



**PLANNING AND ZONING COMMISSION AGENDA**

**Thursday, June 12, 2017**

**Ketchum City Hall**

**480 East Avenue North, Ketchum, ID 83340**

1. **5:15 PM - SITE VISIT: Fire Training Center 219 Lewis Street Ketchum, Idaho (Northgate Sub Lot 7 Block 1)**
2. **5:30 PM - CALL TO ORDER: City Hall, 480 East Avenue North, Ketchum, Idaho**
3. PUBLIC COMMENT - Communications from the public for items not on the agenda.
4. COMMUNICATIONS FROM STAFF
  - a. *Continued from May 23, 2017, Sun Valley Dental Arts Design Review: 100 7th Street East Ketchum, Idaho (Ketchum Townsite, Lot 5, Block 34). The Commission will consider and take action on an application by Ben Franz for Design Review approval of a new mixed use three-story building with ground floor commercial space with two residential units on the second floor and one residential unit on the third floor.*
  - b. Fire Training Center Pre-Application Design Review: 219 Lewis Street Ketchum, Idaho (Northgate Sub Lot 7 Block 1). The Commission will consider an application by the City of Ketchum for a newly proposed 1,280-square foot fire training center on a 12,000-square foot lot.
  - c. *Continued from April 10, 2017 and May 8, 2017, Public Hearing, Zoning Code Amendments: City-initiated text amendments to Chapter 17.08, Definitions, Section 17.12.020, District Use Matrix, and Chapter 17.124, Development Standards to amend regulations for motor vehicle fueling stations in order to protect the health, safety and welfare of the public and to align regulations for the use with the Comprehensive Plan.*
5. CONSENT CALENDAR
6. FUTURE PROJECTS AND NOTICING REQUIREMENTS
7. STAFF REPORTS & CITY COUNCIL MEETING UPDATE
8. COMMISSION REPORTS AND EX PARTE DISCUSSION DISCLOSURE
9. ADJOURNMENT

*Any person needing special accommodations to participate in the meeting should contact the City Clerk's Office as soon as reasonably possible at 726-3841. All times indicated are estimated times, and items may be heard earlier or later than indicated on the agenda.*



City of Ketchum  
Planning & Building

**STAFF REPORT**  
**KETCHUM PLANNING AND ZONING COMMISSION**  
**REGULAR MEETING OF June 12, 2017**

**PROJECT:** Sun Valley Dental Arts

**FILE NUMBER:** #17-010

**OWNERS:** Ben Franz

**REPRESENTATIVE:** Jeff Mathis - Architect

**REQUEST:** Design Review approval for a new three story mixed-use residential and commercial building.

**LOCATION:** (Ketchum Block 34 Lot 5)

**ZONING:** Community Core sub district Traditional Neighborhood (CC-D)

**OVERLAY:** None

**NOTICE:** Notice was mailed to Property owners within 300 foot radius of subject property were mailed notice on May 9<sup>th</sup>, 2017 for a public hearing on May 23, 2017 at 5:30pm. Notice was posted in three public City locations on May 9<sup>th</sup>, 2017.

**REVIEWER:** Carl Anderson, Associate Planner

**STAFF RECOMMENDATION:**

Staff recommends continuing the application to the July 10<sup>th</sup>, 2017 Planning and Zoning Commission meeting.

**ATTACHMENTS:**

- A. Application

BACKGROUND

The site property is located in the Community Core Traditional Neighborhood Sub-district (CC-D). The lot is currently is 5,501 square feet and is located at Ketchum Block 34 Lot 5. The applicant is proposing to construct a three-story mixed use building, which will have a total square footage of 18,672 square feet, and a Floor Area Ratio (FAR) of 2.15 (11,844 sf/5,500 sf). The mixed-use of the proposed building is proposed to contain a Business Office (dental office) on the ground floor, two (2) residential units on the second floor, and one (1) residential unit on the third floor.

The proposed building received Pre-Application Design Review approval on October 26<sup>th</sup>, 2015. The approval was for a 14,376 square foot mixed-use residential and commercial building. The approval expired one year from the approval date and no request to extend the approval was submitted to the City of Ketchum Planning and Building Department. The requirement for Pre-Application Design Review has been waived for this project.

The Design Review application for the Sun Valley Dental Arts project was continued from the May 23<sup>rd</sup>, 2017 P&Z special meeting to the June 12<sup>th</sup>, 2017 P&Z meeting. The applicant has requested that the application be continued from the June 12<sup>th</sup> regular meeting to the next Planning & Zoning Commission regular meeting on July 10<sup>th</sup>, 2017.

COMMISSION OPTIONS

1. **Continuation of the Application:** "Motion to continue the Design Review application from owner Dr. Ben Franz, located at Ketchum Block 34 Lot 5, to a date certain of July 10<sup>th</sup>, 2017."

RECOMMENDED CONDITIONS

None at this time.



City of Ketchum  
Planning & Building

STAFF REPORT  
KETCHUM PLANNING AND ZONING COMMISSION  
SPECIAL MEETING OF June 12<sup>th</sup>, 2017

**PROJECT:** Fire Training Center

**FILE NUMBER:** #17-059

**APPLICANT:** City of Ketchum – Fire Department

**REPRESENTATIVE:** Ketchum Sun Valley Volunteer Firefighters Association (KSVVA)

**OWNER:** City of Ketchum

**REQUEST:** Pre-Application Design Review approval for a new fire training facility to be used for Public Use.

**LOCATION:** 219 Lewis Street  
(Northgate Subdivision Lot 7 Block 1)

**ZONING:** Light Industrial district number 2 (LI-2)

**OVERLAY:** None

**NOTICE:** Notice was mailed to property owners within a 300 foot radius of subject property and posted in three public City locations on May 30<sup>st</sup>, 2017. A second public notice was mailed to all property owners within a 1,500 foot radius of the subject property on June 1<sup>st</sup>, 2017.

**REVIEWER:** Carl Anderson, Associate Planner

BACKGROUND

The site property is located in the Light Industrial zoning district number 2 (LI-2). The Northgate Subdivision Lot 7 Block 1 is 12,000 square feet. The applicant is the City of Ketchum Fire Department, represented by the Ketchum Sun Valley Volunteer Firefighters Association (KSVVA). The property is owned by the City of Ketchum and is currently used as a public use, the proposed fire training center is a public use and therefore will be a continuation of a public use. The project qualifies for Pre-Application Design Review which is required for all new nonresidential construction. The purpose of Pre-Application Design Review is to allow the Commission to exchange ideas and give direction to the applicant on the “design concept”, keeping in mind the purpose of this chapter and the application of the evaluation standards

The applicant is proposing to construct a new three-story fire training facility, which will have a fourth roof attached level to serve as a rappel tower. The building is made up of three floors of single use storage containers, with each container being 8'-6" in height. The proposed Training Center contains four (4) total containers, with 3-40' containers at the first and second level, and 1-20' container at the third level. The



proposed structure complies with the height limitations established for the Light Industrial number two (LI-2) zoning district. The total lot coverage of the proposed building is 8% and is located at the center of the 12,000 square foot lot. The proposed building will have 1,280 square feet of enclosed space and 656 square feet of exterior deck space. As proposed, the structure will have a front setback of 50', a side setback of 24' on the north side, a side setback of 40' on the south side, and a 56' setback at the rear of the lot.

The proposed Fire Training Center has been designed by Fire Training Structures LLC (FTS). FTS states that the structures are constructed utilizing ISO Containers, or shipping containers. FTS states that these structures are specifically designed for live-fire training exercises, which will utilize Class "A" (wood, straw, etc.) or Liquid Petroleum Gas fuel sources. FTS states that the proposed fuel sources have been tested and certified by a Nationally Recognized third party testing laboratory. The applicant states that the fuels utilized in the live fire training and are capable of producing very little smoke. The Liquid Petroleum Gas provides a clean burn with little smoke output and are quick to set up and close down. Class A fires are highly variable in their smoke production, dependent on draft, and fuel, and weather conditions. The applicant states that Class A operations are capable of producing very little smoke when well ventilated and free burning, however, these operations can produce more smoke when oxygen is limited.

#### ANALYSIS

Staff recommends that the Pre-application for the Fire Training Center be advanced to Design Review. A full analysis and explanation of this recommendation is detailed within the staff report. The following items will need to be addressed by the applicant prior to Design Review approval.

#### Outstanding action items:

1. The City of Ketchum will need to secure a location for the existing impound lot and storage area currently located on the subject property. This will need to occur before the project files a Design Review application. See department comment detailed in table 1.
2. The applicant shall coordinate with the Public Works Department to address all concerns detailed in Table 1.
3. 17.96.060.I Landscaping & 17.124.100.A Landscaped Yards, Light Industrial Districts
  - a. The applicant shall comply with the established landscaping requirements established in City Municipal Code, Chapter 17, Sections 17.96.060.I and 17.124.100.A.
  - b. Because the public use is a fire fighter training facility, periodically the structure will be burned, and training operations consistent with fire fighting will occur. The Design Review application should include landscaping to provide a buffer between this use and adjacent land uses, such as the YMCA that is adjacent to the rear property line.
4. 17.124.130. C. Fences, Hedges and Walls: In all other districts, fences, hedges and walls shall not exceed four feet (4') in height when located less than thirty feet (30') from the front lot line and shall not exceed six feet (6') in height when located more than thirty feet (30') from the front lot line.
  - a. The applicant has proposed a six foot (6') within thirty feet (30') of the front lot line which is not permitted.
5. 17.132 Dark Skies
  - a. Any new and existing exterior lighting on the property shall be in compliance with Ketchum Municipal Code, Chapter 17.132, Dark Skies.
6. 17.96.040 Construction management plan showing where staging will occur, where materials are stored, and a parking plan for contractors is required. If any staging or parking shall occur off site, a staging/parking plan must be submitted including materials storage, excavation (backfill) stockpile areas, job trailers, blue rooms, dumpsters and contractor parking.

#### PRE-APPLICATION

The Commission must consider Pre-Application Design Review of the Fire Training Center application as it relates to the criteria used for evaluating such applications and in the context of exchanging ideas and giving direction

to the applicant on the “design concept.” The Commission has the option of moving the application forward to Design Review or continuing the Pre-Application Design Review discussion to a subsequent meeting. If the Planning and Zoning Commission chooses to move the application forward to Design Review, staff recommends requiring any design changes or conditions the Commission would like to see. The commission may require additional conditions based on findings received through public comment, testimony, or other discovery.

Design Review approval may be granted by the Commission only if the applicant demonstrates that:

- The project does not jeopardize the health, safety or welfare of the public.
- The project conforms to all applicable standards and criteria as set forth in this chapter, 17.96: Design Review, and any other standards as adopted or amended by the City of Ketchum from time to time.

The Commission may allow the applicant to move forward with Design Review and may attach additional conditions to approval as it determines necessary to ensure the health, safety, or welfare of the public. All conditions must cite the appropriate standard for imposing such condition. Such conditions include, but are not limited to:

- Ensuring compliance with applicable standards.
- Requiring conformity to approved plans and specifications.
- Requiring security for compliance with the terms of the approval.
- Minimizing adverse impact on other development.
- Controlling the sequence, timing and duration of development and ongoing maintenance.
- Requiring more restrictive standards than those generally found in the Ketchum Municipal Code.

#### COMPREHENSIVE PLAN ANALYSIS

The proposed new construction of the Fire Training Center is consistent with the uses, goals and policies listed below as specified within the 2014 Comprehensive Plan. Staff finds that the proposed Fire Training Facility will generate infrequent traffic and will have an impact comparable to the primary uses listed below. The impact of the facility is contextually appropriate to the Lighting Industrial-two (LI-2) zoning district.

SUPPORTING SECTION	SUMMARY OF COMPLIANCE OF PROPOSED AMENDMENTS WITH THE 2014 COMPREHENSIVE PLAN
<b>Future Land Use</b>	
<b>Mixed-Use Industrial</b>	<p><b>Primary Uses:</b> Light manufacturing, wholesale, services, automotive, workshops, studios, research, storage, construction supply, distribution and offices make up the bulk of development with this district.</p> <p><b>Secondary Uses:</b> A limited range of residential housing types, and supporting retail are provided for within this category. Uses should generate little traffic from tourists and the general public.</p> <p><b>Characteristics and Location:</b> The Mixed-Use industrial category is intended to provide critical lands for Ketchum’s economic growth and entrepreneurial opportunity within a vibrant business district where people can work and live in the same area.</p>
<b>Community Design and Neighborhoods</b>	
<b>Policy CD-1.3 Compatible Infill and Redevelopment Projects</b>	<p>Infill and redevelopment projects should be contextually appropriate to the neighborhood and development in which they will occur. Context refers to the natural and manmade features adjoining a development site; it does not imply a certain style.</p>

#### STAFF RECOMMENDATION

Staff recommends that the advancement of the application for the Fire Training Center to Design Review.

#### COMMISSION OPTIONS

1. **Advance the application to Design Review:** “Motion to advance the application from the applicant for the City of Ketchum Fire Department for the Pre-application Design Review application of the Fire Training Center to Design Review, finding the application meets the standards for approval under Chapter 17.96 of Ketchum Zoning Code, with the following conditions/design changes: [insert conditions of approval here]”
2. **Continuation of the Application:** “Motion to continue the application from the applicant City of Ketchum Fire Department for Pre-Application Design Review for the Fire Training Center to a date certain of [insert date of meeting] in order to address the following design changes [Commission to insert design changes]”.

#### RECOMMENDED CONDITIONS for Pre-Application Design Review

Ketchum City Engineer, Streets, Utilities, Fire, Planning and Building Department requirements shall be met, including:

1. All departmental conditions as described in Table 1, 2 and 3;
2. All building and fire code requirements as dictated by 2012 family of international codes shall apply to all construction onsite;
3. **TERM OF APPROVAL:** The term of Pre-Application design review approval shall be twelve (12) months from the date that findings of fact, conclusions of law and decision are adopted by the Commission or upon appeal, the date the approval is granted by the Council subject to changes in zoning regulations;
4. All exterior lighting on the property shall be in compliance with Ketchum Municipal Code, Chapter 17.132, Dark Skies, and approved prior the issuance of a Certificate of Occupancy;
5. The applicant shall coordinate with the Public Works Department to determine the final sidewalk configuration around the site, prior to the issuance of a Building Permit;
6. The City of Ketchum will need to secure a location for the existing impound lot and storage area currently located on the subject property. This will need to occur before the project files a Design Review application.

#### ATTACHMENTS:

- A. Table 1. Requirements for All Applications
- B. Table 2. Zoning Standards Analysis
- C. Table 3. Design Review Standards
- D. Application
- E. Plans, dated: 05/24/2017
- F. Revised plans, dated: 6/7/2017
- G. Revised materials board, received: 6/8/17
- H. Public Comment received as of 5:00pm Tuesday June 6<sup>th</sup>, 2017

## Attachment A.

Table 1: Requirements for All Applications

Compliant			City Department Comments	
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.040	Complete Application
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<b>Police Department:</b> Currently the relocation of the impound lot has not been finalized.	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>Fire Department:</b> <b>It is the General Contractor's responsibility to understand and adhere to all Fire Protection Ordinance #1125 requirements in addition to any and all other City of Ketchum requirements in effect at the time of Building Permit issuance. Failure to comply with all local ordinances and codes may result in project work stoppage as well as criminal penalties.</b> The above project shall meet all 2012 International Fire Code requirements in addition to specific City Building and Fire Ordinances.  Approved address numbers shall be placed in such a position to be plainly visible and legible from the road fronting the property. Numbers and letters shall be a minimum of four (4) inches tall, contrast with their background and be positioned a minimum of forty-eight (48) inches above final grade.  Vehicle parking and material storage during construction shall not restrict or obstruct public streets or access to any building. A minimum twenty-foot travel lane for emergency vehicle access shall be maintained clear and unobstructed at all times. All required Fire Lanes, including within 15 feet of fire hydrants, shall be maintained clear and unobstructed at all times.  An approved access roadway per 2012 International Fire Code Appendix D ( <a href="http://www.ketchumfire.org">www.ketchumfire.org</a> ) shall be installed prior to any combustible construction on the site. The road shall be a minimum of twenty (20) feet in width and capable of supporting an imposed load of at least 75,000 pounds. The road must be an all-weather driving surface maintained free, clear, and unobstructed at all times.  Fire extinguishers shall be installed and maintained per 2012 IFC Section 906 both during construction and upon occupancy of the building.  Final inspections of all fire department required installations by the Fire Chief or an appointee are required and shall be scheduled at least 48 hours in advance. A Final Inspection Checklist can be found at <a href="http://www.ketchumfire.org">www.ketchumfire.org</a> .	
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<b>Public Works:</b> 1) The proposed plans do not show an impound lot. A plan to relocate the impound lot is required. 2) Proposed curb ramp-The driveway cut in the sidewalk represents an 8.3% cross slope in the 5 foot sidewalk. The proposed curb ramp must meet ADA requirements. Most of Lewis and Northwood streets have rolled curb to eliminate the need for a curb cut. This item needs additional input from staff or engineering.	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>Utilities:</b> The proposed fire hydrant, isolation valve, fittings, and pipe can be purchased by a private contractor or through the Public Works/ Water division. The hydrant will be installed by a private contractor.	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>Parks/Arborist:</b> The proposal won't affect the public access between the YMCA and Lewis Street. If that is the case, no additional comment.	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>Building:</b> The building must meet 2012 International Building Code.	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<b>Planning and Zoning:</b>	

Comments are denoted throughout the staff report.

**Attachment B.**  
**Table 2: Zoning Standards Analysis**

Compliance with Zoning Standards				
Compliant			Standards and Staff Comments	
Yes	No	N/A	Guideline	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	<b>Minimum Lot Area</b>
			<i>Staff Comments</i>	<b>Required:</b> 8,000 square feet minimum. <b>Existing:</b> 12,000 square feet
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	<b>Building Coverage</b>
			<i>Staff Comments</i>	<b>Permitted:</b> 75% <b>Proposed:</b> 8% (960 sf / 12,000 sf)
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	<b>Minimum Building Setbacks</b>
			<i>Staff Comments</i>	<b>Required:</b> Front: 20' Side: 0' for internal side yards and a minimum of 10' for street side yards. Rear: 1' for every 3' in building height, or 10'.  (Note: 1. If the lot adjoins a more restrictive district on the side or rear, the more restrictive setbacks of that district shall apply.) Rear adjoins the Tourist zoning district which establishes a minimum rear setback of the greater of 1' for every 3' in building height, or 10'.  <b>Proposed:</b> Front (Lewis St.): 50' Side: 24' Side: 40' Rear: 56' (The container located at the northwest corner is setback 10' based on the 8' height of the container. The LPG tank is setback 10'.)
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.12.030	<b>Building Height</b>
			<i>Staff Comments</i>	<b>Required:</b> 35'  <b>Proposed:</b> 34'-6" (38" at the top of the guard rail.) The Fire Training Center is proposed to be 34'-6" in height from grade to top of the rappel deck. The guard rail is 42" (3'-6") tall which extends the total height to 38'. Staff finds that the overall height of the structure is under the maximum height permitted in the LI-2 zoning district as defined by City code title 17.08.020. Staff finds that the proposed guard rail will have a similar impact on the overall bulk of the training facility as an antenna or chimney, and is not subject to the overall height of building, provided that the guardrail does not exceed the Osha minimum guardrail height.  City code title 17.08.020 – Definitions HEIGHT OF BUILDING states that the building height provision shall apply to parapets, boston roofs and any other portion of a building roof, but shall not apply to flagpoles, lightning rods, weather vanes, antennas or chimneys.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.125.030.H	<b>Curb Cut</b>
			<i>Staff Comments</i>	<b>Required:</b> A total of 35% of the linear footage of any street frontage can be devoted to access to off street parking. <b>Proposed:</b> 18'/80' of linear footage or 22.5%. The applicant has satisfied this requirement, however, the Public works Department has concerns regarding the proposed curb

			<p>ramp. The applicant shall provide a rolled curb per Public Works Comment copied below.</p> <p><b>Public Works Comment: "Proposed curb ramp-</b> The driveway cut in the sidewalk represents an 8.3% cross slope in the 5 foot sidewalk. The proposed curb ramp must meet ADA requirements. Most of Lewis and Northwood streets have rolled curb to eliminate the need for a curb cut. This item needs additional input from staff or engineering."</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p><b>17.125.020.A.2 &amp; 17.125.050</b></p> <p><b>Parking Spaces</b></p> <p><i>Staff Comments</i></p> <p>Off-street parking standards of this chapter apply to any new development and to any new established uses.</p> <p><b>Required:</b>  <b>Public Use:</b> 1 space per 1,000 gross square feet.  <b>Gross square feet:</b> 1,280 / 1,000 = 1 parking space</p> <p><b>Proposed:</b>  The applicant has not designated a parking space on-site as the property will be able to accommodate multiple vehicles in a variety of parking configurations depending on training needs.</p> <p>The applicant shall paint 1 parking space on site to satisfy this requirement.</p>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<p><b>17.124.130</b></p> <p><b>Fences, Hedges and Walls</b></p> <p>C. In all other districts, fences, hedges and walls shall not exceed four feet (4') in height when located less than thirty feet (30') from the front lot line and shall not exceed six feet (6') in height when located more than thirty feet (30') from the front lot line;</p> <p>The applicant has proposed a six foot (6') within thirty feet (30') of the front lot line which is not permitted.</p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<p><b>17.18.150 &amp; 17.08.020</b></p> <p><b>Zoning Matrix &amp; Definitions</b></p> <p><i>Staff Comments</i></p> <p><b>17.18.150: Light Industrial District Dumber 2 (LI-2)</b>  <b>A. Purpose:</b> The LI-2 light industrial district number 2 is established to provide for a permanent year round employment base and the location of light manufacturing, wholesale trade and distribution, research and development, service industries, limited related, bulk retail and offices related to building, maintenance and construction and which generate little traffic from tourists and the general public. (Ord. 1135, 2015)</p> <p><b>17.08.020 – Definitions</b>  <b>HEIGHT OF BUILDING:</b> The greatest vertical distance measured at any point from the roof to natural, existing, or finished grade, whichever is lowest. The maximum vertical distance from the lowest exposed finished floor to the highest point of the roof (regardless of vertical alignment) shall be no more than five feet (5') greater than the maximum height permitted in the zoning district (see illustration B on file in the office of the city clerk). No facade shall be greater than the maximum height permitted in the zoning district. (See definition of "facade" in this section and illustration B on file in the office of the city clerk.) Facades which step up or down hillsides shall be set back from the lower facade a minimum of fifty percent (50%) of the height of the lower facade; except, that roof overhangs may extend up to three feet (3') into this area (see illustration B on file in the office of the city clerk). This building height provision shall apply to parapets, boston roofs and any other portion of a building roof, but shall not apply to flagpoles, lightning rods, weather vanes, antennas or chimneys.</p> <p><b>PUBLIC USE:</b> A structure or use intended or used for a public purpose by a city, other than the city of Ketchum, a school district, the county, the state, or by any other public agency, or by a public utility.</p> <p>This facility will be used by the City of Ketchum Fire Department and therefore is a public use.</p>

## Attachment C.

Table 3: Design Review Standards for all projects

Design Review Requirements				
IMPROVEMENTS AND STANDARDS: 17.96.060				
Yes	No	N/A	City Code	City Standards and <i>Staff Comments</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(A)(1) Streets	<b>The applicant shall be responsible for all costs associated with providing a connection from an existing city street to their development.</b>
			<i>Staff Comments</i>	<i>The property is served by a public roadway. This standard has been met.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(A)(2) Streets	<b>All street designs shall be approved by the City Engineer.</b>
			<i>Staff Comments</i>	<i>The site is served by an existing street. The applicant is aware of this requirement and the final street design shall be approved by the Public Works Department.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(B)(1)	<b>All projects under 17.96.010(A) that qualify as a "Substantial Improvement" shall install sidewalks as required by the Public Works Department.</b>
			<i>Staff Comments</i>	<i>The site is served by an existing sidewalk.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060 (B)(2)c	<b>Sidewalk width shall conform to the City's right-of-way standards, however the City Engineer may reduce or increase the sidewalk width and design standard requirements at their discretion.</b>
			<i>Staff Comments</i>	<i>See above comment.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060 (B)(3)	<b>Sidewalks may be waived if one of the following criteria is met:</b> <ol style="list-style-type: none"> <li>a. The project comprises an addition of less than 250 square feet of conditioned space.</li> <li>b. The City Engineer finds that sidewalks are not necessary because of existing geographic limitations, pedestrian traffic on the street does not warrant a sidewalk, or if a sidewalk would not be beneficial to the general welfare and safety of the public.</li> </ol>
			<i>Staff Comments</i>	<i>N/A.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060 (B)(4)	<b>The length of sidewalk improvements constructed shall be equal to the length of the subject property line(s) adjacent to any public street or private street.</b>
			<i>Staff Comments</i>	<i>The existing sidewalks have been constructed equal to the length of the subject property line adjacent to Lewis St.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060 (B)(5)	<b>New sidewalks shall be planned to provide pedestrian connections to any existing or future sidewalks adjacent to the site. In addition, sidewalks shall be constructed to provide safe pedestrian access to and around a building.</b>
			<i>Staff Comments</i>	<i>The existing sidewalk serves to provide pedestrian connection to the existing sidewalk in front of the adjacent properties.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060 (B)(6)	<b>The City may approve and accept voluntary cash contributions in-lieu of the above described improvements, which contributions must be segregated by the City and not used for any purpose other than the provision of these improvements. The contribution amount shall be one hundred ten percent (110%) of the estimated costs of concrete sidewalk and drainage improvements provided by a qualified contractor, plus associated engineering costs, as approved by the City Engineer. Any approved in-lieu contribution shall be paid before the City issues a certificate of occupancy.</b>
			<i>Staff Comments</i>	<i>Staff does not recommend a contribution in-lieu for this project.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(C)(1)	<b>All storm water shall be retained on site.</b>
			<i>Staff Comments</i>	<i>As proposed, all storm water will be retained on site.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(C)(2)	<b>Drainage improvements constructed shall be equal to the length of the subject property lines adjacent to any public street or private street.</b>
			<i>Staff Comments</i>	<i>The existing drainage satisfies this requirement. All new drainage improvements shall require approval from the Public Works Department.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(C)(3)	<b>The City Engineer may require additional drainage improvements as necessary, depending on the unique characteristics of a site.</b>
			<i>Staff Comments</i>	<i>Additional drainage improvements are not recommended at this time.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(C)(4)	<b>Drainage facilities shall be constructed per City standards.</b>
			<i>Staff Comments</i>	<i>The Public Works Department has reviewed the drainage facilities and has no concerns at this time.</i>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(D)(1)	<b>All utilities necessary for the development shall be improved and installed at the sole expense of the applicant.</b>
			<i>Staff Comments</i>	<i>The applicant is aware of this requirement and the plans show all applicable utility locations.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(D)(2)	<b>Utilities shall be located underground and utility, power, and communication lines within the development site shall be concealed from public view.</b>
			<i>Staff Comments</i>	<i>The applicant is aware of this requirement. All utilities will be located underground and all utility, power, and communication lines within the development will be concealed from public view.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(D)(3)	<b>When extension of utilities is necessary all developers will be required to pay for and install two (2") inch SDR11 fiber optical conduit. The placement and construction of the fiber optical conduit shall be done in accordance with city of Ketchum standards and at the discretion of the City Engineer.</b>
			<i>Staff Comments</i>	<i>The applicant is aware of this requirement and will comply with these standards and receive approval from the City Engineer prior to the issuance of a building permit.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(E)(1)	<b>The project's materials, colors and signing shall be complementary with the townscape, surrounding neighborhoods and adjoining structures.</b>
			<i>Staff Comments</i>	<i>The property is zoned as Light Industrial-two (LI-2). The adjacent properties at the north and south of the property are currently used for industrial purposes, including materials storage. The YMCA is located adjacent to the rear of the property and the Lewis Street Center is located across the street from the proposed Center. Staff finds that the design of the proposed Fire Training Center is consistent with uses of the Light Industrial zoning district.</i>  <i>The applicant has proposed two options for the exterior color of the facility. Option one is to paint level one in a darker stonewall grey and level 2 &amp; 3 in a lighter half-moon grey. Option two is to paint level 1 in a dark backwoods green and levels 2 and 3 in Cambridge green. The sloped roof will have GAF roofing shingles and all guardrails, to boards, stairs, decks, and miscellaneous trim will be galvanized steel.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060(E)(2)	<b>Preservation of significant landmarks shall be encouraged and protected, where applicable. A significant landmark is one which gives historical and/or cultural importance to the neighborhood and/or community.</b>
			<i>Staff Comments</i>	<i>N/A. There are no identified landmarks on the property.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060(E)(3)	<b>Additions to existing buildings, built prior to 1940, shall be complementary in design and use similar material and finishes of the building being added to.</b>
			<i>Staff Comments</i>	<i>N/A. There is no building located on-site.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(F)(1)	<b>Building(s) shall provide unobstructed pedestrian access to the nearest sidewalk and the entryway shall be clearly defined.</b>
			<i>Staff Comments</i>	<i>The proposed building provides unobstructed pedestrian access to the sidewalk along Lewis St. The building is proposed to be used for fire training purposes and the entryway is designed to accommodate the proposed use.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(F)(2)	<b>The building character shall be clearly defined by use of architectural features.</b>
			<i>Staff Comments</i>	<i>The proposed Fire Training Center has been designed by Fire Training Structures LLC (FTS). The proposed Center contains 3-40' containers at the first and second level, and 1-20' container at the third level. The containers are classified as single trip shipping containers. The shipping containers have been designed to specifically serve as a fire training facility. The Center includes exterior, stair access to the second level. The second level contains a sloped roof for training purposed. The applicant has proposed an 8'-6" metal bar grate panels for rappelling purposes, with a rappel deck and a 3'6" guardrail above.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(F)(3)	<b>There shall be continuity of materials, colors and signing within the project.</b>
			<i>Staff Comments</i>	<i>As proposed, there is continuity of the proposed materials and colors within the project. The applicant has proposed two options for the exterior color of the facility. Option one is to paint level one in a darker stonewall grey and level 2 &amp; 3 in a lighter half-moon grey. Option two is to paint level 1 in a dark backwoods green and levels 2 and 3 in Cambridge green. The applicant has indicated that each option would include</i>



				<i>red windows and doors. The sloped roof will have GAF roofing shingles and all guardrails, to boards, stairs, decks, and misc. trim will be galvanized steel. The applicant has indicated that the final color of the facility may change to a more neutral color.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(F)(4)	<b>Accessory structures, fences, walls and landscape features within the project shall match or complement the principal building.</b>
			<i>Staff Comments</i>	<i>The proposed accessory storage facility will be painted to match the training facility.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(F)(5)	<b>Building walls shall provide undulation/relief, thus reducing the appearance of bulk and flatness.</b>
			<i>Staff Comments</i>	<i>The proposed building walls are stepped back at the second and third level which reduces the appearance of bulk. As proposed, the southeast perspective provides an unbroken façade which currently faces Lewis St. The applicant is encouraged to reorient the building so that the majority of the massing is facing away from Lewis St.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(F)(6)	<b>Building(s) shall orient towards their primary street frontage.</b>
			<i>Staff Comments</i>	<i>The proposed fire training facility orients toward the south most facing property. As the building is proposed to be used as a fire training facility which will include a variety of training exercises, the facility has been oriented maximize circulation around the property. See comment above regarding the orientation of the proposed building.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(F)(7)	<b>Garbage storage areas and satellite receivers shall be screened from public view and located off alleys.</b>
			<i>Staff Comments</i>	<i>The proposed garage storage area is located at the northwest corner of the property, and shall be screened from public view.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060(F)(8)	<b>Building design shall include weather protection which prevents water to drip or snow to slide on areas where pedestrians gather and circulate or onto adjacent properties.</b>
			<i>Staff Comments</i>	<i>N/A. The design of the building does not include weather protection. The proposed building does not include areas that that will serve as pedestrian gathering or circulation space.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(G)(1)	<b>Pedestrian, equestrian and bicycle access shall be located to connect with existing and anticipated easements and pathways.</b>
			<i>Staff Comments</i>	<i>The front of the property will continue to provide street access to Lewis St., and will be served by a sidewalk to accommodate pedestrian access.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060(G)(2)	<b>Awnings extending over public sidewalks shall extend five (5') feet or more across the public sidewalk but shall not extend within two (2') feet of parking or travel lanes within the right of way.</b>
			<i>Staff Comments</i>	<i>N/A. Awnings are not proposed. Per City Code: 17.08.020, an awning is defined as a covered architectural projection that extends from the exterior wall of a building for the purpose of providing shade, shelter or aesthetic value to the building facade.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(G)(3)	<b>Traffic shall flow safely within the project and onto adjacent streets. Traffic includes vehicle, bicycle, pedestrian and equestrian use. Consideration shall be given to adequate sight distances and proper signage.</b>
			<i>Staff Comments</i>	<i>The site has been designed to accommodate a variety of emergency vehicles. Staff finds that the Fire Training Facility will not adversely impact traffic flow from, and onto, Lewis St.</i>
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	17.96.060(G)(4)	<b>Curb cuts and driveway entrances shall be no closer than twenty (20') feet to the nearest intersection of two or more streets, as measured along the property line adjacent to the right of way. Due to site conditions or current/projected traffic levels or speed, the City Engineer may increase the minimum distance requirements.</b>
			<i>Staff Comments</i>	<i>N/A. The proposed curb cut is not closer than 20' to the nearest intersection.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(G)(5)	<b>Unobstructed access shall be provided for emergency vehicles, snowplows, garbage trucks and similar service vehicles to all necessary locations within the proposed project.</b>
			<i>Staff Comments</i>	<i>Emergency and service vehicles can access the site from Lewis St., providing unobstructed access for emergency vehicles, snowplows, and garbage trucks.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(H)(1)	<b>Snow storage areas shall not be less than thirty percent (30%) of the improved parking and pedestrian circulation areas.</b>

			<i>Staff Comments</i>	<i>The applicant states that the improved parking and pedestrian circulation area is approximately 7,915 square feet, with 30% of the paved surface equaling 2,375 square feet. The proposed snow storage area is 2,410 square feet, which satisfies this requirement.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(H)(2)	<b>Snow storage areas shall be provided on site.</b>
			<i>Staff Comments</i>	<i>Snow storage is provided on site. Snow storage is proposed at the southwest corner of the subject property.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(H)(3)	<b>A designated snow storage area shall not have any dimension less than five (5') feet and shall be a minimum of twenty five (25) square feet.</b>
			<i>Staff Comments</i>	<i>The proposed snow storage area satisfies this requirement.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(H)(4)	<b>In lieu of providing snow storage areas, snow melt and hauling of snow may be allowed.</b>
			<i>Staff Comments</i>	<i>Snow storage areas are proposed on site.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	17.96.060(I)(1)	<b>Landscaping is required for all projects.</b>
			<i>Staff Comments</i>	<i>Landscaping is not proposed. The applicant shall identify landscaping suitable for the location at the time of Design Review. Additionally, the proposed project shall comply with the standards established in 17.124.100: Landscaped Yards, Light Industrial Districts.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	17.96.060(I)(2)	<b>Landscape materials and vegetation types specified shall be readily adaptable to a site's microclimate, soil conditions, orientation and aspect, and shall serve to enhance and complement the neighborhood and townscape.</b>
			<i>Staff Comments</i>	<i>See comment above.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	17.96.060(I)(3)	<b>All trees, shrubs, grasses and perennials shall be drought tolerant. Native species are recommended but not required.</b>
			<i>Staff Comments</i>	<i>See comment above.</i>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	17.96.060(I)(4)	<b>Landscaping shall provide a substantial buffer between land uses, including, but not limited to, structures, streets and parking lots. The development of landscaped public courtyards, including trees and shrubs where appropriate, shall be encouraged.</b>
			<i>Staff Comments</i>	<i>The proposed Fire Training Center will include operations consistent with firefighting. Therefore, landscaping should be used strategically to provide a buffer from the adjacent land uses, such as the YCMA adjacent to the rear of the property.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.96.060(J)(1)	<b>Where sidewalks are required, pedestrian amenities shall be installed. Amenities may include, but are not limited to, benches and other seating, kiosks, bus shelters, trash receptacles, restrooms, fountains, art, etc. All public amenities shall receive approval from the Public Works Department prior to design review approval from the Commission.</b>
			<i>Staff Comments</i>	<i>Sidewalks are existing. Additional amenities are not proposed.</i>

**ATTACHMENT D. APPLICATION**



City of Ketchum  
Planning & Building

OFFICIAL USE ONLY
File Number
Date Received
By
Pre-Application Fee Paid
Design Review Fee Paid
Approved Date
Denial Date
By
ADRE Fee \$

Design Review Application

**APPLICANT INFORMATION**

Project Name: Fire Training Center	Phone: 208-720-3448
Owner: City of Ketchum	Mailing Address: PO Box 4171, Ketchum
Email: See Representative Email	
Architect/Representative: KSVVA	Phone: 208-720-3448
Email: dl@pdisv.com	Mailing Address: PO Box 1262, Ketchum
Architect License Number: N/A	
Engineer of Record: Benchmark Associates, P.A.	Phone: 208-726-9512
Email: sam@bma5b.com	Mailing Address: PO Box 733, Ketchum
Engineer License Number: 9392	

All design review plans and drawings for public commercial projects, residential buildings containing more than four (4) dwelling units and development projects containing more than four (4) dwelling units shall be prepared by an Idaho licensed architect or an Idaho licensed engineer.

**PROJECT INFORMATION**

Legal Land Description: Northgate Subdivision Lot 7 Block 1	
Street Address: 219 Lewis Street	
Lot Area (Square Feet): 12,000	
Zoning District: LI-2	
Overlay District: <input type="checkbox"/> Floodplain <input type="checkbox"/> Avalanche <input type="checkbox"/> Mountain	
Type of Construction: <input checked="" type="checkbox"/> New <input type="checkbox"/> Addition <input type="checkbox"/> Remodel <input type="checkbox"/> Other	
Anticipated Use: GOVERNMENT	Number of Residential Units: 0

**TOTAL FLOOR AREA**

	Proposed	Existing
Basements	Sq. Ft.	Sq. Ft.
1 <sup>st</sup> Floor 640 inside + 160 storage container	Sq. Ft.	Sq. Ft.
2 <sup>nd</sup> Floor 320 inside; 216 exterior deck	Sq. Ft.	Sq. Ft.
3 <sup>rd</sup> Floor 160 inside; 160 exterior deck	Sq. Ft.	Sq. Ft.
Mezzanine 160 + 120 exterior deck	Sq. Ft.	Sq. Ft.
Total 1280 enclosed sf; 656 exter. decks	Sq. Ft.	Sq. Ft.

**FLOOR AREA RATIO**

Community Core:	Tourist:	General Residential-High:
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**BUILDING COVERAGE/OPEN SPACE**

Percent of Building Coverage: 16 x 40 footprint + 8 x 20 storage = 960 sf / (lot 80 x 150) = 8% lot coverage

**DIMENSIONAL STANDARDS/PROPOSED SETBACKS**

Front: 50'	Side: 24' (North)	Side: 40' (South)	Rear: 56'
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Building Height:

**OFF STREET PARKING**

Parking Spaces Provided: N/A

Curb Cut: 18 LF Sq. Ft. 22.5 %

**WATER SYSTEM**

<input checked="" type="checkbox"/> Municipal Service	<input type="checkbox"/> Ketchum Spring Water
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The Applicant agrees in the event of a dispute concerning the interpretation or enforcement of the Design Review Application in which the city of Ketchum is the prevailing party, to pay the reasonable attorney fees, including attorney fees on appeal and expenses of the city of Ketchum. I, the undersigned, certify that all information submitted with and upon this application form is true and accurate to the best of my knowledge and belief.

Paul ZA  
Signature of Owner/Representative

5-24-2017  
Date

**ATTACHMENT E. PLANS 05/24/2017**

# FIRE TRAINING CENTER

## MAY 2017

**OWNER**

CITY OF KETCHUM  
C/O KETCHUM SUN VALLEY VOLUNTEER ASSOCIATION (KSWA)  
P.O. BOX 1262  
KETCHUM, ID, 83340  
(208) 726-3841  
CONTACT: DAVID LISTER

**STRUCTURAL ENGINEERING**

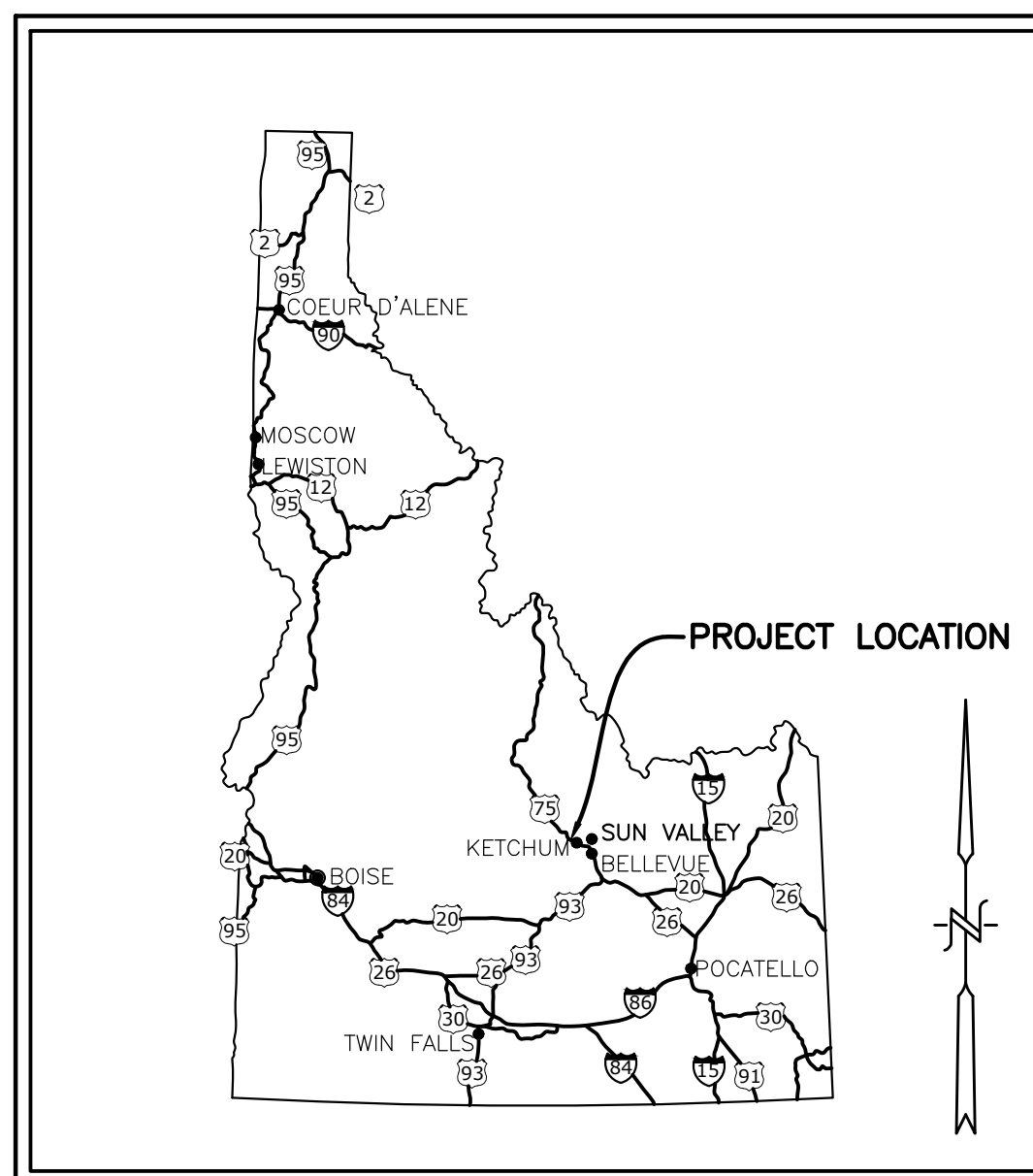
MORELL ENGINEERING/ FIRE TRAINING STRUCTURES, LLC  
(208) 726-2844  
MORELLENGINEERING@COX.NET  
CONTACT: MATT MORELL, P.E.

**CIVIL ENGINEERING**

BENCHMARK ASSOCIATES, P.A.  
P.O. BOX 733  
KETCHUM, ID, 83340  
(208) 726-9512  
CONTACT: STEVE BUTLER, P.E.

**SHEET INDEX**

SHEET NUMBER	SHEET TITLE
C-0	COVER SHEET
C-1	SITE AND UTILITY PLAN
C-2	GRADING AND DRAINAGE PLAN
C-3	DETAILS
A-001	ISOMETRICS
A-002	PERSPECTIVES
A-101	PLANS
A-102	PLANS
A-201	ELEVATIONS
A-202	ELEVATIONS
A-301	SECTIONS



VICINITY MAP



REVISIONS	No.	DESCRIPTION	DATE	BY



PREPARED BY:  
BENCHMARK ASSOCIATES, P.A.  
P.O. BOX 733 100 BELL DRIVE  
KETCHUM, IDAHO 83340  
(208) 726-9512  
FAX 726-9514  
WEB: WWW.BMA5B.COM  
MAIL: WWW.BMA5B.COM

**COVER SHEET**

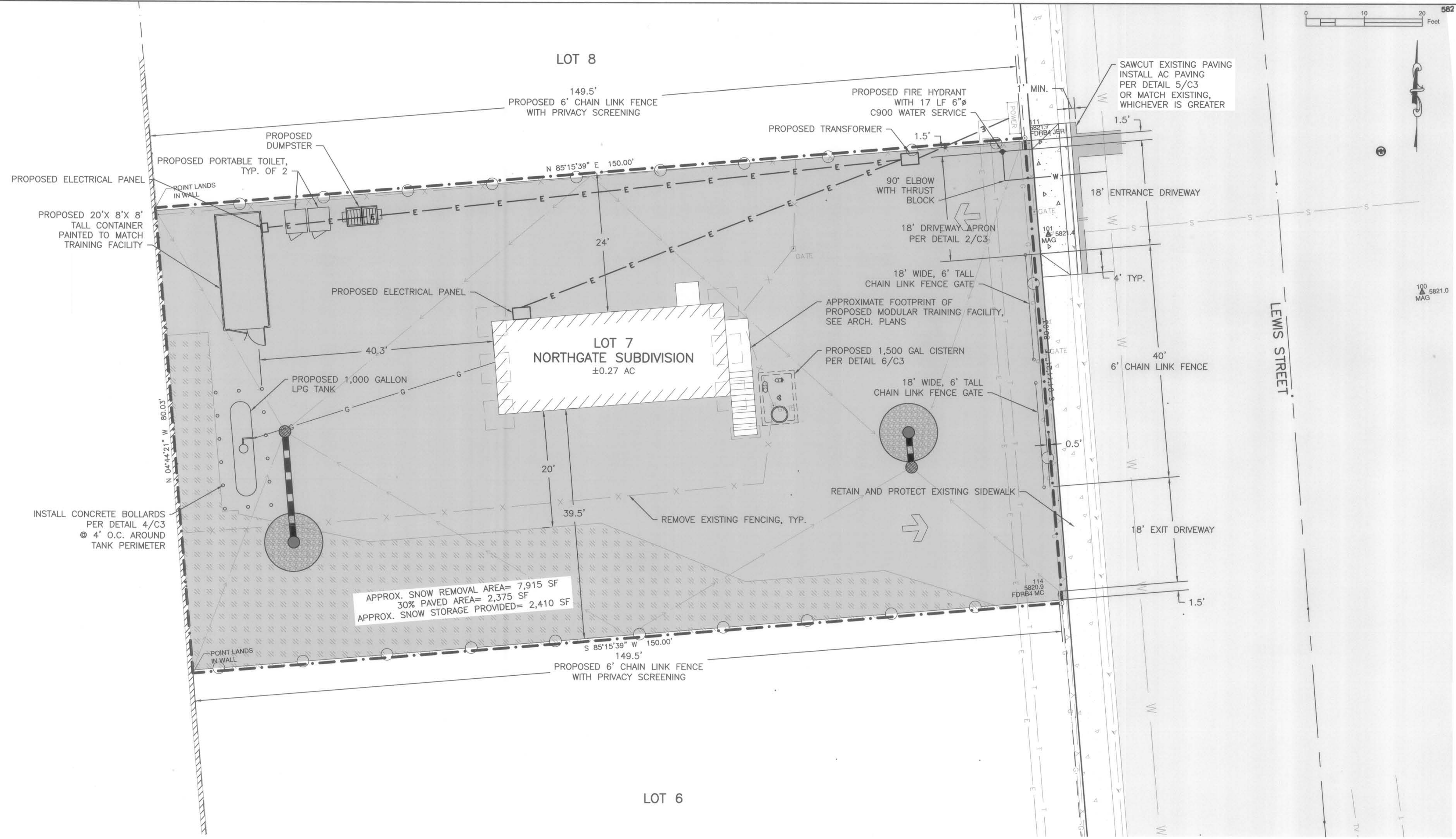
NORTHGATE SUBDIVISION LOT 7  
T4N, R17E, SEC 12, B.M., BLAINE COUNTY, IDAHO

PREPARED FOR: KSWVA

DRAWN BY: SKS  
DESIGNED BY: SKS  
CHECKED BY: SB  
DATE: 05/08/17  
PROJECT NO.: 17031

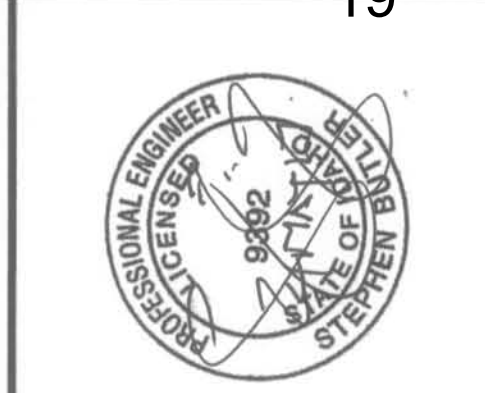
SHEET NUMBER  
**C-0**





**LEGEND**

	EXISTING PROPERTY LINE
	EXISTING ADJOINING PROPERTY LINE
	EXISTING CENTERLINE
	EXISTING ASPHALT PAVEMENT
	EXISTING WALL
	EXISTING FENCE
	EXISTING SEWER MAIN
	EXISTING WATER MAIN
	EXISTING GAS LINE
	EXISTING POWER LINE
	EXISTING TELEPHONE LINE
	EXISTING CABLE TV LINE
	EXISTING 5/8" REBAR
	EXISTING TRANSFORMER
	EXISTING TELEPHONE RISER
	PROPOSED ASPHALT PAVEMENT
	PROPOSED SAWCUT
	PROPOSED CONCRETE SIDEWALK
	PROPOSED DRYWELL
	PROPOSED STORM DRAIN PIPE
	PROPOSED CATCH BASIN
	PROPOSED ELECTRICAL
	PROPOSED GAS
	PROPOSED 8" WATER LINE
	PROPOSED FLOW LINE



**REVISIONS**

No.	DESCRIPTION	DATE	BY



PREPARED BY:  
 BENCHMARK ASSOCIATES, P.A.  
 P.O. BOX 733 100 BELL DRIVE  
 KETCHUM, IDAHO 83340  
 (208) 726-9512  
 FAX 726-9514  
 WEB: WWW.BMA5B.COM  
 MAIL: WWW.BMA5B.COM

**GENERAL NOTES:**

- CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS OF ALL EXISTING UTILITIES BEFORE COMMENCING CONSTRUCTION. ANY CONFLICT SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER.
- THE CONTRACTOR SHALL NOTIFY DCLINE (1-800-342-1585) AND THE CITY OF KETCHUM UTILITIES DEPT. (1-208-726-7825) AT LEAST 48 HOURS PRIOR TO BEGINNING CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING UTILITIES ENCOUNTERED DURING CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR DUST CONTROL DURING THE CONSTRUCTION OF ALL ITEMS HEREON. DUST CONTROL SHALL BE CONTINUOUS DURING CONSTRUCTION, 24 HOURS PER DAY 7 DAYS PER WEEK. THE CONTRACTOR SHALL FOLLOW THE REQUIREMENTS OF THE STORM WATER POLLUTION PREVENTION PROGRAM AT ALL TIMES UNTIL PERMANENT EROSION CONTROL IS ESTABLISHED.
- THE CONTRACTOR SHALL INSTALL TEMPORARY BMP'S AS SHOWN AT THE BEGINNING OF CONSTRUCTION AND REMOVE UPON PROJECT COMPLETION.
- TRENCH BACKFILL AND ROAD RECONSTRUCTION SHALL MEET KETCHUM ROAD DEPARTMENT REQUIREMENTS. CONTRACTOR SHALL CONTACT KETCHUM ROAD DEPARTMENT PRIOR TO CONSTRUCTION COMMENCEMENT.
- CONSTRUCTION OF WATER MAINS AND ALL OTHER RELATED APPURTENANCES SHALL BE IN ACCORDANCE WITH THE IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION (ISPMC), IDAPA 58.01.08, IDAHO RULES FOR PUBLIC DRINKING WATER SYSTEMS AND THE CITY OF KETCHUM STANDARDS.
- CONTRACTOR SHALL PRESSURE TEST, DISINFECT, AND CONDUCT BIOLOGICAL TESTING IN ACCORDANCE WITH THE IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION (ISPMC), AMERICAN WATER WORKS ASSOCIATION (AWWA) STANDARDS, AND THE PRESSURE TESTING, DISINFECTION, AND MICROBIOLOGICAL TESTING PROCEDURES.
- THE CONSTRUCTION OF WATER MAINS SHALL BE IN ACCORDANCE WITH IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION (ISPMC) AND THE CITY OF KETCHUM UTILITIES DEPARTMENT STANDARDS.
- ALL WATER SERVICE FIXTURES, FITTINGS, PIPING AND ALL RELATED APPURTENANCES SHALL BE ANSI/NSF STD. 61 COMPLIANT.
- ALL WATER SUPPLY FIXTURES, FITTINGS, PIPING AND ALL RELATED APPURTENANCES SHALL COMPLY WITH THE LOW LEAD ACT REQUIRING ALL MATERIALS TO HAVE A LEAD CONTENT EQUAL TO OR LESS 0.25%.
- CONTRACTOR SHALL USE ANSI/NSF STANDARD 60 CHEMICALS AND COMPOUNDS DURING INSTALLATION & DISINFECTION OF POTABLE WATER MAIN.
- STORM DRAIN PIPE SHALL HAVE 2' MIN. COVER AND A SLOPE OF 2% MIN.
- 25' MINIMUM SEPARATION BETWEEN WATER LINES AND DRYWELLS.
- UTILITY LINES SHOWN ARE GENERALLY DIAGRAMMATIC AND DO NOT SHOW EVERY BEND OR CLEANOUT NECESSARY FOR INSTALLATION. CONTRACTOR SHALL REFER TO THE MOST CURRENT EDITION OF THE UNIVERSAL PLUMBING CODE FOR REQUIREMENTS.
- CONTRACTOR SHALL RETAIN AND PROTECT ALL MONUMENTS, ACCESSORIES TO CORNERS, BENCHMARKS AND/OR SURVEY CONTROL POINTS. IF ANY OF THE ABOVE LISTED ITEMS MUST BE REPLACED, THE CONTRACTOR SHALL NOTIFY ENGINEER PRIOR TO THE REMOVAL.
- ELECTRICAL SERVICE TO BE DESIGNED BY IDAHO POWER AT THE REQUEST OF THE DEVELOPER FOR SPECIFIC PROJECT.
- ALL UTILITIES INCLUDING, IF SHOWN, NATURAL GAS, ELECTRICAL, TELEPHONE AND CABLE TO BE EXTENDED UNDERGROUND.
- CONSTRUCTION STAKING SHALL BE PROVIDED BY BENCHMARK ASSOCIATES.
- EXISTING CONDITIONS PER SURVEY CONDUCTED BY BENCHMARK ASSOCIATES DATED 4/3/17.

**SITE AND UTILITY PLAN**  
 NORTHGATE SUBDIVISION LOT 7  
 T4N, R17E, SEC 12, B.M., BLAINE COUNTY, IDAHO  
 PREPARED FOR: KSWVFA

DRAWN BY:	SKS
DESIGNED BY:	SKS
CHECKED BY:	SB
DATE:	05/08/17
PROJECT NO.:	17031

SHEET NUMBER

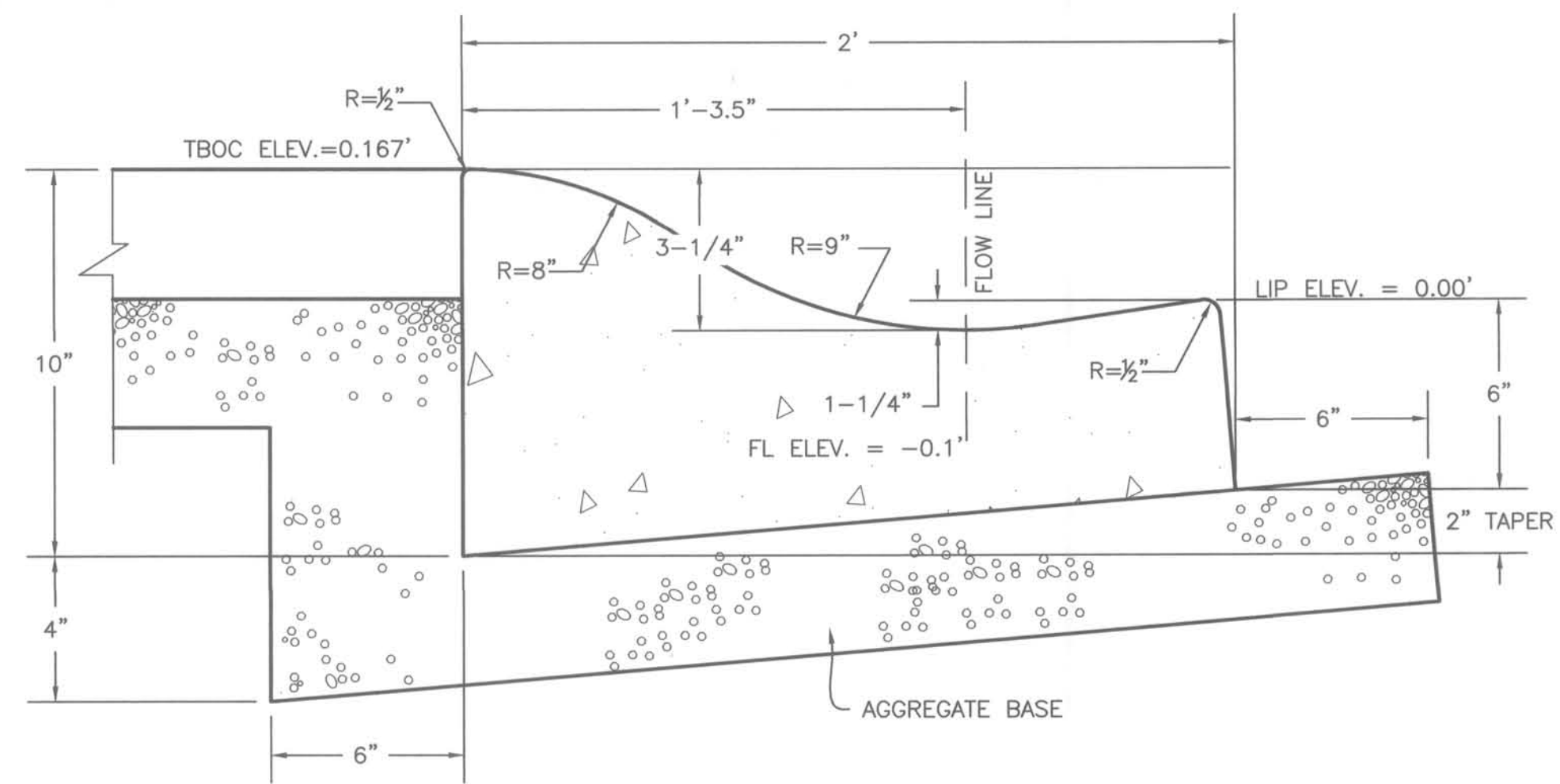
**C-1**

PRELIMINARY ONLY, NOT FOR CONSTRUCTION  
 DESIGN REVIEW SUBMITTAL 5/24/17

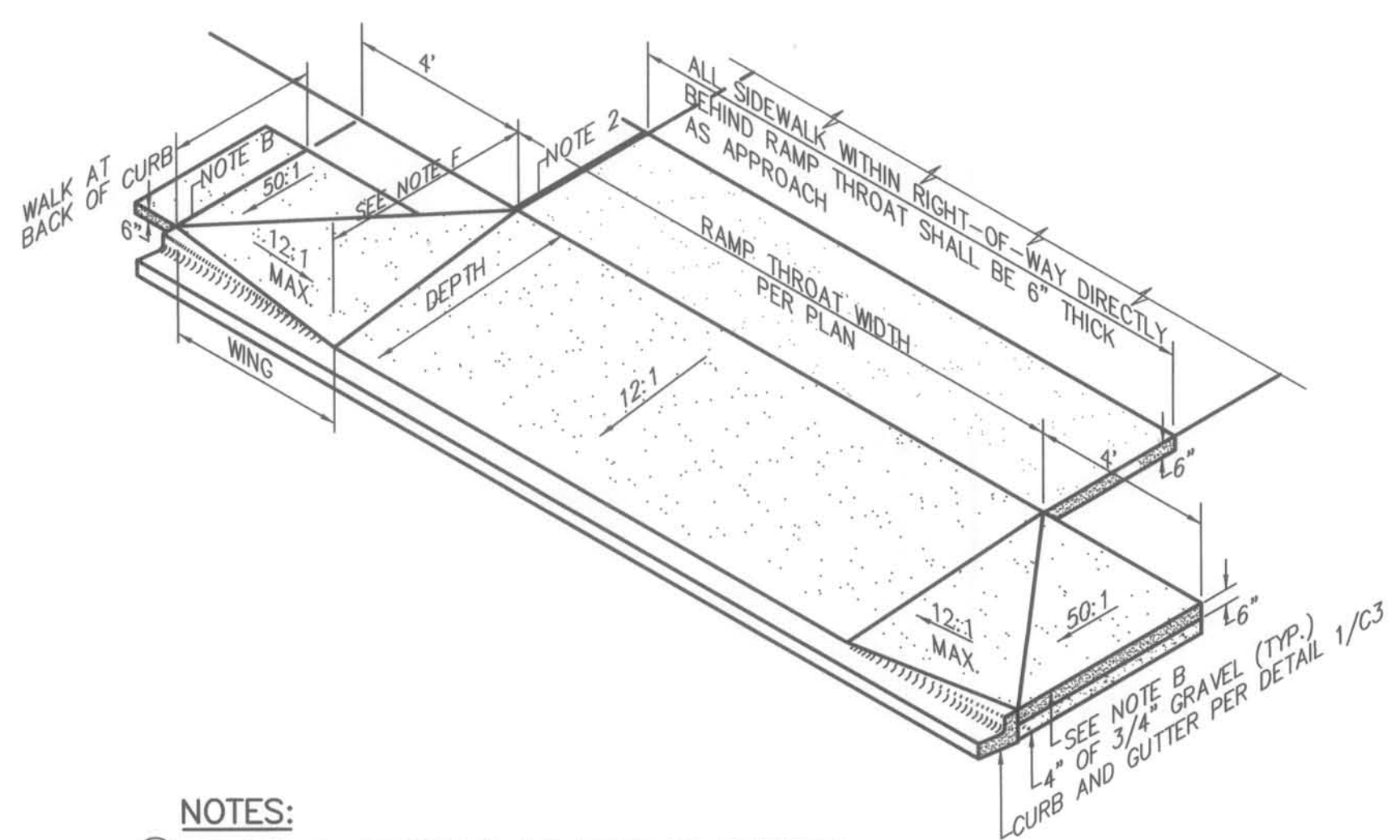




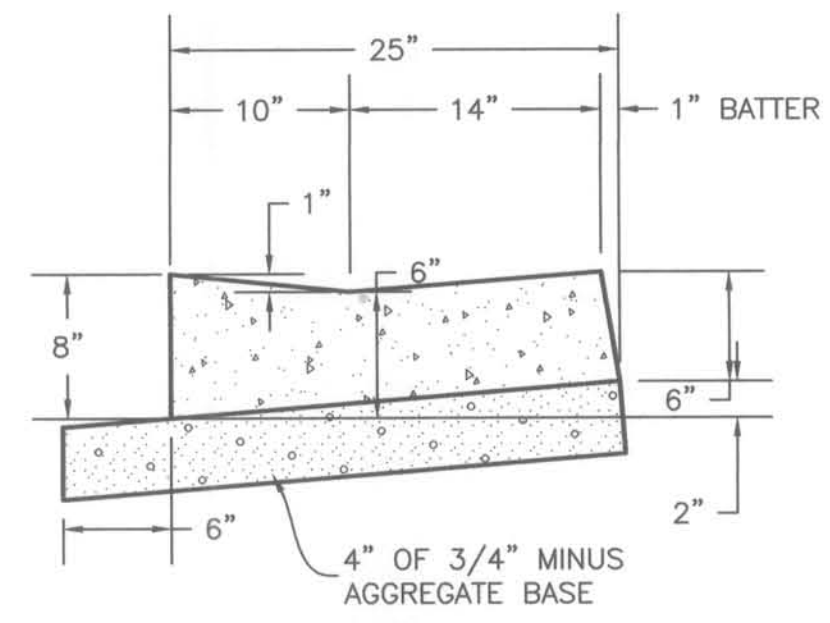
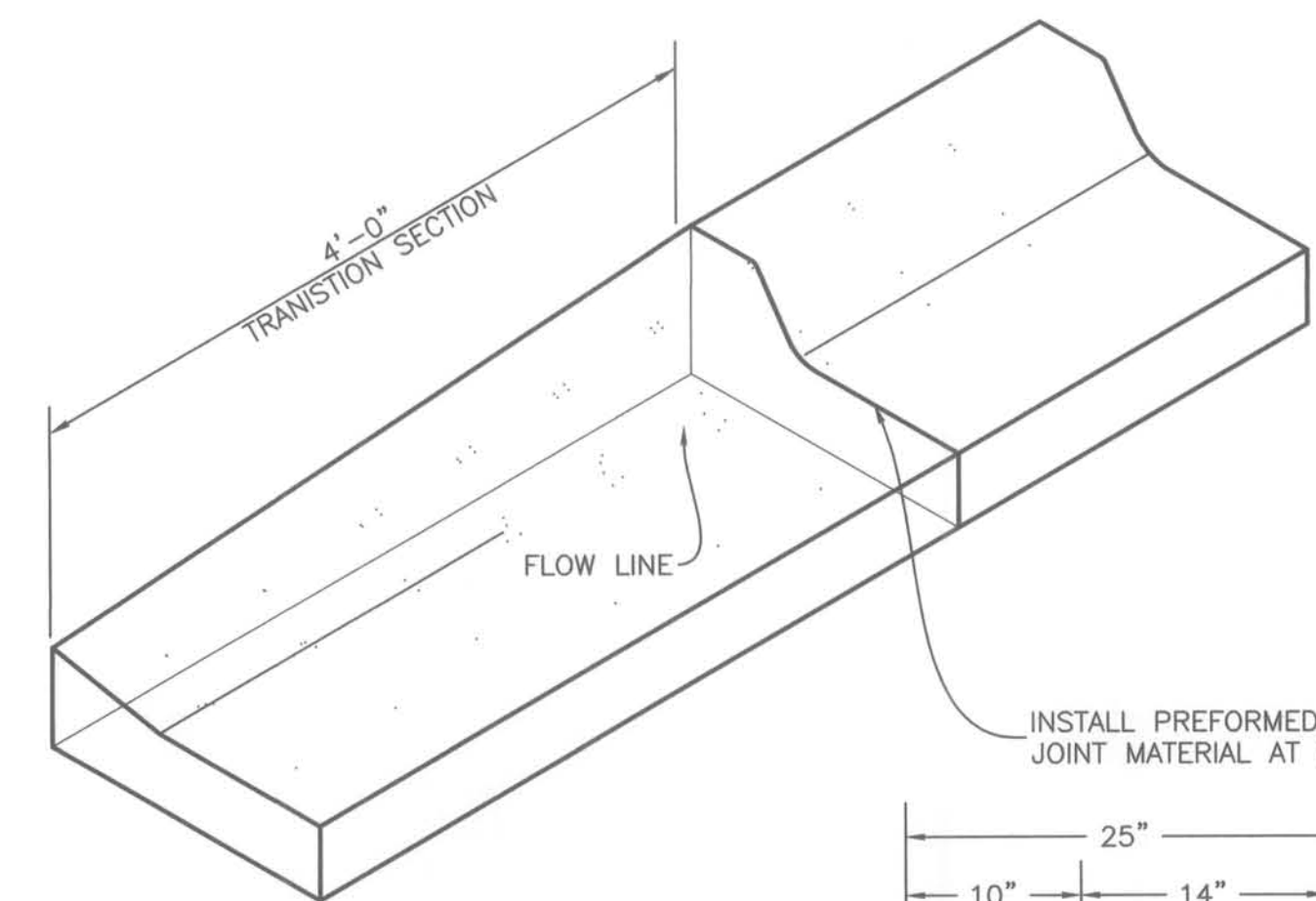




- NOTES:**
- (A) GRADE AND ALIGNMENT TO BE ESTABLISHED OR APPROVED BY THE ENGINEER AND THE PUBLIC AGENCY HAVING JURISDICTION.
  - (B) BASE: 4-INCH COMPACTED DEPTH OF 3/4-INCH MINUS CRUSHED AGGREGATE BASE MATERIAL, PLACED AS SPECIFIED AND PAID UNDER SECTION-802 ISPC; COMPACTED TO EXCEED 95% OF THE MAXIMUM DRY DENSITY OF THE MATERIAL ACCORDING TO ASTM-698; A MINIMUM WIDTH OF 3- FEET TO GRADE, PRIOR TO SETTING CURB FORMS.
  - (C) 1/2-INCH PREFORMED EXPANSION JOINT MATERIAL (AASHTO M213) AT TERMINAL POINTS OF RADII.
  - (D) CONTINUOUS PLACEMENT PREFERRED, SCORE INTERVALS 10- FEET MAXIMUM SPACING (8- FEET W/ SIDEWALK).
  - (E) MATERIALS AND CONSTRUCTION IN COMPLIANCE WITH ISPC SPECIFICATIONS.
  - (F) BACKFILL AS PER ISPC SECTION-706.
  - (G) SECURE RIGHT-OF-WAY PERMIT BEFORE BEGINNING CONSTRUCTION IN PUBLIC RIGHT-OF-WAY.

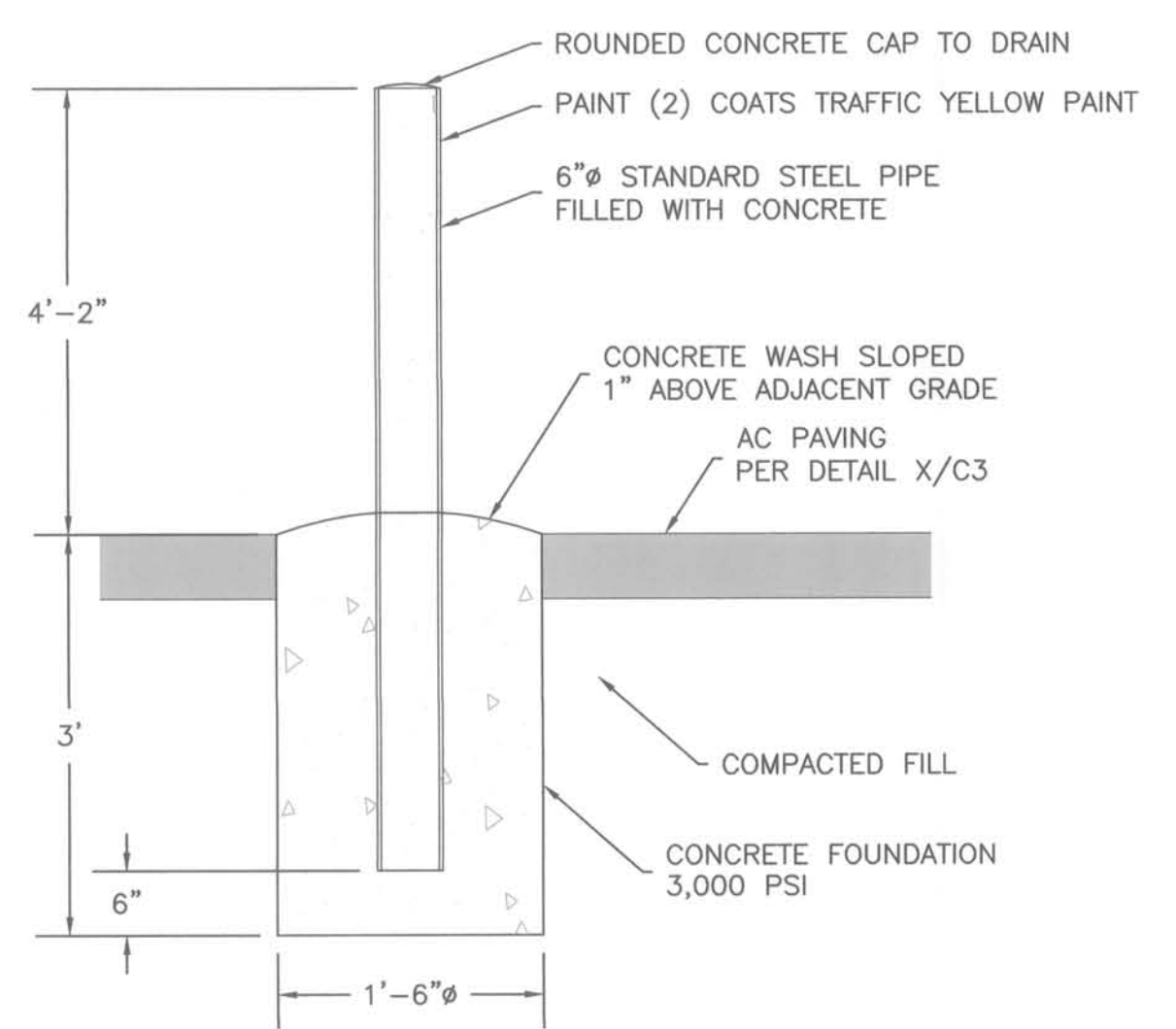


- NOTES:**
- (A) APPROACH TO CONFORM TO THE LATEST ADA STANDARDS.
  - (B) INSTALL EXPANSION JOINT AT TIP OF APPROACH WINGS WHERE SIDEWALK CHANGES THICKNESS.
  - (C) BASE TO BE 4" THICK OF 3/4" MINUS CRUSHED AGGREGATE COMPACTED TO EXCEED 95% OF THE MAXIMUM DRY DENSITY OF THE MATERIAL ACCORDING TO ASTM-698.
  - (D) ALL CONCRETE TO BE 6" THICK FROM TIP OF WING TO TIP OF WING UP TO THE EXPANSION JOINT. WHEN SIDEWALK IS SEPARATE FROM CURB THE SIDEWALK IMMEDIATELY BEHIND THE APPROACH THROAT SHALL BE 6" THICK ALSO.
  - (E) ALL CONCRETE SHALL BE CLASS 3000 PER SECTION-703.

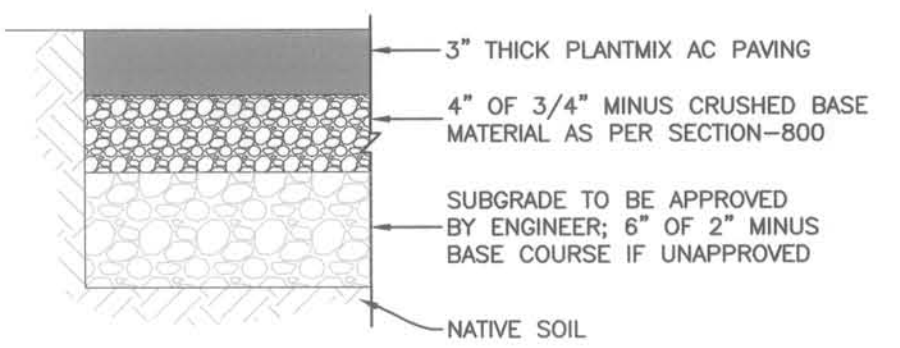


DETAIL AT END OF TRANSITION

1 4" CONCRETE ROLLED CURB  
C3 SCALE: N.T.S.



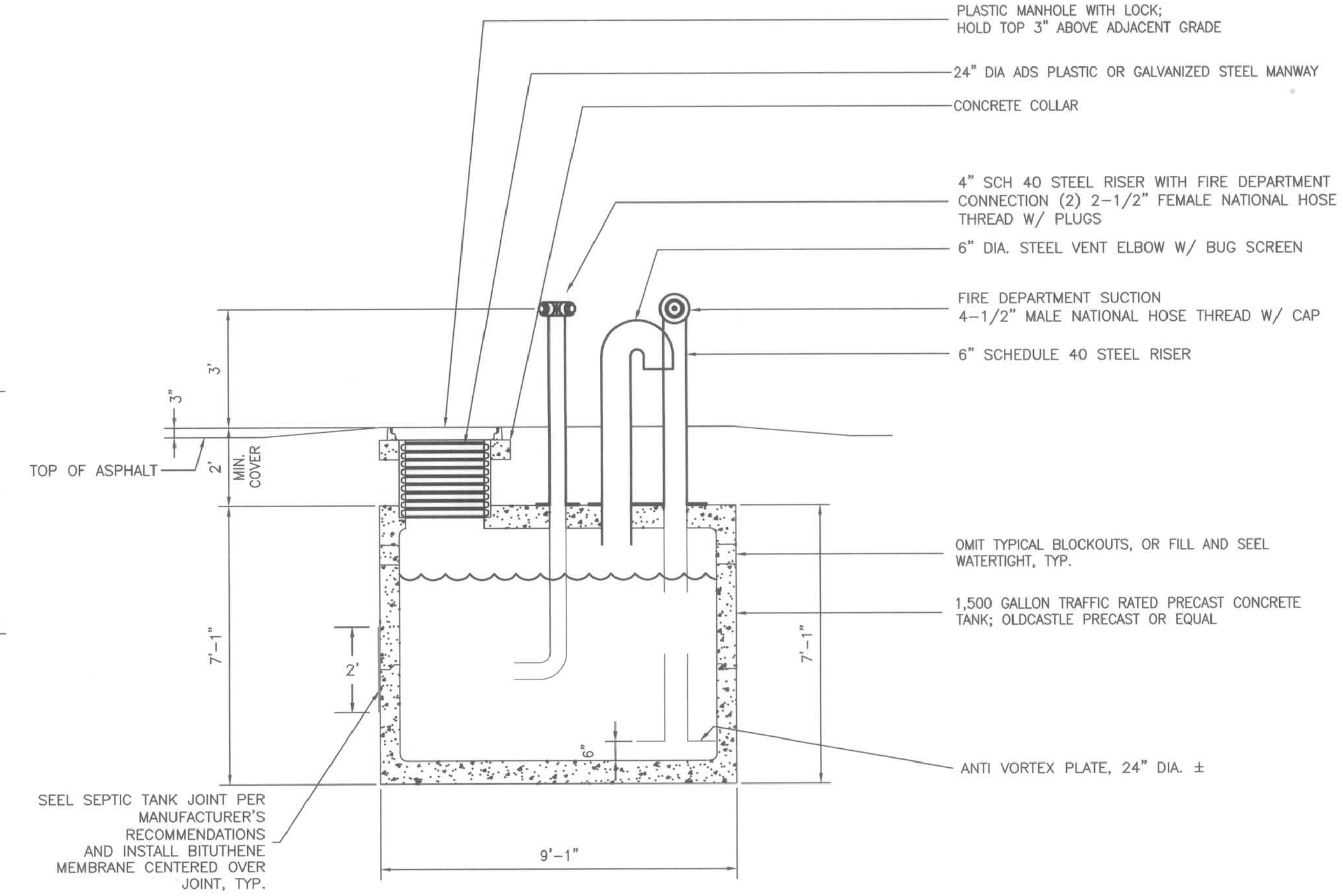
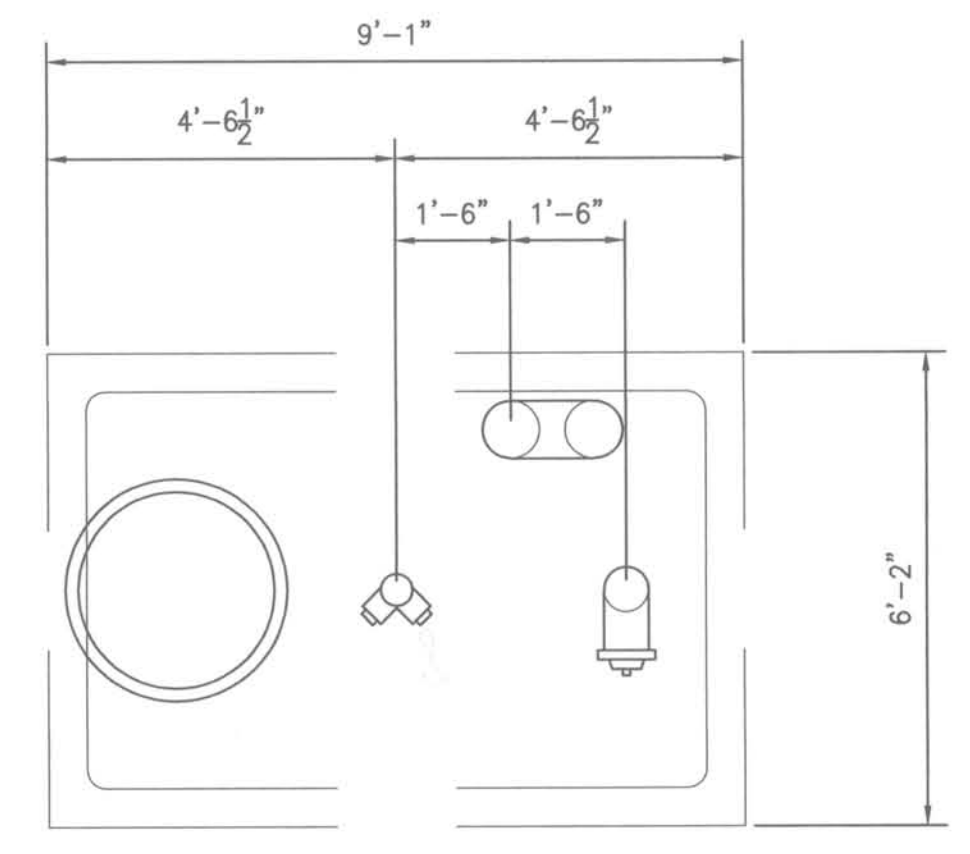
4 CONCRETE BOLLARD  
C3 SCALE: N.T.S.



- NOTES:**
1. GEOTECHNICAL ENGINEER TO VERIFY THE STRUCTURAL INTEGRITY OF THE EXISTING SUBGRADE.
  2. BASE TO BE COMPACTED TO EXCEED 95% OF THE MAXIMUM DRY DENSITY OF THE MATERIAL ACCORDING TO ASTM-698.

5 ASPHALT PAVING SECTION  
C3 SCALE: N.T.S.

2 DRIVEWAY APRON  
C3 SCALE: N.T.S.



6 1500 GALLON CISTERN  
C3 SCALE: N.T.S.

3 CURB TERMINUS  
C3 SCALE: N.T.S.

No.	REVISIONS DESCRIPTION	BY	DATE



PREPARED BY:  
BENCHMARK ASSOCIATES, P.A.  
P.O. BOX 733 100 BELL DRIVE  
KETCHUM, IDAHO 83340  
(208) 726-9512  
FAX 726-9514  
WEB: WWW.BMA5B.COM  
MAIL: WWW.BMA5B.COM

**DETAILS**  
NORTHGATE SUBDIVISION LOT 7  
T4N, R.17E, SEC 12, B.M., BLAINE COUNTY, IDAHO  
PREPARED FOR: KSWVFA

DRAWN BY: SKS  
DESIGNED BY: SKS  
CHECKED BY: SB  
DATE: 04/18/17  
PROJECT NO.: 17031

SHEET NUMBER

C-3



# BULLHEAD SYSTEM

Drawing Index	
A-001	Isometrics
A-002	Perspectives
A-101	Plans
A-102	Plans
A-201	Elevations
A-202	Elevations
A-301	Sections



3233 E. Corona Avenue  
 Phoenix, AZ 85040  
 1-877-268-8303  
 www.mobilefireunits.com



- 3 – 40' CONTAINERS
- 1 – 20' CONTAINER
- 2 – BURN ROOMS
- TEMPERATURE MONITORING SYSTEM
- RECONFIGURABLE SCBA MAZE
- SHORING PROP
- WALL BREACH PROP
- INTERIOR STAIRS
- EXTERIOR STAIRS
- ACTIVE ROOF
- OSHA COMPLIANT GUARDRAIL SYSTEM
- RAPPELLING TOWER
- STANDPIPE SYSTEM
- SPRINKLER SYSTEM
- PITCHED ROOF PROP
- FORCED ENTRY DOOR
- REBAR CUTTING PROP
- GARAGE DOOR PROP
- PERSONNEL DOORS
- DENVER WINDOW

Ketchum Sun Valley Volunteer Association Training Center

219 Lewis Street; Lot 7 Northwood Sub.  
 Ketchum, ID 83340

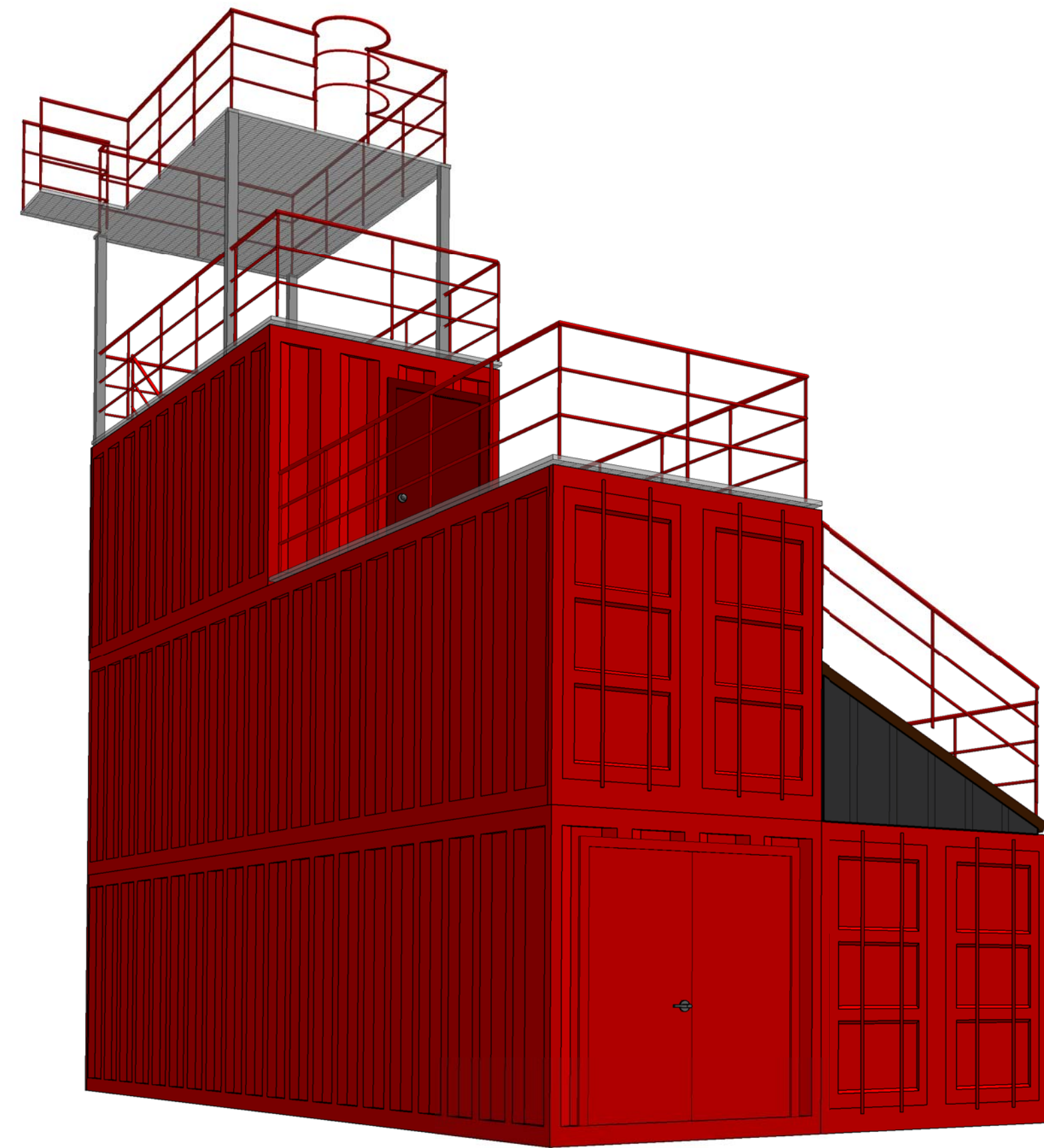
Design Review Set

5/24/2017

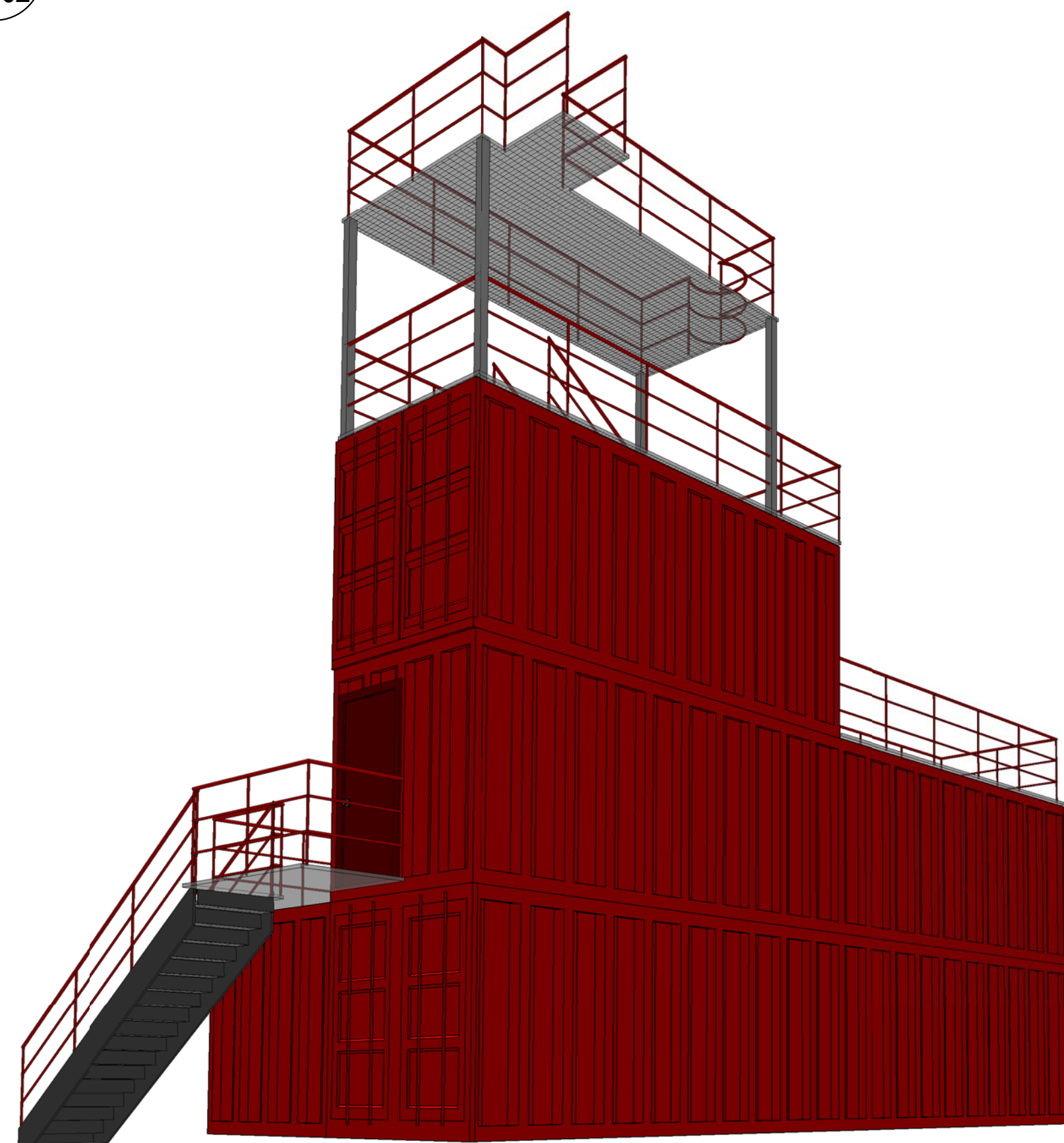
SHEET NUMBER  
**A-001**  
 Isometrics

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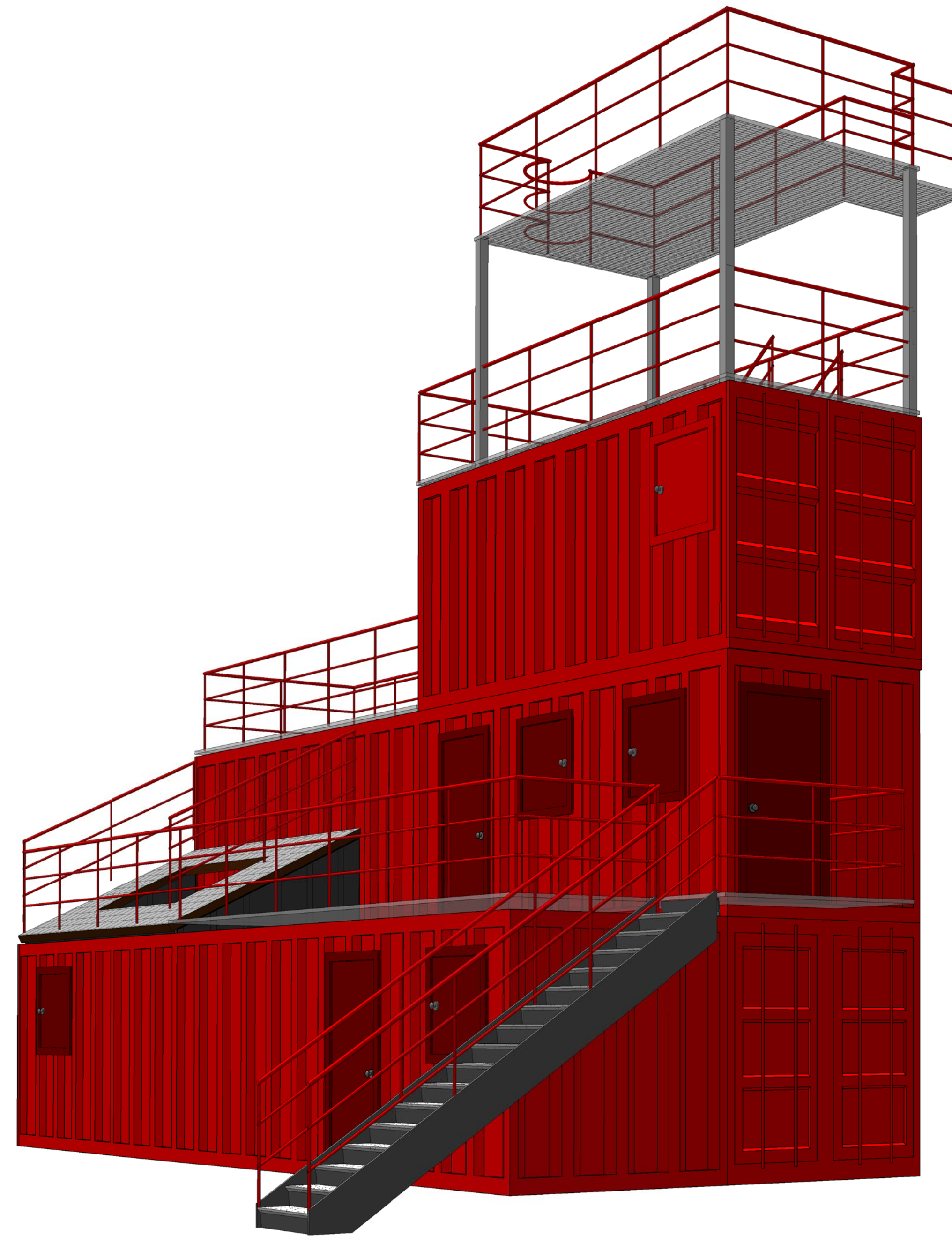




1 Northwest Perspective  
A-002



3 Northeast Perspective  
A-002



4 Southeast Perspective  
A-002



Ketchum Sun Valley Volunteer Association Training Center

219 Lewis Street; Lot 7 Northwood Sub.  
Ketchum, ID 83340

Design Review Set

5/24/2017

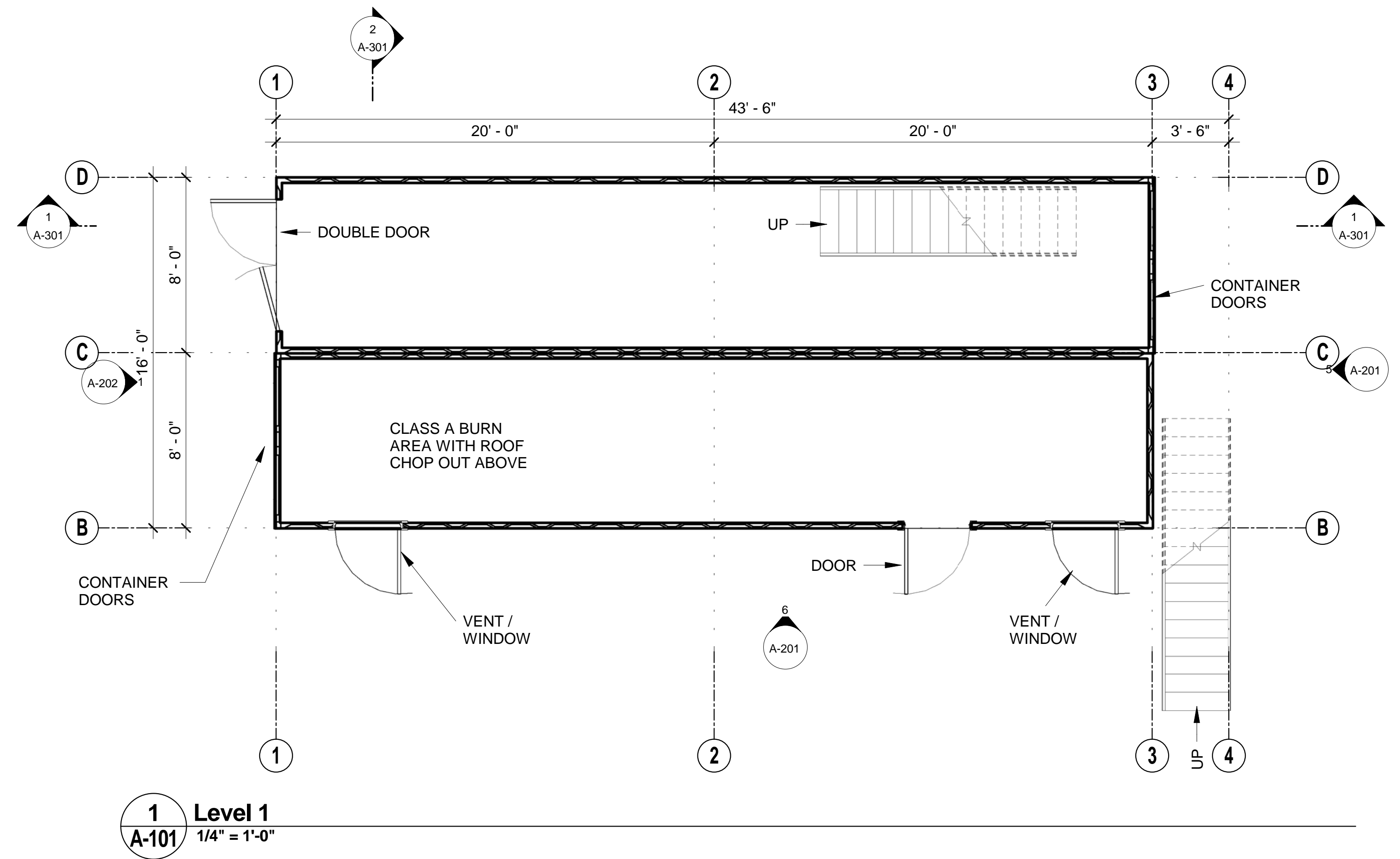
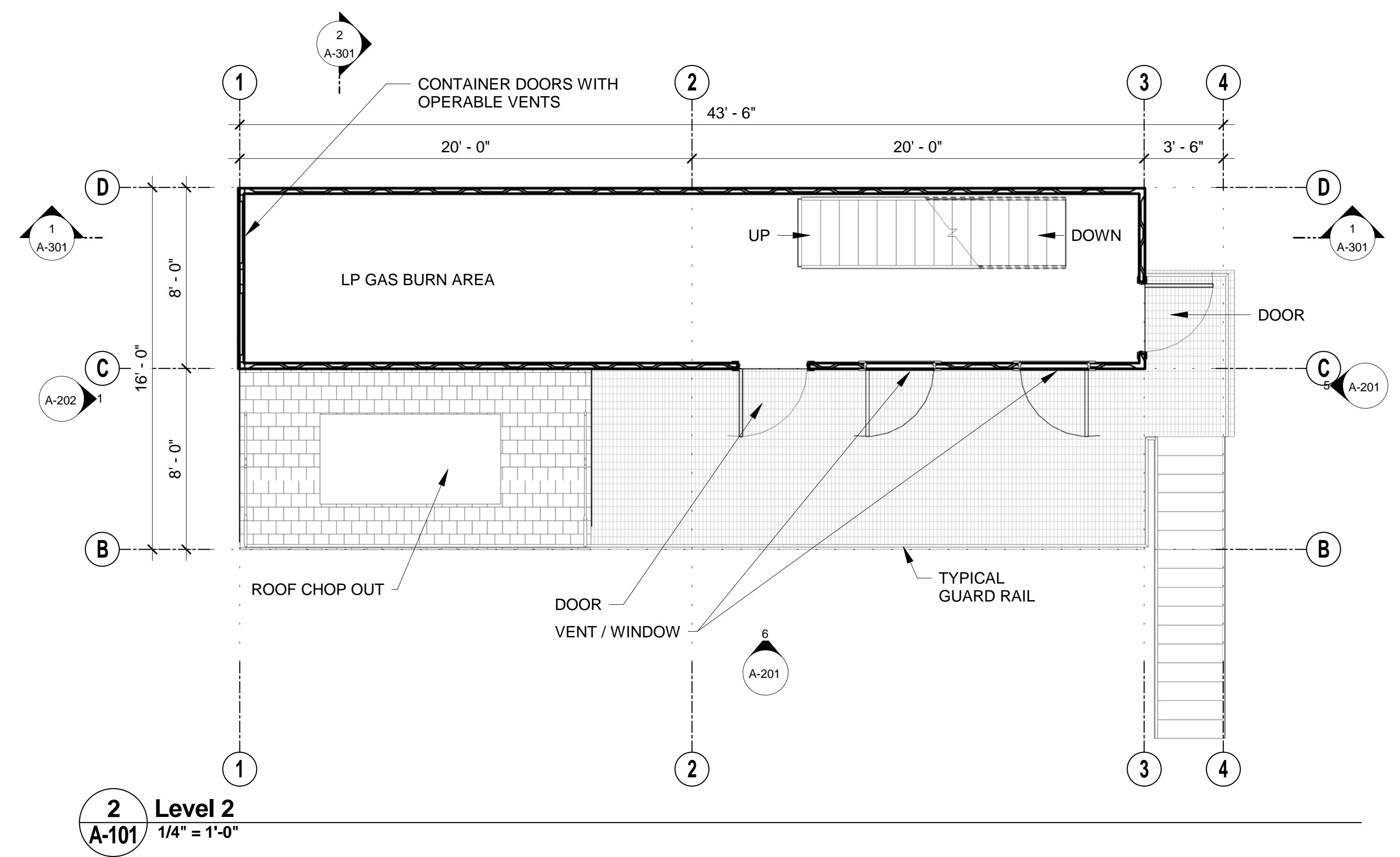
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A-002

Perspectives

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Ketchum Sun Valley Volunteer Association Training Center

219 Lewis Street; Lot 7 Northwood Sub.  
Ketchum, ID 83340

Design Review Set

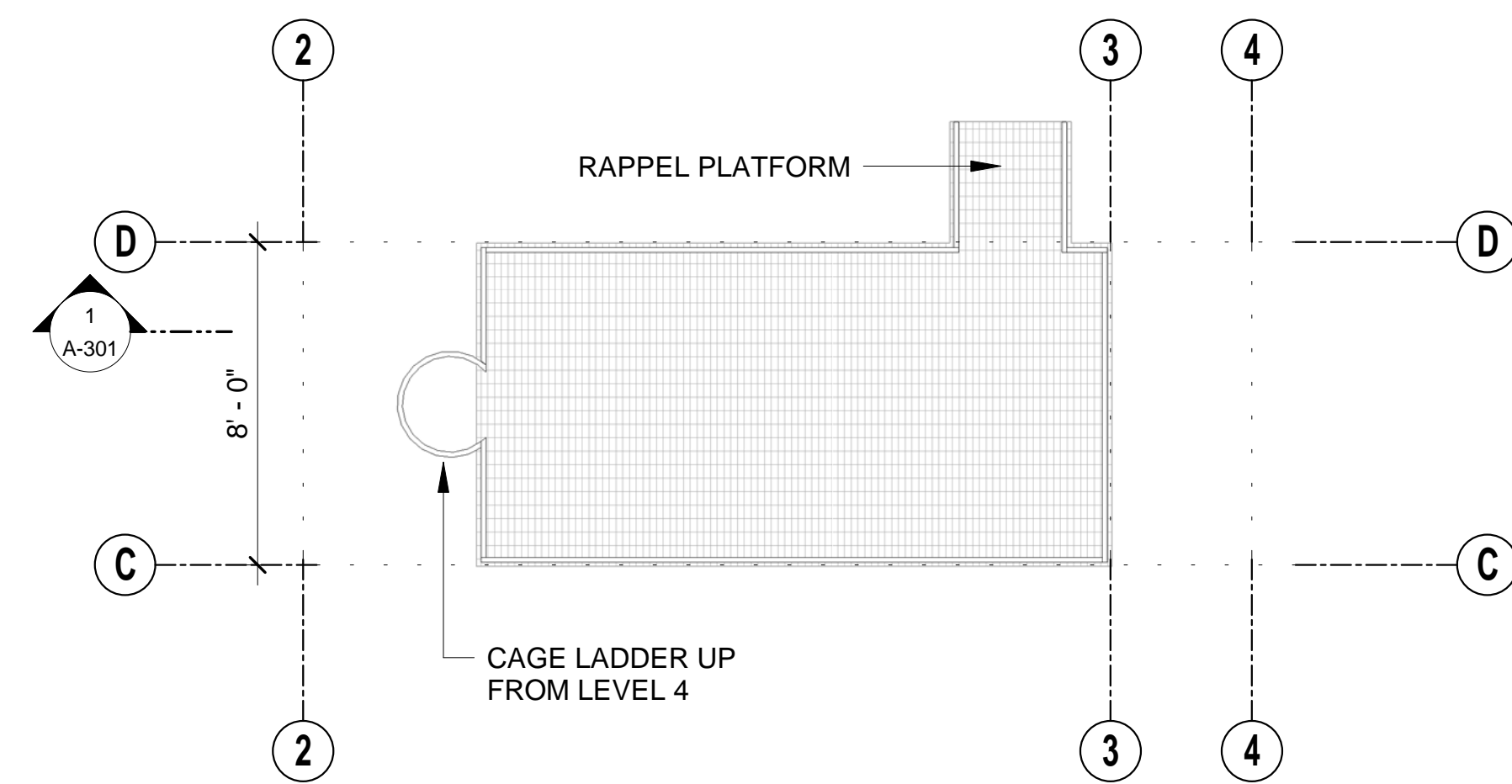
5/24/2017

SHEET NUMBER

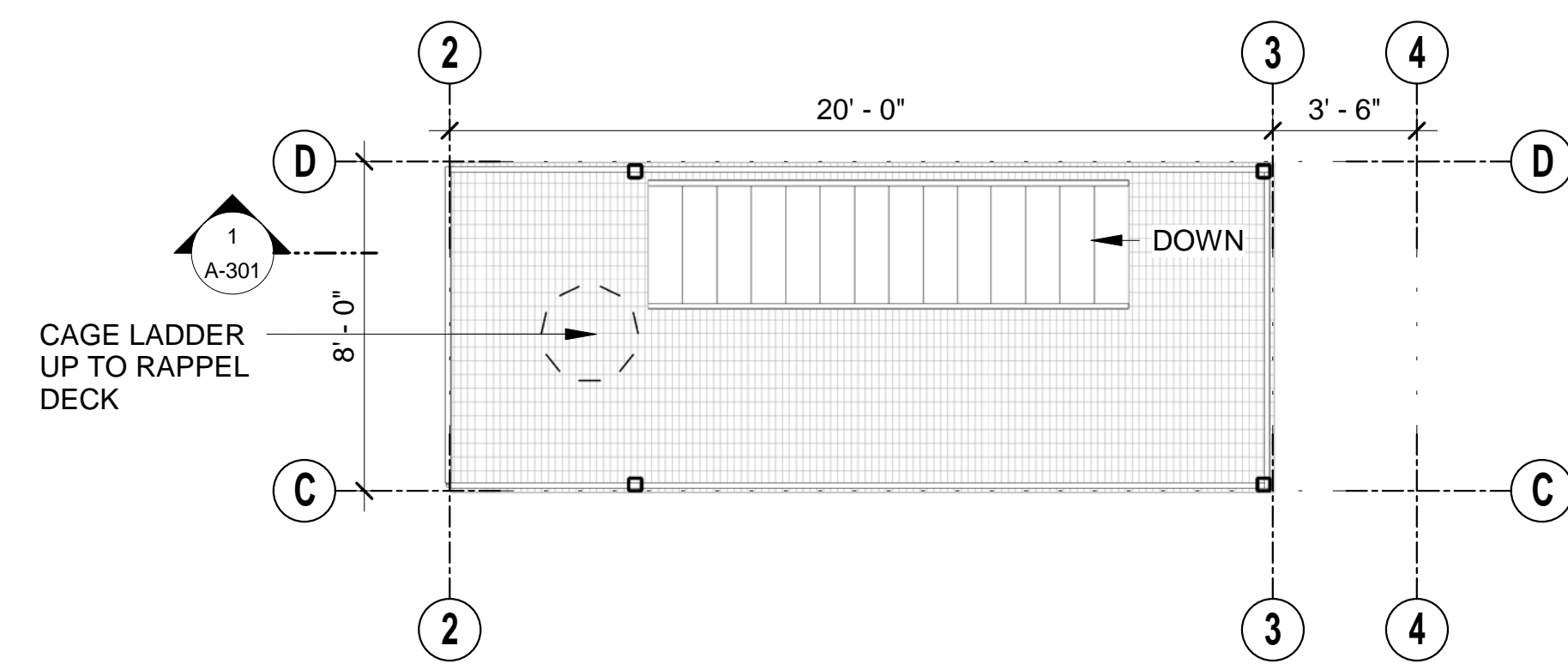
**A-101**

Plans

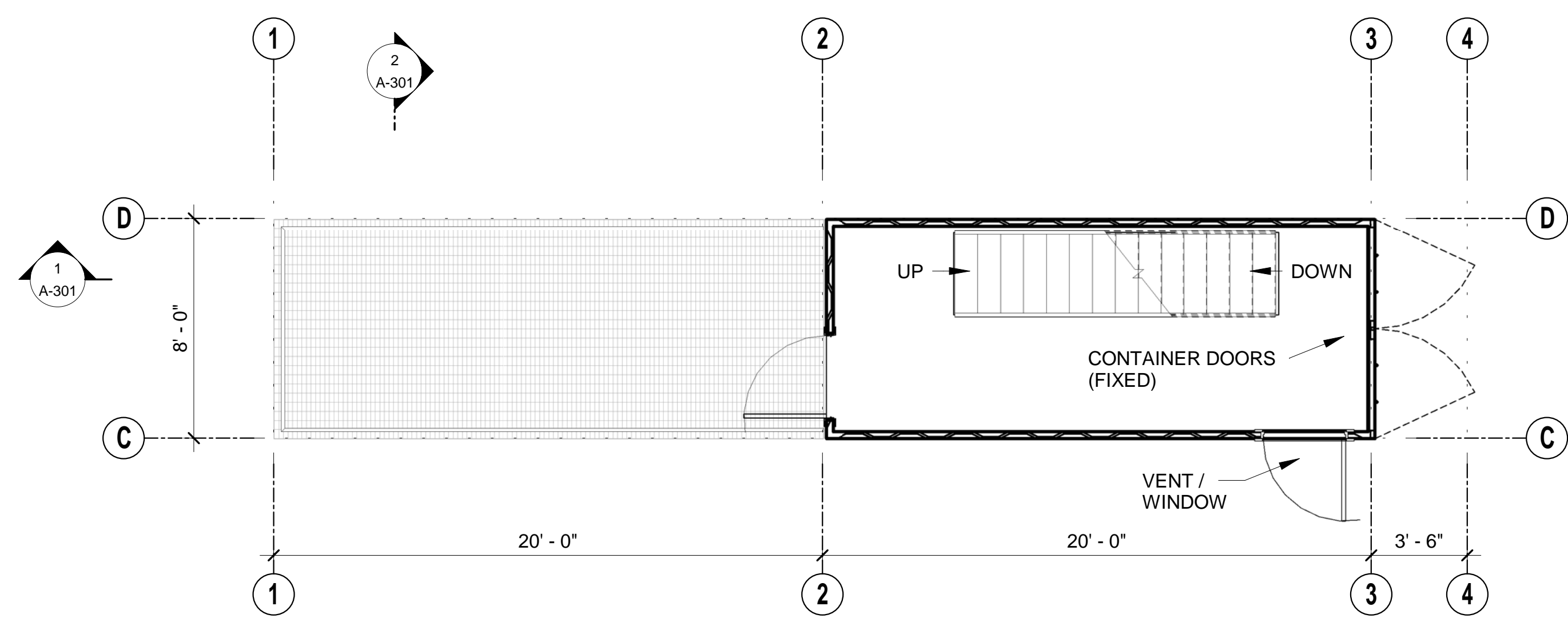
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**5 Level 5 Rappel Deck**  
A-102 1/4" = 1'-0"



**4 Level 4 Roof Deck**  
A-102 1/4" = 1'-0"



**3 Level 3**  
A-102 1/4" = 1'-0"

Ketchum Sun Valley Volunteer Association Training Center

219 Lewis Street; Lot 7 Northwood Sub.  
Ketchum, ID 83340

Design Review Set

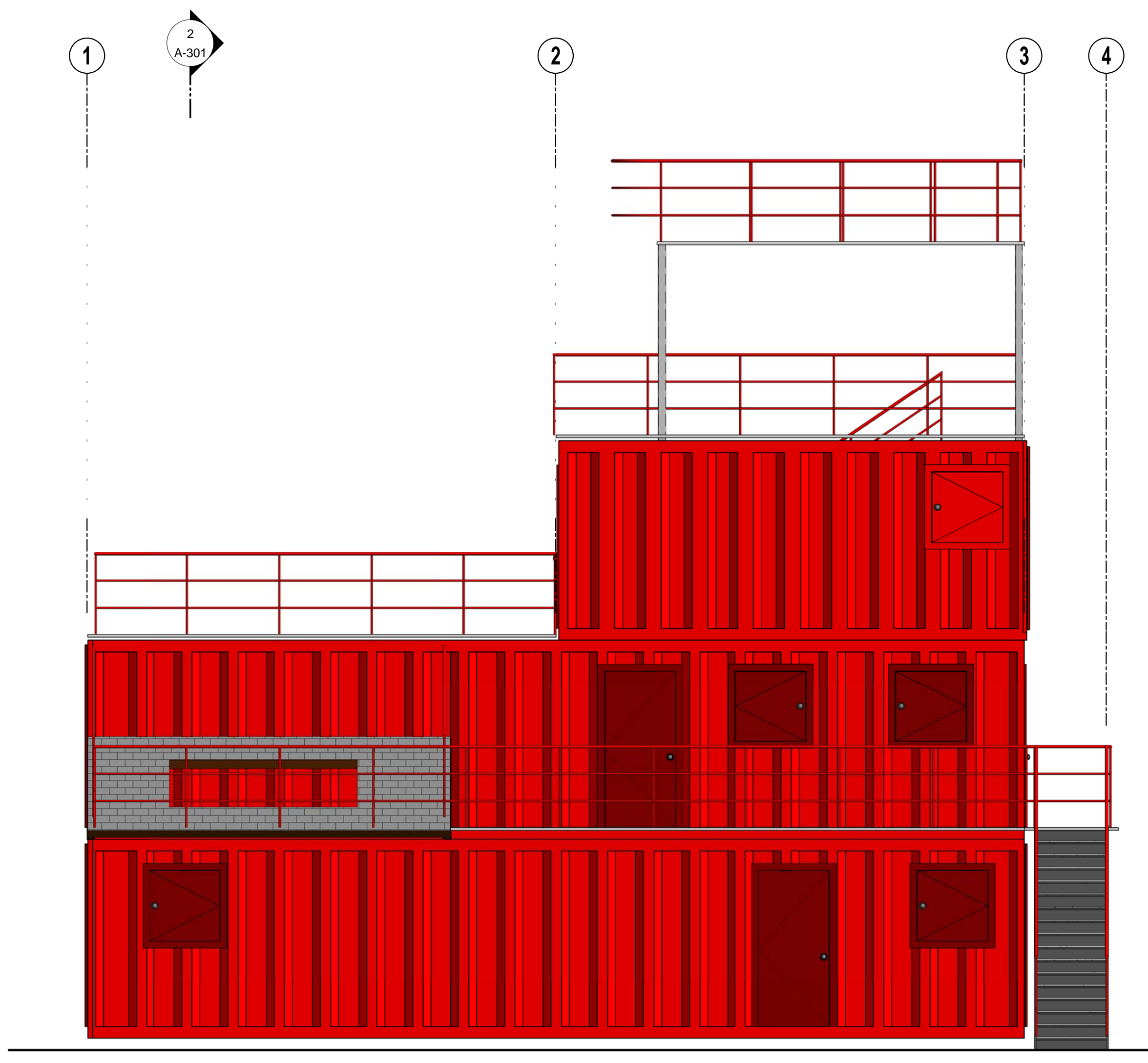
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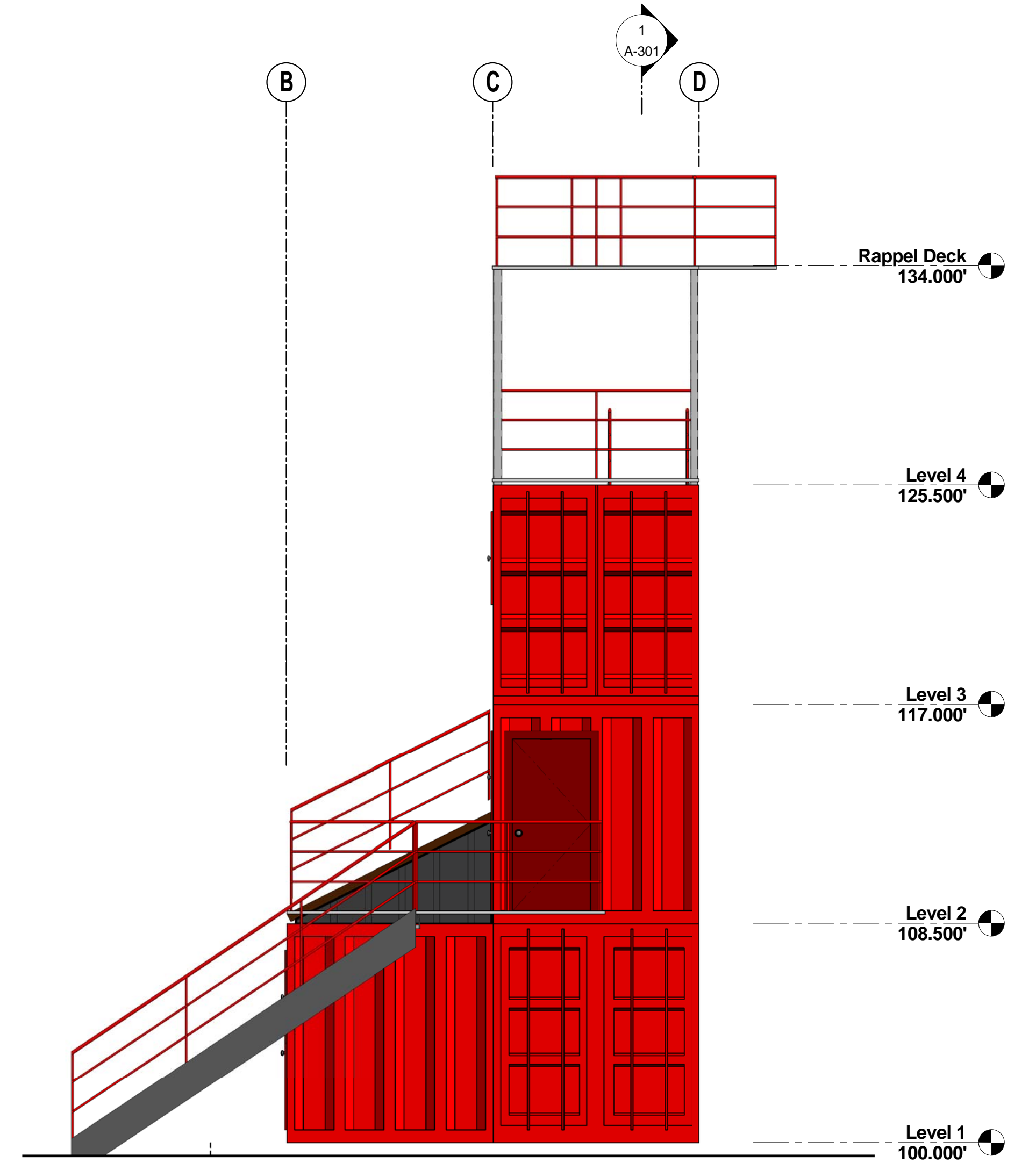
**A-102**  
Plans

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**6 South Elevation**  
A-201 1/4" = 1'-0"



**5 East (Lewis Street) Elevation**  
A-201 1/4" = 1'-0"



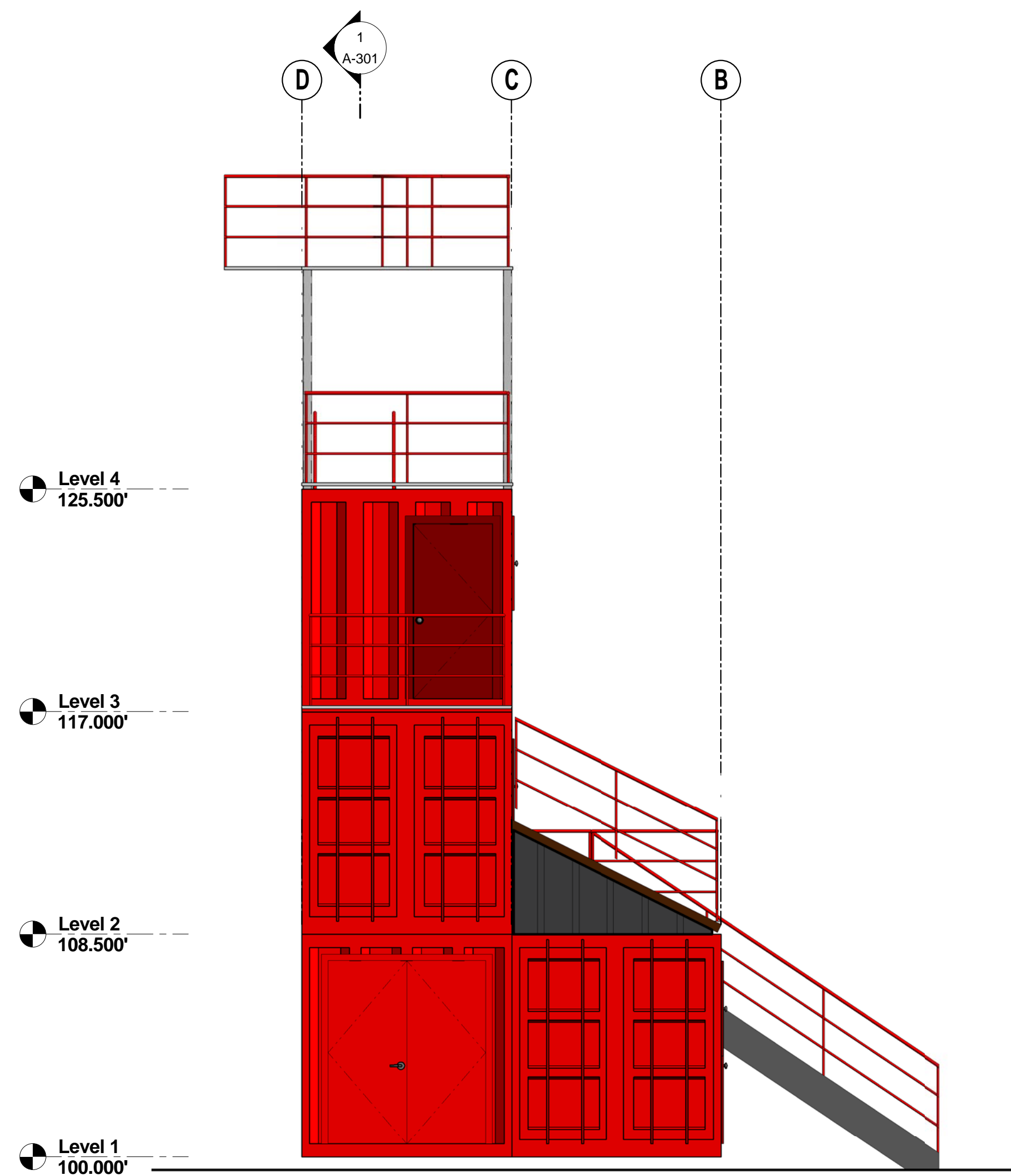
Ketchum Sun Valley Volunteer Association Training Center  
219 Lewis Street; Lot 7 Northwood Sub.  
Ketchum, ID 83340

Design Review Set

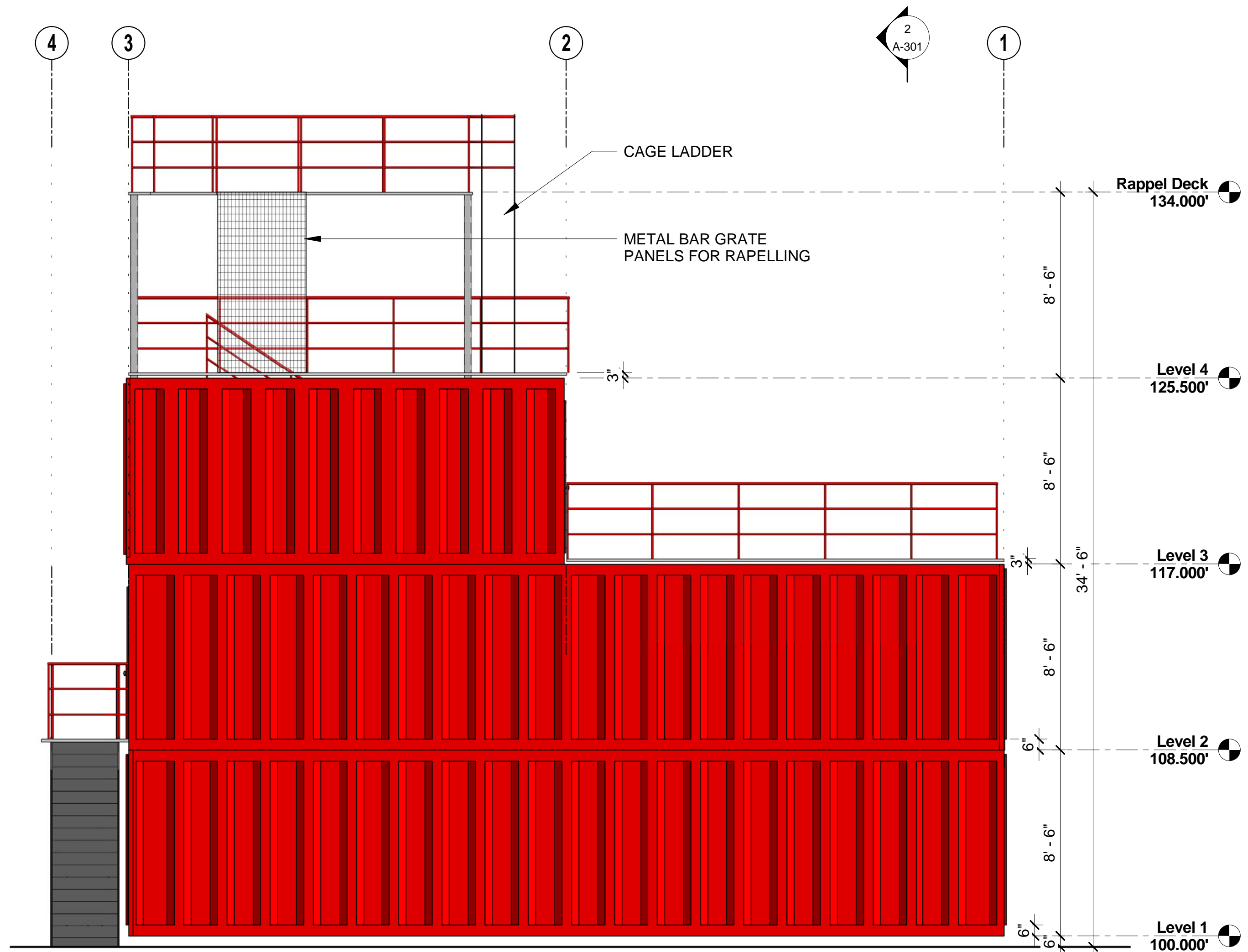
5/24/2017

SHEET NUMBER  
**A-201**  
Elevations





**1** West (YMCA) Elevation  
**A-202** 1/4" = 1'-0"



**2** North Elevation  
**A-202** 1/4" = 1'-0"



Ketchum Sun Valley Volunteer Association Training Center

219 Lewis Street; Lot 7 Northwood Sub.  
 Ketchum, ID 83340

Design Review Set

5/24/2017

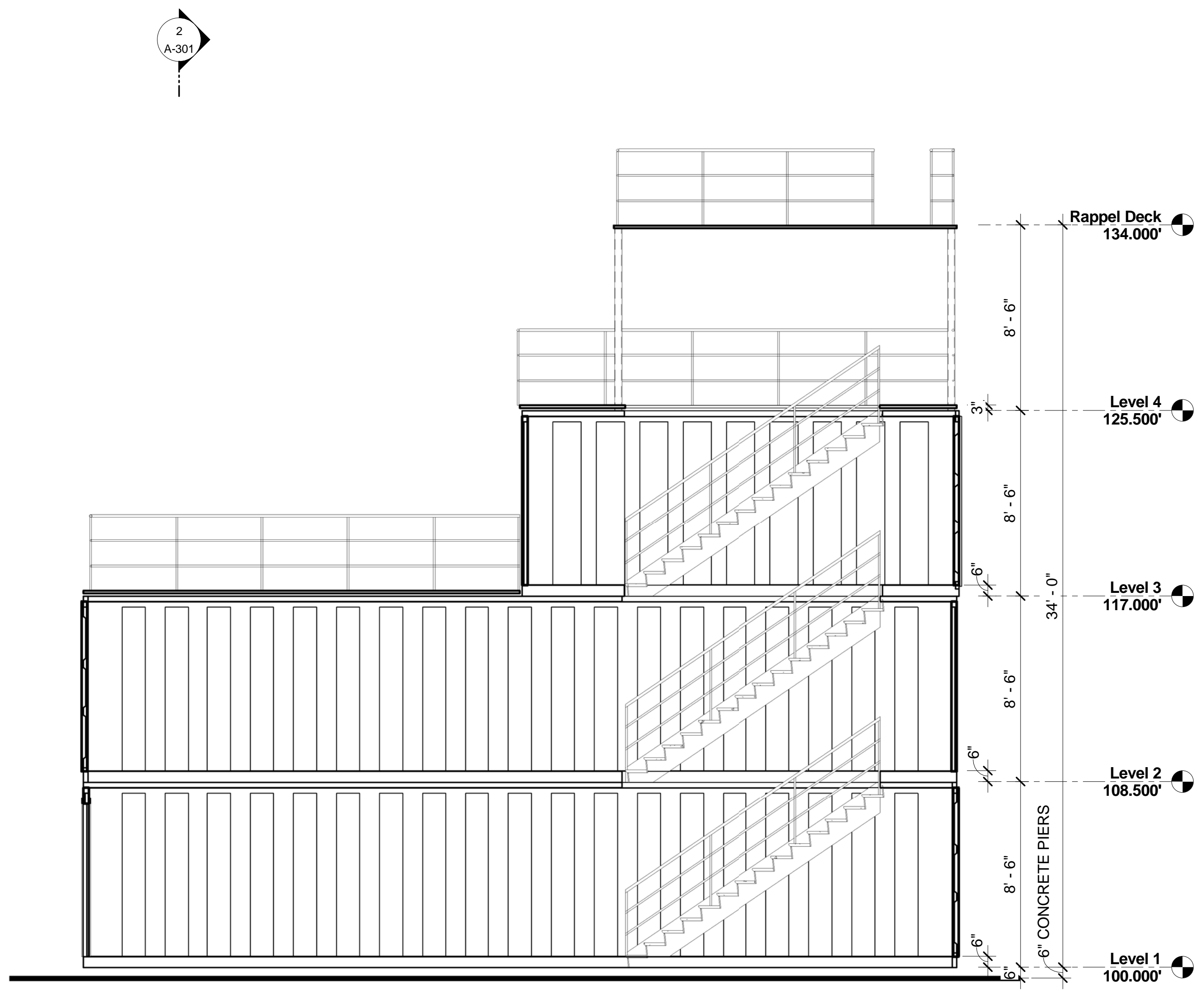
SHEET NUMBER

**A-202**

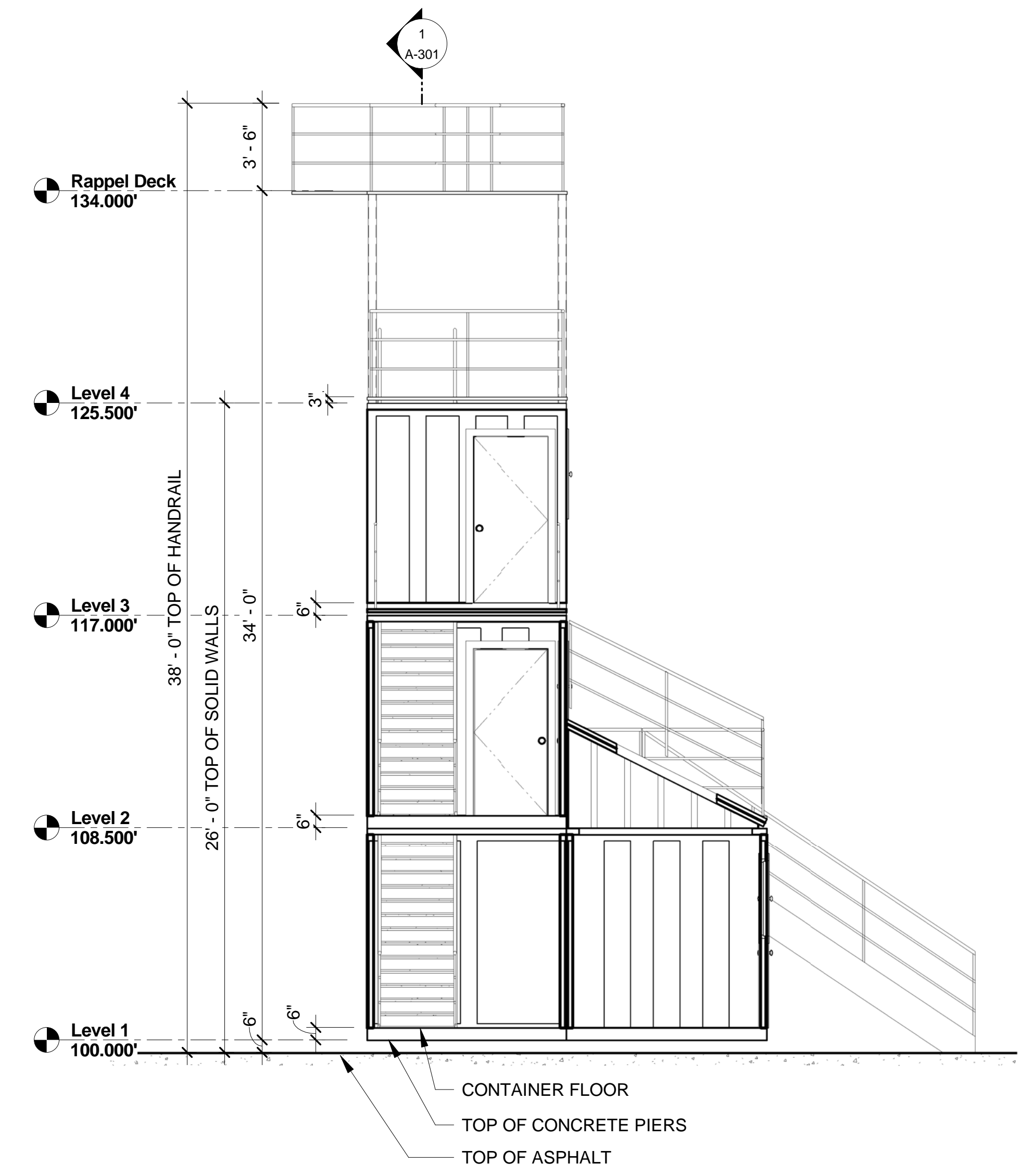
Elevations



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**1 Long Section**  
**A-301** 1/4" = 1'-0"



**2 Transverse Section**  
**A-301** 1/4" = 1'-0"



Ketchum Sun Valley Volunteer Association Training Center  
 219 Lewis Street; Lot 7 Northwood Sub.  
 Ketchum, ID 83340

Design Review Set

5/24/2017

SHEET NUMBER  
**A-301**  
 Sections





Galvanized Steel:  
Guardrails, Toe Boards, Stairs,  
Decks, Misc. Trim



Painted Steel:  
Benjamin Moore  
Bullseye Red 2002-20  
Typical Exterior of Containers



Sloped Roof:  
GAF Roofing Shingles  
Timberline Cool Series  
Weathered Wood

**Color and Material Images**



Ketchum Sun Valley Volunteer Association Training Center

219 Lewis Street; Lot 7 Northwood Sub.  
Ketchum, ID 83340

Design Review Set

5/24/2017

SHEET NUMBER

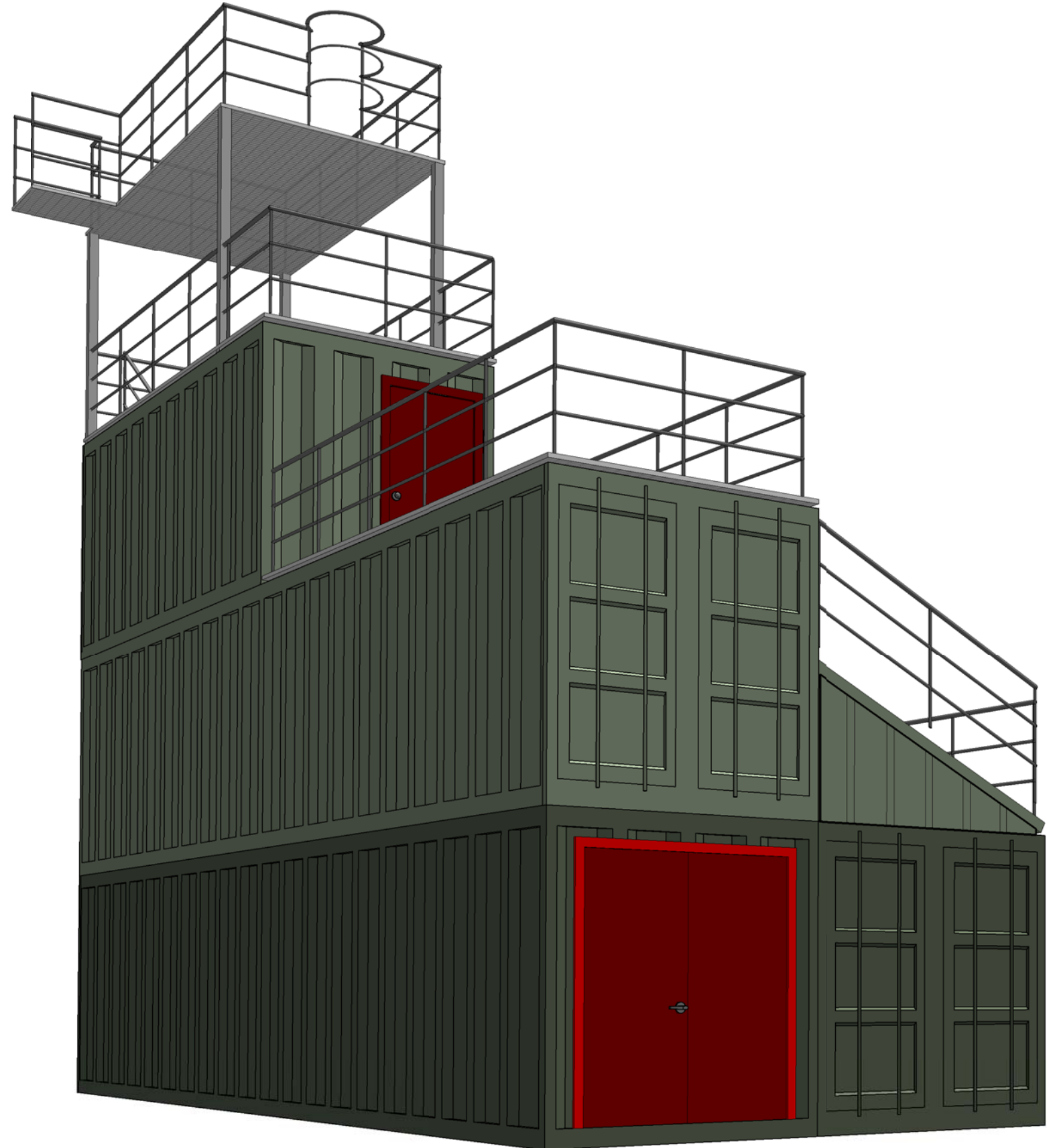
**A-003**

Material Images

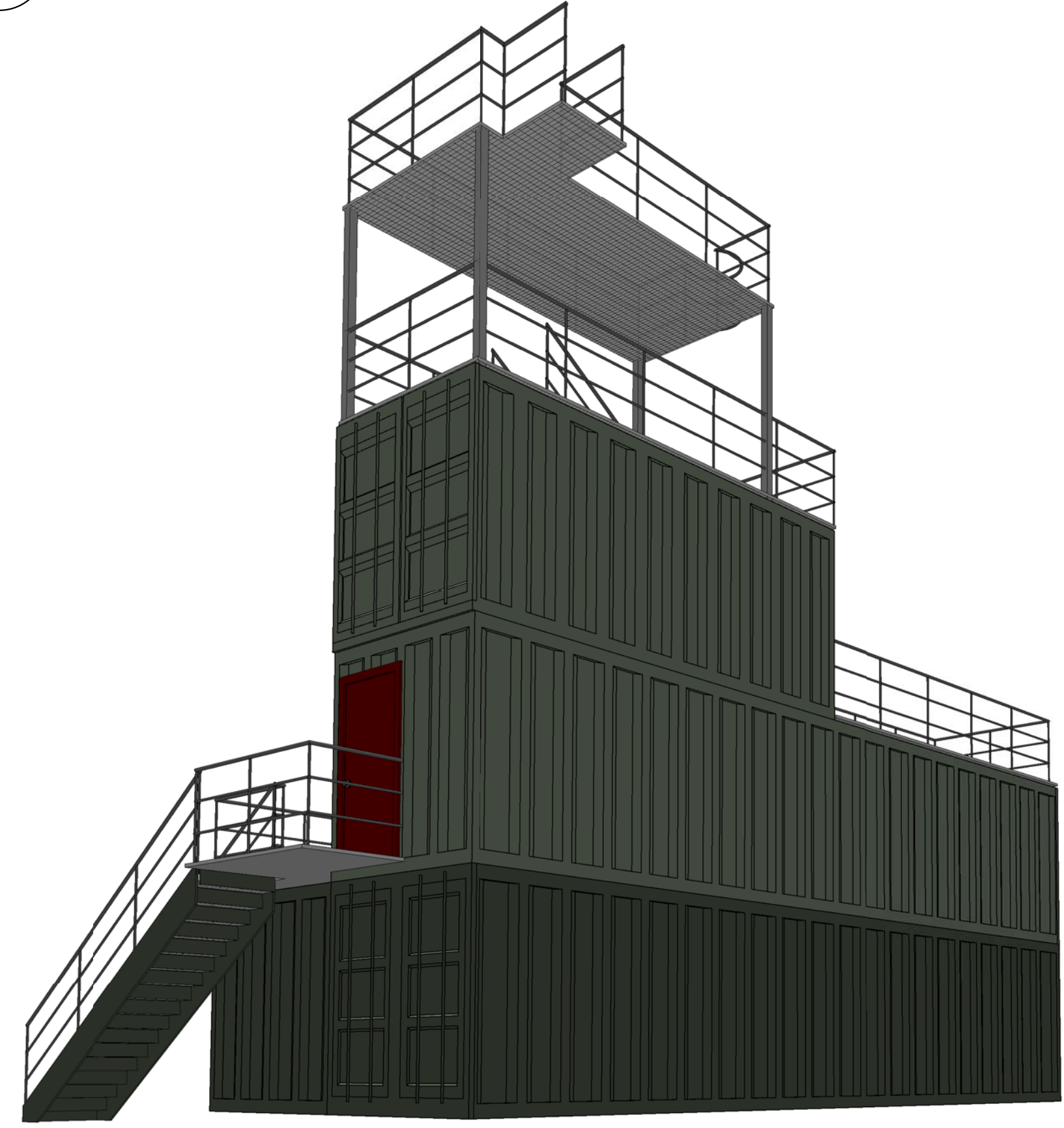
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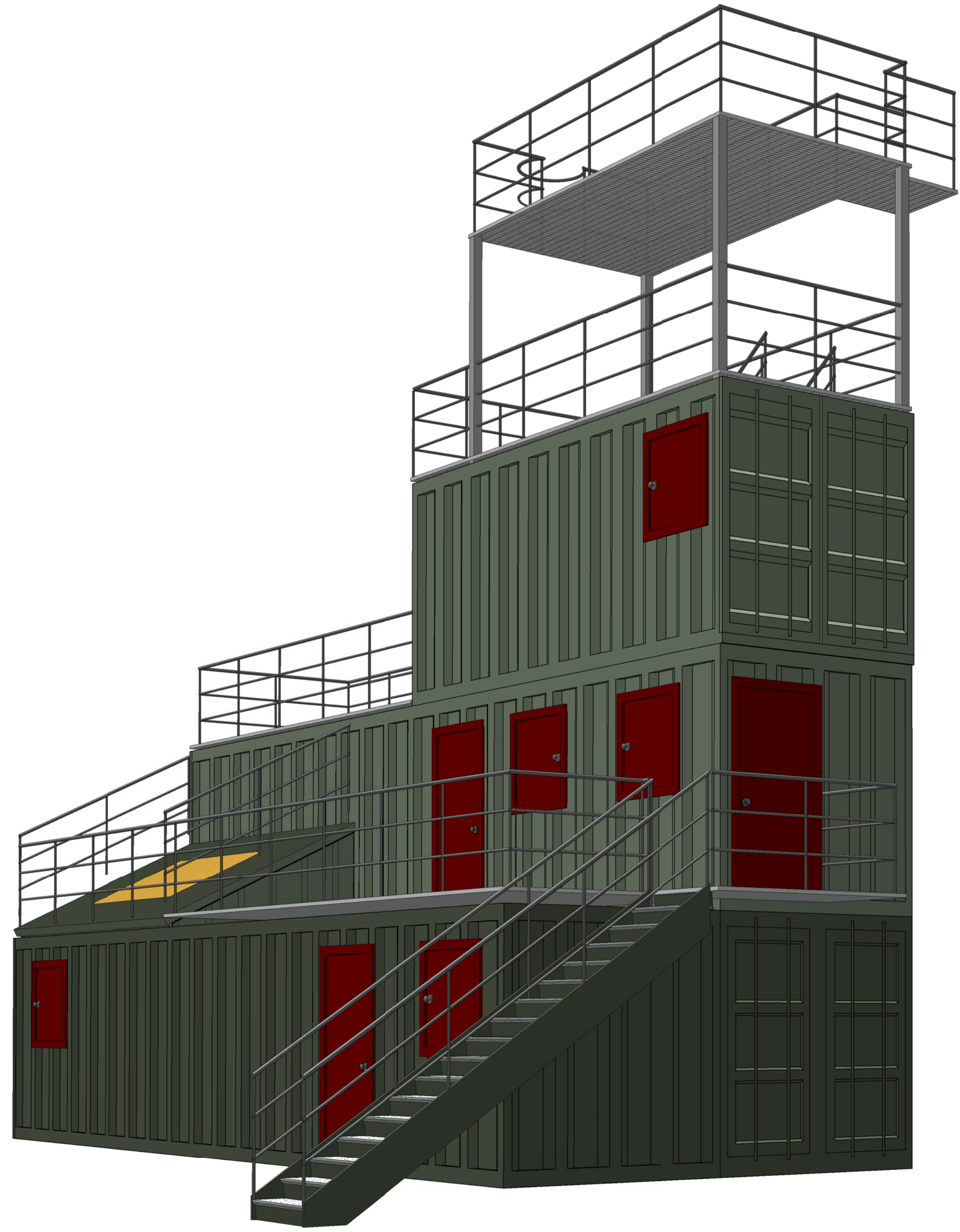
**ATTACHMENT F. REVISED PLANS, 6/7/2017**



1 Northwest Perspective  
A-002



3 Northeast Perspective  
A-002



4 Southeast Perspective  
A-002



Ketchum Sun Valley Volunteer Association Training Center

219 Lewis Street; Lot 7 Northwood Sub.  
Ketchum, ID 83340

Design Review Set

6/7/2017

SHEET NUMBER

A-002

Perspectives

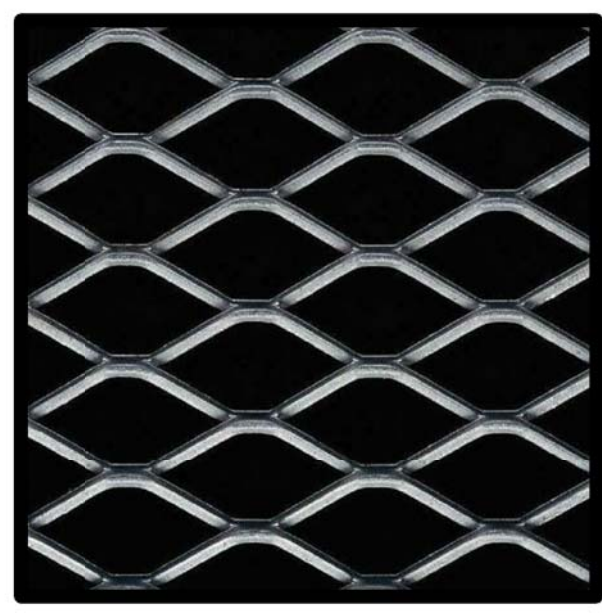
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Vertical Panels at Rappel Deck:

Galvanized Expanded Metal Mesh



Sloped Roof at Level 2:

Galvanized Expanded Metal Mesh Over Steel Plate; all painted to match Level 2 Container.

With 4' x 8' unfinished plywood or OSB insert for cutting

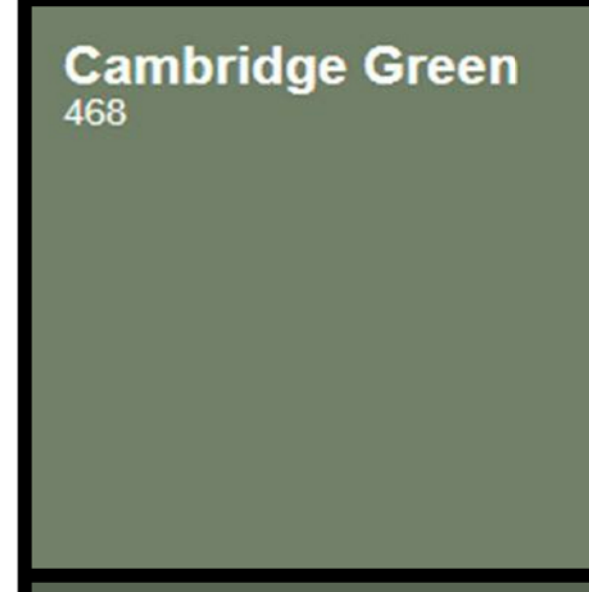


Rappel Deck and all Guardrails, Toe Boards, Stairs, Etc.

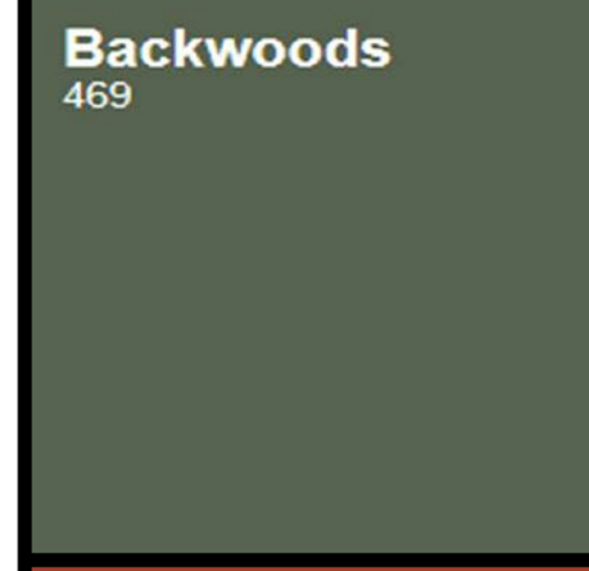
Galvanized Steel Bar Grate  
Galvanizes Steel Tube and Plate



Level 3 Container:  
  
Painted Steel,  
Benjamin Moore  
Cambridge Green 468



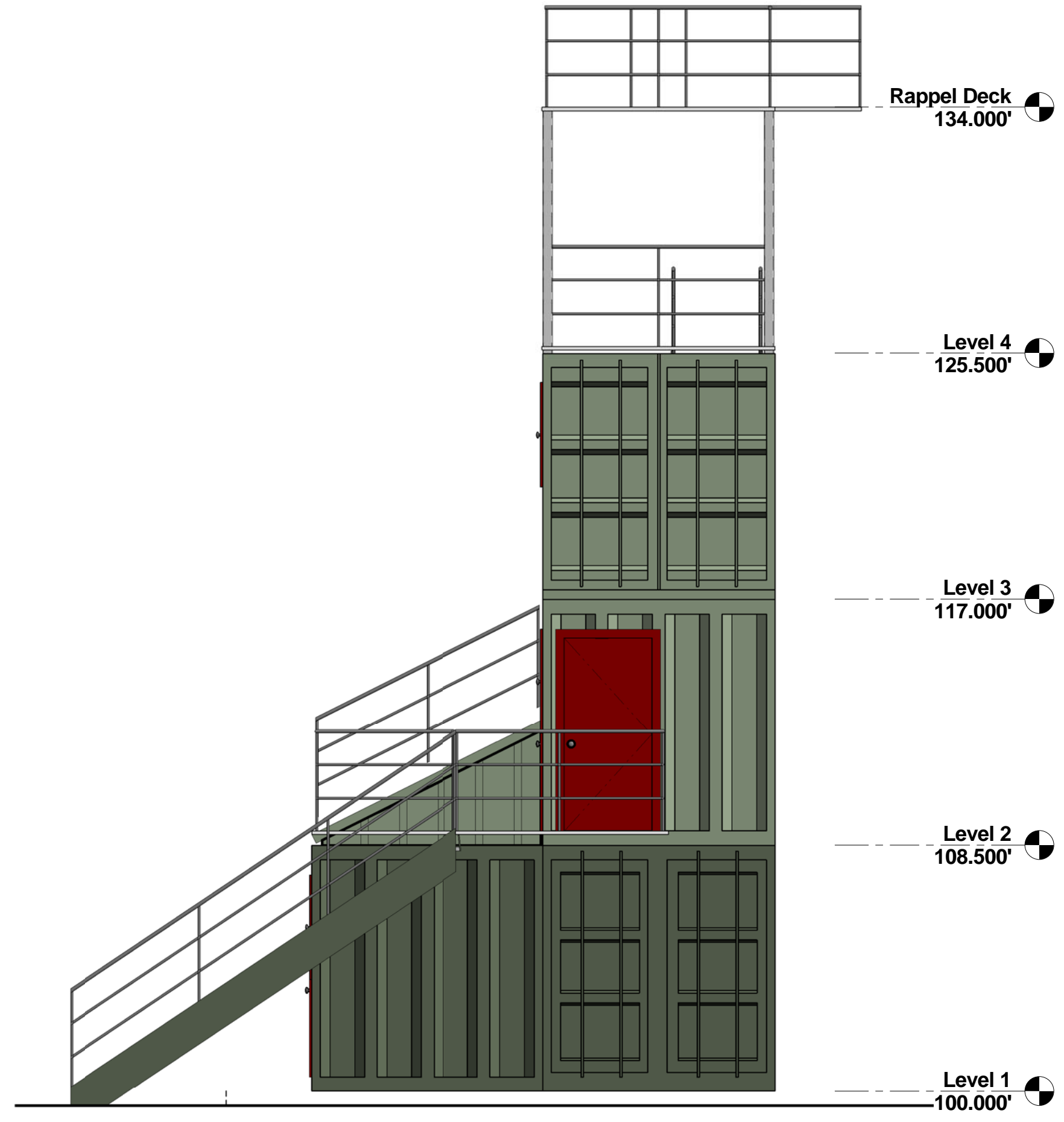
Level 2 Container  
  
Painted Steel,  
Benjamin Moore  
Cambridge Green 468



Level 1 Containers and Level 1 Exterior Stair  
  
Painted Steel,  
Benjamin Moore  
Backwoods 469



Windows and Doors  
  
Painted Steel,  
Benjamin Moore  
Merlot Red 2006-10



Ketchum Sun Valley Volunteer Association Training Center

219 Lewis Street; Lot 7 Northwood Sub.  
Ketchum, ID 83340

Design Review Set

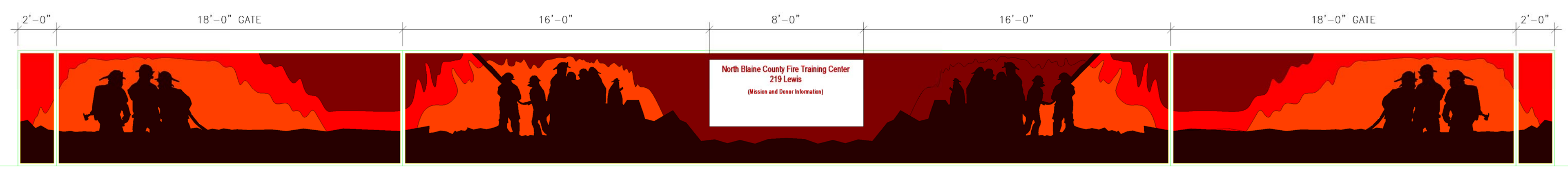
6/7/2017

SHEET NUMBER

**A-003**

Material Images & Graphics

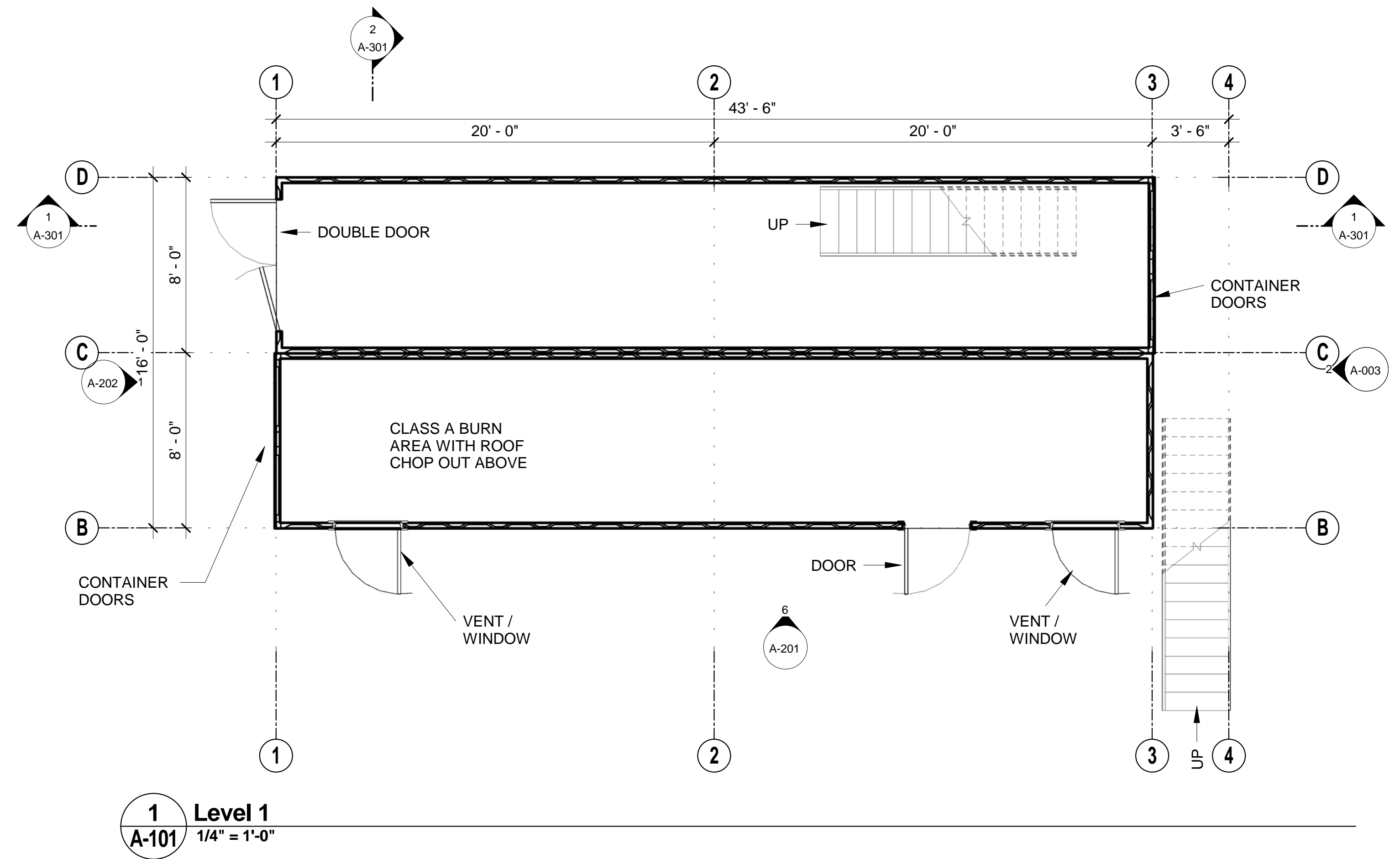
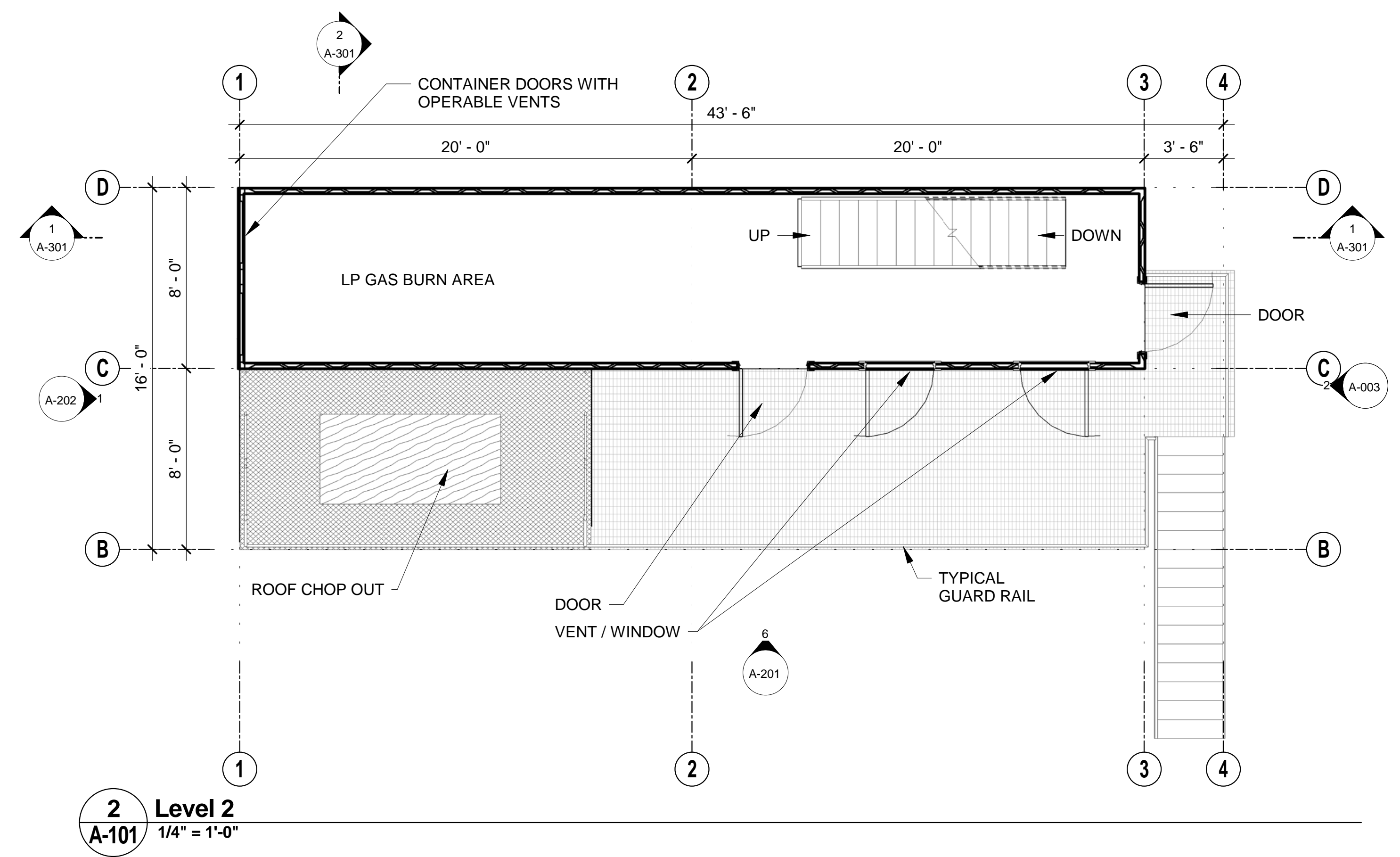
**1 Color and Material Images - Greens**  
A-003 6" = 1'-0"



**3 Lewis Street Fence Graphics**  
A-003 1/4" = 1'-0"

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6/7/2017 4:58:33 PM ORIGINAL SHEET SIZE 22" x 34"





Ketchum Sun Valley Volunteer Association Training Center

219 Lewis Street; Lot 7 Northwood Sub.  
Ketchum, ID 83340

Design Review Set

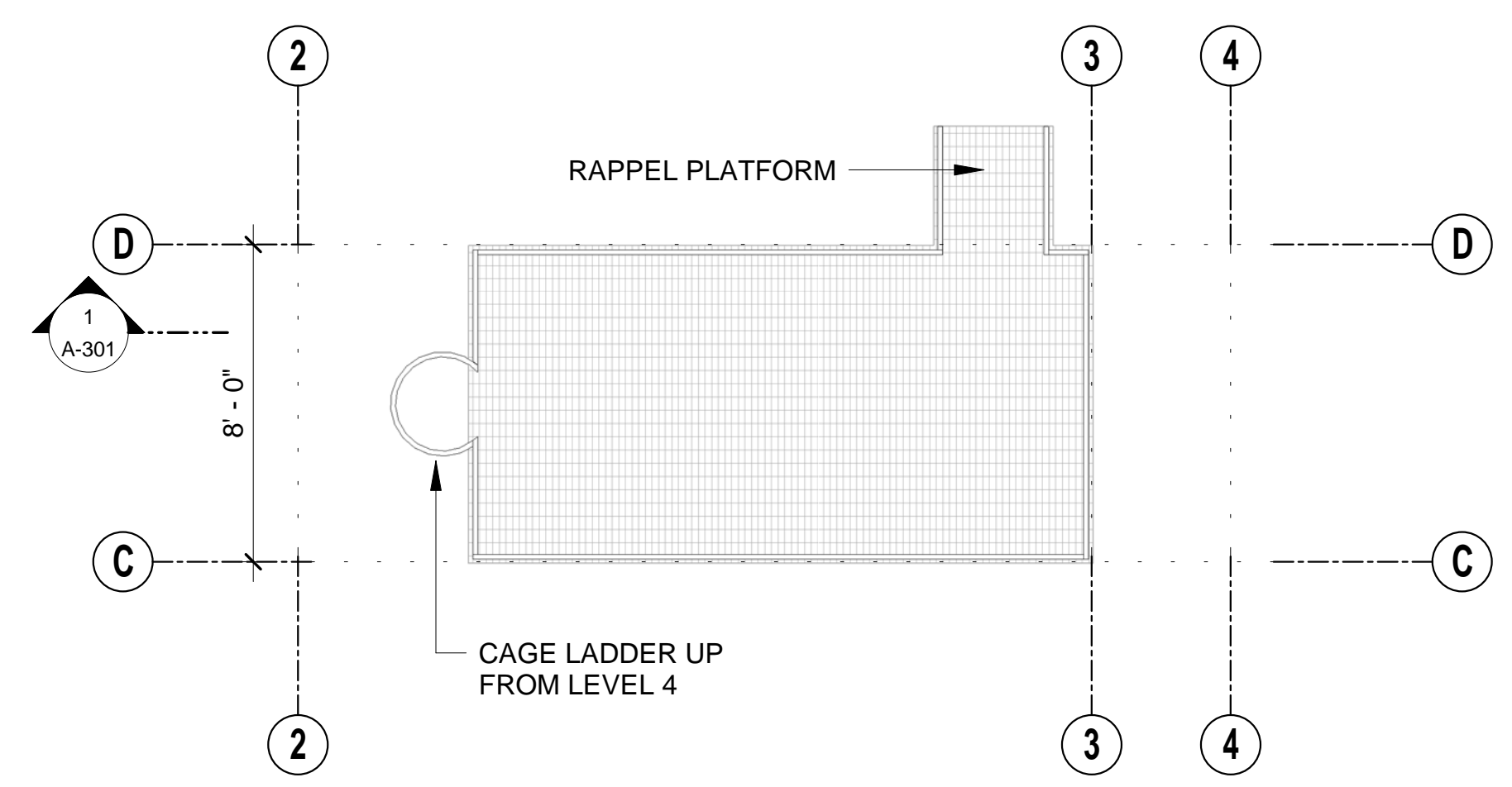
6/7/2017

SHEET NUMBER

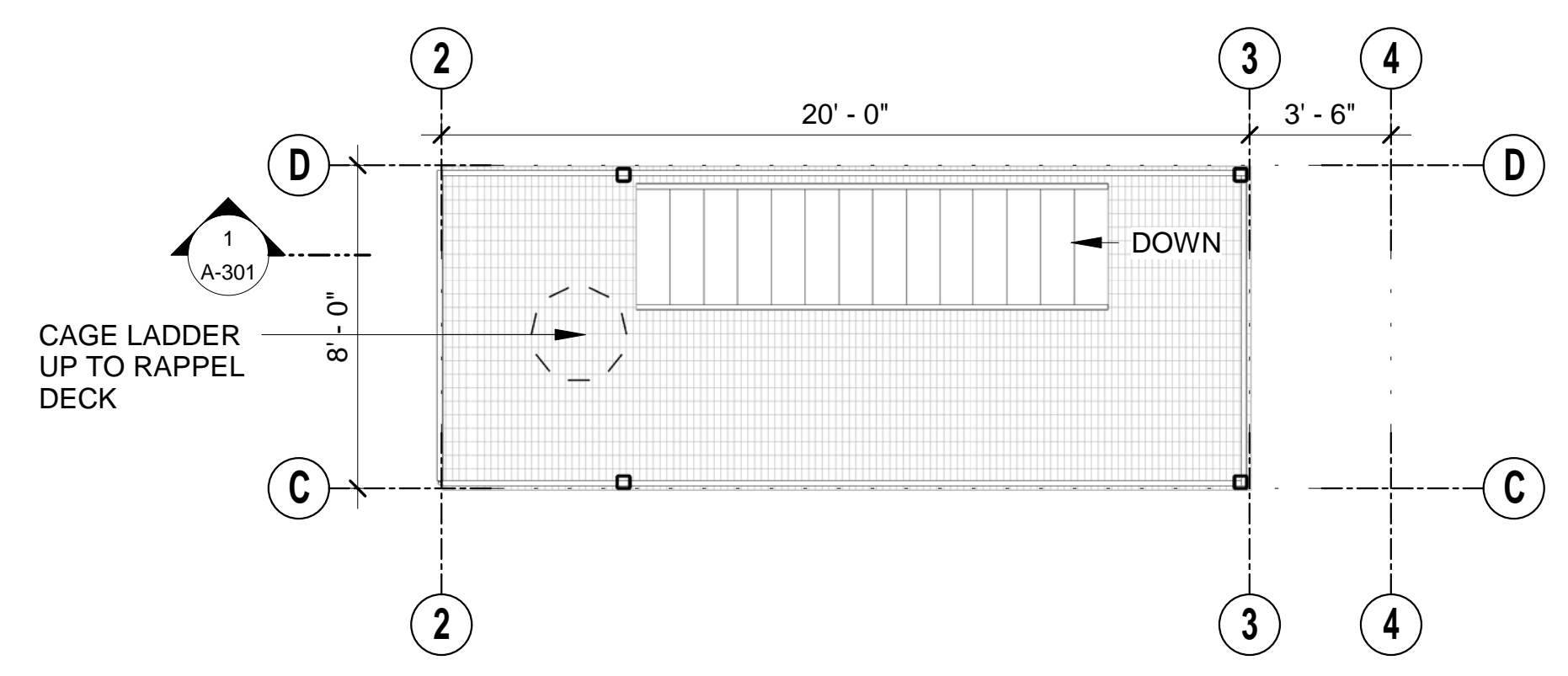
**A-101**

Plans

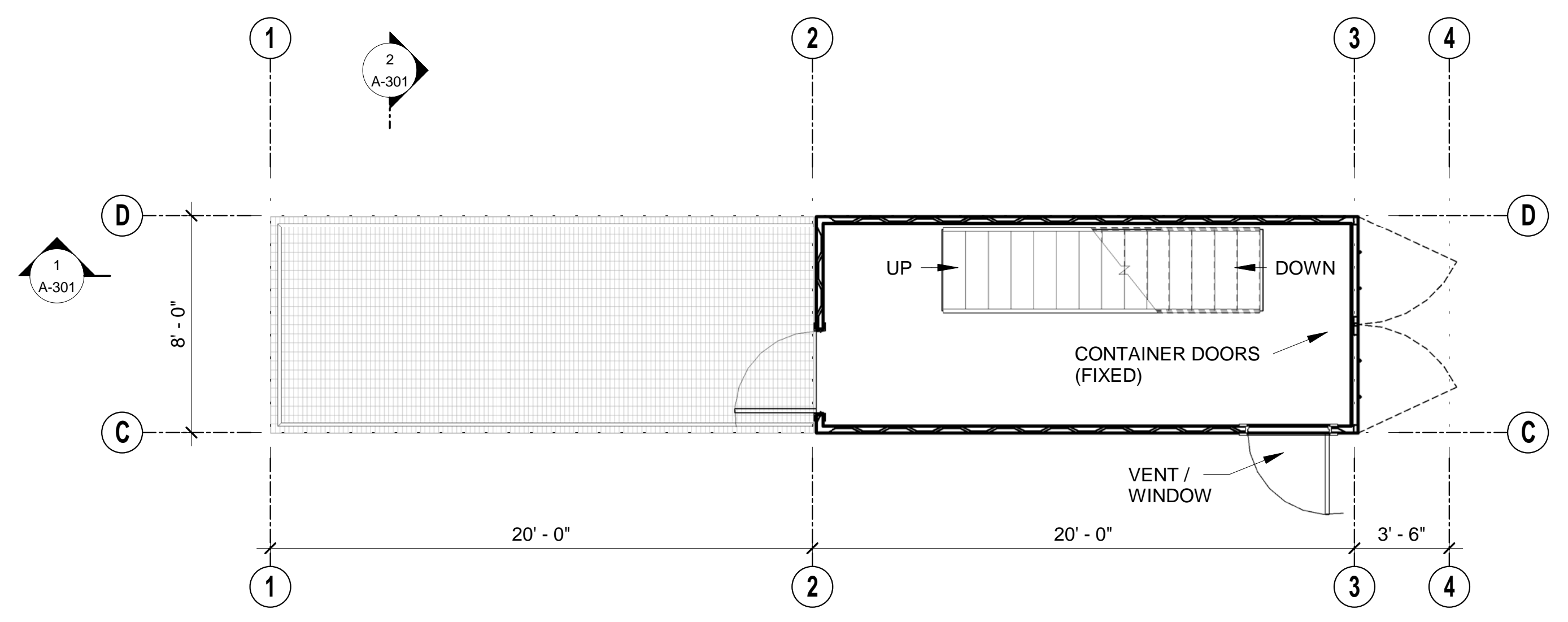
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**5 Level 5 Rappel Deck**  
A-102 1/4" = 1'-0"



**4 Level 4 Roof Deck**  
A-102 1/4" = 1'-0"



**3 Level 3**  
A-102 1/4" = 1'-0"

Ketchum Sun Valley Volunteer Association Training Center

219 Lewis Street; Lot 7 Northwood Sub.  
Ketchum, ID 83340

Design Review Set

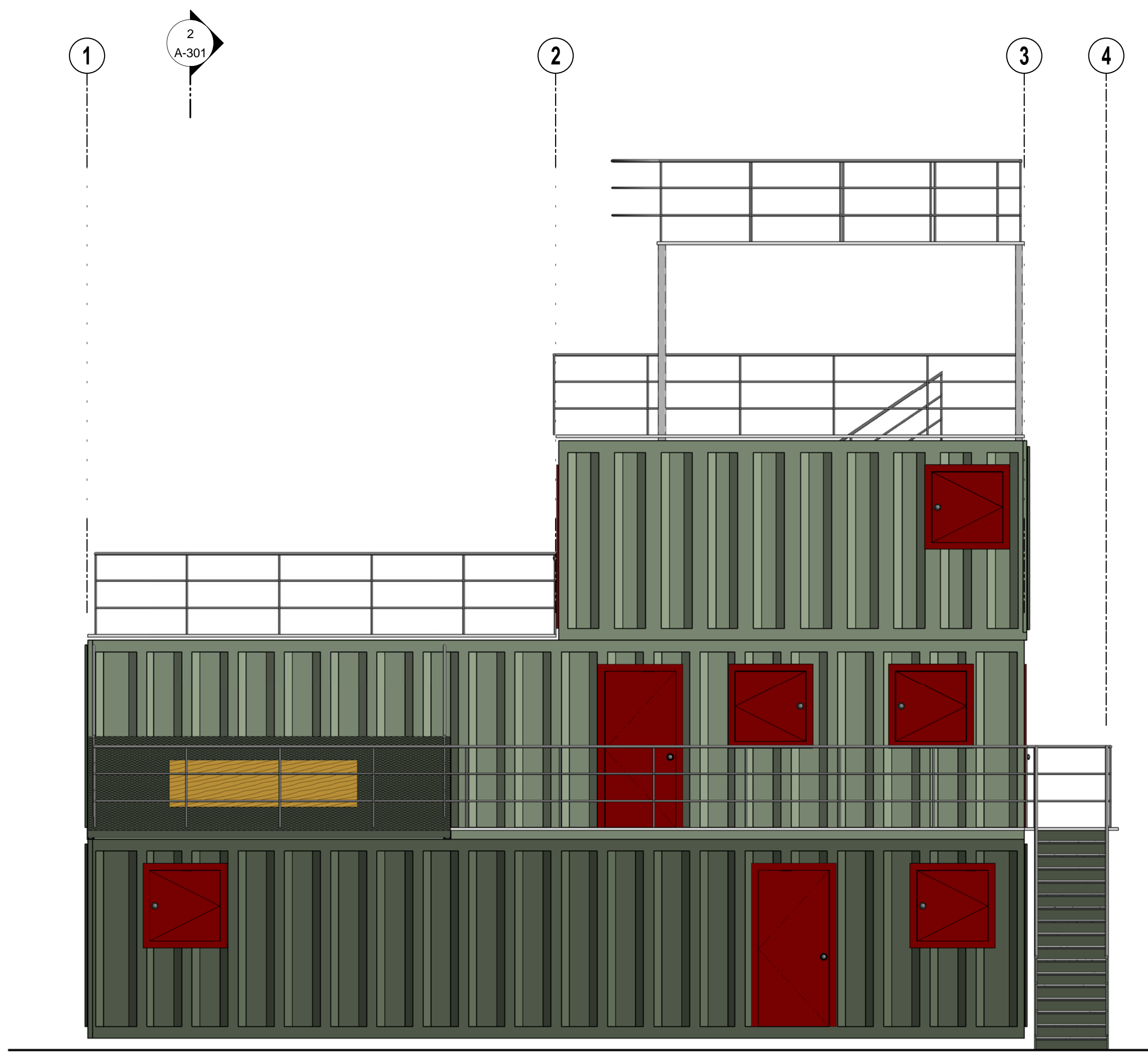
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SHEET NUMBER

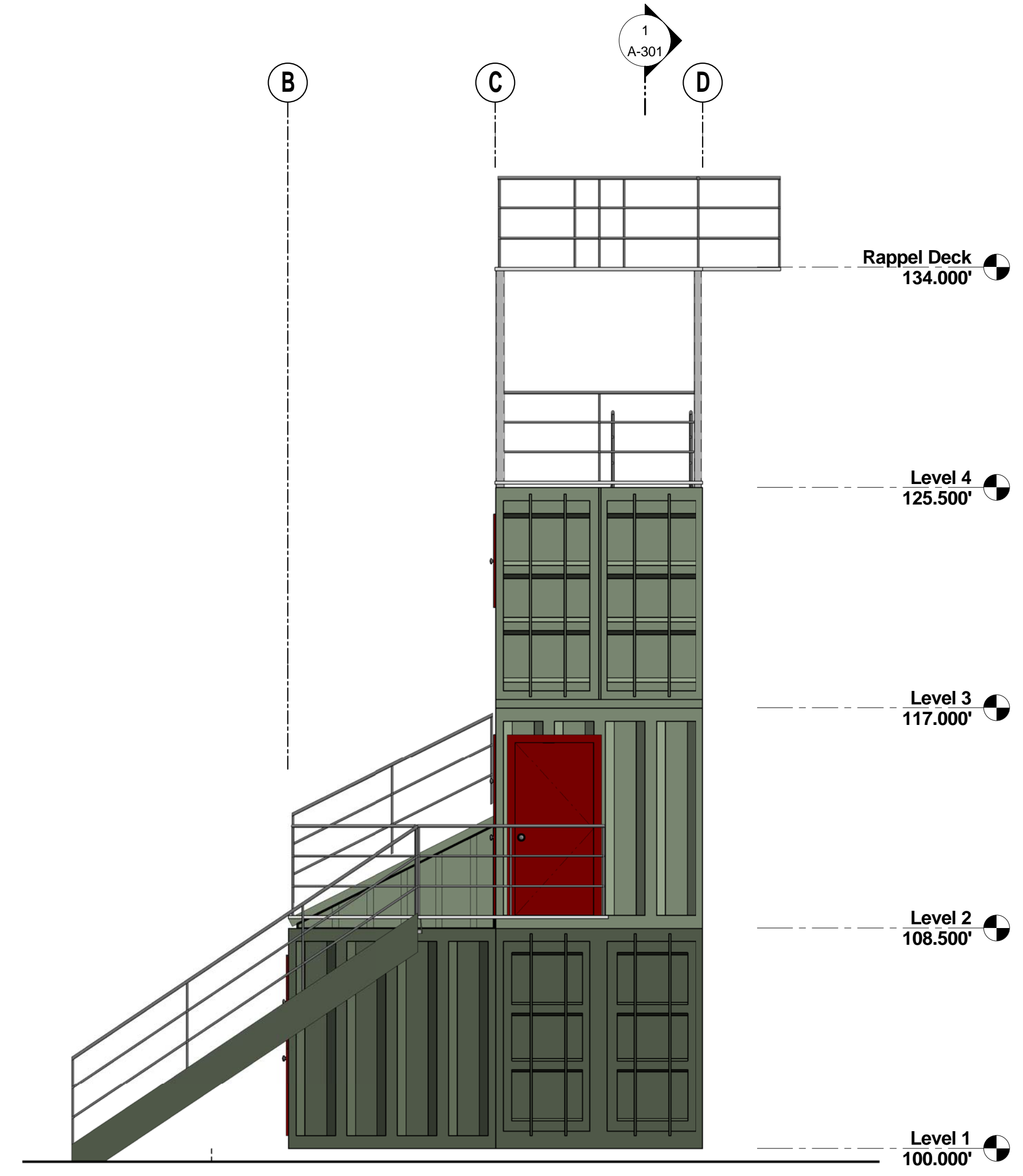
**A-102**  
Plans

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6/7/2017 4:58:49 PM ORIGINAL SHEET SIZE 22" x 34"



**6** South Elevation  
A-201 1/4" = 1'-0"



**5** East (Lewis Street) Elevation  
A-201 1/4" = 1'-0"



Ketchum Sun Valley Volunteer Association Training Center  
219 Lewis Street; Lot 7 Northwood Sub.  
Ketchum, ID 83340

Design Review Set

6/7/2017

SHEET NUMBER  
**A-201**  
Elevations





Ketchum Sun Valley Volunteer Association Training Center

219 Lewis Street; Lot 7 Northwood Sub.  
Ketchum, ID 83340

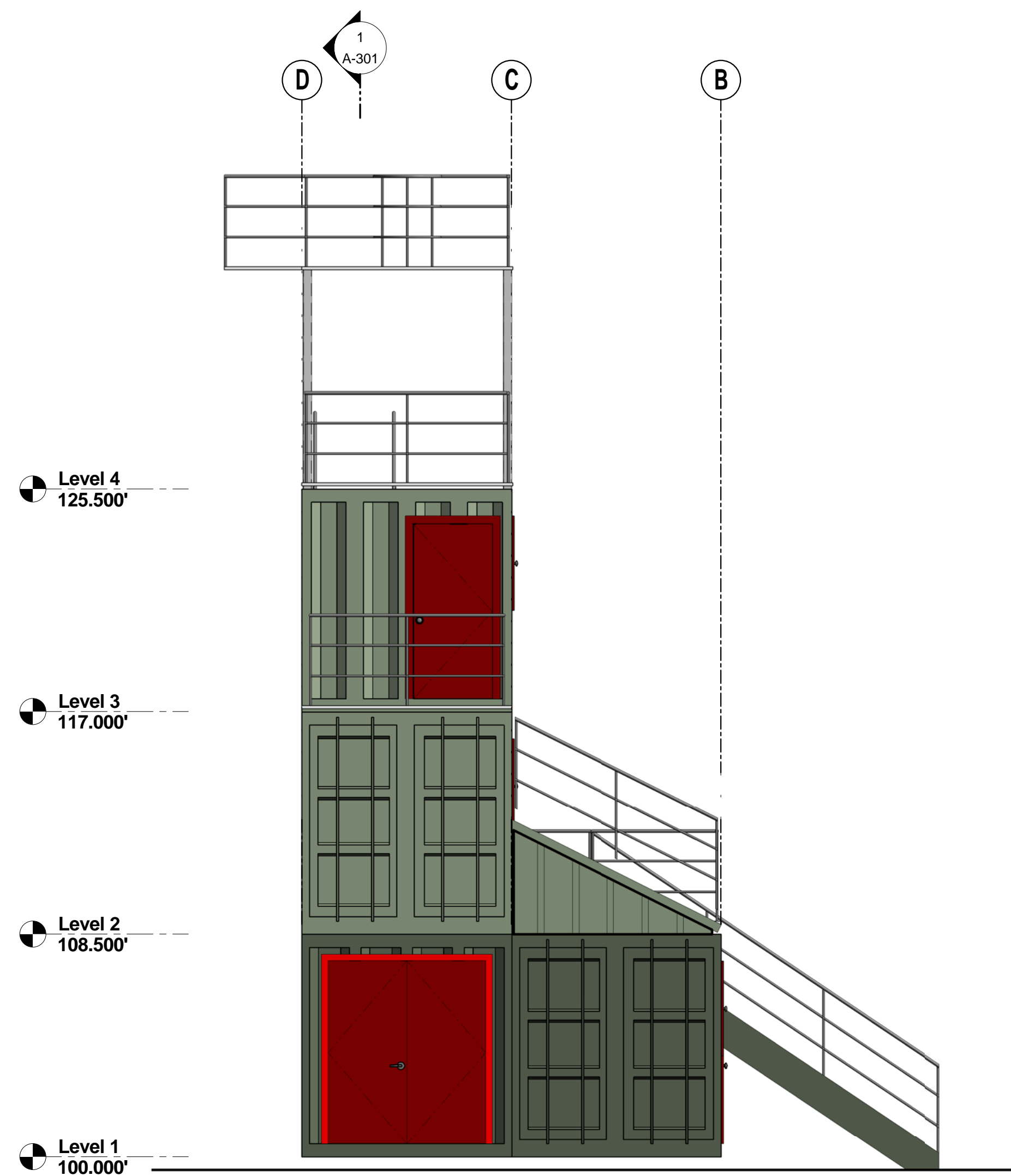
Design Review Set

6/7/2017

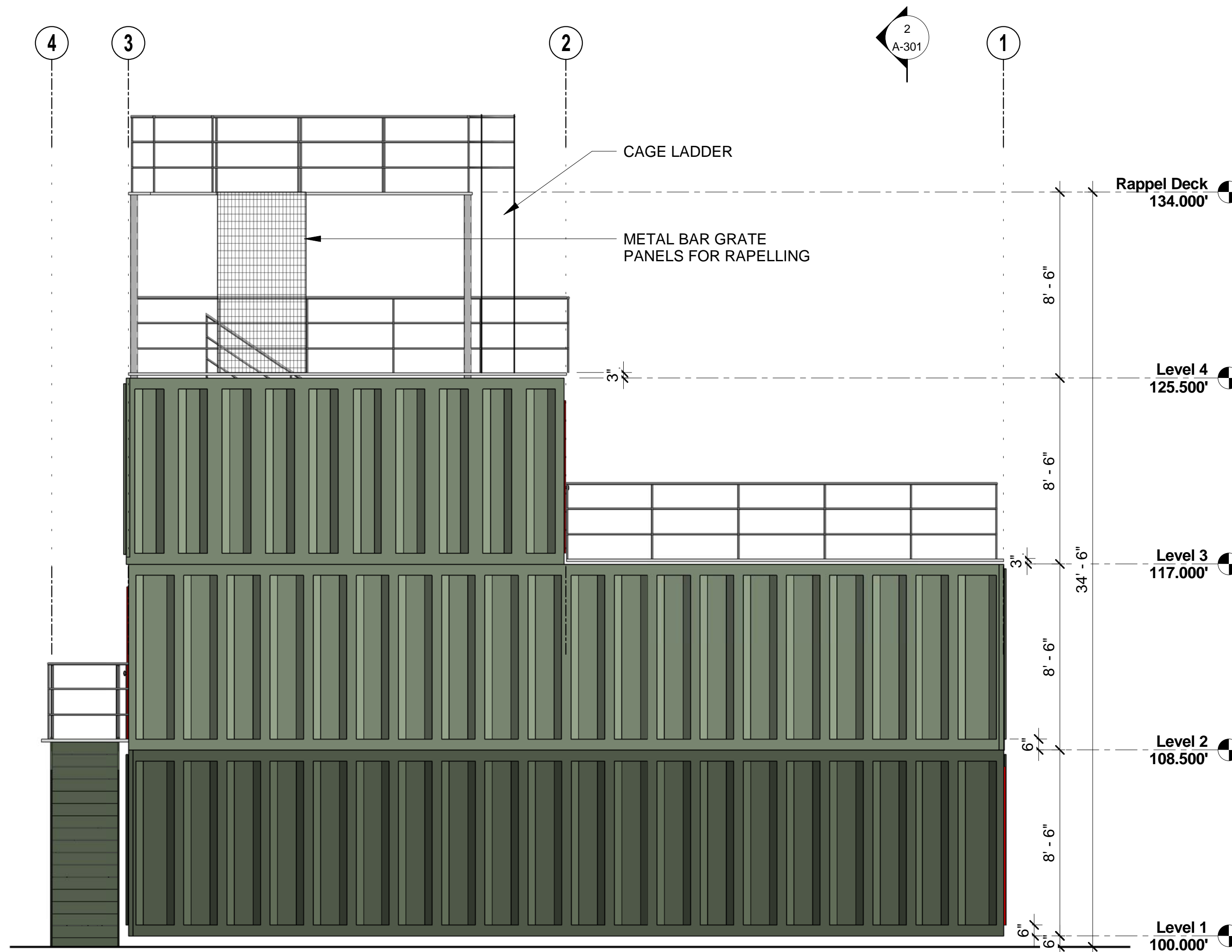
SHEET NUMBER

**A-202**

Elevations

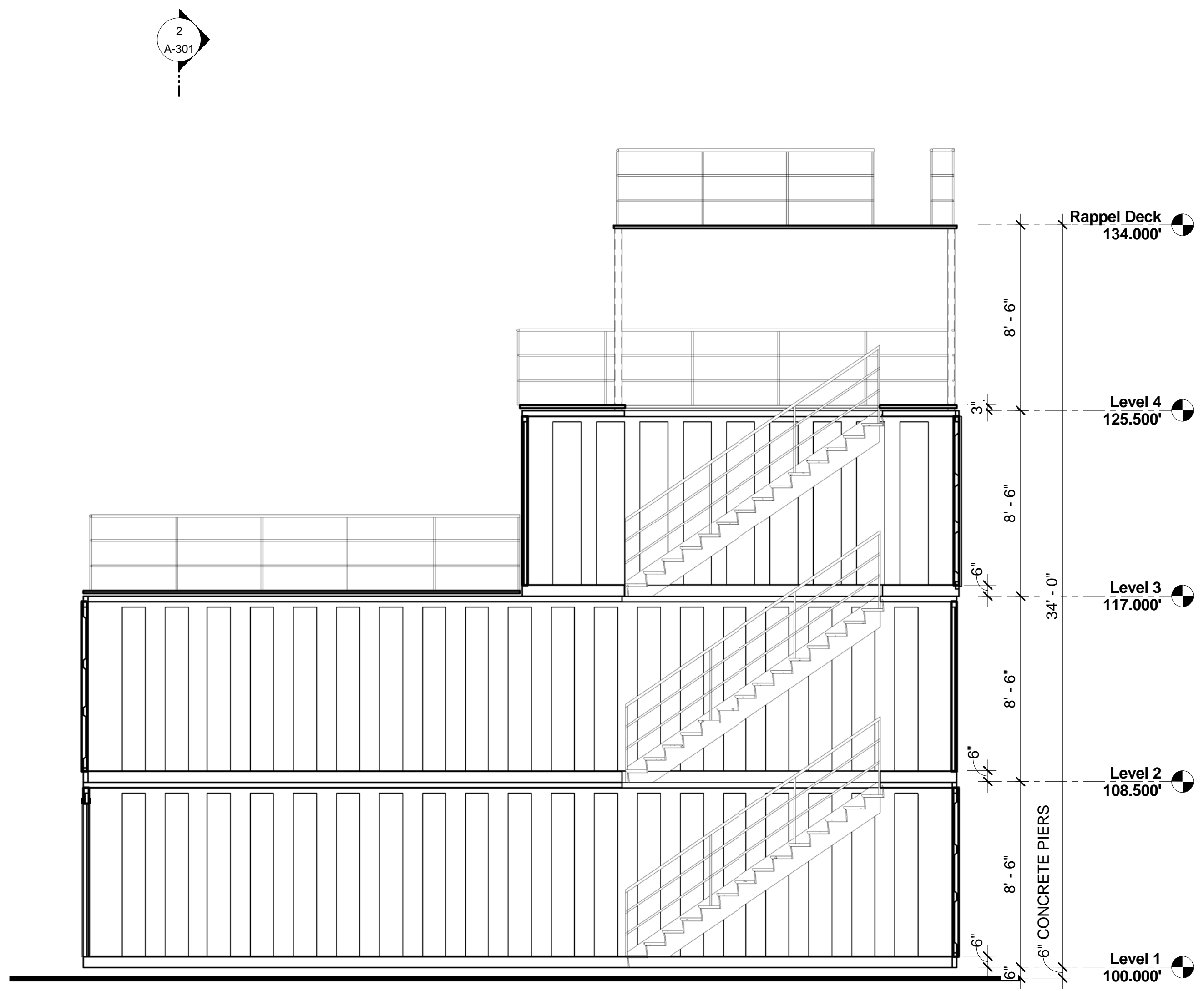


**1** West (YMCA) Elevation  
A-202 1/4" = 1'-0"

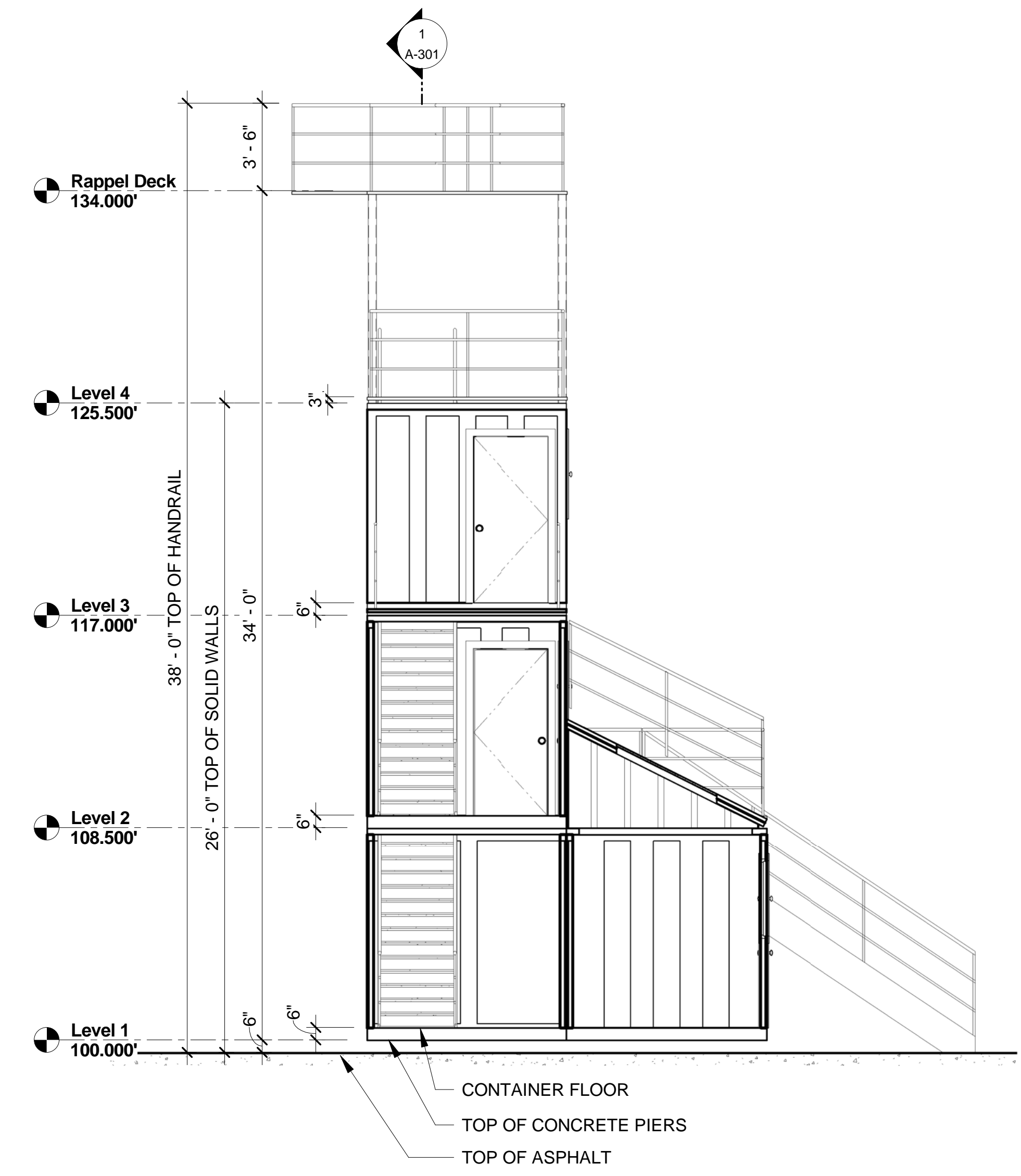


**2** North Elevation  
A-202 1/4" = 1'-0"

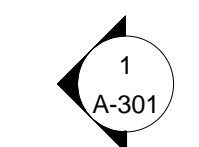
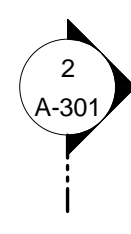
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**1 Long Section**  
 A-301 1/4" = 1'-0"



**2 Transverse Section**  
 A-301 1/4" = 1'-0"



Ketchum Sun Valley Volunteer Association Training Center  
 219 Lewis Street; Lot 7 Northwood Sub.  
 Ketchum, ID 83340

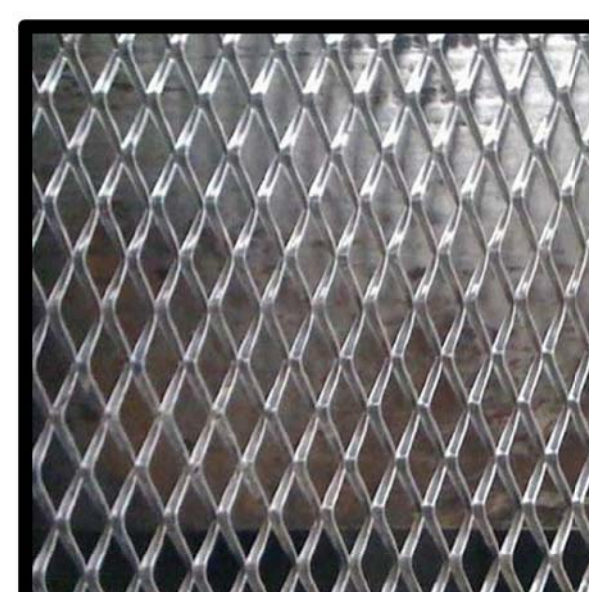
Design Review Set

6/7/2017

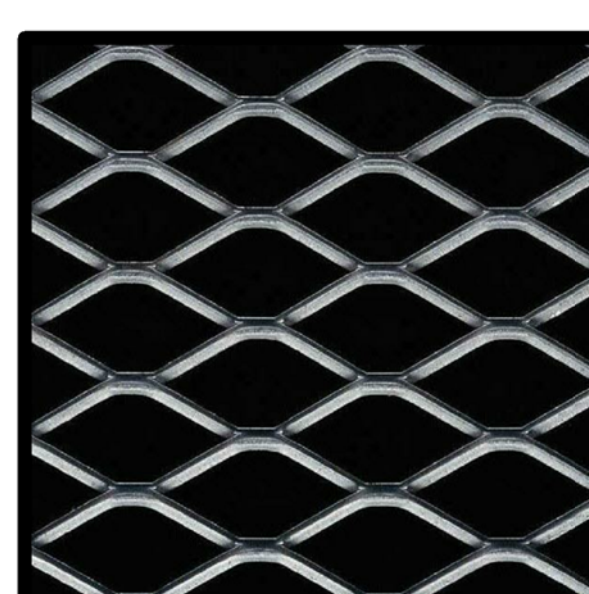
SHEET NUMBER  
**A-301**  
 Sections

**ATTACHMENT G. REVISED MATERIALS, 6/8/2017**





Vertical Panels at Rappel Deck:  
Galvanized Expanded Metal Mesh



Sloped Roof at Level 2:  
Galvanized Expanded Metal Mesh Over Steel Plate; all painted to match Level 2 Container.  
With 4' x 8' unfinished plywood or OSB insert for cutting

	<p>Rappel Deck and all Guardrails, Toe Boards, Stairs, Etc.</p> <p>Galvanized Steel Bar Grate Galvanizes Steel Tube and Plate</p>
<p><b>Half Moon Crest</b> 1481</p>	<p>Level 3 Container:</p> <p>Painted Steel, Benjamin Moore Half Moon Crest 1481</p>
<p><b>Half Moon Crest</b> 1481</p>	<p>Level 2 Container</p> <p>Painted Steel, Benjamin Moore Half Moon Crest 1481</p>
<p><b>Cos Cob Stonewall</b> 1483</p>	<p>Level 1 Containers</p> <p>Painted Steel, Benjamin Moore Cos Cob Stonewall</p>

**1** Color and Material Images Stonewall / Half Moon Crest  
A-005 6" = 1'-0"



Ketchum Sun Valley Volunteer Association Training Center  
219 Lewis Street; Lot 7 Northwood Sub.  
Ketchum, ID 83340

Design Review Set

5/24/2017

SHEET NUMBER  
**A-005**  
Material Images

Z:\Dropbox\KS\ATC 2015\Drawings\Revit\2017-05-26 Training Center rev G.rvt  
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**ATTACHMENT H. PUBLIC COMMENT**

---

**From:** Keshia Owens  
**Sent:** Tuesday, June 06, 2017 11:48 AM  
**To:** Carl Anderson  
**Subject:** FW: Proposed Fire Training Center

-----Original Message-----

From: Lisa Enourato On Behalf Of Participate  
Sent: Tuesday, June 06, 2017 9:11 AM  
To: Micah Austin <maustin@ketchumidaho.org>; Suzanne Frick <sfrick@ketchumidaho.org>  
Cc: Keshia Owens <kowens@ketchumidaho.org>  
Subject: FW: Proposed Fire Training Center

LISA ENOURATO | CITY OF KETCHUM  
Assistant City Administrator  
P.O. Box 2315 | 480 East Ave. N. | Ketchum, ID 83340  
o: 208.726.7803 | f: 208.726.7812  
lenourato@ketchumidaho.org | www.ketchumidaho.org

-----Original Message-----

From: Linda McCoy [mailto:leontopardus@cableone.net]  
Sent: Monday, June 05, 2017 6:33 PM  
To: Participate <participate@ketchumidaho.org>  
Subject: Proposed Fire Training Center

To the Ketchum Department of Planning and Building:

We wish to express our opposition to the planned Fire Training Center at 219 Lewis st. Although that address is technically inside the light industrial zone, it is less than a couple blocks from residential areas and is essentially right behind the Wood River YMCA. It is entirely inappropriate for children, as well as many adults, to be exposed to smoke, excessively loud noise and chemical smoke stimulants, especially while recreating or participating in wellness programs.

There are many folks who either reside in or visit the mountains in order to get away from noise and air pollution. Individuals with asthma or other respiratory conditions can be harmed by smoke and by "non-toxic" smoke machines, sometimes severely. We should not be subjected to serious assaults on our health resulting from city activities. Also, noise above 85 decibels causes hearing damage, and nearby residents and customers of the YMCA should not be subject to hearing damage merely as a result of location, especially since they occupied the surrounding area prior to the inception of a fire training facility.

In short, this function does not belong anywhere near a residential area. Such facilities should be located where they cannot adversely impact the public.

Sincerely  
Linda McCoy  
John Yankeelov

222D Pinewood Ln,  
Ketchum, ID

Sent from my iPad



## City of Ketchum

June 12, 2017

Planning and Zoning Commission  
City of Ketchum  
Ketchum, Idaho

**STAFF REPORT**  
**KETCHUM PLANNING AND ZONING COMMISSION**  
**REGULAR MEETING OF JUNE 12, 2017**

- PROJECT:** City-initiated Text Amendments to Title 17, Chapter 17.08, Definitions, and Section 17.12.020, District Use Matrix
- REPRESENTATIVE:** City of Ketchum Planning and Building Department
- DESCRIPTION:** City-initiated text amendments to the City of Ketchum Municipal Code to amend Title 17 - Zoning Regulations, Chapter 17.08, Definitions, and Section 17.12.020, District Use Matrix, to amend the regulations for motor vehicle fueling stations in order to protect the health, safety and welfare of the public and to align regulations for the use with the Comprehensive Plan.
- PLANNER:** Brittany Skelton, Senior Planner
- ATTACHMENTS:**
- A. Table 1: High Volume Trip Generating Uses Permitted in Light Industrial Zones
  - B. Table 2: High Volume Trip Generating Uses Permitted in Light Industrial, Community Core, and Tourist Zones
  - C. Table 3: Bracken Station Trip Generation Evaluation
  - D. Table 4: Limelight Hotel Trip Generation
  - E. Table 5: Use Specific Trip Generation, PM Peak Hour Trips
  - F. Table 6: Compliance with the 2014 Comprehensive Plan
  - G. Table 7: Compliance with Zoning Ordinance
  - H. Table 8: Compliance with Subdivision Ordinance
  - I. Draft Ordinance
- NOTICE:** **Planning and Zoning**
- Public notice for the public hearing on April 10, 2017 was published in the Idaho Mountain Express and was mailed to outside agencies on March 22, 2017. Notice was posted in three public locations on March 21, 2017. Continuation of the hearing to May 8, 2017 was announced at the April 10, 2017 meeting and continuation of the hearing to June 12, 2017 was announced at the May 8, 2017 meeting.
- PUBLIC HEARINGS:** **Planning and Zoning**
- April 10, 2017
  - May 08, 2017
  - June 12, 2017



## Section 1: Overview

### Amendment Objectives

The objective of the proposed text amendments is to align the zoning code, with regards to motor vehicle fueling stations and electric vehicles, to the 2014 Comprehensive Plan.

### Background

A public hearing on city-initiated text amendments to prohibit vehicular access from Highway 75 to motor vehicle fueling stations and to define and allow Electric Vehicle Charging Stations as a use were considered during a public hearing with the Planning and Zoning Commission on April 10, 2017.

The hearing was noticed for text amendments to Chapter 17.08, Definitions, Section 17.12.020, District Use Matrix, and Chapter 17.124, Development Standards, although only amendments to Chapter 17.08, Definitions, and Section 17.12.020, District Use Matrix were considered during the hearing.

The Commission directed staff to conduct the following additional research for the May 8, 2017 hearing:

1. Potential standards for Highway 75 access based on the number of trips generated by a use;
2. Potential standards for Highway 75 access based on type of curb cut access required by the Idaho Transportation Department;
3. Review standards that peer cities have adopted for motor vehicle fueling stations; and
4. Additional Comprehensive Plan research.

At the May 8, 2017 meeting the Commission directed staff to conduct the following research for the June 12, 2017 hearing:

1. Consider noise and visual standards for specific uses permitted in the Light Industrial zones that could be located on parcels fronting Highway 75; and
2. Consider trip generation for uses that are permitted in the Community Core and Tourist zoning districts in order to compare the impact that such zoning designations could have in terms of trip generation if Light Industrial parcels with Highway 75 access were rezoned.

### Additional Public Comment

As of 5:00 p.m. on Wednesday, June 7th, 2017, no additional written public comment has been received.

### Summary of Findings

Key findings are summarized below.

#### **Trips Generated by Use**

1. Staff evaluated 89 iterations of uses; the two categories of uses that generate the highest peak p.m. hour trips per 1,000 square feet or feature (such as per fueling position) and are permitted in the Light Industrial zoning districts are gas stations and daycares.
  - a. Daycares are already prohibited by ordinance from having vehicular access from Highway 75 or Saddle Road.
  - b. A gas station with convenience market, where the primary function of the business is fueling motor vehicles, generates approximately 8x more trips per 1,000 square feet than a daycare.
2. The highest peak p.m. hour trip generating use permitted in the Light Industrial zoning districts that is not a gas station or a daycare is an automobile parts and service center.
  - a. A gas station with convenience market, where the primary function of the business is fueling motor vehicles, generates 16x more trips per 1,000 square feet than an automobile parts and service center.

- i. A gas station with convenience market, where the primary function of the business is selling convenience items, 2,000 square feet in size would generate approximately the same number of p.m. peak hour trips as an automobile parts and service center 32,000 square feet in size.
3. The majority of uses permitted in the Light Industrial zones generate far less trips per gross square feet or unit of measure (such as per fueling position or per apartment) than motor vehicle fueling stations. Trip generation for such uses is detailed in Table 1.

**Table 1: High Volume Trip Generating Uses Permitted in Light Industrial Zones**

Uses highlighted in yellow have a motor vehicle fueling component and require Conditional Use approval

All other uses are permitted in the Light Industrial zoning districts

ITE Land Use Code	Use	Peak P.M. Generator - Generated from Adjacent Street	Unit of Measure
*945	Gasoline/Service Station with Convenience Market, where the primary activity of business is the fueling of motor vehicles.	97.08	per 1,000 square feet
*853	Convenience Market with Gasoline Pumps, where the primary activity of business is the selling of convenience items.	59.69	per 1,000 square feet
*853	Convenience Market with Gasoline Pumps, where the primary activity of business is the selling of convenience items.	19.07	per fueling position
946	Gas Station with Car Wash and Convenience Market	13.94	per fueling position
*945	Gasoline/Service Station with Convenience Market, where the primary activity of business is the fueling of motor vehicles.	13.38	per fueling position
565	Daycare Center	12.46	per 1,000 square feet
**932	High turnover sit down restaurant	11.15	per 1,000 square feet
843	Automobile Parts and Service	5.98	per 1,000 square feet
816	Hardware / Paint Store	4.84	per 1,000 square feet
812	Building Materials and Lumber Store	4.49	per 1,000 square feet
848	Tire Store	4.15	per 1,000 square feet
710	General Office	1.49	per 1,000 square feet
811	Construction Equipment Rental Store	0.99	per 1,000 square feet
110	Industrial - General Light Industrial	0.97	per 1,000 square feet
**220	Apartments	0.62	Per unit
150	Warehouse	0.32	per 1,000 square feet
151	Storage Units	0.26	Per storage unit
152	Delivery Distribution Center	0.12	per 1,000 square feet

\* Source = ITE Trip Generation, 8th Edition. All other uses sourced from ITE Trip Generation, 9th Edition.

\*\* = Requires Conditional Use approval

### Curb Cuts / Access from Highway 75

4. ITD's decision making is driven by safety, mobility, and economic concerns, in that order.
  - a. ITD considers Boulevard Approaches, which are a standard 84' width, for high daily trip generating uses that will have large commercial vehicles using the approach.
  - b. Wider access points are often preferred, because vehicles do not have to slow down as much in a travel or turn lane in order to navigate through the access.
  - c. Unless the District Engineer waives the requirement, any project projected to generate more than 100 peak hour trips, based on ITE traffic manual standards, is required to complete a Traffic Impact Study that ITD finds acceptable. The Traffic Impact Study is required to describe the type of access and other right-of-way improvements necessary for safety.
  - d. ITD sends all permit applications for new and redeveloped accesses to the City of Ketchum for review and approval. ITD will not approve a permit that the City of Ketchum does not approve.

### Gas Station Standards – Peer Cities

5. The City of Aspen, CO permits Gasoline Stations as a Conditional Use in one zoning district, the Commercial Core, subject to Commercial Design Standards and Guidelines and Conditional Use standards.
  - a. The Conditional Use standards grant the Commission wide discretion to condition the location, size, design, and operating characteristics of a conditional use in order to ensure it is compatible with the development in the immediate vicinity in terms of density, height, bulk, architecture, landscaping, open space, and any criteria in an applicable regulatory master plan that has been adopted.
6. The Town of Jackson, WY allows gas stations in three zones with a Conditional Use Permit and in two zones by-right with administrative review for conformance with the zoning code. However, all uses, including gas stations, are subject to the extensive form based regulations adopted with the town's complete overhaul of the zoning code in 2016. The form based regulations dictate physical development standards for queuing spaces, the location of off street parking and loading spaces, the distance of parking spaces from access drives, and so forth.

### Comprehensive Plan Analysis

7. The Comprehensive Plan analysis is contained in Attachment F, Table 6: Compliance with the 2014 Comprehensive Plan. Sustainability was the driving principle behind the 2014 Comprehensive Plan, with the Brundtland Commission of the United Nation's definition for sustainable development, "Development that meets the needs of the present without compromising the ability of future generations to meet their own needs," referenced in the introduction to the plan. The Comprehensive Plan expanded the Brundtland Commission's definition to encompass physical development of buildings and places, economic resiliency and prosperity, the conservation of natural resources, and the protection of history and culture.
  - a. Three major objectives were identified to guide the creation of the Comprehensive Plan, with the objective most relevant to the proposed text amendment being "Focus on sustainability."
  - b. Ten key values drove the Comprehensive Plan; the values most relevant to the proposed text amendments are:
    - i. Community Character
    - ii. Environmental Quality and Scenic Beauty
    - iii. A "Greener" Community
  - c. Chapters in the Comprehensive Plan with a vision, goal, or policy related to the proposed text amendments are:
    - i. CHAPTER 4: COMMUNITY DESIGN AND NEIGHBORHOODS
    - ii. CHAPTER 5: NATURAL RESOURCE STEWARDSHIP
    - iii. CHAPTER 7: MOBILITY
    - iv. CHAPTER 10: COMMUNITY HEALTH AND WELLNESS
    - v. CHAPTER 12: FUTURE LAND USE

## Noise and Visual Standards

8. The City of Ketchum's Main Street happens to be State Highway 75, and currently two parcels of land with Light Industrial zoning designations have frontage and primary vehicular access to the right-of-way that is both Main Street and State Highway 75. In addition to both parcels adjoining other parcels zoned Light Industrial, other adjacent zoning districts and land uses include Community Core (residential condominiums), Recreation Use (Ketchum cemetery) and Tourist (a hotel and restaurant and a residential single family home). Due to the Main Street / highway fronting location and the proximity to other zoning districts, the character in the immediate vicinity is not exclusively Light Industrial. In addition to traffic volume standards, character standards were evaluated for appropriateness.
  - a. Noise standards – One specific use, animal boarding/kennel, was evaluated in terms of noise standards. Title 9, Public Peace, Morals and Welfare regulations nuisances noises. Section 9.08.040.C.3 prohibits frequent or long continued noises by animals that disturb the peace and therefore there is sufficient regulation to mitigate the potential noise nuisance an animal boarding facility or kennel could cause.
  - b. Visual standards – The importance of protecting and enhancing the visual character of Ketchum's gateways and corridors, as supported by the 2014 Comprehensive Plan, is detailed in Attachment F, Table 6: Compliance with the 2014 Comprehensive Plan.
    - i. Staff finds that the parcels with Light Industrial zoning that have vehicular access from Highway 75 / Main Street do warrant augmented attention to development character.

## Trip Generation for a Wider Variety of Uses Permitted in Other Zoning Districts

9. The trip generation rates of a variety of commercial uses common in Commercial Core and permitted in the Tourist zoning districts were evaluated to inform the Commission in making a request for direction from City Council regarding potential zone changes for properties in the Light Industrial districts that are adjacent to Main Street.
  - a. Per 1,000 square feet of floor area, the highest trip generating uses permitted in the CC and T zoning districts are as follows:
    - i. Convenience Market (open 24 hours) – 52.4 p.m. peak hour trips
    - ii. Bank with drive-through window – 24.3 p.m. peak hour trips
    - iii. Pharmacy / Drug Store with drive-through window – 9.91 p.m. peak hour trips
    - iv. Retail dollar store / variety store – 6.82 p.m. peak hour trips
  - b. Uses that are not permitted anywhere in Ketchum were evaluated for comparison. The highest trip generating uses per 1,000 square feet are as follows:
    - i. Fast food drive through with no interior seating – 153.85 p.m. peak hour trips
    - ii. Coffee / donut shop with drive through window and no interior seating – 75.0 p.m. peak hour trips
    - iii. Coffee shop with drive-through window and interior seating – 42.8 p.m. peak hour trips
    - iv. Fast food restaurant with drive through window and interior seating – 33.84 p.m. peak hour trips

A table comparing high trip generating uses across zoning districts follows on the next page.

**Table 2: High Volume Trip Generating Uses Permitted in Light Industrial, Community Core, and Tourist Zones**

Uses highlighted in yellow have a motor vehicle fueling component
Uses highlighted in grey are not permitted in Ketchum City Limits
Uses highlighted in red are permitted in the CC and T zones
All other uses are permitted in the Light Industrial zoning districts

ITE Land Use Code	Use	Peak P.M. Generator - Generated from Adjacent Street	Unit of Measure
935	<i>Fast Food Drive Through with no interior seating</i>	153.85	<i>per 1,000 square feet</i>
945	Gasoline/Service Station with Convenience Market, where the primary activity of business is the fueling of motor vehicles.	97.08	per 1,000 square feet
938	<i>Coffee / Donut Shop with Drive-Through Window and No Indoor Seating</i>	75.0	<i>per 1,000 square feet</i>
851	<i>Convenience Market (open 24 hours)</i>	52.4	<i>per 1,000 square feet</i>
853	Convenience Market with Gasoline Pumps, where the primary activity of business is the selling of convenience items.	50.92	per fueling position
853	Convenience Market with Gasoline Pumps, where the primary activity of business is the selling of convenience items.	50.92	per 1,000 square feet
937	<i>Coffee Shop with Drive-Through Window</i>	42.8	<i>per 1,000 square feet</i>
934	<i>Fast Food Restaurant with Drive Through Window</i>	33.84	<i>per 1,000 square feet</i>
912	<i>Bank with Drive-Through Window</i>	24.3	<i>per 1,000 square feet</i>
946	Gas Station with Car Wash and Convenience Market	13.94	per fueling position
945	Gasoline/Service Station with Convenience Market, where the primary activity of business is the fueling of motor vehicles.	13.51	per fueling position
565	Daycare Center	12.46	per 1,000 square feet
932	High turnover sit down restaurant	11.15	per 1,000 square feet
881	<i>Pharmacy / Drugstore with Drive-Through Window</i>	9.91	<i>per 1,000 square feet</i>
814	<i>Dollar/Variety Store</i>	6.82	<i>per 1,000 square feet</i>
843	Automobile Parts and Service	5.98	per 1,000 square feet
816	Hardware / Paint Store	4.84	per 1,000 square feet
812	Building Materials and Lumber Store	4.49	per 1,000 square feet

848	Tire Store	4.15	per 1,000 square feet
876	Apparel Store	3.86	per 1,000 square feet
820	Shopping Center	3.71	per 1,000 square feet
826	Specialty Retail Center	2.71	per 1,000 square feet
710	General Office	1.49	per 1,000 square feet
811	Construction Equipment Rental Store	0.99	per 1,000 square feet
110	Industrial - General Light Industrial	0.97	per 1,000 square feet
220	Apartments	0.62	Per unit
150	Warehouse	0.32	per 1,000 square feet
151	Storage Units	0.26	Per storage unit
152	Delivery Distribution Center	0.12	per 1,000 square feet

**Section 2: Detailed Analysis**

Trips Generated by a Specific Use – Bracken Station and Limelight Hotel

Staff began by comparing the different trip generation volumes that were submitted by the applicant and received through public comment for the Bracken Station Conditional Use Permit application (CUP #16-034). Staff began with an evaluation of trip generation associated with Bracken Station because the Planning and Zoning Commission denied the Conditional Use Permit application finding the vehicle composition attracted to the proposed use and the volume of vehicle trips generated by the use would be hazardous and conflict with existing and anticipated pedestrian, cyclist, and vehicular traffic in the vicinity.

The five different trip generation scenarios made available to the Commission for evaluation for the Bracken Station proposal are summarized in Table 3. The data based on Institute of Transportation Engineers (ITE) Trip Generation Manual, 9<sup>th</sup> edition, Land Use Codes differ depending on the classification of the proposed development’s primary function – either convenience store sales or gasoline sales.

Staff also evaluated the trip generation volume prepared for the Limelight Hotel (Development Agreement #07-015), which was last updated in and submitted to the city in 2013. While hotels are not permitted in the Light Industrial zoning districts, trip generation data was included for comparison purposes since the hotel is the largest scale project in the city’s history that also has frontage on Highway 75 (Main Street) and the trip generation data is relatively recent.

**Table 3: Bracken Station Trip Generation Evaluation**

Use	ITE Land Use Code	Variable	Weekday Trips	AM Peak Trips	PM Peak Trips	Source / Description
Hailey, ID - Gas station with 8 fueling positions fronting Main Street and a	n/a	X	X	X	90	Based on observation of an actual location in Hailey, Idaho. This PM peak trip generation was the basis for the last versions of the vehicle circulation diagrams provided by the applicant.

food truck parked on site						
Provo, UT - Gas station with attached sandwich shop	n/a	X	X	X	185	Alternate potential trip generation suggested by the applicant due to the greater similarity between the use and the uses proposed for the Bracken Station project and because the use was under consideration for inclusion in a forthcoming ITE manual. However, this data point was not used to generate vehicle circulation diagrams.
Bracken Station - 2,084 square foot convenience store and 8 fueling positions	Land Use 945, Service Station with Convenience Market	Fueling Positions	1,304	82	110	The first version of data provided by applicant for the Bracken Station application, based on ITE Trip Generation, 9 <sup>th</sup> Edition
Bracken Station - 2,084 square foot convenience store and 8 fueling positions	Land Use 853, Convenience Market with Gas Pumps	Fueling Positions	4,341	X	153	Alternate characterization of the Bracken Station proposed uses. Provided to city in public comment by a Professional Engineer retained by citizens to analyze the Bracken Station application.
Bracken Station - 2,084 square foot convenience store and 8 fueling positions	Land Use 853, Convenience Market with Gas Pumps	Store square footage	1,776	X	107	Alternate characterization of the Bracken Station proposed uses. Provided to city in public comment by a Professional Engineer retained by citizens to analyze the Bracken Station application.

**Table 4: Limelight Hotel Trip Generation**

Use	ITE Land Use Code	Weekday Trips	PM Peak Trips	Source / Description
Limelight Hotel	330 - Resort Hotel and 233 - Luxury Condos	1,108	87	Last trip generation information analyzed for the Limelight Hotel (source: February 12, 2013 memo from Transportation Consultants, Inc. to the City of Ketchum) <sup>1</sup>

1. Although the trip generation provided is based on 119 hotel rooms and 11 residential units, the program for the building was revised slightly and 108 hotel rooms and 14 residential units were constructed. The project consists of 132,411 square feet (square footage of the below ground parking area excluded).

The highest trip generation for the Bracken Station project, based on ITE standards, was 4,341 daily weekday trips and 153 p.m. peak hour trips, however, the trip generation evaluated in the final traffic circulation diagrams were based on 90 p.m. peak hour trips and not total weekday trip generation. The trip generation for the Limelight Hotel was 1,108 weekday trips and 87 p.m. peak hour trips.



### Trips Generated by Other Uses Permitted in the Light Industrial Zones

Staff then analyzed other uses permitted in Ketchum's light industrial zoning districts with respect to developable square footage for several possible lot sizes, detailed in Attachment E, Table 5: Use Specific Trip Generation, PM Peak Hour Trips.

Trip generation is provided in the ITE manual and on TripGeneration.org as either a specified number of trips per every 1,000 square feet in gross floor area of the use, or as a specified number of trips per feature associated with the use, such as fueling positions or car wash station. The higher factor for trips generated by the use or trips generated from the adjoining street was used for each land use. PM peak hour trips were used for all uses. When "0" is indicated for weekday trips or PM trips staff was unable to source ITE or TripGeneration.org data for the use.

#### Sites analyzed:

- Generic minimum area lot size for the zone (8,000 s.f. / 0.18 acre),
- 18,590 s.f. / 0.43 acre site (the size of 911 N. Main Street, where Bracken Station was proposed)
- 21,780 s.f. / 0.5 acre generic site
  - A half-acre (0.5 acre) site was selected because one of the two sites in the Light Industrial zones that currently has access from Highway 75 is the 10th Street Light Industrial development and the site is approximately 1.75 acres in size. Because the development is condominimized and there are approximately 24 individual units, it is unlikely that the entire site will be redeveloped in the near future. A smaller portion of the site, such as a half-acre, has a higher likelihood of being redeveloped. Additionally, sites of unspecified size were analyzed for some uses that were dependent on a characteristic of the use such as number of fueling stations or number of vehicle wash bays.

#### Uses evaluated included:

- Half-Acre (21,780 s.f.) and minimum lot size (8,000 s.f.) lots
  - A general office building developed to the maximum density permitted
  - A general warehouse building developed to the maximum density permitted
  - A general light industrial building developed to the maximum density permitted on a site
  - An automobile parts and service center developed to the maximum density permitted
  - Mixed-use buildings built to the maximum density containing the maximum residential density coupled with either the maximum office or light industrial square footage that could be developed
- Bracken Station site
  - An automobile parts and service center developed to the maximum density permitted
  - Mixed-use buildings built to the maximum density containing the maximum residential density coupled with either the maximum office or light industrial square footage that could be developed
- Day care facilities ranging from 5,000 to 27,885 square feet in size
- Gas stations with 1-10 fueling positions
- Gas stations with convenience stores 500 – 5,500 square feet in size
- Automated car washes with 1-10 vehicle wash stations
- Gas stations with carwashes and markets with 1-10 fueling positions

A total of 89 different configurations of uses were evaluated and are summarized below; peak p.m. trip data was available for 88 of 88 uses and daily trip data was available for 45 of 89 uses:

- Highest peak p.m. trips generated
  - 13 of the 20 highest peak p.m. trip generating uses were gas stations with convenience stores, with convenience stores ranging in size from 2,084 – 5,500 square feet, where the size of the convenience store was the variable; two additional uses in the top twenty were gas stations with convenience stores of unspecified sizes.

- A maximum density (45,738) square foot auto parts and service center on a half-acre site was the 16<sup>th</sup> highest peak p.m. trip generating use.
- Four (4) of the 20 highest peak p.m. generating trips were daycare centers. Section 17.124.120, Daycare Business, already prohibits any daycare from having access from State Highway 75 or Saddle Road; therefore, this use was included for comparison purposes only.
- Lowest peak p.m. trips generated
  - Seven (7) of the 20 lowest peak p.m. trip generating uses were iterations of gas stations with between one (1) to three (3) fueling positions; one (1) of the 20 lowest peak p.m. trip generating uses was a gas station with a convenience store 500 square feet in size where the size of the convenience store was the variable.
  - Three (3) of the 20 lowest peak p.m. trip generating uses were iterations of a restaurant (sit-down, fast food and fast-casual) 1,000 s.f. in size, which is the largest size restaurant permitted in the Light Industrial Zones.
  - The Bracken Station site (18,590 s.f.), and the minimum lot size in the zone (9,000 s.f. in size) developed to the maximum residential density and mixed use with office use were among the 20 lowest peak p.m. trip generating uses.
  - Storage units, warehouses, and delivery distribution developed to the maximum density on half-acre and minimum lot size lots were among the 20 lowest peak p.m. trip generating uses.
- Highest total daily trips generated and lowest daily trips generated
  - As opposed to p.m. peak trips, where data was available for 85 or 89 uses, daily trips were only available for 45 of 89 uses.
  - When comparing the 20 highest peak p.m. generating uses, 16 of the 20 did not have daily trip data available. Five (5) of the 20 lowest peak p.m. generating uses did not have daily trip data available.
    - Therefore, highest and lowest daily trips have not been categorized and staff's recommendations are based largely on p.m. peak trip generating uses.

#### Idaho Transportation Department Highway Access Standards

Staff consulted with the Idaho Transportation Department (ITD) District-4 access permitting staff persons Dave Jensen (retiring) and Deb Pierson (incoming). ITD staff provided the current Rules Governing Highway Right-of-Way Encroachments on State Rights-of-Way document and general feedback on highway access points.

Generally:

- ITD's decision making is driven by safety, mobility, and economic concerns, in that order.
- Wider access points are often preferred, because vehicles do not have to slow down as much in a travel or turn lane in order to navigate through the access.
- Unless the District Engineer waives the requirement, any project projected to generate more than 100 peak hour trips, based on ITE traffic manual standards, is required to complete a Traffic Impact Study that ITD finds acceptable. The Traffic Impact Study is required to describe the type of access and other right-of-way improvements necessary for safety.
- ITD sends all permit applications for new and redeveloped accesses to the City of Ketchum for review and approval. ITD will not approve a permit that the City of Ketchum does not sign off on.

In terms of standards, minimum and maximum curb cuts for access points are dependent on the speed of travel at the specific location and broad use categories. Where the speed limit is 35 m.p.h. or less, multi-family curb cuts are 28' minimum and 40' maximum, one-way commercial accesses are 15' minimum and 30' maximum and two-way commercial accesses are 25' minimum and 40' maximum. Two-way boulevard Approaches are a standard 84' width and are considered for high daily trip generating uses that will have large commercial vehicles using the approach.

### **Section 3: Recommended Amendments and Commission Options**

#### Text Amendments Proposed April 10, 2017

Primarily due to the high volume of vehicle trips generated by motor vehicle fueling stations, especially motor vehicle fueling stations that include sales of items of convenience to the motoring public, and due to anticipated conflict between pedestrian, cyclist, and vehicular traffic and safety that would result if such a use were to locate at any site with access from Highway 75 in city limits, staff proposed amending the zoning code to prohibit such access. Secondly, staff recommended such amendments because locating motor vehicle fueling stations on Highway 75 conflicts with goals and objectives stated in the 2014 Comprehensive Plan. Finally, because the only place in the zoning code that references electric vehicle charging stations is the definition of motor vehicle fueling station, staff recommended defining electric vehicle fueling stations separately.

Staff had proposed the following amendments:

- 1. Amend Section 17.12.020, District Use Matrix, to include a new footnote #30. to appear at the bottom of the table.**

The footnote would state, "Vehicular access from Highway 75 to motor vehicle fueling stations is prohibited." The footnote would be added to the row for the motor vehicle fueling station within the district use matrix. The proposed revised District Use Matrix is attached as Exhibit A to Attachment E, proposed Ordinance.

- 2. Amend Section 17.08.020, Terms Defined, as follows:**

**MOTOR VEHICLE FUELING STATION:** A facility providing the retail sale and direct delivery to motor vehicles of fuel, including electric charging stations associated with a motor vehicle fueling station, lubricants and minor accessories, and retail sales for the convenience of the motoring public.

**ELECTRIC VEHICLE CHARGING STATION:** Electric vehicle charging station means a public or private parking space that is served by battery charging station equipment that has as its primary purpose the transfer of electric energy (by conductive or inductive means) to a battery or other energy storage device in an electric vehicle.

- 3. Amend Section 17.12.020, District Use Matrix, to include Electric Vehicle Charging Station as an accessory use and to permit Electric Vehicle Charging Stations as an accessory use in all zoning districts.**

While the Commission supported the Electric Vehicle Charging Stations amendments, the Commission requested that staff conduct additional research vehicular access to Highway 75.

#### Text Amendments Proposed June 12, 2017

With respect to all analysis completed to date, at this time staff continues to recommend the text amendments proposed for the April 10, 2017 meeting, as detailed above, for the following reasons:

1. After evaluating a variety of uses permitted in both the Light Industrial zones and other zoning districts, gas stations with convenience stores have the potential to generate a far higher volume of p.m. peak hour trips than other uses permitted in the Light Industrial zones, such as automobile parts and service centers, hardware and paint stores, building material and lumber stores, tire stores, and office uses, because the trip generation per 1,000 gross square feet of floor area for gas stations with convenience stores is far higher.

Stand alone convenience stores are not permitted in the Light Industrial zoning districts, but the broad language in the definition of motor vehicle fueling station allows for "retail sales for the convenience of the motoring public" and does not contain a limitation on the gross floor area that may be devoted

to such use. A text amendment could be considered that would prohibit the sale of convenience items in association with a gas station, or limit the gross floor area devoted to such use, and such an amendment could limit the potential number of trips generated. If such an amendment were pursued, an amendment limiting the number of fueling positions would also have to be considered and adopted simultaneously, because the number of fueling positions and the gross floor area devoted to the sale of convenience items both influence the trips generated.

However, if such amendments were pursued in order to mitigate impacts from the use from a traffic standpoint only, there would still be outstanding conflicts with gas stations located on Highway 75 with respect to the Comprehensive Plan's goals and policies and the intent of the Light Industrial zones as currently stated in the zoning code to "generate little traffic from tourists and the general public."

2. The second highest p.m. peak hour trip generating use permitted in the Light Industrial zones, daycare centers, are already prohibited by the zoning code from having vehicular access from Highway 75 or Saddle Road.
3. The value of sustainability that is interwoven into the 2014 Comprehensive Plan and the explicitly stated goal of promoting and supporting energy conservation and reduction of greenhouse gasses are supported by defining Electric Vehicle Charging Stations as an accessory use permitted in all zones, and by defining Electric Vehicle Charging Stations separately from motor vehicle fueling stations.

#### Additional Amendments to Consider

As a separate scope of work, the Commission may consider directing staff to seek input from Council regarding additional text amendments aimed at protecting and enhancing the character of Main Street / Highway 75 with respect to properties that are zoned Light Industrial and are adjacent to Main Street / Highway 75.

#### **STAFF RECOMMENDATION**

Staff recommends approval of the text amendments as proposed.

#### **OPTIONAL MOTIONS**

1. "I MOVE TO RECOMMEND approval to City Council of the text amendments to Title 17, Section 17.08.020, Terms Defined and Section 17.12.020 District Use Matrix as proposed, finding the proposed amendments consistent with the Comprehensive Plan, Zoning Ordinance, and Subdivision Ordinance."
2. "I MOVE TO RECOMMEND approval to City Council of the text amendments to Title 17, Section 17.08.020, Terms Defined and Section 17.12.020 District Use Matrix with the following modifications, [insert modifications], finding the proposed amendments consistent with the Comprehensive Plan, Zoning Ordinance, and Subdivision Ordinance."
3. I MOVE TO RECOMMEND denial to City Council of the text amendments to Title 17, Section 17.08.020, Terms Defined, and Section 17.12.020, District Use Matrix."

**Attachment E:**

Table 5: Use Specific Trip Generation, PM Peak Hour Trips

## Use Specific Trip Generation, Peak P.M. Hour

\* = 90 PM peak trips is the trip generation rate referenced in the last versions vehicle circulation diagrams submitted for the Bracken Station Condition Use Permit application.

\*\* = Section 17.124.120 Daycare Business prohibits any daycare from having access from State Highway 75 or Saddle Road.

<i>Sorted by peak p.m. hour trips generated</i>			
Address	Use	Weekday Trips	PM Peak Trips
Any site in the LI zones	5500 s.f. convenience store and gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity	0	1602
Any site in the LI zones	5000 s.f. convenience store and gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity	0	1214
Any site in the LI zones	5500 s.f. convenience store and gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	985
Any site in the LI zones	4500 s.f. convenience store and gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	910
Any site in the LI zones	5000 s.f. convenience store and gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	746
Any site in the LI zones	4000 s.f. convenience store and gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	582
Any site in the LI zones	4500 s.f. convenience store and gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	560
Half Acre Site (21,780 s.f.)	1.5 FAR (27,885 sq. ft.) Daycare - ITE Land Use Daycare Center, Use 565, Square Footage Variable	2065	383
Minimum lot size site (9,000 s.f.)	5,000 square foot Daycare - ITE Land Use Daycare Center, Use 565, Square Footage Variable	69	371
Any site in the LI zones	4000 s.f. convenience store and gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	358
Any site in the LI zones	3500 s.f. convenience store and gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	340
Half Acre Site (21,780 s.f.)	1.0 FAR (21,780 sq. ft.) Daycare - ITE Land Use Daycare Center, Use 565, Square Footage Variable	1613	299
Any site in the LI zones	3000 s.f. convenience store and gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	291

Any site in the LI zones	2500 s.f. convenience store and gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	243
Any site in the LI zones	3500 s.f. convenience store and gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	209
Half Acre Site (21,780 s.f.)	Maximum density (45,738 s.f.), ITE Land Use Code 943, Automobile Parts and Service, square foot variable	0	204
Any site in the LI zones	2084 s.f. convenience store and gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	202
Half Acre Site (21,780 s.f.)	1.5 FAR (27,885 sq. ft.) Daycare - ITE Land Use Daycare Center, Use 565, Square Footage Variable, real world conditions courtesy of TripGeneration.org	874	195
Any site in the LI zones	10 fuel pump gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	191
911 North Main	Provo, UT - Gas station with attached sandwich shop	0	185
Any site in the LI zones	3000 s.f. convenience store and gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	179
911 Bracken Station	Maximum density (39,039 s.f.), ITE Land Use Code 943, Automobile Parts and Service, square foot variable	0	175
Any site in the LI zones	9 fuel pump gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	172
Any site in the LI zones	Gas Station, 10 fuel pump, w/carwash and market - ITE Land Use Gas Station w/Carwash & Market, Use 946, variable is fueling positions, real world conditions courtesy of TripGeneration.org	2118	170
911 North Main	Bracken Station - 2,084 square foot convenience store and 8 fuel pumps - ITE Land Use 853, Convenience Market with Gas Pumps, Fuel Pumps Variable	4,341	153
Any site in the LI zones	8 fuel pump gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	153
Any site in the LI zones	Gas Station, 9 fuel pump, w/carwash and market - ITE Land Use Gas Station w/Carwash & Market, Use 946, variable is fueling positions, real world conditions courtesy of TripGeneration.org	1906	153
Half Acre Site (21,780 s.f.)	1.0 FAR (21,780 sq. ft. ) Daycare - ITE Land Use Daycare Center, Use 565, Square Footage Variable, real world conditions courtesy of TripGeneration.org	683	152
Any site in the LI zones	2500 s.f. convenience store and gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	149



Any site in the LI zones	1500 s.f. convenience store and gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	146
Minimum lot size site (9,000 s.f.)	10,000 square foot Daycare - ITE Land Use Daycare Center, Use 565, Square Footage Variable	741	138
Any site in the LI zones	Gas Station, 8 fuel pump, w/carwash and market - ITE Land Use Gas Station w/Carwash & Market, Use 946, variable is fueling positions, real world conditions courtesy of TripGeneration.org	1694	136
Any site in the LI zones	10 fuel pump gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	134
Any site in the LI zones	7 fuel pump gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	133
Any site in the LI zones	2084 s.f. convenience store and gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	124
Any site in the LI zones	9 fuel pump gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	120
Any site in the LI zones	Gas Station, 7 fuel pump, w/carwash and market - ITE Land Use Gas Station w/Carwash & Market, Use 946, variable is fueling positions, real world conditions courtesy of TripGeneration.org	1483	119
Any site in the LI zones	6 fuel pump gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	114
911 North Main	Bracken Station - 2,084 square foot convenience store and 8 fuel pumps - ITE Land Use 953, Service Station with Convenience Market,	1,304	110
911 North Main	Bracken Station - 2,084 square foot convenience store and 8 fuel pumps - ITE Land Use 853, Convenience Market with Gas Pumps, Convenience Store Square Footage Variable	1,776	107
Any site in the LI zones	8 fuel pump gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the	0	107
Any site in the LI zones	Gas Station, 6 fuel pump, w/carwash and market - ITE Land Use Gas Station w/Carwash & Market, Use 946, variable is fueling positions, real world conditions courtesy of TripGeneration.org	1271	102
Any site in the LI zones	1000 s.f. convenience store and gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	97

Any site in the LI zones	5 fuel pump gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	95
Any site in the LI zones	7 fuel pump gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	94
911 North Main	Hailey, ID - Gas station with 8 gas pumps fronting Main Street and a food truck parked on site	0	90
Any site in the LI zones	1500 s.f. convenience store and gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	90
Limelight Hotel	330 - Resort Hotel and 233 - Luxury Condos	1,108	87
Any site in the LI zones	Gas Station, 5 fuel pump, w/carwash and market - ITE Land Use Gas Station w/Carwash & Market, Use 946, variable is fueling positions, real world conditions courtesy of TripGeneration.org	1059	85
Minimum lot size site (9,000 s.f.)	Maximum density (18,900 s.f.), ITE Land Use Code 943, Automobile Parts and Service, square foot variable	0	85
Any site in the LI zones	6 fuel pump gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	80
Half Acre Site (21,780 s.f.)	Maximum residential density (24,502 s.f. of residential development, 61 units), remaining 24,502 s.f. is office - ITE Land Uses Apartments, Use 220, Number of Units Variable and Land Use 710, General Office	611	76
Any site in the LI zones	4 fuel pump gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	76
Half Acre Site (21,780 s.f.)	Maximum density (39,039 square feet), ITE Land Use General Office, square footage variable	505	69
Any site in the LI zones	Fast food restaurant 1,000 square feet (maximum permitted by code) - ITE Land Use Fast Food, Use 934, variable square feet, real world conditions courtesy of TripGeneration.org	482	68
Any site in the LI zones	Gas Station, 4 fuel pump, w/carwash and market - ITE Land Use Gas Station w/Carwash & Market, Use 946, variable is fueling positions, real world conditions courtesy of TripGeneration.org	847	68
Any site in the LI zones	5 fuel pump gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	67
Any site in the LI zones	1000 s.f. convenience store and gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	60

911 North Main	Maximum residential density (19,519 square feet of residential development, 48 units), remaining square footage is 19,159 is office - ITE Land Uses Apartments, Use 220, Number of Units Variable and Land Use 710, General Office	540	60
Any site in the LI zones	3 fuel pump gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	57
Any site in the LI zones	4 fuel pump gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	54
Any site in the LI zones	Gas Station, 3 fuel pump, w/carwash and market - ITE Land Use Gas Station w/Carwash & Market, Use 946, variable is fueling positions, real world conditions courtesy of TripGeneration.org	635	51
Any site in the LI zones	500 s.f. convenience store and gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	49
Any site in the LI zones	Fast food restaurant 1,000 square feet (maximum permitted by code) - ITE Land Use Fast Food, Use 934, variable square feet	497	48
Any site in the LI zones	Fast Casual restaurant 1,000 square feet (maximum permitted by code) - Fast Casual Restaurant, variable square feet	317	43
Any site in the LI zones	3 fuel pump gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	40
Any site in the LI zones	2 fuel pump gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	38
911 North Main	Maximum residential density (19,519 square feet of residential development, 48 units), remaining square footage is 19,159 is office - ITE Land Uses Apartments, Use 220, Number of Units Variable and Land Use 710, General Office, real world use observations by Spack Consulting	276	34
Any site in the LI zones	Gas Station, 2 fuel pump, w/carwash and market - ITE Land Use Gas Station w/Carwash & Market, Use 946, variable is fueling positions, real world conditions by TripGeneration.org	424	34
Half Acre Site (21,780 s.f.)	Maximum density (45,738 s.f.), ITE Land Use 151 - Delivery Distribution, real world observations by TripGeneration.org	265	33
Minimum lot size site (9,000 s.f.)	Maximum residential density (10,125 square feet of residential development, 25 units), remaining 10,125 s.f. is office - ITE Land Uses Apartments, Use 220, Number of Units Variable and Land Use 710, General Office, square footage variable	273	31

Any site in the LI zones	500 s.f. convenience store and gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	30
Any site in the LI zones	Maximum density (39,039 square feet), ITE Land Use General Office, square footage variable, real world conditions observed by Spack Consulting	183	28
Any site in the LI zones	2 fuel pump gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	27
Half Acre Site (21,780 s.f.)	Maximum density (45,738), ITE Land 150 - Warehouse	163	21
Any site in the LI zones	1 fuel pump gas station, ITE Land Use Code 853 (Convenience Market with Gasoline Pumps), primary activity of business is the selling of convenience items.	0	19
Any site in the LI zones	Restaurant 1,000 square feet (maximum permitted by code) - ITE Land Use Restaurant, Use 932, variable square footage	127	18
Any site in the LI zones	Gas Station, 1 fuel pump, w/carwash and market - ITE Land Use Gas Station w/Carwash & Market, Use 946, variable is fueling positions, real world conditions courtesy of TripGeneration.org	212	17
Minimum lot size site (9,000 s.f.)	Maximum density (20,250), ITE Land Use 152 - Delivery Distribution, real world conditions courtesy of TripGeneration.org	117	15
Any site in the LI zones	1 fuel pump gas station, ITE Land Use Code 945 (Gasoline/Service Station with Convenience Market), primary activity of business is the fueling of motor vehicles.	0	13
Minimum lot size site (9,000 s.f.)	Maximum density (20,250), ITE Land Use 150 - Warehouse	72	10
Half Acre Site (21,780 s.f.)	Maximum density (45,738 s.f.), ITE Land Use 151 - Storage Units	11	3
Minimum lot size site (9,000 s.f.)	Maximum density (20,250), ITE Land Use 151 - Storage Units	5	1
Minimum lot size site (9,000 s.f.)	Maximum residential density (10,125 square feet of residential development, 25 units), remaining 10,125 s.f. is General Light Industrial - ITE Land Uses Apartments, Use 220, Number of Units Variable and General Light Industrial, square footage variable	234	0
911 North Main	Maximum residential density (19,519 square feet of residential development, 48 units), remaining square footage is 19,159 is general light industrial - ITE Land Uses Apartments, Use 220, Number of Units Variable and General Light Industrial, square footage variable	459	0
Half Acre Site (21,780 s.f.)	Maximum density (45,738), ITE Land Use Industrial, General Light	319	0

Half Acre Site (21,780 s.f.)	Maximum residential density (24,502 s.f. of residential development, 61 units), remaining 24,502 s.f. is light industrial - ITE Land Uses Apartments, Use 220, Number of Units Variable and General Light Industrial, square footage variable	474	0
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*Color of cell indicates Bracken Station site*

**Attachment F:**

**Compliance with the 2014 Comprehensive Plan**

TABLE 6: Compliance with the 2014 Comprehensive Plan

<b>Premise of 2014 Comprehensive Plan</b>
<p>“This major update to the Ketchum Comprehensive Plan not only has a new look and organization, it charts a new way forward. Ketchum’s new planning perspective is based on the principle of sustainability. While sustainability embodies numerous definitions that vary depending on context, simply put it is about a community’s stability, resilience, social health, and the ability to thrive. As defined by the Brundtland Commission of the United Nations, sustainable development is “development that meets the needs of the present without compromising the ability of future generations to meet their own needs.” Many view sustainability as an ongoing effort to achieve balance between three or more “pillars” or core components: economic vitality, environmental stewardship, and social responsibility.</p> <p>“Sustainability” refers to not only the physical development of buildings and places, but also economic resiliency and prosperity, the conservation of natural resources, and the protection of history and culture. Basically, it means a strong, vibrant Ketchum.” (pg. iii, 2014 Comprehensive Plan)</p>
<p>Staff finding: Staff finds the facilitation of electric vehicle charging stations and the additional regulation of motor vehicle fueling stations to be consistent with the principle of conserving natural resources and the intent in the Brundtland Commission’s definition to meet the needs of the present without compromising the ability of future generations to meet their own needs.</p>
<b>Comprehensive Plan Objectives</b>
<p>Three major objectives were identified to guide the creation of the Plan:</p> <ul style="list-style-type: none"> <li>• Address the community’s most challenging issues;</li> <li>• Focus on sustainability; and</li> <li>• Provide direction on how the Plan will be achieved.</li> </ul>
<p>Staff finding: The proposed text amendments align with the objective to focus on sustainability; the proposed text amendments do not conflict with the other two objectives.</p>
<b>Key Values</b>
<p>Ten key values are identified, with three being most relevant to the proposed text amendments:</p> <p>3. Community Character</p> <ul style="list-style-type: none"> <li>• “You know when you have entered Ketchum; this is a place centered on the “town” and identifiable from the “country” by distinct edges. Residents and visitors desire this clear division that has been lost in so many American cities through strip commercial development and sprawling residential subdivisions. Protecting and enhancing the visual character of our community gateways, the undeveloped hillsides, and night skies is a priority.</li> </ul> <p>Geographically, downtown is a focal point and plays a key role in how our community looks and feels to locals and visitors. People value the opportunity to come together in the city’s well-defined community spaces.”</p> <p>5. Environmental Quality and Scenic Beauty</p> <ul style="list-style-type: none"> <li>• “Ketchum’s citizens place great value on the exceptional natural setting and resources of the Wood River Valley. The community is surrounded by rugged alpine peaks, forested and sage-covered open spaces, pristine wildlife habitat, and beautiful rivers and riparian areas. Key open spaces create visual buffers between the built and natural environment. Unobstructed views exist in every direction in large part due to Ketchum’s wide streets and lack of hillside development. These environmental features and resources sustain our</li> </ul>

economy and are why many people choose to live in Ketchum. We will be excellent stewards of these resources in order to preserve them for the future. We will do so by raising awareness and collaborating with other regional entities that care for our natural resources.”

10. A “Greener” Community

- “Ketchum understands that there are global challenges too large for any one community to solve, but believes in doing its share to address them. We will strive to integrate best practices in energy conservation, renewable energy use, multimodal transportation, waste reduction and recycling, low-impact development, storm water management, tree preservation, and local food production, among other areas.

We value the quality and quantity of our water resources that we have and will work to conserve them. We will work to build partnerships with businesses, the resort and lodging industry, the development sector, and others in order to raise awareness about the importance of being a “greener” community.”

Staff finding: As detailed in the next section, staff finds that properties adjacent to Main Street / Highway 75 and located on the edges of the Community Core warrant enhanced consideration of development character. Staff finds promotion of electric vehicle charging stations and regulation of motor vehicle fueling stations to be complementary to caring for the environmental quality, scenic beauty, and natural resources present in the Wood River Valley and Ketchum city limits. Staff finds promotion of electric vehicles and regulation of motor vehicle fueling stations to work toward achieving the vision stated in A “Greener” Community.

**Key Challenges**

Chapter 4: Community Design and Neighborhoods

“Challenge: Protecting Community Gateways

Ketchum's primary entrance from the south is a corridor transitioning from more rural sections of the County, past the Reinheimer Ranch, into downtown Ketchum. This entryway forms many visitors’ first impressions of the community. The south entrance corridor is pressured by development, and the burden of visitor and commuter traffic on Highway 75 as it is the only entrance to Ketchum from southern Idaho.

The northern entrance corridor leading from the Sawtooth National Recreation Area into Ketchum provides the feeling of expansive open space with its low-density residential housing and Bigwood Golf Course. This corridor begins north of Hulen Meadows along Highway 75 and extends southward to Tenth Street. Although portions of SH 75 have been designated nationally as the Sawtooth Scenic Byway, no specific protection measures are afforded to properties visible from SH 75 or other scenic byways. It is up to the local jurisdiction to implement adequate safeguards.”

Staff finding: With the proximity to the Big Wood Golf Course and the passive land use of the Ketchum Cemetery, and the fact that the two parcels of land with Light Industrial zoning that currently have vehicular access to Highway 75 being located on the north and south sides of 10<sup>th</sup> Street, staff finds that consideration of the character of development on both parcels is warranted and supported by the Comprehensive Plan.

<b>SUPPORTING SECTION</b>	<b>SUMMARY OF COMPLIANCE OF PROPOSED AMENDMENTS WITH THE 2014 COMPREHENSIVE PLAN</b>
<b>Prohibition of access to motor vehicle fueling stations from Highway 75</b>	
<b>Chapter 12: Future Land Use</b>	<p>The Light Industrial #1 and #2 zones are designated for the future I and use “Mixed-Use Industrial.” The description of this land use includes, “Uses should generate little traffic from tourists and the general public.”</p> <p>Providing highway access to a motor vehicle fueling station from Highway 75 facilitates the</p>



	<p>generation of traffic from tourists and the general public because Highway 75 is the main thoroughfare that carries tourists and the general public through Ketchum. Prohibiting access to motor vehicle fueling stations from Highway 75 limits the accessibility, and therefore traffic generated, from tourists and the general public.</p>
<p><b>Goal CD-3: Ketchum will maintain and improve the appearance of its entryway corridors and gateways.</b></p>	<ul style="list-style-type: none"> <li>• Policy CD-3.1 Scenic Corridors and the Community's Key Gateways.</li> </ul> <p>Along SH 75, there are a wide variety of panoramic vistas, rather than specific views to a fixed background area. Any development along the rural portions of the highway should retain these vistas. The gateways to the community presently convey a positive image because of their vast openness and distinct separation of urban and rural areas.</p> <p>The community would like to maintain views from major gateways including the following: SH 75, Sun Valley Road, Warm Springs Road</p>
<p><b>Goal M-1: Promote land use patterns, densities and mobility planning that maximizes investments and promotes safe and efficient mobility.</b></p>	<ul style="list-style-type: none"> <li>• Policy M-1.2 Transportation Planning and Access Management on Highway 75 and Arterials.</li> </ul> <p>Develop and implement a City-wide Transportation Plan. Control new curb cuts on Highway 75, Warm Springs Road and Saddle Road and reduce access points where possible (especially in the Mortgage Row area) to promote safe and efficient circulation.</p>
<p><b>Goal M-3: Monitor and manage traffic volume.</b></p>	<p>Monitor and manage traffic volume.</p> <ul style="list-style-type: none"> <li>• Policy M-3.2 Roadway Safety</li> </ul> <p>The City will strive to maintain an acceptable level of service for roads, which will generally place a priority on pedestrian, bicycle, and vehicle safety.</p>
<p><b>Addition of Electric Vehicle Charging Station as a distinct and defined accessory use</b></p>	
<p><b>Goal NR6: Promote and support energy conservation and reduction of greenhouse gases.</b></p>	<p>Promote and support energy conservation and reduction of greenhouse gases.</p> <ul style="list-style-type: none"> <li>• Policy NR6.2 Energy Alternatives</li> </ul> <p>The City should implement policies and programs that enhance opportunities for individuals, businesses, and public organizations to conserve energy and convert to renewable resources. The City should support energy conservation in City buildings, vehicles, operations and processes through its own policies, and provide information about techniques for energy efficiency.</p>
<p><b>Goal CHW-6 Reduce generation of air pollutants and noise</b></p>	<p>The City will promote reductions in air pollution to minimize impact to human health, sustain or improve the local economy, improve air quality, and reduce the impact of greenhouse gases.</p> <ul style="list-style-type: none"> <li>• Policy CHW-6.1 Air Emissions</li> </ul> <p>The City will continue to pursue reductions in air emissions/airborne particulates by regulating idling vehicles, street sanding, construction pollution, and other sources. Further, the City will reduce vehicle trips and vehicle miles travelled, and support renewable energy sources.</p>

**Attachment G:**

**Compliance with Zoning Ordinance**

Table 7: Compliance with Zoning Ordinance

SECTION	SUMMARY OF COMPLIANCE OF PROPOSED AMENDMENTS WITH THE ZONING ORDINANCE
<b>Prohibition of access to motor vehicle fueling stations from Highway 75</b>	
<b>17.18.140: LIGHT INDUSTRIAL DISTRICT NUMBER 1 (LI-1)</b>	A. Purpose: The LI-1 light industrial district number 1 is established as a transition area providing limited commercial service industries, limited retail, small light manufacturing, research and development, and offices related to building, maintenance and construction and which generate little traffic from tourists and the general public.
<ul style="list-style-type: none"> <li>Prohibiting access to motor vehicle fueling stations from Highway 75 limits the accessibility, and therefore traffic generated, from tourists and the general public.</li> </ul>	
<b>17.18.150: LIGHT INDUSTRIAL DISTRICT NUMBER 2 (LI-2)</b>	A. Purpose: The LI-2 light industrial district number 2 is established to provide for a permanent year round employment base and the location of light manufacturing, wholesale trade and distribution, research and development, service industries, limited related, bulk retail and offices related to building, maintenance and construction and which generate little traffic from tourists and the general public.
<ul style="list-style-type: none"> <li>Prohibiting access to motor vehicle fueling stations from Highway 75 limits the accessibility, and therefore traffic generated, from tourists and the general public.</li> </ul>	

**Attachment H:**

**Compliance with Subdivision Ordinance**

Table 8: Compliance with Subdivision Ordinance

SECTION	SUMMARY OF COMPLIANCE OF PROPOSED AMENDMENTS WITH THE SUBDIVISION ORDINANCE
<p><b>16.04.010: GENERAL PROVISIONS</b></p>	<p>B. Purposes: The general purpose of this chapter is to protect and promote the public health, safety, convenience and welfare by establishing regulations and a process of review for all proposed subdivisions of land. This chapter establishes standards for land subdivision in order to accomplish the following:</p> <ol style="list-style-type: none"> <li>1. To promote orderly, harmonious and integrated development of land;</li> <li>2. To provide safe, adequate and efficient pedestrian and vehicular traffic systems and circulations;</li> <li>3. To provide adequate all weather ingress and egress to subdivisions and lots;</li> <li>4. To prevent overcrowding of land and congestion on streets and highways;</li> <li>5. To provide for adequate air, light, solar access, privacy, and open space;</li> <li>6. To provide for adequate fire protection;</li> <li>7. To prevent inadequate or inappropriate provision of water, sewer, streets, pedestrian easements and public expenditures to provide such improvements;</li> <li>8. To protect and conserve wildlife, streams, natural topography, and other desirable natural features by providing for maximum retention of natural topographic features and qualities such as, but not limited to, skyline and ridge tops, knoll ridges, established trees and shrub masses, topsoil, streambeds and banks, drainage swales, and preventing damage to the natural environment or scenic beauty;</li> <li>9. To safeguard and enhance the character, appearance, and economic stability of the community;</li> <li>10. To provide adequate and uniform monumenting of land subdivisions and promote accurate legal descriptions;</li> <li>11. To protect the economic base of the community, including property values;</li> <li>12. To provide access to public lands and waters;</li> <li>13. To ensure the provision and construction of adequate improvements including, but not limited to, water, sewer, and other utilities, streets, bridges, drainage, street lighting and easements;</li> <li>14. To encourage and promote energy conservation and alternative energy sources as well as other advanced building technology;</li> <li>15. To ensure conformance of proposed subdivisions with the above stated purposes and to ensure design and construction of improvements in conformance with the standards and purposes of this chapter and all other municipal ordinances relating to this chapter, including subsequent amendments.</li> </ol>
<ul style="list-style-type: none"> <li>• The proposed text amendments do not conflict with the Subdivision Ordinance.</li> </ul>	

**Attachment I:**  
**Draft Ordinance**

**ORDINANCE NO. [TBD]**

**AN ORDINANCE OF THE CITY OF KETCHUM, BLAINE COUNTY, IDAHO AMENDING TITLE 17, THE ZONING ORDINANCE, OF THE KETCHUM MUNICIPAL CODE BY AMENDING SECTION 17.12.020, DISTRICT USE MATRIX; SECTION 17.08.020, TERMS DEFINED; PROVIDING A REPEALER CLAUSE; PROVIDING A SAVINGS AND SEVERABILITY CLAUSE, AND PROVIDING AN EFFECTIVE DATE.**

WHEREAS, the City of Ketchum is authorized to amend the city zoning ordinance pursuant to Idaho Code § 67-6511; and

WHEREAS, motor vehicle fueling stations generate a high volume of vehicle trips; and

WHEREAS, motor vehicle fueling stations with accessory retail sale of items of convenience to the motoring public generate an even higher volume of vehicle trips; and

WHEREAS, where Highway 75/Main Street in Ketchum, Idaho is adjacent to the zoning districts where motor vehicle fueling stations are permitted has only two lanes of vehicular travel, and lacks bicycle lanes and adequate pedestrian facilities; and

WHEREAS, Goal M-3 of the 2014 Comprehensive Plan is to monitor and manage traffic volumes and Policy M-3.2 Roadway Safety states, “The City will strive to maintain an acceptable level of service for roads, which will generally place a priority on pedestrian, bicycle and vehicle safety”; and

WHEREAS, Prohibiting vehicular access from Highway 75/Main Street supports Goal M-3 and Policy M-3.2 of the 2014 Comprehensive Plan; and

WHEREAS, Goal NR-6 of the 2014 Comprehensive Plan is to promote and support energy conservation and reduction of greenhouse gasses and Goal CHW-6 is to reduce generation of air pollutants and noise; and

WHEREAS, defining Electric Vehicle Charging Station as a use in the zoning code and permitting Electric Vehicle Charging Stations as an accessory use in all zoning districts supports Goal NR-6 and Goal CHW-6 of the 2014 Comprehensive Plan; and

WHEREAS, the Ketchum City Council, having reviewed the proposed text amendments, held public hearings on \_\_\_\_\_ and found that the request, on the whole, was in compliance with the 2014 Comprehensive Plan; and

WHEREAS, the Ketchum City Council having considered the recommendation of the Planning and Zoning Commission and submitted comments and testimony from the public, having determined that it is in the best interests of the public and adopt the proposed text amendments to Title 17, Zoning Ordinance;

**NOW, THEREFORE, BE IT ORDAINED BY THE MAYOR AND COUNCIL OF THE CITY OF KETCHUM**

**Section 1: AMENDMENTS TO SECTION 17.12.020, DISTRICT USE MATRIX.** That Title 17 of the Ketchum Municipal Code be amended to add a new footnote to Section 17.12.020, District Use Matrix as attached and incorporated as Exhibit A to this Ordinance.

**Section 2: AMENDMENTS TO SECTION 17.08.020, TERMS DEFINED.** That Title 17 of the Ketchum Municipal Code be amended to add a new definition to Section 17.02.020, Definitions, as attached and incorporated as Exhibit B to this Ordinance.

**Section 3: SAVINGS AND SEVERABILITY CLAUSE.** It is hereby declared to be the legislative intent that the provisions and parts of this Ordinance shall be severable. If any paragraph, part, section, subsection, sentence, clause or phrase of this Ordinance is for any reason held to be invalid for any reason by a Court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Ordinance.

**Section 4: REPEALER CLAUSE.** All City of Ketchum Ordinances or parts thereof which are in conflict herewith are hereby repealed.

**Section 5: PUBLICATION.** This Ordinance, or a summary thereof in compliance with Section 50-901A, Idaho Code, substantially in the form annexed hereto as Exhibit C, shall be published once in the official newspaper of the City, and shall take effect immediately upon its passage, approval, and publication.

**Section 6: EFFECTIVE DATE.** This Ordinance shall be in full force and effect after its passage, approval and publication, according to law.

PASSED BY the CITY COUNCIL and APPROVED by the MAYOR of Ketchum, Idaho, on this \_\_\_\_\_ day of \_\_\_\_\_, 2017.

APPROVED BY the Mayor of the City of Ketchum, Idaho, this \_\_\_\_\_ day of \_\_\_\_\_, 2017.

APPROVED:

\_\_\_\_\_  
Nina Jonas, Mayor



ATTEST:

\_\_\_\_\_  
Robin Crotty, Interim City Clerk

**EXHIBIT A**

17.12.020: DISTRICT USE MATRIX:

"P" = PERMITTED "C" = CONDITIONAL "A" = ACCESSORY

DISTRICT USE MATRIX

DISTRICT USES		L	L	L	G	G	S	S	S	T	T	T	C	C	C	C	L	L	L	R	A	
		R	1	2	L	R	O	O	O	T	3000	4000	SD	SD	SD	SD	I	I	I	U	F	
RES.	Dwelling, Multi-family				P <sup>1</sup>	P			P	P	P	P	P <sup>26</sup>	P <sup>26</sup>	P	P	C <sup>14</sup>	C <sup>14</sup>	C <sup>14</sup>	C <sup>19</sup>		
	Dwelling, One-Family	P	P	P	P <sup>2</sup>	P	P	P	P	P	P	P	See Note 28	See Note 28	See Note 28	See Note 28				C <sup>19</sup>	P	
	Residential Care Facility	P <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>	P <sup>26</sup>	P <sup>26</sup>	P	P					
COMMERCIAL	Agriculture, Commercial																				P	
	Adult Only Business																		P			
	Business Support Service												P	P	P	P <sup>27</sup>	P	P				
	Convenience Store									P			P	P	P		P <sup>12</sup>	P <sup>16</sup>				
	Daycare Center				C <sup>4</sup>	C <sup>4</sup>				P <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>	P	P	P		C <sup>17</sup>		C <sup>17</sup>			
	Daycare Facility				C <sup>4</sup>	P <sup>4</sup>			C <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>	P <sup>4</sup>					C <sup>17</sup>		C <sup>17</sup>	P <sup>4</sup>		
	Drive-Through Facility												P <sup>9</sup>	P <sup>9</sup>	P <sup>9</sup>							
	Equestrian Facility																				C	C
	Food Service									P	P <sup>6</sup>	P <sup>6</sup>	P	P	P		PC <sup>15</sup>	PC <sup>15</sup>		C <sup>29</sup>		
	Golf Course	P	P	P	P	P	P	P	P	P	P	P									C	
	Grocery Store												P	P	P							
	Health and Fitness Facility									P			P	P	P		C	C				
	Hotel									P <sup>25</sup>	P <sup>25</sup>	P <sup>25</sup>	P <sup>25</sup>	P <sup>25</sup>								
	Hybrid Production Facility												P	P	P		P	P				
	Instructional Service												P	P	P		P	P				
	Kennel, Boarding																P	P				
	Laundry, Industrial																P	P				
	Lodging Establishment									P	P	P	P	P								
	Maintenance Service Facility																P	P			C	
	Manufacturing																P	P				
	Mortuary													C	C	C						
	Motor Vehicle Fueling Station																	C <sup>30</sup>	C <sup>30</sup>			
	Motor Vehicle Sales																	C	C			
	Motor Vehicle Service																P	P				
	Office, Business												C	P <sup>10</sup>	P	P	P <sup>24</sup>			P		
	Outdoor Entertainment									P	P	P	P	P	P							
	Personal Service									P	P <sup>6</sup>	P <sup>6</sup>	P	P	P		P <sup>13</sup>					
	Professional Service																P	P				
	Recreation Facility, Commercial									C	C	C	P <sup>20</sup>	P <sup>20</sup>	P <sup>20</sup>						C	
	Repair Shop									P	P <sup>6</sup>	P <sup>6</sup>	P	P	P		P	P				
	Retail Trade									P <sup>5</sup>			P	P	P		P <sup>12</sup>	P <sup>16</sup>		C <sup>29</sup>		
	Self-Service Storage Facility																P	P				
	Ski Facility									C	C	C									C	C
Storage Yard																P	P					
Studio, Commercial												P	P	P		P	P	P				
Tourist House									P	P	P	P <sup>11</sup>	P <sup>11</sup>	P <sup>11</sup>	P <sup>11</sup>							
Tourist Housing Accommodation						P	P	P	P	P	P											
Truck Terminal																P	P					
TV and Radio Broadcasting Station																P	P	P				
Veterinary Service Establishment																P	P		C <sup>21</sup>			
Warehouse																P	P	P				
Wholesale																P	P					
Wireless Communication Facility	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	C <sup>23</sup>	
INSTITUTIONAL	Assembly, Place of				C <sup>3</sup>	C <sup>3</sup>							C	C	C							
	Cemetery												P	P	P					C	C	
	Cultural Facility																			C		
	Geothermal Utility											C <sup>7</sup>										
	Hospital												C	C	C							
	Medical Care Facility					C				P			P	P	P							
	Nature Preserve	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P
Parking Facility, Off-Site									C	C	C	C	C	C	C							

DISTRICT USES		L	L	L	G	G	S	S	S	T	T	T	C	C	C	C	L	L	L	R	A		
		R	1	2	R	R	O	O	O	T	3000	4000	SD	SD	SD	SD	I	I	I	U	F		
PUBLIC & I	Parking, Shared											C <sup>8</sup>	C <sup>8</sup>	C <sup>8</sup>	P <sup>8</sup>	P <sup>8</sup>	P <sup>8</sup>	P <sup>8</sup>					
	Performing Arts Production													P	P	P					C		
	Public Use	C	C	C	C	C	C	C	C	C	C	C	C	P	P	P	C	C	C	C	C	C	
	Public Utility	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	
	Recreation Facility, Public	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	
	Recycling Center																			P			
Semi-Public Use						C						C	C	C	P	P	P				C	C	
ACCESSORY	Agriculture, Urban	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	A <sup>22</sup>	
	Daycare Home	A <sup>4</sup>	A <sup>4</sup>	A <sup>4</sup>	A <sup>4</sup>	A <sup>4</sup>	A <sup>4</sup>	A <sup>4</sup>	A <sup>4</sup>	A <sup>4</sup>	A <sup>4</sup>	A <sup>4</sup>	A <sup>4</sup>					C <sup>4</sup>				A <sup>4</sup>	
	Daycare, Onsite Employees																		A	A	A		
	Dwelling Unit, Accessory	A <sup>18</sup>	A <sup>18</sup>	A <sup>18</sup>	A <sup>18</sup>	A <sup>18</sup>	A <sup>18</sup>	A <sup>18</sup>	A <sup>18</sup>	A <sup>18</sup>	A <sup>18</sup>	A <sup>18</sup>	A <sup>18</sup>	A <sup>18</sup>	A <sup>18</sup>	A <sup>18</sup>	A <sup>18</sup>					A <sup>18</sup>	
	Electric Vehicle Charging Station	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A
	Energy System, Solar	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A
	Energy System, Wind	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A
	Fallout Shelter	A	A	A	A	A	A	A	A	A	A	A	A										A
	Guesthouse	A	A	A	A	A	A	A	A	A	A	A	A										
	Home Occupation	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A
	Recreation Facility, Residential	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A			
	Equestrian Facility, Residential	A	A	A	A	A	A	A	A	A	A	A	A										A
	Sawmill, Temporary																						C

- A multi-family development containing up to two (2) dwelling units is permitted.
- Two (2) one-family dwellings are permitted.
- Religious institutions are allowed through the provision of a conditional use permit. No other assembly uses as defined in Chapter 17.08 are permitted.
- Use is not permitted in the Avalanche Zone. Reference Zoning Map.
- Retail trade is permitted but must not exceed 2,500 square feet.
- Uses must be subordinate to and operated within tourist housing and not to exceed ten percent (10%) of the gross floor area of the tourist housing facility.
- Utility for offsite use.
- See section 17.125.070 for shared parking standards.
- Drive-throughs are not allowed in association with food service establishments.
- This is a permitted use, however offices and professional services on the ground floor with street frontage require a conditional use permit.
- Tourist houses shall only be located in existing one-family dwellings. Additions to the home shall not exceed 20 percent (20%) of the existing square footage.
- The following forms of retail trade are permitted: (a) Equipment rental, including sporting equipment and entertainment equipment, (b) Building, construction and landscaping materials; small engines with associated sales (c) Retail in conjunction with manufacturing, warehousing or wholesaling not to exceed 30% gross floor area or 800 square feet, whichever is less; no advertising is displayed from windows or building facades; and no access onto a major arterial is allowed if an alternative access is available.
- Personal service is not allowed except for laundromats and dry cleaning establishments.
- See section 17.124.090 of this title for industrial districts residential development standards.
- Catering and food preparation is permitted. Restaurants require a conditional use permit and shall not exceed 1,000 square feet and serve no later than 9:00 P.M. unless expressly permitted through approval of the conditional use permit.
- The following forms of retail trade are permitted: (a) Equipment rental, including sporting equipment and entertainment equipment (b) Building, construction and landscaping materials; small engines with associated sales (c) Furniture and appliances in conjunction with warehousing not to exceed 18% gross floor area or 900 square feet, whichever is less; (d) Other retail in conjunction with manufacturing, warehousing or wholesaling; it is limited to 10% gross floor area or 500 square feet, whichever is less. ---- Retail uses (c) & (d) shall have no advertising displayed from windows or building facades; and no access will be permitted onto a major arterial if an alternative access is available.
- See section 17.124.120.C of this title for industrial districts daycare development standards.
- See section 17.124.070 of this title for accessory dwelling unit development standards.
- A maximum of five (5) dwelling units are allowed through a conditional use permit and shall be a minimum of 400 square feet and not exceed 1,200 square feet in size.
- Indoor only.
- Only allowed in conjunction with an equestrian facility.
- See section 17.124.080 of this title for urban agriculture development
- See chapter 17.140 for wireless communications facility provisions.
- Allowed on the ground floor only.
- See section 17.124.050 of this title for hotel development standards.
- Ground floor street frontage uses are limited to retail and/or office uses. In subdistrict A office uses require a conditional use permit.
- Ground floor only.
- Through the provision of a conditional use permit, the planning and zoning commission may approve a 20% increase to the total existing square footage of an existing nonconforming one-family dwelling.
- Use is allowed as an accessory use through the provision of a conditional use permit.
- 30. Vehicular access from Highway 75 to motor vehicle fueling stations is prohibited.**

**EXHIBIT B**

**17.02.020 TERMS DEFINED:**

MOTOR VEHICLE FUELING STATION: A facility providing the retail sale and direct delivery to motor vehicles of fuel, including electric charging stations associated with a motor vehicle fueling station, lubricants and minor accessories, and retail sales for the convenience of the motoring public.

ELECTRIC VEHICLE CHARGING STATION: Electric vehicle charging station means a public or private parking space that is served by battery charging station equipment that has as its primary purpose the transfer of electric energy (by conductive or inductive means) to a battery or other energy storage device in an electric vehicle.



**EXHIBIT C**

**PUBLICATION OF SUMMARY OF ORDINANCE NO. [tbd]**

**AN ORDINANCE OF THE CITY OF KETCHUM, BLAINE COUNTY, IDAHO AMENDING TITLE 17, THE ZONING ORDINANCE, OF THE KETCHUM MUNICIPAL CODE BY AMENDING SECTION 17.12.020, DISTRICT USE MATRIX; SECTION 17.08.020, TERMS DEFINED; PROVIDING A REPEALER CLAUSE; PROVIDING A SAVINGS AND SEVERABILITY CLAUSE, AND PROVIDING AN EFFECTIVE DATE.**

A summary of the principal provisions of Ordinance No. 1170 of the City of Ketchum, Blaine County, Idaho, adopted on \_\_\_\_\_, 2017, is as follows:

**SECTION 1.** Amends Section 17.12.020, District Use Matrix, to prohibit vehicular access from Highway 75 to motor vehicle fueling stations and to add Electric Vehicle Charging Stations as a permitted accessory use in all zones.

**SECTION 2.** Amends Section 17.08.020, Terms Defined, to add a new definition for Electric Vehicle Charging Station and to amend the definition of Motor Vehicle Fueling Station to clarify that Electric Vehicle Charging Stations are permitted in association with motor vehicle charging stations.

**SECTION 3.** Provides for a savings and severability clause.

**SECTION 4.** Provides for a repealer clause.

**SECTION 5.** Provides for publication of this Ordinance by Summary.

**SECTION 6.** Establishes an effective date.

The full text of this Ordinance is available at the City Clerk's Office, Ketchum City Hall, 480 East Avenue North, Ketchum, Idaho 83340 and will be provided to any citizen upon personal request during normal office hours.

APPROVED:

\_\_\_\_\_  
Nina Jonas, Mayor

ATTEST:

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Robin Crotty, Interim City Clerk