCHA and the Findings of Fact for the Conditional Use Permit staff did not affirm that the Conditional Use Permit is meeting the following objectives of the approval:
 - a. Low-income persons are being adequately served; and
 - b. Persons in need of transitional housing are being adequately served.
- 12. The Commission should evaluate the information submitted and determine whether the project is in compliance with the Conditional Use Permit. If the Commission determines non-compliance they may consider the following options:
 - a. Rejection of the CUP, at which point the use could revert back to a motel;
 - b. Request compliance within a given timeline; or
 - c. Impose conditions that would further compliance of the project.

Conditional Use Requirements		
1. EVALUATION STANDARDS: 17.116.030 A conditional use permit shall be granted by the commission only if the applicant demonstrates that:		
Compliant	Standards and Staff Comments	

Yes	No	N/A	Guideline	City Standards and Staff Comments	
\boxtimes			17.116.030(A)	The characteristics of the conditional use will not be unreasonably	
			CONDITIONAL	incompatible with the types of uses permitted in the applicable zoning	
			USE	district.	
			Staff Comments	The conditional use of an extended stay length motel for low-income	
				and seasonal resident and transitional housing is not unreasonably	
				incompatible with the allowed uses in the Tourist zoning district, which	
				include hotels, motels, lodges, tourist homes and tourist housing	
				accommodations.	
				In light of deliberation at the August 22, 2016, meeting the	
				Commission considered whether the current operations of the Lift	
				Tower Lodge best meet the definition and intent of "public use"; it is	
				BCHA's status as a public agency operating the Lift Tower for a public	
				use that allows BCHA to rent rooms in excess of the 30-day time period	
				private hotels, motels, and other lodging establishments are limited to.	
				The Commission considered the following points: whether renting	
				blocks of rooms to companies at a flat rate serves, or best serves, the	
				intent of a public use, and the degree to which renting to low income	
				persons should be required.	
			17.116.030(B)	The conditional use will not materially endanger the health, safety and	
\boxtimes				welfare of the community.	
			Staff Comments	The proposed use is very similar to the prior use at the property and to	
				allowed uses in the Tourist zoning district. The Fire Chief placed	
				conditions of approval, in line with the House Rules and Regulations,	
				that limit use of cooking devices and barbeque grills and require	
				maintenance of smoke detectors and fire extinguishers. These were	
				included in Condition of Approval #1 and as stated in BCHA's letter	
				dated August 17, 2016, the conditions have been adhered to.	
\boxtimes			17.116.030(C)	The conditional use is such that pedestrian and vehicular traffic associated	
	ш			with the use will not be hazardous or conflict with existing and anticipated	
				traffic in the neighborhood.	
			Staff Comments	The conditional use has not increased the pedestrian and vehicular	
				traffic impacts above those created by the prior motel use. The site	
				has more than adequate parking and has adequate access to Highway	
				75. There is adequate space for vehicles to turn around without	
				impacting Highway 75 traffic.	
\boxtimes			17.116.030(D)	The conditional use will be supported by adequate public facilities or	
	_			services and will not adversely affect public services to the surrounding area	
				or conditions can be established to mitigate adverse impacts.	
			Staff Comments	Public utilities, emergency and essential services are available and can	
				serve the subject property. City departments have reviewed the	
				proposal and have responded with no concerns for adequately serving	
				the project.	
\boxtimes			17.116.030(E)	The conditional use is not in conflict with the policies of the Comprehensive	
	_			Plan or the basic purposes of this Section.	

Staff Comments	Goals in the 2014 Comprehensive Plan include:	
	Goal H-1: Ketchum will increase its supply of homes, including rental and special-needs housing for low-, moderate- and median-income households.	
	 Policy H-1.1: Affordable Housing Monitoring Policy H-1.2: Local Solutions to Attainable Housing Policy H-1.3: Integrated Affordable Housing in Neighborhoods Policy H-1.4: Integrated Housing in Business and Mixed-Use Areas 	
	Goal H-2: The Ketchum community will support affordable housing programs. Policy H-2.1: Blaine County Housing Authority, ARCH Community Housing Trust and Ketchum Community Development Corporation (The City will partner with the above organizations to fulfill housing goals.)	
	• Goal H-3: Ketchum will have a mix of housing types and styles. BCHA's current operations of the Lift Tower Lodge, which include serving low-income persons and seasonal workers and persons in need of transitional housing who may or may not have low-incomes, are largely in alignment with goals H-1, H-2 and H-3.	

CONCLUSIONS OF LAW

- 1. The City of Ketchum is a municipal corporation organized under Article XII of the Idaho Constitution and the laws of the State of Idaho, Title 50, Idaho Code.
- 2. Under Chapter 65, Title 67 of the Idaho Code, the City has passed a land use and zoning ordinance, Title 17.
- 3. The Planning and Zoning Commission has authority to hear the applicant's Conditional Use Permit application pursuant to Idaho Code Section 67-6512 of the Local Land Use Planning Act and Chapters 17.52, 17.80 and 17.116 of Ketchum Zoning Code Title 17.
- 4. The public hearing and consideration of the applicant's Conditional Use Permit application was properly noticed pursuant to the Local Land Use Planning Act, Idaho Code Section 67-6512.
- 5. The application does comply with Ketchum Zoning Code Title 17 and the Ketchum Comprehensive Plan.

DECISION

THEREFORE, the Ketchum Planning and Zoning Commission **approves** this Conditional Use Permit application this 26th day of September, 2016 provided the following conditions are met:

- 1. Ketchum City Engineer, Streets, Utilities, Fire and Building Department requirements shall be met, including:
 - The fire department will require strict adherence to the Lift Tower Lodge House Rules and Regulations Tenant Guide;
 - The smoke detectors are the owner's responsibility to maintain and as noted shall not be disabled or tampered with at any time;
 - No cooking devices other than the microwaves will be allowed in the rooms at any time;
 - Barbeques and other open flame cooking devices are required to be at least 10 feet from the building and may not be used on combustible decks or balconies;
 - A minimum of one 10-pound fire extinguisher is required in each rental room and in the manager's apartment; and
 - The Lodge shall post a notice informing residents of snow removal operations and the associated noise;
- An on-site manager shall reside in the two (2) bedroom apartment at the Lift Tower Lodge and the manager and/or a BCHA representative shall be available twenty-four (24) hours per day;
- 3. Per Title 17, Section 17.116.080: TERM OF PERMITS: Conditional Use Permit approval shall expire one (1) year from the date of approval if not acted upon within that time frame;
- 4. The maximum term of stay at the Lift Tower Lodge for any occupant, except the on site manager, shall be limited to one year;
- This Conditional Use Permit approval is based on representations made and other components of the application presented and approved at the meeting on the date noted herein; and
- 6. This Conditional Use Permit shall be reevaluated in a public hearing by the Planning and Zoning Commission for compliance with all of the KMC Section 17.116.030 CUP evaluation standards and compliance with the above conditions of approval twelve (12) months from the date of the Findings of Fact.

Findings of Fact adopted this 10 th day of October, 2016.	
	Chara Caali Waa Chair
	Steve Cook, Vice Chair
	Planning and Zoning Commission